

**LAND USE APPLICATION SUMMARY**

*Property Location:* 1221 Nicollet Mall, 1226 Marquette Avenue, 1200 Marquette Avenue  
*Project Name:* Westminster Presbyterian Church Project  
*Prepared By:* Aaron Hanauer, Senior City Planner, (612) 673-2494  
*Applicant:* Rob Hunter, James Dayton Design  
*Project Contact:* Rob Hunter, James Dayton Design  
*Request:* Church building addition and outdoor plaza space  
*Required Applications:*

<b>Variance</b>	Variance to reduce the minimum floor area ratio from 2.0 to 1.2.
<b>Variance</b>	Variance to increase the maximum off-street parking from 144 spaces to 264 spaces.
<b>Variance</b>	Variance to reduce the drive aisle width from 18 feet to 15'-6".
<b>Variance</b>	Variance to increase the number of freestanding signs allowed from one to three.
<b>Site Plan Review</b>	For a 49,244 square foot building addition and new outdoor plaza space

**SITE DATA**

<b>Existing Zoning</b>	B4S-1, Downtown Service District DP, Downtown Parking Overlay District
<b>Lot Area</b>	117,080 square feet / 2.69 acre
<b>Ward(s)</b>	7
<b>Neighborhood(s)</b>	Loring Park borders Downtown West
<b>Designated Future Land Use</b>	Mixed Use
<b>Land Use Features</b>	Commercial Corridor (Nicollet Avenue/Nicollet Mall) Downtown Growth Center
<b>Small Area Plan(s)</b>	<u>Loring Park Neighborhood Master Plan (2013)</u>

<b>Date Application Deemed Complete</b>	May 19, 2016	<b>Date Extension Letter Sent</b>	N/A
<b>End of 60-Day Decision Period</b>	July 18, 2016	<b>End of 120-Day Decision Period</b>	N/A

## BACKGROUND

### SITE DESCRIPTION AND PRESENT USE.

The project site is an entire city block located at the south end of Downtown. The city block is bounded by Nicollet Mall to the west, 12th Street to the north, Alice Rainville Place to the south, and Marquette Avenue to the east.

The original Westminster Presbyterian Church at this location was built in 1897 and takes up the northern half of the city block. There have been subsequent building additions. Westminster is listed on the National Register of Historic Places and is eligible for local designation for its social and architectural significance.

In 2012, Westminster purchased the southern half of the city block (1226 Marquette Avenue and 1221 Nicollet Mall). The following year, they received City Council approval for an interim use permit to allow for an expansion of a surface parking lot from 32 to 53 spaces. As part of the parking lot proposal, the five-story Marimark Apartment Building (1226 Marquette Avenue) was demolished. The eight-story office building at the southwest corner of this city block is currently in the process of being demolished.

### SURROUNDING PROPERTIES AND NEIGHBORHOOD.

Nicollet Mall, the main pedestrian/transit spine for Downtown Minneapolis, is going through a redesign project from Washington Avenue to the north to Grant Street to the south from building face to building face. The redesign is broken into five themes. The project site is within the Loring Woods Area. The plan for the Loring Woods Area is to have more park-like settings including intensive greening, additional trees and amenities to strengthen the connection to the green hubs of the Loring Greenway and Loring Park. In addition to Nicollet Mall being an important pedestrian corridor, Alice Rainville Place provides a direct pedestrian route from the Minneapolis Convention Center to the Loring Greenway, Loring Park, and the Walker Art Center.

The project site is near the southern terminus of Nicollet Mall. It is located just to the south of Orchestra Hall and Peavey Plaza. The access/exit point for the Loring Greenway, a prominent pedestrian route, is located just to the west of the project site. The Minneapolis Convention Center campus is located to the east, the Hyatt Regency is located to the west, and the Millennium Hotel is located to the north of the project site.

### PROJECT DESCRIPTION.

Westminster is proposing a renovation/rehabilitation of the existing structure and an expansion project that includes the demolition of an office building and construction of an approximately 49,244 square foot, two-story building addition. The building addition would come close to the property line along Alice Rainville Place and would be centered between Nicollet Mall and Marquette Avenue. The applicants are proposing to reuse the existing skyway that connects the new building addition to the Millennium Hotel. The proposed addition would accommodate worship and gathering space, a multipurpose room, classrooms, youth spaces, church offices, community partnership space, and two floors of underground parking.

The project also includes the installation of a large plaza area along Nicollet Mall and a large greenspace along the southeastern portion of the site. A major landscape plan is proposed for the entire southern portion of the project site. Westminster has been working with the Nicollet Mall redevelopment team to ensure their landscaping plans are compatible with the Nicollet Mall Project in terms of design and timing.

**RELATED APPROVALS.**

Planning Case #	Application	Description	Action
BZZ-6084 (1226 Marquette)	Interim Use Permit	temporary expansion of a surface parking lot 32 spaces to 53 spaces.	June 14, 2013, set to expire on June 14, 2018

**PUBLIC COMMENTS.**

No public comments were received as of the publication of the staff report on June 7. Any correspondence received after the publication of the staff report and prior to the public meeting will be forwarded on to the Planning Commission for consideration.

**ANALYSIS**

**VARIANCE**

The Department of Community Planning and Economic Development has analyzed the application for a variance to reduce the minimum floor area ratio from 2.0 to 1.2. based on the following findings:

- 1. Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

The minimum floor area ratio requirement in the B4S-1, Downtown Service District is 2.0. The existing Westminster Church has 94,254 square feet of gross floor area on a lot that is 58,434 square feet, which equates to a floor area of 1.6. When the proposed 49,244 square foot addition is built on the expanded property on the south end of the block, the Westminster Presbyterian Church will have 143,498 square feet of floor area on a lot that is 117,080 square feet, which equates to a floor area ratio of 1.2.

Practical difficulties exist in compliance with the minimum 2.0 floor area ratio requirement because of circumstances unique to the property. The Westminster Presbyterian Church is historically significant for its architecture (and social history). It is listed on the National Register of Historic Places and is eligible for local historic landmark designation. In 1997, the Minnesota Historical Society stated that “Westminster Presbyterian Church is an important surviving late 19th century ecclesiastical design in the city of Minneapolis.” The original architects and those that completed renovations are considered master architects including Charles S. Sedgwick, Warren H. Hayes, Purcell and Elmslie, and Magney and Tusler. As outlined in the Secretary of Interior’s Standards for the Treatment of Historic Properties-Guidelines for Rehabilitation, it is recommended that new additions are completed so that there is the least possible loss of historic materials, that character-defining features are not obscured, damaged, or destroyed, and limiting the size and scale of the addition in relationship to the historic building.

- 2. The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The property owner is proposing to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan. The intent of the minimum

floor area ratio regulation is to prevent the underutilization of downtown properties. The applicant has taken efforts to mitigate this issue while building a sensitive addition to a historically and architecturally significant building. This includes building a large public plaza area with an enhanced landscape plan along Nicollet Mall that will ideally activate the street. In addition, the applicant is removing all surface parking and has placed the new construction away from Marquette Avenue leaving an approximately 17,000 square foot open space that could accommodate a future expansion. The applicant states that they are proposing to structurally design the underground parking ground structure to accommodate a future expansion up to three stories in this open space area, thus potentially bringing the building closer to the zoning district's minimum floor area requirement.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

The proposed variance to reduce the minimum floor area ratio will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity nor will it be detrimental to the health, safety or welfare of the general public or those utilizing nearby properties. As previously mentioned in Finding 1 and 2 above, the size, placement, and design of the proposed addition does a good job of striking a balance being sensitive to a historic landmark and having an addition and outdoor space that activates the street and blends in with the surrounding area, including the major renovation of Nicollet Mall.

## VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance to increase the maximum off-street parking from 144 spaces to 264 spaces based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

As outlined in Table 541-2-Specific off-street parking requirements-downtown districts, the maximum off-street parking requirement is 1 space per 1,000 square feet of gross floor area for an institutional use such as a place of assembly. With the proposed 49,244 square foot addition, the building will have approximately 143,498 square feet of gross floor area. Therefore, the maximum allowed parking is 144 off-street parking spaces. The applicant is proposing to provide 264 off-street parking spaces. Therefore, the project requires a variance to exceed the maximum off-street parking requirement.

Practical difficulties exist in complying with the ordinance because of circumstances unique to the property that were not created by persons presently having an interest in the property. The project site is located at the south end of downtown. This part of downtown does not have the levels of mass transit service compared to the heart of downtown and/or near the Blue and Green light rail lines, especially during weeknights and weekends when the church often has their large events and peak demand for parking.

In addition, the historically significant church, which was built well before the maximum off-street parking requirement was in place, has a main sanctuary that seats 1,400 people. This type of space has unique peak demands in terms of off-street parking needs.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The property owner proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan. The intent of the maximum parking requirement is to ensure that parking does not consume an inordinate amount of downtown space, does not create dead space, nor eliminate eyes on the street by having a massive amount of parking above ground, and to recognize the substantial public investment in transit, which converges in downtown Minneapolis.

The applicant has taken efforts to mitigate this issue by not having any surface parking and having all parking enclosed in the basement levels of the building. Even though there are portions of the parking garage that are exposed above ground, the applicant is proposing a large amount of glazing on each elevation which will provide natural surveillance of the area. As mentioned in Finding 1, the church is unique in terms of being at the south end of downtown which does not have as great of a level of mass transit service as the heart of downtown on weeknights and weekends, particularly Sunday morning. Finally, allowing for extra parking that does not adversely impact the historic landmark, may help preserve this locally architecturally significant gem.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Granting of this variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. In addition, granting of the variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties. All of the proposed parking will be enclosed and located below the ground floor. Given the parking location below ground, the development is able to have active uses line most of the building perimeter ground floor along all elevations. In addition, as previously mentioned, the development has a proposed fenestration pattern that will large amounts of glazing on each elevation. These building elements will be a benefit to those using the property and the general public in the area.

## VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance to reduce the drive aisle width from 18 feet to 15'-6", based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

As required by Section 541.330-Size, the minimum width for a one-way drive aisle when there is 60 degree angled parking is 18 feet. The applicant is proposing a one-way drive aisle with of 15.5 feet. Practical difficulties exist in complying with the ordinance requirement for drive aisle width because of circumstances unique to the property. The applicant is proposing to tie in the proposed parking garage into an existing parking garage that is underneath a historic structure. The placement of the existing Westminster Presbyterian Church and existing underground parking creates limits and constraints in terms of meeting all parking area design requirements outlined in Chapter 541.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The property owner proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan. The intent of the drive aisle width requirement is to ensure that proper access to off-street parking spaces is provided. Although the applicant is proposing to reduce the drive aisle width from 18 feet to 15.5 feet, the applicant is proposing to have the parking stalls meet the minimum width, curb length, and stall depth requirements. In addition, the applicant is proposing that all parking stalls be standard dimensions (rather than compact spaces). Furthermore, the applicant has provided documentation that a 15.5 foot wide, one-way drive aisle meets the recommended aisle width dimensional guidelines as outlined by the National Parking Association in their "Guidelines for Parking Geometrics."

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Granting of this variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. In addition, granting of the variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties. While a portion of the drive aisle is less than the required 18 feet in width, access to the underground parking garage will meet the minimum driveway width requirement.

## VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance to increase the number of freestanding signs allowed from one to three, based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

As outlined in Section 543.225-Institutional and public uses, signs for religious institutions are regulated by Table 543-1. Table 543-1 states that a place of assembly of a lot of 20,000 square feet or more is allowed one (1) monument sign not exceeding thirty-two (32) sq. ft. in area and eight (8) ft. in height. The applicant is proposing to retain an existing monument sign along Nicollet Mall near the 12<sup>th</sup> Street South intersection and proposing to install two additional monument signs at the Marquette Avenue and Alice Rainville Place intersection; a total of three monument signs.

Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The project site is unique in terms of taking up an entire city block. The project site is 117,080 square feet in size, almost six times the minimum lot area requirement set by the zoning code for a place of assembly that allows one monument sign. In addition, the project site is unique in terms of having a building that is recognized being a historic landmark for its architectural significance. The architectural significance limits where it would be appropriate for building and site signage.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The property owner proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan. The intent of the sign regulations are to allow effective signage appropriate for uses and zoning districts, to promote an attractive environment by minimizing visual clutter and confusion, and to minimize adverse effects on nearby property.

The proposed placement of the additional monuments signs at the Marquette and Alice Rainville intersection is intended to clearly identify destination arrival and provide direction to those not familiar with the area at key arrival locations without creating sign clutter. In addition, with the proposed signs positioned at a right angle, they will help reinforce street frontage along both streets. Although additional monumental signs are requested than what is allowed by the zoning code, the applicant's proposed sign package is well below the square footage allowance for a place of assembly and at 5.8' in height the monument signs are well below the 8' height requirement. The proposed signs will meet all other zoning code requirements. The requirement that the base shall be screened is not applicable given it is only six inches high.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Granting the variance would not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity and would not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties. As previously mentioned, the applicant's proposed signage is well below what is allowed for an institutional use. The only signs proposed for the entire city block are the retention of one monumental sign and the installation of two additional monumental signs. It should also be noted that the applicant is proposing to provide approximately three feet of space between the new monumental signs in order to provide visibility into and out of the project site at this corner.

### **Additional Standards for Sign Adjustments**

In addition, the Planning Commission shall consider, but not be limited to, the following factors when considering an adjustment to the number, type, height, area, or location of allowed signs on property located in an OR2 or OR3 District or a commercial, downtown, or industrial district:

1. *The sign adjustment will not significantly increase or lead to sign clutter in the area or result in a sign that is inconsistent with the purpose of the zoning district in which the property is located.*

The B4S-1 Downtown Neighborhood District is established to provide an environment that promotes the development of mixed-use neighborhoods in a higher density, transit- and pedestrian-oriented, urban environment with a wide range of retail and office activities and high density residential uses and hotels. A place of assembly that spans an entire city block and has a variety of uses has unique identification needs. The proposed signs have been placed to provide appropriate identification and wayfinding while at the same time avoiding sign clutter. Although the additional freestanding signs are requested, the total amount of signage is well below what is allowed.

2. *The sign adjustment will allow a sign that relates in size, shape, materials, color, illumination and character to the function and architectural character of the building or property on which the sign will be located.*

The proposed signs would relate in size, shape, materials, color, illumination and character to the existing building. The building will be clad in honed limestone, stucco, and zinc. The proposed signs would be monument signs with a stone surround and a black metal (factory-prefinished aluminum) signage area. The signs would be 32 square feet in area, 5.8' in height, horizontally oriented, and be externally illuminated with a small light in the ground shining on the sign face. These design details will relate to the historic and contemporary buildings. As a condition of approval, the applicant shall show on final plans that the sign cabinet is no greater than 32 square feet in size.

## SITE PLAN REVIEW

The Department of Community Planning and Economic Development has analyzed the application based on the required findings and applicable standards in the site plan review chapter:

### **I. Conformance to all applicable standards of Chapter 530, Site Plan Review.**

#### **BUILDING PLACEMENT AND DESIGN**

##### **Building placement – Requires alternative compliance**

- The first floor of building has street frontage on Nicollet Mall, Alice Rainville Place, and Marquette Avenue. The addition is located within 8 feet of the property line along Alice Rainville Place; however, it is located more than 8 feet from the Nicollet Mall and Marquette Avenue property lines. Alternative compliance is required.
- The placement of the building addition maximizes natural surveillance and visibility, facilitates pedestrian access and circulation along the three elevations. However, it does not reinforce the street wall, Alternative compliance is required.
- The area between the building and lot line includes amenities. This includes a large plaza area along Nicollet Mall and a large landscaped greenspace along Marquette Avenue that has an allée of trees, shrubs, and benches that line an interior walkway.
- All on-site accessory parking is located within the principal building served.

##### **Principal entrances – Meets requirements**

- The building is oriented so there is an entrance point that faces the public street on Nicollet Mall, Alice Rainville Place, and Marquette Avenue.
- The entrances are clearly defined and emphasized through the use of canopies and large amounts of glazing.
- There are eight pedestrian entrance points to the Westminster Church.

##### **Visual interest – Requires alternative compliance**

- The building walls provide architectural detail to create visual interest including high quality materials, articulation, canopies, overhangs, and large glazing areas.
- Architectural elements, including recesses or projections, and changes in material are used effectively to divide the building into smaller identifiable sections.
- There is a blank, uninterrupted walls that exceed 25 feet in length in three places: along Nicollet Mall, Alice Rainville Place, and Marquette Avenue. Alternative compliance is required.

##### **Exterior materials – Meets requirements**

- Besides glazing, the applicant is proposing honed limestone (stone), Thermocromex Plaster (stucco), and zinc-coated copper (metal panels). These materials are considered durable.
- Each elevation will have three exterior materials (excluding glazing and accent materials) and will comply with the exterior material percentage requirements outlined in the Guide to Exterior Building Walls and Materials (see Table I).
- The exterior materials and contemporary appearance on the three sides will be compatible with each other and will complement the historic portion of the church building.
- The rear and side walls of the building will be similar to and compatible with the front of the building.
- Plain face concrete block is not proposed along any public streets, sidewalks, or adjacent to a residence or office residence district.

**Table 1. Percentage of Exterior Materials per Elevation**

Material	Allowed Max	North	South	East	West
Glass	100%	N/A	24 %	29 %	29 %
Stone	100%	N/A	27 %	20 %	30 %
Stucco	75%	N/A	19 %	15 %	11 %
Metal Panel	75%	N/A	30 %	36 %	30 %

**Windows – Requires alternative compliance**

- More than 30 percent of the walls on the first floor on the west and east elevations contain glazing. In addition, the walls on the upper floors of the west, south, and east elevation contain more than 10 percent glazing (see Table 2). The proposed fenestration on these elevations and floors meets the requirements outlined in Section 530.120 including being evenly distributed along the building walls. As condition of approval, the first floor windows shall have clear or lightly tinted glass with a visible light transmittance ratio of six-tenths (0.6) or higher. However, the 1<sup>st</sup> floor of the south elevation does not meet the 30 percent requirement given that the bottom of the windows used to satisfy the ground floor window requirement are more than four (4) feet above the adjacent grade. Alternative compliance is required.

**Table 2. Percentage of Windows per Applicable Elevation**

	Code Requirement		Proposed	
<b>Nonresidential Uses</b>				
1st floor-west	30% minimum	424 sq. ft.	52%	730 sq. ft.
1st floor-south	30% minimum	394 sq. ft.	47 %	620 sq. ft.
1 <sup>st</sup> floor-east	30% minimum	374 sq. ft.	0 %	0 sq. ft.
2 <sup>nd</sup> floor-west	10% minimum	399 sq. ft.	18%	726 sq. ft.
2 <sup>nd</sup> floor-south	10% minimum	389 sq. ft.	19%	743 sq. ft.
2 <sup>nd</sup> floor-east	10% minimum	375 sq. ft.	22%	822 sq. ft.

**Ground floor active functions – Meets requirements**

- The ground floor facing Nicollet Mall, Alice Rainville Place, and Marquette Avenue meets the requirement that at least 70 percent of the first floor building frontage facing the public street contains active functions.

**Roof line – Meets requirements**

- The principal roof line of the building will be flat, which is similar to that of surrounding building. The Bell Carillon will provide a unique upper element to the building; however this complements the original existing church towers and is subordinate to the new construction in terms of size and area.

**Parking garages – Meets requirements**

- The exterior design of parking garage ensures that sloped floors do not dominate the appearance of the walls and that vehicles are screened from view. Given the proposed height of the parking garage and the grading, the parking garage portion of the building is considered part of the basement level and not the ground level. Therefore, the project is not subject to the B4S-Downtown Service District requirement that the ground floor of a principal parking structure

shall have commercial, residential, office, or hotel uses located between the parking garage and the public sidewalk.

## **ACCESS AND CIRCULATION**

### **Pedestrian access – Meets requirements**

- There are clear and well-lit walkways at least four feet in width connecting building entrances to the adjacent public sidewalk and on-site parking facilities.

### **Transit access – Meets requirements**

- No transit shelters are proposed as part of this development. The existing bus stop along 12<sup>th</sup> Street South is proposed to remain.

### **Vehicular access – Requires alternative compliance**

- Vehicular access and circulation has been designed to minimize conflicts with pedestrian traffic and with surrounding residential uses.
- Curb cuts for vehicles shall be consolidated wherever possible. There are currently two curb cuts. With the proposed project there would be three curb cuts. Alternative compliance is required.
- There are no public alleys adjacent to the site.
- Service vehicle access does not conflict with pedestrian traffic. Truck loading areas are not located next to residence or office residence districts.
- The proposed site plan minimizes the use of impervious surfaces with the large planting areas, pervious pavers, greenspace, and green roofs.

## **LANDSCAPING AND SCREENING**

### **General landscaping and screening – Meets requirements**

- The overall composition and location of landscaped areas complement the scale of development and its surroundings including the proposed plans for Nicollet Mall. The project site is within the Loring Woods Area of the Nicollet Mall remodeling. The plan for the Loring Woods Area is to have more park-like settings including intensive greening, additional trees and amenities to strengthen the connection to the green hubs of the Loring Greenway and Loring Park. The applicant is proposing an enhanced planting plan and outdoor plaza along Nicollet Mall to tie in with the Nicollet Mall redesign.
- As outlined in Section 530.30-Buildings and uses subject to site plan review, any building containing 50,000 square feet or more of gross floor area located in the Downtown districts shall be exempt from the general landscaping and screening requirement. With the proposed addition, Westminster Presbyterian Church will have 143,498 square feet of gross floor area above grade. Therefore, the project is not subject to the general landscaping and screening requirements. Even though the proposed development is not required to have landscape area, the proposed project will have approximately 13,300 square feet of green space.
- There is no canopy tree requirement for the proposed project; however, the applicant is proposing to plant 28 canopy trees onsite and an addition 5 canopy trees within the public right of way.
- There is no shrub requirement for the proposed project; however, the applicant is proposing to plant approximately 393 shrubs onsite and 98 within the public right of way.
- The landscaped areas will be covered with turf grass, native grasses, perennial flowering plants, vines, shrubs and other trees.

**Table 3. Landscaping and Screening Requirements**

	<b>Code Requirement</b>	<b>Proposed</b>
<b>Lot Area</b>	--	117,080 sq. ft.
<b>Building Footprint</b>	--	67,948 sq. ft.
<b>Remaining Lot Area</b>	--	49,132 sq. ft.
<b>Landscaping Required</b>	N/A	13,300 sq. ft.
<b>Canopy Trees (1:500 sq. ft.)</b>	N/A	28 trees
<b>Shrubs (1:100 sq. ft.)</b>	N/A	393 shrubs

**Parking and loading landscaping and screening – Meets requirements**

- There is no surface parking proposed for the site. However, there is a proposed drop off area in the southeast portion of the site. This area is required to comply with the requirements outlined in section 530.170 in terms of landscape yard, screening, and trees. The project includes a landscape yard that is at least 60 feet wide between the drop off area and Marquette Avenue. The project also includes landscape screening that is at least three feet in height and not less than 60 percent opaque. In addition, there will be not less than one (1) tree shall be provided for each twenty-five (25) linear feet or fraction thereof of parking or loading area lot frontage.
- Since there is no surface parking, the project is not required to comply with the distance to tree requirement that no parking space shall be located more than fifty (50) feet from the center of an on-site deciduous tree.
- There are no tree islands as part of the proposed project.
- As conditioned, the plant materials, and the installation and maintenance of the plant materials, shall comply with sections 530.200 and 530.210 of the zoning code.

**ADDITIONAL STANDARDS**

**Concrete curbs and wheel stops – Meets requirements**

- The drop off area is defined by a continuous concrete curb. However, the applicant is proposing a permeable paver drive that will help with onsite water retention.

**Site context – Meets requirements**

- The proposed addition has minimized the blocking of views of important elements of the city such as the Loring Greenway, Nicollet Mall, and the original Westminster Presbyterian Church.
- The proposed two-story building should have minimal shadowing effects on public spaces and adjacent properties.
- This building will not have an adverse impact in terms of wind currents at ground level.

**Crime prevention through environmental design – Meets requirements**

- The proposed development employs best practices to increase natural surveillance and visibility with a large amount of glazing on all elevations. The proposed project also helps control and guide movement on the site with defined walkways, and would distinguish between public and non-public spaces through entrance and fence locations.
- The project provides lighting on site, at all building entrances, and along walkways. As a condition of approval, a lighting plan shall be submitted that shows the site maintains a minimum acceptable level of security while not creating glare or excessive lighting of the site, and complying with the lighting standards outlined in Chapter 535 and Chapter 541.

- The landscaping, sidewalks, lighting, fencing, and building features are located to clearly guide pedestrian movement on or through the site and to control and restrict people to appropriate locations.
- The entrances, exits, signs, fencing, landscaping, and lighting are located to distinguish between public and private areas, to control access, and to guide people coming to and going from the site.

**Historic preservation – Meets requirements**

- The site plan includes the rehabilitation of the original Westminster Presbyterian Church which is eligible for local designation for its architectural and social significance.

**2. Conformance with all applicable regulations of the zoning ordinance.**

The proposed principal use, place of assembly, is a permitted use in the B4S-1, Downtown Service District.

**Off-street Parking and Loading – Requires variance(s)**

- Table 541-2 Specific Off-Street Parking Requirements-Downtown Districts outlines the minimum parking requirement and the maximum parking allowed. There is no minimum parking requirement for an institutional and public use in downtown.
- The maximum off-street parking requirement is 1 space per 1,000 square feet of gross floor area. With the 49,244 square foot proposed addition, the building will have approximately 143,498 square feet of gross floor area. Therefore, the maximum allowed parking is 144 off-street parking spaces. The applicant is proposing to provide 264 off-street parking spaces. The project requires a variance to exceed the maximum off-street parking requirement. (see Table 4).
- Bike parking: Development downtown that is under 500,000 square feet is required to provide one (1) secure bicycle parking space for every twenty (20) automobile spaces provided, but in no case shall fewer than four (4) or more than thirty (30) bicycle parking spaces be required. For the purposes of this section, a secure bicycle parking space shall include a bicycle rack which permits the locking of the bicycle frame and one (1) wheel to the rack, and which supports the bicycle in a stable position without damage to wheels, frame or components. With the 264 off-street parking spaces proposed, the project is required to provide 13 bike parking spaces. The applicant is proposing 30 (8 at grade and 22 in parking level P1). (see Table 5).
- As outlined in Table 541-9 Specific Off-Street Loading Requirements, places of assembly have a low loading requirement. With approximately 143,498 square feet of gross floor area is required to provide space for 2 small loading spaces. (see Table 7). The applicant is proposing two small loading spaces that meet the dimension requirements for small loading spaces.
- Snow removal will be off-site.
- The proposed parking spaces in the parking garage would be at a 65 degree angle. Table 541-6- Minimum space and aisle dimension requirements, provides the width, depth, and drive aisle width minimums for parking provided at a 45, 60, and 90 degree angle. Off-street parking that is at a 65 degree angle requires that a one-way drive aisle be 18 feet in width. The proposed one-way drive aisle widths would be 15.5 feet in width. A variance is required.

**Table 4. Vehicle Parking Requirements Per Use (Chapter 541)**

Use	Minimum	Reductions	Total with Reductions	Maximum Allowed	Proposed
Place of Assembly	0	--	0	144	264

**Table 5. Bicycle Parking Requirements (Chapter 541)**

Use	Minimum	Short-Term	Long-Term	Proposed
Place of Assembly	13	--	22	30

**Table 6. Loading Requirements (Chapter 541)**

Use	Loading Requirement	Minimum Requirement	Proposed
Place of Assembly	Low	2 small spaces	2 small spaces

**Building Bulk and Height – Requires variance(s)**

- With the proposed lot combination the project site will have 117,080 square feet of area (2.7 acres). With the proposed addition, the entire Westminster Presbyterian Church will have 143,498 square feet of gross floor area. This equates to a floor area ratio of 1.2. The minimum floor area required in the B4S-1, Downtown Service District is 2.0. Therefore, a variance is required to go below the minimum requirement.
- The maximum floor area ratio allowed in the B4S-1, Downtown Service District is 8.0. (see Table 7).
- The building complies with the height requirement of the B4S-1, Downtown Service District. The natural grade 10 feet from the center of the building along 12<sup>th</sup> Street South is 856 feet above sea level (the lots are platted towards 12<sup>th</sup> Street South). The top of the first floor slab is 856.6 feet above sea level. The height of the building from the first floor slab to the Bell Carillon is 91 feet (947.6 feet above sea level). Therefore, the height of the addition at its tallest point is 91.6 feet in height.
- The first floor of the parking garage does not count as a story. There is no portion that has an exposure of 12 feet in height and after grading, 50 percent of the total perimeter is not more than 6 feet above grade.

**Table 7. Building Bulk and Height Requirements**

	Code Requirement	Proposed
Lot Area	--	117,080 sq. ft. / 2.7 acres
Gross Floor Area	--	143,498 sq. ft.
Floor Area Ratio (Minimum)	2.0	1.2
Floor Area Ratio (Maximum)	8.0	
Building Height (Maximum)	10 stories or 140 feet, whichever is less	91.6 feet

**Lot Requirements – Not applicable**

- There is no impervious surface area maximum, lot coverage maximum or lot width or lot size requirements in the B4S-1, Downtown Service District.

**Yard Requirements – Not applicable**

- There are no yard requirements for the project site and proposed use. The project site comprises an entire block and is zoned B4S-1, Downtown Service District.

**Table 8. Minimum Yard Requirements**

Setback	Zoning District	Overriding Regulations	Total Requirement	Proposed
<b>Marquette Avenue</b> (East)	0 ft.	--	0 ft.	102 ft.
<b>Alice Rainville Place</b> (South)	0 ft.	--	0 ft.	0 ft.
<b>Nicollet Mall</b> (West)	0 ft.	--	0 ft.	83 ft.

**Signs – Requires variance(s)**

- Westminster Presbyterian Church is proposing to have a total of three signs with the completion of the new construction project; all of them monumental freestanding signs. As outlined in Table 543-1, a place of assembly with a lot area of 20,000 square feet or more is allowed one monument sign. This proposal requires a variance.

**Table 9. Signage Summary**

	Number Allowed/ Lot	Proposed Number	Maximum Size Allocation	Maximum Area Per Sign	Proposed Area	Maximum Allowed Height	Proposed Height
<b>Monument</b>	1	3	32	sq. ft.	XX sq. ft.	8 ft.	5.8 ft.

**Screening of Mechanical Equipment – Meets requirements with Conditions of Approval**

- There are two existing underground vaults on the project site that are proposed to remain as part of the project. One is located on Marquette Avenue and the other is located on Alice Rainville Place. No new transformers are proposed. There are four rooftop mechanical units proposed that are proposed to be 9.5 feet in height. These units, except rooftop unit 4 would be fully screened by a proposed 10’ tall screen wall and the structure itself. Half of RTU 4 is screened from Alice Rainville. As a condition of approval, all portions of the mechanical equipment shall be screened in compliance from 535.70.

**Refuse Screening – Meets requirements**

- Trash, recycling, and all other refuse storage will in compliance with Chapter 535 as they will be stored within the underground parking garage.

**Lighting – Meets requirements with Conditions of Approval**

- The applicant submitted an exterior light layout plan and light fixture schedule. It appears that the proposed lighting plan will comply with Chapter 535 and Chapter 541 of the zoning code. As a condition of approval, the applicant shall submit a lighting plan prior to building permit issuance that shows full compliance with Section 535.590-Lighting and Chapter 541.340-Lighting.

**Fences – Meets requirements**

- The proposed fencing complies with the fence requirements outlined in Chapter 535. The applicant is proposing to install fencing along a portion of the building at the Alice Rainville Place property line. It would be located just to the west of the parking garage access point. This fence would be four feet in height. As outlined in Section 535.420-Fence height, fences located along public streets are not allowed to exceed 6 feet in height.
- A portion of the existing fence along 12<sup>th</sup> Street South will be removed as part of this project.

**Plazas – Meets requirements**

- The Minneapolis zoning code defines a plaza as an exterior open space designed for community gathering that is primarily hardscaped and accessible to the public, fronting along a public street or public sidewalk. The applicant is proposing two plaza spaces. The southern outdoor space along Nicollet Mall was reviewed for compliance with the plaza standards as it was identified as a community gathering space and accessible to the public. The outdoor area along Marquette Avenue was not considered a plaza as it is primarily greenspace. An analysis of the plaza standards is provided below:
  - *Size:* The Nicollet Mall Plaza is 17,880 square feet which meets the minimum square foot requirement outlined in Section 535.790.
  - *Placement:* The placement of plaza would not detract from and will enhance natural surveillance and visibility of adjacent spaces and public sidewalks and facilitate pedestrian access and circulation. Multiple building entry points and abundant windows would be oriented to the plaza to enhance natural surveillance, visibility and usability.
  - *Access:* The plaza is designed to allow pedestrian and bicycle access through and around the plaza. The plaza contains multiple access points from the public sidewalk. In addition, the plaza will be designed in accordance with the accessibility requirements of the Americans with Disabilities Act, and unobstructed walkways a minimum of four (4) feet in width would connect the plaza to the building. The plaza would not be adjacent to a transit stop.
  - *Natural surveillance and visibility:* The plaza is designed to promote natural surveillance with a large amount of windows and numerous building entrances leading directly to the plaza. In addition, the plaza is designed for people to observe adjacent spaces and the public sidewalks.
  - *Aerial obstructions:* The standards that apply to aerial obstructions require that plazas are unobstructed to the sky except for seating, arbors, trellises, kiosks, lighting, water features, public art and landscaping. Up to 30 percent of the plaza area may include umbrellas, awnings or an arcade. None of the plaza would have aerial obstructions with the exception of umbrellas.
  - *Surface materials:* The plaza areas that are not landscaped would be surfaced with concrete, permeable pavers, and other pavers.
  - *Seating:* The plaza complies with the seating requirements (see Table 10 below).

**Table 10. Plaza Seating Requirements**

	<b>Code Requirement</b>	<b>Proposed</b>
<b>Plaza Area</b>	--	17,880 sq. ft.
<b>Overall seating (1:50 sq. ft.)</b>	358 linear feet	359 linear feet

<b>Fixed seating (20% of required)</b>	72 linear feet	174 linear feet
<b>Seating with backs (20% of required)</b>	72 linear feet	160 linear feet
<b>Seating types</b>	2 types	2 types Seat wall planters and benches

- *Encroachments and setbacks:* The proposed plaza does not encroach into the public right-of-way.
- *Plantings:* Plazas shall provide a minimum of one (1) tree for each one thousand (1,000) square feet of plaza area. However, a landscaped area equivalent to ten (10) percent of the total plaza area may be provided in lieu of trees. The applicant is compliance with this requirement as they would have 26 percent of the plaza landscaped (4,577 square feet). The plaza would also include three trees.
- *Additional amenities:* The plaza is required to provide a total a minimum of two amenities since it is greater than 5,000 square feet. The applicant is proposing three amenities: a water feature, a sculpture (i.e. art feature) within the water, and permeable pavers to help with stormwater functionality.
- *Lighting:* As a condition of approval, a lighting plan shall be submitted prior to building permit issuance showing compliance with Chapter 535-Regulations of General Applicability.
- *Winter use:* The plaza is designed for winter use and relates to the built form with consideration given to elements such as providing shelter from winds, utilizing seasonally appropriate materials, maximizing access to sunlight and providing for snow and ice removal. The plaza would be located in an area that would be sheltered from northerly winds. The openness of the plaza would maximize access to the sun in the morning and afternoon hours.
- *Trash receptacles:* A minimum of 1 trash receptacle and 1 recycling receptacle are required to be provided for each two thousand (2,000) square feet of plaza area. The 17,880 square foot plaza would be in compliance with this requirement as it will have nine trash and recycling receptacles.
- *Mechanical equipment:* There are no proposed exhaust fans or mechanical equipment located adjacent to the plaza.
- *Maintenance:* As a condition of approval, the plaza shall be maintained in good order by the property owner for the life of the plaza. Proper maintenance shall include, but not be limited to, snow and ice removal, annual maintenance of vegetation and green space and annual inspection and repair and/or replacement of furnishings. Minimum landscaping and seating requirements shall be maintained for the life of the plaza. All adjacent streets, sidewalks and pathways shall be inspected regularly for purposes of removing any litter found thereon.

**Specific Development Standards – Meets requirements**

- There are no development standards for a place of assembly in the downtown zoning districts.

**DP/Downtown Parking Overlay District Standards – Meets requirements**

- The proposal is in compliance with the DP Downtown Parking Overlay District standards. The proposed project does not have surface parking or a commercial parking lot. With the proposed drop off area along Marquette Avenue, the applicant is encouraged to place “no parking” or “loading/unloading” signs to ensure that this area is not used for long term parking.

**3. Conformance with the applicable policies of The Minneapolis Plan for Sustainable Growth.**

The Minneapolis Plan for Sustainable Growth identifies the site as mixed use on the future land use map. The proposed development is consistent with the following principles and policies outlined in the comprehensive plan:

**Urban Design Policy 10.1: Promote building designs and heights that enhance and complement the image and form of the Downtown skyline, provide transition to the edges of Downtown and protect the scale and quality in areas of distinctive physical or historical character.**

- 10.1.2 Building placement should preserve and enhance public view corridors that focus attention on natural or built features, such as landmark buildings, significant open spaces or water bodies.
- 10.1.3 Building placement should allow light and air into the site and surrounding properties.

**Urban Design Policy 10.2: Integrate pedestrian scale design features into Downtown site and building designs and infrastructure improvements.**

- 10.2.1 The ground floor of buildings should be occupied by active uses with direct connections to the sidewalk.
- 10.2.2 The street level of buildings should have windows to allow for clear views into and out of the building.
- 10.2.7 Locate buildings so that shadowing on public spaces and adjacent properties is minimized.
- 10.2.8 Coordinate site designs and public right-of-way improvements to provide adequate sidewalk space for pedestrian movement, street trees, landscaping, street furniture, sidewalk cafes and other elements of active pedestrian areas.

**Urban Design Policy 10.3: Use skyways to connect buildings Downtown.**

- 10.3.4 Provide convenient and easily accessible vertical connections between the skyway system and the public sidewalks, particularly along primary transit and pedestrian routes.
- 10.3.5 Maintain functional links in the skyway system while adjoining properties undergo redevelopment or renovation.

**Urban Design Policy 10.14: Encourage development that provides functional and attractive gathering spaces.**

- 10.14.1 Increase resident access to and use of facilities and meeting spaces in parks, libraries, schools, and not-for-profit institutions and places of worship.

10.14.2 Investigate existing gathering spaces on publicly owned land that are underutilized and make recommendations about how they could be improved. Peavey Plaza in downtown Minneapolis is an example of a popular plaza and gathering space in the city.

10.14.3 Encourage the creation of new parks and plazas.

**Urban Design Policy 10.18: Reduce the visual impact of automobile parking facilities.**

10.18.2 Parking lots should maintain the existing street face in developed areas and establish them in undeveloped areas through the use of fencing, walls, landscaping or a combination thereof along property lines.

10.18.5 Design parking structures so sloping floors do not dominate the appearance of the walls.

**Urban Design Policy 10.19: Landscaping is encouraged in order to complement the scale of the site and its surroundings, enhance the built environment, create and define public and private spaces, buffer and screen, incorporate crime prevention principles, and provide shade, aesthetic appeal, and environmental benefits.**

10.19.1 In general, larger, well-placed, contiguous planting areas that create and define public and private spaces shall be preferred to smaller, disconnected areas.

10.19.2 Plant and tree types should complement the surrounding area and should include a variety of species throughout the site that include seasonal interest. Species should be indigenous or proven adaptable to the local climate and should not be invasive on native species.

10.19.6 Green roofs, living walls, and porous pavement are encouraged but are not meant to be a substitute for ground-level landscaping of sites as landscaping provides both a natural amenity and aesthetic beauty to the urban landscape.

10.19.7 Boulevard landscaping and improvements, in accordance with applicable city polices, are encouraged.

**Urban Design Policy 10.22 Use Crime Prevention Through Environmental Design (CPTED) principles when designing all projects that impact the public realm, including open spaces and parks, on publicly owned and private land.**

10.22.1 Integrate “eyes on the street” into building design through the use of windows to foster safer and more successful commercial areas in the city.

10.22.3 Design the site, landscaping, and buildings to promote natural observation and maximize the opportunities for people to observe adjacent spaces and public sidewalks.

10.22.4 Provide on-site lighting at all building entrances and along walkways that maintains a minimum acceptable level of security while not creating glare or excessive lighting of the site.

10.22.5 Locate landscaping, sidewalks, lighting, fencing and building features to clearly guide pedestrian movement on or through the site and to control and restrict people to appropriate locations.

10.22.6 Use innovative building designs and landscaping to limit or eliminate the opportunity for graffiti tagging.

- 10.22.7 Locate entrances, exits, signs, fencing, landscaping, and lighting to distinguish between public and private areas, control access, and to guide people coming to and going from the site.

CPED finds that the proposed development is in conformance with the above policies of The Minneapolis Plan for Sustainable Growth.

#### **4. Conformance with applicable development plans or objectives adopted by the City Council.**

The Loring Park Neighborhood Master Plan (2013) provides additional guidance for the project site. The plan is divided into three subdistricts. The project site is within the Loring Greenway Subdistrict. The master plan recommends that the future land use of the existing church parcel be reclassified as public and institutional and that the southern portion of the project site remain mixed use.

The land use and built form recommendations for the Loring Greenway District is to maintain the quality of the current building stock and pursue opportunities to increase energy efficiency and general sustainability and to encourage transparent building frontage at ground level. The proposed project is consistent with this policy. Although the proposed project includes the demolition of one building, the proposed project preserves the historic structure and the new addition is built with sustainability in mind. In addition, the project will have a transparent building frontage at ground level along the public streets.

The small area plan places emphasis on protection of historic structures and sustainable design, which the proposed project is intending to do. The project will also be in compliance with public realm improvements of having streetscape improvements along Marquette Avenue.

#### **5. Alternative compliance.**

The Planning Commission or zoning administrator may approve alternatives to any site plan review requirement upon finding that the project meets one of three criteria required for alternative compliance. Alternative compliance is requested for the following requirements:

- **First floor proximity to property line | building placement shall reinforce the street wall.** As required by Section 530.110-Building placement, corner lots shall have the buildings within eight feet of each street it fronts and reinforce the street wall. Therefore, the proposed project is required to meet this requirement along Nicollet Mall, Alice Rainville Place, and Marquette Avenue. The purpose of these requirement are to help orient buildings to pedestrians by having them being built near public sidewalks, assist in having buildings be built in relation to each other, and create a sense of enclosure. The proposed addition is at or near the Alice Rainville Place property line. However, it is located approximately 82 feet from the Nicollet Mall property line and 101 feet from the Marquette Avenue property line. Alternative compliance is required. CPED finds that the applicant's proposed alternatives meet the intent of this chapter and the site plan includes amenities or improvements that address adverse effects of the first floor proximity to the property line and the building placement in proximity to the property lines with street frontage. As outlined by the applicant, the greater setback from Nicollet Mall and Marquette Avenue helps the contemporary addition respect the historic significance of the original church building which is eligible for local designation. The applicant's proposed amenities that address the adverse impact of this alternative includes a large plaza area with enhanced landscaping and decorative pavers along Nicollet Mall and a large green space area along Marquette Avenue with an allée of trees and shrubs placement that will help create a sense of enclosure for pedestrians.
- **Blank walls:** As required by Section 530.120-Building design, blank, uninterrupted walls that do not include windows, entries, recesses or projections, or other architectural elements, shall not exceed twenty-five (25) feet in length. The purpose of this requirement is to limit the areas that do

not provide natural surveillance and are not architecturally interesting. With the applicant's proposal, there is a blank, uninterrupted walls that exceed 25 feet in length in three places: along Nicollet Mall (31 feet), Alice Rainville Place (33 feet), and Marquette Avenue (32 feet). Alternative compliance is required. CPED finds that the applicant's proposed alternatives meet the intent of this chapter and the site plan includes amenities or improvements that address any adverse effects of the alternative. This includes a proposed fenestration on all three elevations which will provide natural surveillance opportunities, enhanced landscaping along these blank wall areas and the installation of a public art pieces along Alice Rainville Place that will also mitigate the blank wall condition. In addition, as the applicant points out, the use of exterior materials are arranged in a series of masses that break up the building into distinct components to create visual interest.

- **Window percentage.** As required by Section 530.120, nonresidential buildings require thirty (30) percent of the walls on the first floor that face a public street to be windows. In order to qualify the bottom of any window used to satisfy the ground floor window requirement may not be more than four (4) feet above the adjacent grade. The purpose of this requirement is to encourage eyes on the street and to make elevations interesting for pedestrians by allowing views into and out of the building. The applicant's proposal on the south elevation includes windows where the bottom of the windows are at least nine feet above adjacent grade. Alternative compliance is required. CPED finds that the applicant's proposed alternatives meet the intent of this chapter and the site plan includes amenities or improvements that address any adverse effects of the alternative. The applicant is proposing large glazing areas on the first floor on the south elevation. When calculating window coverage between two and ten feet on the first floor elevation, the applicant is proposing to have 47 percent window coverage. In addition, to help provide visual interest and soften the edge of the south elevation the applicant is proposing an extensive landscape plan along the building wall. This will help provide visual interest for pedestrians.
- **Curb cuts:** As required by Section 530.150-Vehicular access, curb cuts for vehicles shall be consolidated wherever possible. The purpose of this requirement is to minimize conflicts with pedestrian traffic. The applicant is proposing to increase the number of overall curb cuts to the project site from two to three and one to two along Alice Rainville Place. Alternative compliance is required. CPED finds that the applicant's proposed alternatives meet the intent of this chapter and the site plan includes amenities or improvements that address any adverse effects of the alternative. As the applicant outlines, the vehicular access locations proposed are designed to provide an off-street vehicle and bus drop off area, eliminate the need for a surface parking lot, maximize open space and landscaping, and provide for pedestrian amenities. The proposal along Alice Rainville Place will also reduce the linear footage of curb from 48 feet to 41 feet. Furthermore, the design affords site amenities including open space, landscaping, decorative pavers, and a permeable paver driveway. Even though CPED is recommending granting alternative compliance, staff does have concerns about the number of vehicles that may exit the drop off area on Alice Rainville Place and then enter the underground parking along Alice Rainville Place. The department does recognize that the drop off area will accommodate bus loading; however, CPED encourages the applicant to eliminate the loading curb cut for vehicles exiting and have an onsite access point to the underground parking.

## RECOMMENDATIONS

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt staff findings for the applications by Rob Hunter of James Dayton Design at 1221 Nicollet Mall, 1226 Marquette Avenue, 1200 Marquette Avenue.

Applicant Name for the properties located at addresses:

**A. Variance to reduce the minimum floor area ratio.**

Recommended motion: **Approve** the application for a variance to reduce the minimum floor area ratio from 2.0 to 1.2.

**B. Variance to increase the maximum off-street parking.**

Recommended motion: **Approve** the application for a variance to increase the maximum off-street parking from 144 spaces to 264 spaces.

**C. Variance to reduce the drive aisle width.**

Recommended motion: **Approve** the application for a variance to reduce the drive aisle width from 20 feet to 15'-6".

**D. Variance to increase the number of freestanding signs allowed on a zoning lot.**

Recommended motion: **Approve** the application for a variance to increase the number of freestanding signs allowed from one to three, subject to the following condition:

1. The applicant shall show on final plans that the sign cabinet is no greater than 32 square feet in size.

**E. Site Plan Review for a new church addition and outdoor plaza.**

Recommended motion: **Approve** the site plan review application to allow for a 49,244 square foot church addition and new outdoor plaza space, subject to the following conditions:

1. All site improvements shall be completed by June 13, 2018, unless extended by the Zoning Administrator.
2. CPED staff shall review and approve the final site, elevation, landscaping, and lighting plans before building permits may be issued.
3. The first floor windows shall have clear or lightly tinted glass with a visible light transmittance ratio of six-tenths (0.6) or higher.
4. The plant materials, and installation and maintenance of the plant materials, shall comply with sections 530.200 and 530.210 of the zoning code.
5. All portions of the mechanical equipment shall be screened in compliance from 535.70.
6. A lighting plan shall be submitted prior to building permit issuance that shows full compliance with Section 535.590-Lighting and Chapter 541.340-Lighting.
7. The plaza shall be maintained in good order by the property owner for the life of the plaza. Proper maintenance shall include, but not be limited to, snow and ice removal, annual maintenance of vegetation and green space and annual inspection and repair and/or replacement of furnishings. Minimum landscaping and seating requirements shall be maintained for the life of the plaza. All adjacent streets, sidewalks and pathways shall be inspected regularly for purposes of removing any litter found thereon.

## ATTACHMENTS

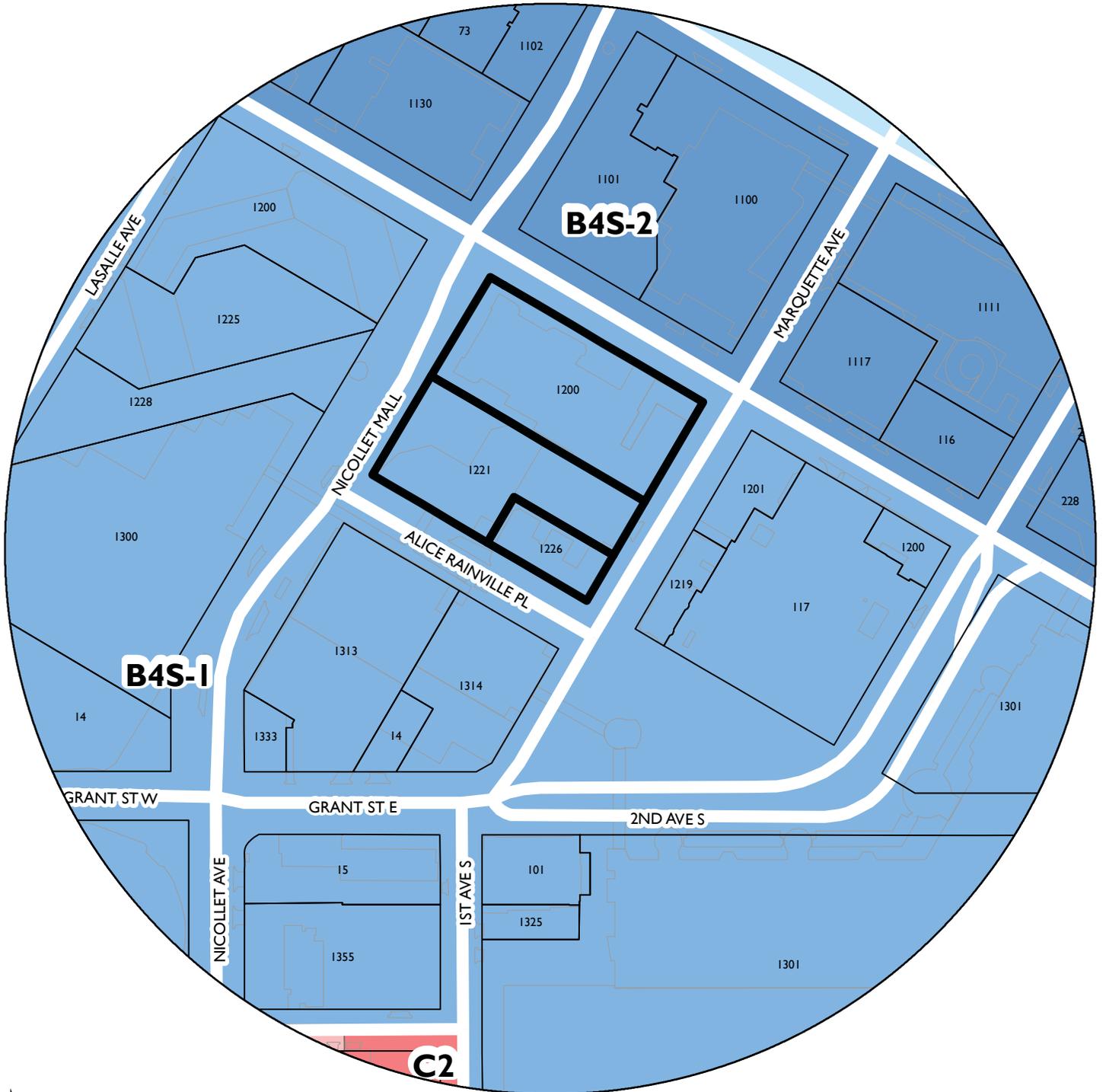
1. Zoning map
2. Aerial
3. Written description | statement to findings | statement to alternative compliance requests
4. Travel demand management plan
5. Site photos
6. Architectural plan set
7. Landscape plan set
8. Civil plan set
9. Plaza set
10. PDR report

# Westminster Presbyterian Church

7th

NAME OF APPLICANT

WARD



PROPERTY ADDRESS

**1200 Marquette, 1226 Marquette, and 1221 Nicollet Mall**

FILE NUMBER

**BZZ-7688**





# WESTMINSTER PRESBYTERIAN CHURCH

MINNEAPOLIS, MN

SUBMITTAL FOR GENERAL LAND USE APPLICATION  
AND SITE PLAN REVIEW

APRIL 20, 2016

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April 20, 2016

Mr. Aaron Hanauer, AICP  
Senior City Planner – Land Use, Design and Preservation  
City of Minneapolis – Community Planning and Economic Development  
250 South 4<sup>th</sup> Street – Room 300  
Minneapolis, MN 55415

*RE: Property Owner Authorization for Land Use Application to the City of Minneapolis for the:  
Westminster Presbyterian Church Project (1200 & 1226 Marquette Ave. and 1221 Nicollet Mall)*

Dear Mr. Hanauer,

Please accept this letter as authorization by Westminster Presbyterian Church, the owner of the above referenced properties, for James Dayton Design, Ltd. to submit on Westminster's behalf the attached General Land Use Application to the City of Minneapolis.

Thank you for your consideration of this proposal. Please do not hesitate to contact me if you have any questions.

Sincerely,

A handwritten signature in black ink, appearing to be 'P. Hyde', enclosed within a circular scribble.

Paul Hyde  
Chair, Board of Trustees  
Westminster Presbyterian Church  
1200 Marquette Ave  
Minneapolis, MN 55403

## **SECTION 1**

- STATEMENT OF PROPOSED USE AND PROJECT DESCRIPTION
- GENERAL PROPERTY INFORMATION AND PROJECT DATA

## STATEMENT OF PROPOSED USE AND PROJECT DESCRIPTION

### *WESTMINSTER PRESBYTERIAN CHURCH PROJECT*

#### **Proposed Use**

Westminster Presbyterian Church proposes a project involving renovations and additions to its church building (“Westminster Presbyterian Church Project” or “Project”) that includes:

- A two-story, 49,000 square-foot, church addition housing a large worship, performance, and gathering hall, multi-purpose room, classroom, youth spaces, church offices, and community partner spaces;
- 30,000 square feet of interior renovations to the historic church building to provide two new libraries, office space, and improved life safety and accessibility features;
- Two levels of underground parking accommodating 237 vehicles;
- Two outdoor plazas – one along Nicollet Mall and Alice Rainville Place and one along Marquette Avenue and Alice Rainville Place;
- A direct connection to the existing skyway that crosses Alice Rainville Place; and
- A range of stormwater Best Management Practices (“Stormwater BMPs”).

#### **Project Description**

Westminster Presbyterian Church is a vibrant, open-minded community of 3,100 members. An engaged, urban partner, Westminster has focused its Mission on being “A Telling Presence” in the City since 1857. The original Westminster Church on this site was constructed in 1897, with subsequent additions made in the 1930’s and late 1990’s. The historic nature of the existing church building is reflected by its inclusion on the National Register of Historic Places.

This proposed building addition engages the south side of the existing building at its middle third, creating centralized interior circulation and fellowship space in the heart of the church with entrances that connect to the two exterior plazas. Within this space are a large ornamental stair and elevators connecting the underground parking with the floors above. This circulation and fellowship space is flanked by principal church functions – the existing main sanctuary, the new worship, performance, and gathering space, the two new libraries, a multi-purpose room, and a new classroom. Additional spaces being created are a new youth room, an internal meditation garden, and space on the second floor for church offices, community partners, and new restrooms.

The new worship, performance, and gathering hall will be a flexible space that can be configured for various worship forms, musical and theatre performances, meetings, dinners, community events, and church gatherings. It will seat up to 300 persons and replaces the Great Hall in the existing church that could seat up to 225 persons.

The community partner space, which comprises 25% of the space in the new addition, will house one of Westminster's existing community partners and one or more new partners whose services will benefit the community. The principle entrance to this space will be from Alice Rainville Place through the tower at the southwest corner of the new addition or through the existing skyway that will attach to the tower.

Renovations to the existing church building focus on replacing the two existing libraries, which must be removed to make the connection to the new addition. To do so, the current Great Hall will be renovated to hold two new libraries and open gathering space. Other renovations will open up circulation space, replace existing bathroom space with larger bathrooms sized to accommodate current and future uses, renovate and enlarge the existing kitchen, and make other adjustments occasioned by connecting the new addition to the existing church building.

The existing church exterior will see two major improvements. First, the walls along Nicollet Mall will be removed and replaced with sloping green spaces and new stairs that lead directly to the sanctuary's center entry doors. Second, that portion of the existing copper screen extending from the Nicollet Mall to the 12<sup>th</sup> Street church entrance will be removed and this area reworked to present a more open view of the historic church building. However, most of the existing church building - including its 1,400 person capacity main sanctuary and its 180 person capacity chapel, church school spaces, choir spaces, and other meeting rooms – will not be affected by the Project.

The new addition's design sets the structure back from both Nicollet Mall and Marquette Avenue. The Nicollet Mall setback both respects the significance of the historic church building and restores the original design intent of the church by exposing the historic south façade to view from Nicollet Mall and Alice Rainville Place. This view has been largely obstructed for over thirty years by the adjacent office structure.

The Nicollet Mall and Marquette Avenue setbacks enable the creation of two plazas that are owned by the church but designed as if they belong to the City. The Nicollet plaza addresses Nicollet Mall, the Loring Greenway, the new Loring Woods, and Alice Rainville Place to help create a consequential southern node to Nicollet Mall. The Marquette plaza addresses Marquette Avenue and Alice Rainville Place with a shaded tree alley to reinforce the street edges, provide shaded pedestrian areas, and enclose the flat lawn area of the plaza that will be used by youth and other congregants for gatherings. The drive lane in this plaza accommodates school buses, coach busses, general vehicular drop off, and deliveries.

Together, these plazas provide an open and welcoming streetscape along Alice Rainville Place, a major thoroughfare between the Minneapolis Convention Center and Nicollet Mall. These plazas and the large expanses of glass on all facades of the building reflect Westminster's core principles of openness, inclusivity, and civic engagement.

**GENERAL PROPERTY INFORMATION AND PROJECT DATA**  
*WESTMINSTER PRESBYTERIAN CHURCH PROJECT*

**Zoning Districts:**

Minneapolis Code of Ordinances, Title 20 – Zoning Code (“Zoning Code”) in Section 521.30, Primary Zoning Districts Plate 19 and Overlay Zoning Districts Plate 19 zones the Westminster Presbyterian Church property as follows:

- Primary: B4S-1
- Overlay: DP (downtown parking)

**Parcels And Site Area:**

1. 1200 Marquette Ave. (existing church) <i>PID# 27-029-24-13-0266</i>	1.34 acres	58,434 SF
2. 1221 Nicollet Mall (existing office building) <i>PID# 27-029-24-13-0276</i>	1.06 acres	46,271 SF
3. 1226 Marquette Ave. (existing parking lot) <i>PID# 27-029-24-13-006</i>	0.28 acres	12,375 SF
<hr/>		
TOTAL SITE AREA OF ALL PARCELS (1,2,3)	2.68 acres	117,080 SF

**Building Areas As Proposed:**

A. Existing building above grade	= 94,254 GSF
B. New building addition above grade	= 49,244 GSF
C. Existing building below grade	= 35,411 GSF
D. Existing below grade parking under church	= 12,074 GSF
E. New P1 parking level below grade	= 56,215 GSF
F. New P2 parking level below grade	= 41,574 GSF
<hr/>	
TOTAL GROSS SQUARE FOOTAGE (A thru F)	= 288,772 GSF
TOTAL GSF OF ABOVE GRADE ONLY (A & B)	= 143,498 GSF

**Floor Area Ratio As Proposed:**

The FAR, based on above grade floors only = 1.23

**Vehicular Parking:**

The Project proposes 237 new vehicular parking spaces all located in a new underground parking structure. Existing surface parking will be removed, and no surface parking is proposed. All proposed parking spaces are standard size, and no compact spaces are proposed.

PARKING DATA AS PROPOSED (*spaces are all for church use – none commercial*)

1. Existing underground parking under church	= 27 stalls
2. New proposed underground parking	= 237 stalls
3. <u>Existing and proposed surface parking</u>	= 0 stalls
<b>TOTAL PARKING AS PROPOSED</b>	<b>= 264 stalls</b>

**Bicycle Parking:**

Bicycle parking will be provided in locations both at grade in the plaza along Marquette and in the P1 level of the underground parking ramp. Per Zoning Code Section 541.180 (c), one bicycle parking space is required for every 20 vehicular parking spaces.

- Required Bicycle Spaces = 14 (*264 vehicular spaces/20*)
- Proposed Bicycle Spaces = 30 (*8 at grade, 22 in parking level P1*)

**Building Height:**

- A. Existing Building
  - 3 stories; 111'-7" to top of tower
- B. New Addition
  - 2 stories; 61'-7" to top of tower (99'-6" to top of bell carillon)

**Exterior Materials By Elevation**

Material	Allowed Max.	Description	North	South	East	West
Stone	100%	Honed Limestone	N/A	27%	20%	30%
Stucco	75%	Thermocromex Plaster	N/A	19%	15%	11%
Metal Panels	75%	Zinc-Coated Copper	N/A	30%	36%	30%
Glass	100%		N/A	24%	29%	29%

**Trash Storage And Snow Removal:**

Trash, recycling, and all other refuse storage will be entirely within the underground parking garage, and all will be removed from the parking garage by the church's contracted waste disposal services. Snow removal will be off-site.

**Mechanical Equipment And Transformers:**

All mechanical building equipment is located on the building roof and screened from view. All electrical transformers are located in existing underground vaults located on Marquette Avenue and Alice Rainville Place. There are no grilles or other mechanical protuberances on any façade of the new addition.

## **SECTION 2**

- REQUESTED VARIANCES

## REQUESTED VARIANCE TO REDUCE MINIMUM FLOOR AREA RATIO

### *WESTMINSTER PRESBYTERIAN CHURCH PROJECT*

#### **Summary Of Variance Request:**

Westminster Presbyterian Church requests approval of the following variance:

Zoning Code Table 549-5 *Downtown Service District Building Bulk Requirements*: Establishes 2.0 as the minimum floor area ratio for non-residential buildings in the B4S-1 District.

The applicant requests a variance to permit a minimum floor area ratio of 1.23 due to unique circumstances presented by the property that require a density that is impractical for the expansion of a unique and long established existing use in the District.

#### **Findings:**

- 1) *Requested variance is authorized.*  
Zoning Code Section 525.520(3) authorizes variances to “the gross floor area, floor area ratio and seating requirements of a structure or use.”
- 2) *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*
  - a) The proposed variance is due to unique circumstances presented by the property. The existing church is a unique and long established use located within the Downtown Service District. The original church building, constructed in 1897, preceded current minimum density requirements and does not meet them. The minimum density requirement which governs the only available property for church expansion presents a unique and prohibitive circumstance as expansion of the church to meet this density would exceed any current expansion needs or reasonable density expectations for this type of use.
  - b) The Project includes provisions for a future density increase that would bring the density closer to compliance with the ordinance. Specifically, the structure under the plaza space fronting Marquette Avenue is designed to accommodate future building expansion of up to three stories.
  - c) We understand that the Zoning Code does not allow basement space to be counted in determining the floor area ratio (“FAR”) because basement spaces are not normally used heavily. This is not the case with the existing church basement, which includes classrooms, a full service kitchen, a large refectory, and other spaces that are used extensively for church programs. If the existing basement square footage were to be counted, the proposed FAR would be 1.53.

- 3) *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*
  - a) The spirit and intent of the ordinance and comprehensive plan is to promote mixed-use neighborhoods and development along commercial corridors that enhance the street's character and foster pedestrian movement. An engaged, urban partner, Westminster is a unique use in the District that has dynamic programs in worship, social justice, education, and art and music, all of which will expand with this Project.
  - b) The proposed plazas provide pedestrian amenities addressing Nicollet Mall, Marquette Avenue, Alice Rainville Place, and the Convention Center, protect the historic church, and afford exceptional and innovative sustainable design elements, all of which meet the spirit and intent of both the ordinance and Loring Park Neighborhood Master Plan.
  - c) The proposed Stormwater BMPs meet the intent of the comprehensive plan.
  
- 4) *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*
  - a) The proposed variance would permit expansion of an engaged community partner with a 120-year history on the property allowing a greater number of services.
  - b) The plazas would replace existing hardscape areas of roughly the same size with lush plantings, permeable paving, water features, art, and a range of sustainable design elements and provide public, pedestrian oriented amenities. This would enhance rather than alter or injure the essential character of the locality and nearby property.
  - c) The two-story plus height of the church addition would not be appreciably different from the height of Orchestra Hall and the YWCA to the immediate north or the two-to three-story office structures to the immediate east.
  - d) The proposed variance would have no impact on the health, safety, or welfare of the general public or those using the building or nearby properties.

**REQUESTED VARIANCE TO INCREASE MAXIMUM OFF-STREET PARKING**  
*WESTMINSTER PRESBYTERIAN CHURCH PROJECT*

**Summary of Variance Request:**

Westminster Presbyterian Church requests approval of the following variance:

Zoning Code Table 541-2 *Specific Off-Street Parking Requirements—Downtown Districts*: Establishes the maximum amount of off-street parking for Institutional and Public Uses as 1 space per 1,000 square feet of GFA. Pursuant to this Table, the maximum amount of off-street parking permitted for the project is 144 spaces.

The applicant requests a variance to permit an increase in the maximum amount of off-street parking to 264 spaces. This request is due to the unique nature of the use within the District and unique parking demands associated with this use.

**Findings:**

- 1) *Requested variance is authorized.*  
Zoning Code Section 525.520(6) authorizes variances to “vary the applicable minimum and maximum number of required off-street parking, stacking or loading spaces.”
- 2) *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*
  - a) The church is a long established use in the District having unique parking demands. The church’s main sanctuary seats 1,400 persons; its chapel seats 180; and the new performance and gathering space will accommodate up to 300. These spaces are and will be used not only on Sundays but also on other days of the week for events including worship services, weddings, funerals, church meetings neighborhood gatherings, and the Westminster Town Hall Forum. The latter, for example, often fills the sanctuary at noon on Thursdays with overflow seating in the Great Hall. The maximum parking amounts, which govern the only available property for church expansion, represent a unique circumstance as they are below the minimum parking demands associated with a church use, especially this large and vibrant church.
  - b) Off-site ramps do exist in reasonable proximity to the church, and the church will continue to use off-site ramp parking. However, another unique circumstance in the church’s vicinity is that large events at surrounding institutions, including Orchestra Hall and the Convention Center, often and unpredictably compromise

off-street, ramp parking. The requested variance allows the minimum amount of on-site parking necessary to ensure current and long-term church access.

- c) Unique demands also include accommodating parking for a place of assembly, which is not addressed in the governing District table. Tables governing other Districts identify Religious Institutions as a specific use, recognize the unique parking demands of assembly places, and provide far greater parking maximums than those in the Downtown Service District table. The proposed variance, which is well below what other District tables permit for places of assembly, accounts for other transportation and parking options available in the District and would provide parking amounts appropriate for this unique assembly use.
- d) Another unique circumstance is the disconnect between transit availability in Downtown and the timing of most church events. Transit to Downtown is maximized to handle the morning and evening rush hours on the weekdays. However, most church events either do not occur during rush hours or do not occur on weekdays. The obvious example is Sunday worship. However, weddings and funerals either occur on Saturdays or are scheduled at 10:00 a.m. to noon, in the afternoons from 1:00 to 4:00 p.m., or in the evenings on weekdays. This means far fewer transit options are available for church events, underlining the need for adequate parking.

- 3) *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

All proposed parking is located in an underground garage and no surface parking is proposed, enhancing the compatibility of parking with the building's surroundings. The proposed variance to increase the amount of underground parking will secure the continued longevity of the church as an engaged, urban partner, and is in keeping with the spirit and intent of the ordinance and comprehensive plan.

- 4) *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

- a) The additional underground parking would not alter the essential character of the locality nor would it be injurious to use or enjoyment of other property in the area. Rather, by allowing sufficient church parking underground, the variance actually benefits the character of the locality and surrounding property by eliminating the existing surface parking lot on the church block.
- b) The proposed variance would permit additional underground parking with typical peak time of operation on Sunday mornings, when other traffic and pedestrian activity in the vicinity is limited. Most church events during weekdays will not coincide with morning or evening rush hours. So, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or those using the property or nearby properties.

**REQUESTED VARIANCE TO PERMIT ALTERNATIVE PARKING ANGLE AND DRIVE AISLE DIMENSION**  
*WESTMINSTER PRESBYTERIAN CHURCH PROJECT*

**Summary of Variance Request:**

Westminster Presbyterian Church requests approval of the following variance:

Zoning Code Table 541-6 *Minimum Parking Space and Aisle Dimensions*: Establishes minimum dimensions for off-street parking spaces and drive aisles for the following parking angles: 0°, 45°, 60° and 90°, having 12', 12', 18' and 20' wide drive aisles for one-way traffic, respectively. Minimum standard parking space width for all parking angles is 8'-6".

The applicant requests a variance to permit 65° angle parking stalls, 8'-9" wide, with 15'-6" wide drive aisles for one-way traffic flow. This request is made to provide a high level of service that balances the existing dimensional limitations of the property with the operational capacity needs for on-site parking.

**Findings:**

- 1) *Requested variance is authorized.*  
Zoning Code Section 525.520(14) authorizes variances to "reduce the minimum required width of parking aisles or to increase the maximum width of driveways in any zoning district, as regulated in Chapter 541, Off-Street Parking and Loading . . ."
- 2) *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*
  - a) The proposed variance is due to unique circumstances presented by the property. Using the permitted tabular parking angles, the existing dimensions of the property do not allow for an operationally efficient parking design with a high enough service level to meet the church's parking demands. The proposed variance would provide wider, more comfortable parking stalls exceeding City stall width requirements, and a slightly narrower drive aisle for a more balanced service level.
  - b) The proposed dimensions meet or exceed the recommended aisle width dimensional guidelines for the National Parking Association as published in their "Guidelines for Parking Geometrics", "Figure 5: Common Parking dimensions", published in April of 2002. This demonstrates that the proposed variance will function properly and efficiently.
  - c) Additionally, the variance would permit all spaces to be standard size.

Eliminating the need for compact spaces increases the functionality of the parking ramp, since users will not have to avoid or seek out compact spaces.

- 3) *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

All proposed parking is located in an underground garage, and the existing surface parking is eliminated, enhancing the compatibility of parking with the building's surroundings. The proposed variance as described above will help secure the continued longevity of the church as an engaged, urban partner, and is in keeping with the spirit and intent of the ordinance and comprehensive plan.

- 4) *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

- a) The proposed variance allows a practical enhancement of level of service for parking users, allows all standard size parking spaces, and provides the necessary underground stall capacity to meet church needs on-site.
- b) By allowing sufficient church parking underground, the variance actually benefits the character of the locality and surrounding property by eliminating the existing surface parking lot on the church block.
- c) The proposed variance to permit 8'-9" wide, 65° parking stalls and 15'-6" one-way drive aisles meets or exceeds National Parking Association Guidelines. This means the parking ramp would operate in manner not detrimental to the health, safety, or welfare of the general public or of those using the church property or nearby properties.

**REQUESTED VARIANCE TO INCREASE MAXIMUM NUMBER OF FREESTANDING SIGNS**  
*WESTMINSTER PRESBYTERIAN CHURCH PROJECT*

**Summary of Variance Request:**

Westminster Presbyterian Church requests approval of the following variance:

Zoning Code Table 543.3 *Specific Standards for Signs in the Downtown District*: Permits a maximum of one freestanding sign per zoning lot.

The applicant requests a variance to permit three freestanding signs. Two freestanding signs already exist – one in front of the historic church at Nicollet Mall and 12<sup>th</sup> Street and one at the intersection of Marquette Avenue and Alice Rainville Place. The third would be an additional sign at the intersection of Marquette Avenue and Alice Rainville Place. The Project property encompasses an entire city block, and this request is made to provide effective signage commensurate with the size, wayfinding, and identification needs of the property.

**Findings:**

- 1) *Requested variance is authorized.*  
Zoning Code Section 525.520(21) authorizes variances to “the number, type, height, area or location of allowed signs on property located in an OR2 or OR3 District or a commercial, downtown or industrial district . . .”
- 2) *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*  
The proposed variance is due to circumstances unique to the property in that it encompasses an entire city block and is surrounded by major Downtown vehicular and pedestrian thoroughfares. Having only two signs limits the opportunity for identification and wayfinding. The proposed variance would provide signage that is both proportionate to the size of the property and appropriate to meet the identification and wayfinding needs to address these thoroughfares.
- 3) *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*
  - a) The spirit and intent of the ordinance is to provide effective, attractive signage, appropriate to the character of the District that minimizes clutter and confusion.

The proposed variance meets this intent by providing signage that is commensurate to the size of the property at locations where signage is most needed to avoid confusion, and that is appropriate to the character of the District.

- b) The two signs at the corner of Marquette Avenue and Alice Rainville Place would be placed at right angles to each other paralleling and reinforcing each street frontage. The signs would also align with trees in the allee, which also serve to define the street frontages. This design approach matches the spirit and intent of the ordinance and comprehensive plan.

4) *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

- a) The proposed variance would permit signs in the most practical, safest, and informative locations on the property, addressing surrounding vehicular and pedestrian thoroughfares where signage is most needed. The signs are designed to work in concert with the building and landscaping, are modest in scale relative to the size of the property, and will not materially impact nearby properties.
- b) The placement of the signs will promote wayfinding and be set back from the sidewalks to avoid interfering with pedestrians. So, the additional signs will benefit wayfinding by the general public and area users and will not be detrimental to the health, safety, or welfare of the general public or those using the property or nearby properties.

## **SECTION 3**

- ALTERNATIVE COMPLIANCE REQUESTS

## **ALTERNATIVE COMPLIANCE REQUEST FOR BUILDING PLACEMENT**

### *WESTMINSTER PRESBYTERIAN CHURCH PROJECT*

#### **Summary of Alternative Compliance Request:**

The following requests are made for consideration for alternative compliance from site plan requirements:

Zoning Code Section 530.110 *Building Placement*: Establishes that the first floor of buildings shall be located not more than eight (8) feet from the lot line.

See the attached site plan (sheet C4.0) for setback dimensions to the property lines, which are greater than eight feet. The proposed siting of the building addition respects the historic church facades, provides space to meet vehicular drop-off demands on site, affords the creation of public plaza and landscape amenities, and mitigates practical challenges associated with the existing first floor level of the church being greater than six feet above sidewalk grade.

#### **Findings:**

- 1) The greater setback from Nicollet Mall both respects the historic significance of the original church building and restores the original urban design intent of the building by exposing the historic south façade. The south façade has been largely obstructed for over thirty years by the adjacent office building.
- 2) The greater setback from Marquette allows for a drop off drive to accommodate school buses, coach busses, and general vehicular drop off on-site without the need for surface parking. This setback also provides space for possible future building expansion as the parking structure below is designed to accommodate a building of up to three stories in height over either a portion of, or the entirety of, this area.
- 3) The greater setback distances allow for the creation of two public plazas having lush landscape plantings, trees, water features, art, decorative pavers, seating, and a variety of stormwater Best Management Practices. The Nicollet plaza addresses Nicollet Mall, the Loring Greenway and the new Loring Woods to help create a consequential southern node to the Nicollet Mile, and both plazas together provide for an open and welcoming streetscape along Alice Rainville Place.
- 4) The existing first floor of the church building is more than six feet above sidewalk grade. The greater setbacks allow for a gradual transition between the sidewalk and the existing first floor. Bridging this distance within the plazas allows stairs and accessible

ramps to provide well-designed transitions for pedestrians while also avoiding tall blank walls that would result on street frontages if these transitions were interior to the building.

- 5) The proposed building placement is consistent with Alternative Compliance Guidelines. The greater setback distances afford site amenities, including open space, landscaping, decorative pavers, and other enhancements while also addressing the impracticalities of certain existing site conditions.

## **ALTERNATIVE COMPLIANCE REQUEST FOR VEHICLE ACCESS**

### *WESTMINSTER PRESBYTERIAN CHURCH PROJECT*

#### **Summary of Alternative Compliance Request:**

The following requests are made for alternative compliance from site plan requirements:

Zoning Code Section 530.150 *Vehicular Access*: States “Vehicular access and circulation shall be designed to minimize conflicts with pedestrian traffic and with surrounding residential uses. Curb cuts for vehicles shall be consolidated wherever possible.”

The applicant proposes alternative compliance to permit three curb cuts to allow vehicular site access for:

- A proposed one-way, vehicular, drop-off lane within the plaza along Marquette Avenue and Alice Rainville Place having entry from Marquette and exit onto Alice Rainville Place.
- Proposed access to the underground parking on Alice Rainville Place located 30’ to the west of the drop-off lane exit.

The vehicular access locations proposed are designed to provide an off-street vehicle and bus drop off area, eliminate the need for a surface parking lot, maximize open space and landscaping, provide for pedestrian amenities, and accommodate vehicle movements that do not interfere with regular traffic patterns, pedestrian traffic, or surrounding uses.

#### **Findings:**

- 1) The proposed vehicular access design reduces the linear footage of existing curb cuts on the property. The proposed 24’ curb cut on Marquette is the same width as currently exists. The two curb cuts proposed for Alice Rainville total 41’, which is less than the existing single 48’ wide curb. The separation of the existing curb cut on Alice Rainville into two curb cuts provides a more pedestrian friendly environment and allows for streetscaping plantings between the two access points.
- 2) The proposed one-way drop-off lane accommodates school buses, coach busses, and general vehicular drop off on-site without the need for surface parking or a large paved area for turning maneuvers. This maximizes the amount of open space, increases the amount of landscape plantings, and affords more pedestrian friendly amenities such as seating, tree canopy, and a large flat lawn for events and gatherings.
- 4) As indicated in the Traffic Demand Management Plan (“TDMP”), drop off demand during church services is modest with limited traffic traversing the drop off lane. The majority of vehicular traffic will be concentrated at the parking garage access.

- 5) As illustrated in the TDMP, vehicle movement may be readily executed into and out of the proposed site access points without tracking into adjacent or opposing lanes. This includes vehicle movement exiting the drop-off lane and entering the underground parking garage on Alice Rainville Place.
- 6) The TDMP indicates that the future conditions level of service remains unchanged and high at adjacent street intersections and site access points. Only the level of service at parking garage access, while remaining high, decreases slightly from its current status, but would be in alignment with the parking garage level of service.
- 7) The proposed vehicular site access design and curb cuts are consistent with Alternative Compliance Guidelines. The design affords site amenities including open space, landscaping, decorative pavers, and other enhancements while benefitting the character of the locality and surrounding property by eliminating the existing surface parking lot on the church block.

## **ALTERNATIVE COMPLIANCE REQUEST FOR BLANK WALLS**

### *WESTMINSTER PRESBYTERIAN CHURCH PROJECT*

#### **Summary of Alternative Compliance Request:**

The following requests are made for consideration for alternative compliance from site plan requirements:

Zoning Code Section 530.120 *Building Design*: Establishes that the building shall not have blank, uninterrupted walls that do not include windows, entries, recesses, projections or other architectural elements that exceed twenty-five feet in length.

As illustrated in the attached elevations, there are blank walls exceeding twenty-five feet on three facades. It is proposed that the 30'-9" blank wall facing Nicollet, the 33'-1" blank wall facing Alice Rainville Place, and the 31'-7" blank wall facing Marquette be permitted.

#### **Findings:**

- 1) The building facades are comprised of four building materials: stone, zinc panels, plaster, and glass. These materials are arranged in a series of masses that break up the building into distinct components to create visual interest and respond to the interior functions of the building.
- 2) The room behind the blank wall facing Nicollet is a large assembly space accommodating over 300 people for worship, musical and theatrical performances, dinners, meetings, community events, and other large gatherings. A large glass wall is provided into this room on the Nicollet façade, however, additional openings would prohibitively limit the uses and functionality of this space. The first floor of this façade consists of 43% windows, exceeding the 30% minimum City requirement.
- 3) The upper portion of the 33'-1" blank stone wall along Alice Rainville Place is left blank for a commissioned piece of artwork. The lower portion of the stone wall is the outside wall of the underground parking garage and not appropriate for windows (the first floor level of the existing church, being matched by the addition, is 6-8 feet above grade). Windows in the lower portion of the stone wall would also detract from the artwork to be placed immediately above. The first floor of this façade consists of 32% windows, exceeding the 30% minimum City requirement.
- 4) The space behind the blank wall facing Marquette is the parking garage entry, which is not appropriate for windows. As shown on renderings, coniferous trees will be planted in front of this wall to provide visual interest and largely conceal this blank wall. The first floor of this façade consists of 30% windows, meeting the 30% minimum City requirement.

**SECTION 4**  
▪ TRAFFIC DEMAND MANAGEMENT PLAN

**Travel Demand Management Plan (TDMP)**

Westminster Presbyterian Church - Building Addition  
and Renovation

Minneapolis, Minnesota

SEH No. WALKP 130895

April 18, 2016



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Engineers | Architects | Planners | Scientists

**Certification Page**

Westminster Presbyterian Church - Building Addition and Renovation  
Travel Demand Management Plan (TDMP)  
Minneapolis, Minnesota

SEH No. WALKP 130895

April 18, 2016

I (we), on behalf of the following agencies, accept the data and findings presented in the following report as valid conclusions and an appropriate response for the implementation of the proposed project.

I (we) furthermore agree to the implementation of the recommendations described herein as a condition of the project approval.

**Signed by:**

\_\_\_\_\_  
Steve P. Mosing, PE, PTOE  
City of Minneapolis, Traffic Operations Engineer  
Date: \_\_\_\_\_

\_\_\_\_\_  
Steve Poor  
City of Minneapolis, CPED Development Services Director  
Date: \_\_\_\_\_

\_\_\_\_\_  
Dennis Alfton, Director of Operations  
Westminster Presbyterian Church  
1200 Marquette Avenue  
Minneapolis, MN 55403  
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## Distribution List

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4	Dennis Allton, Director of Operations Westminster Presbyterian Church 1200 Marquette Avenue Minneapolis, MN 55403 <i>(3 to sign and return, 1 to keep)</i>
1	Robert A. Hunter AIA, ALA James Dayton Design, Ltd. 1515 Central Ave NE Suite 150 Minneapolis, MN 55413 <i>(For your records)</i>

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April 2016

# Travel Demand Management Plan (TDMP) Westminster Presbyterian Church - Building Addition and Renovation

Prepared for Westminster Presbyterian Church

## 1.0 Introduction

Westminster Presbyterian Church is currently undergoing design activities for an addition and renovation project. Westminster Presbyterian Church is located at 1200 Marquette Avenue in Downtown Minneapolis as shown in Figure 1. The church building currently occupies one half of the block and the church owns the adjacent vacant 1221 Nicollet Tower.

The Westminster Presbyterian Church (Westminster) project will include a major addition on the 1221 Nicollet Tower property connecting to the historic church building. Renovations and additions to Westminster include:

A 30,000 square foot renovation of the interior of the existing building to provide new:

- Libraries
- Office Space
- Collaborative Work Space
- Improved life safety and accessibility features

A two-story, 41,000 square foot church addition to provide:

- Gathering space
- Multi-purpose room
- Classroom
- Youth spaces
- Church offices
- Community partnership spaces
- Two levels of underground parking
- Significant outdoor plazas and gardens
- Stormwater Best Management Practices (BMPs)

Figure 2 shows the site plan for the addition and renovation project.

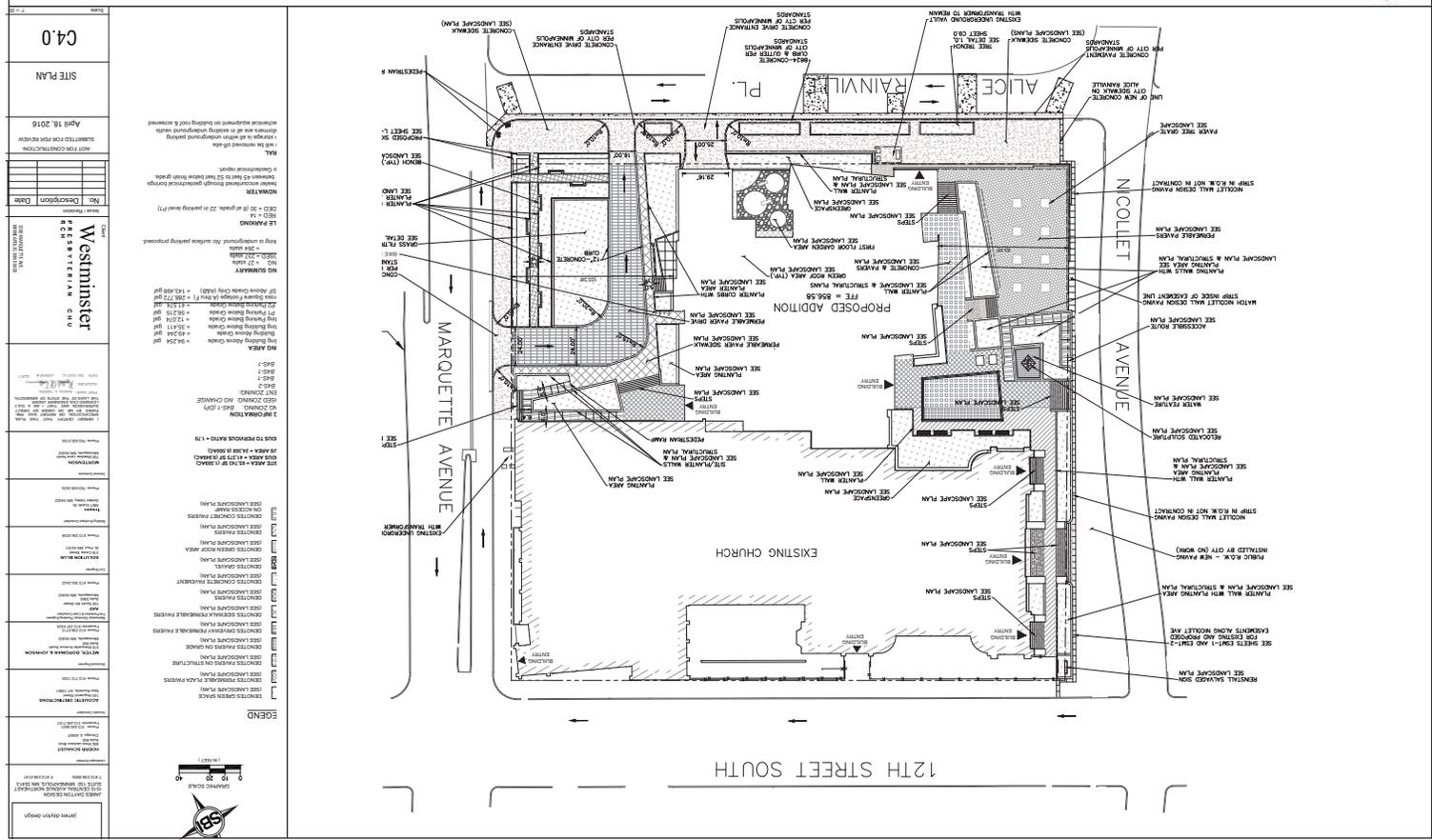
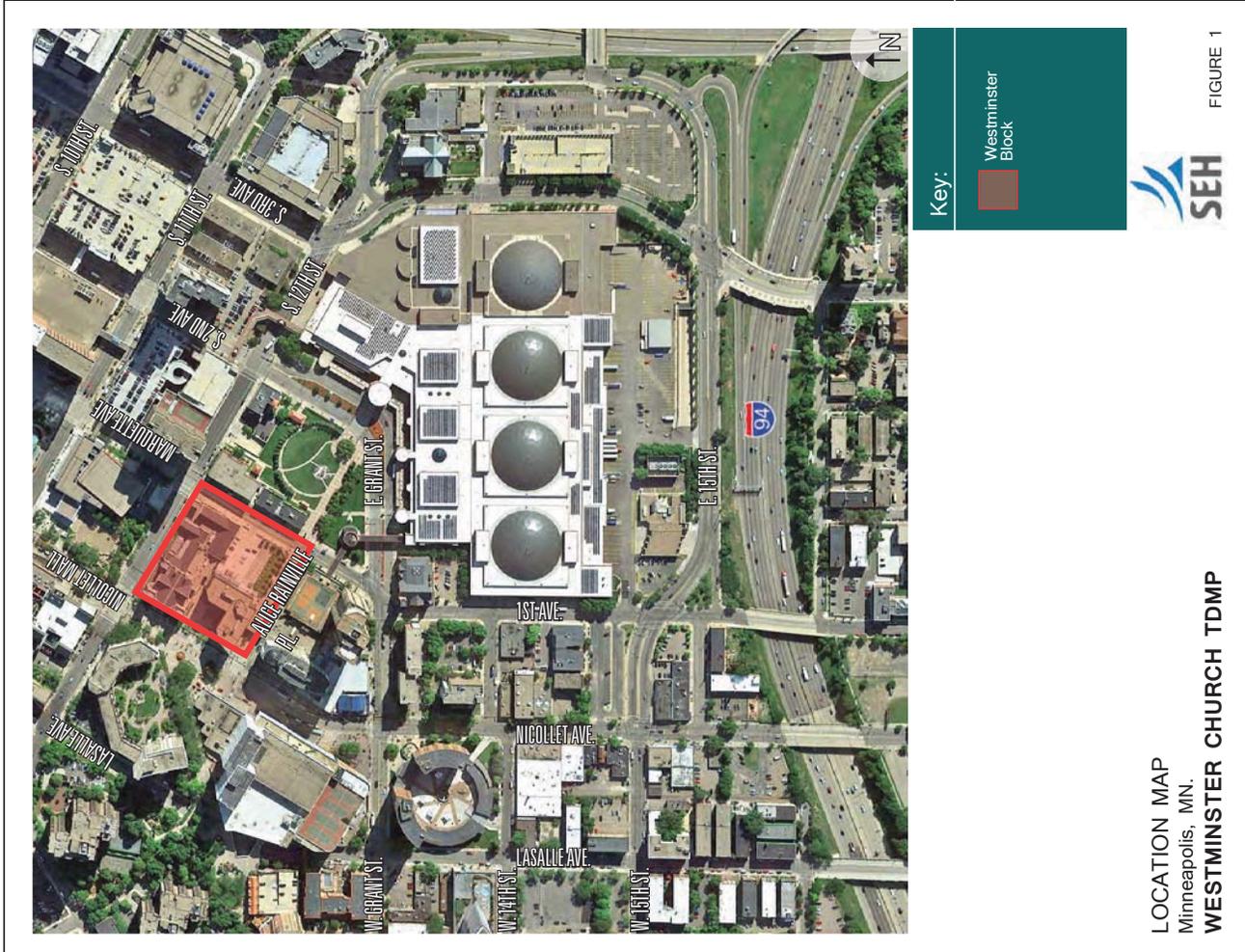


FIGURE 1



LOCATION MAP  
Minneapolis, MN.  
WESTMINSTER CHURCH TDMP

## 2.0 Travel Demand Management Plan

The City of Minneapolis requires that Westminster Presbyterian Church prepare a Travel Demand Management Plan (TDMP) for the proposed building addition and renovation plan.

## 2.1 Travel Demand Management Plan Scope

The purpose of the Travel Demand Management Plan for Westminster Presbyterian Church is to support the City of Minneapolis in accomplishing their sustainability transportation goals as discussed in the Minneapolis Plan for Sustainable Growth adopted in 2009. The objective of the Westminster Presbyterian Church TDM plan is to document the site operations during the peak period for the church, Sunday mornings. In addition the TDMP addresses the planned access, circulation, parking, church programming and transportation impacts related to the addition and renovation of the church.

This TDM plan was prepared with assistance from the City of Minneapolis Community Planning and Economic Development (CPED) and Public Works Departments to ensure that key issues were addressed accurately and sufficiently.

The primary focus of a TDMP is the management of effects of the proposed addition and renovation on the local street network, parking demand, transit system, and other multimodal facilities and initiatives. During a February 25, 2015 meeting and subsequent conversations, City staff identified the following primary issues for the Westminster Presbyterian Church TDMP:

- Site operations, access and circulation
- Parking
- Typical Sunday street operations - existing and future

## 3.0 Site Operations, Access and Circulation

Access and circulation of motorists, transit, bicyclists and pedestrians at Westminster Presbyterian Church are described in the following sections.

## 3.1 Existing Site Access and Operations

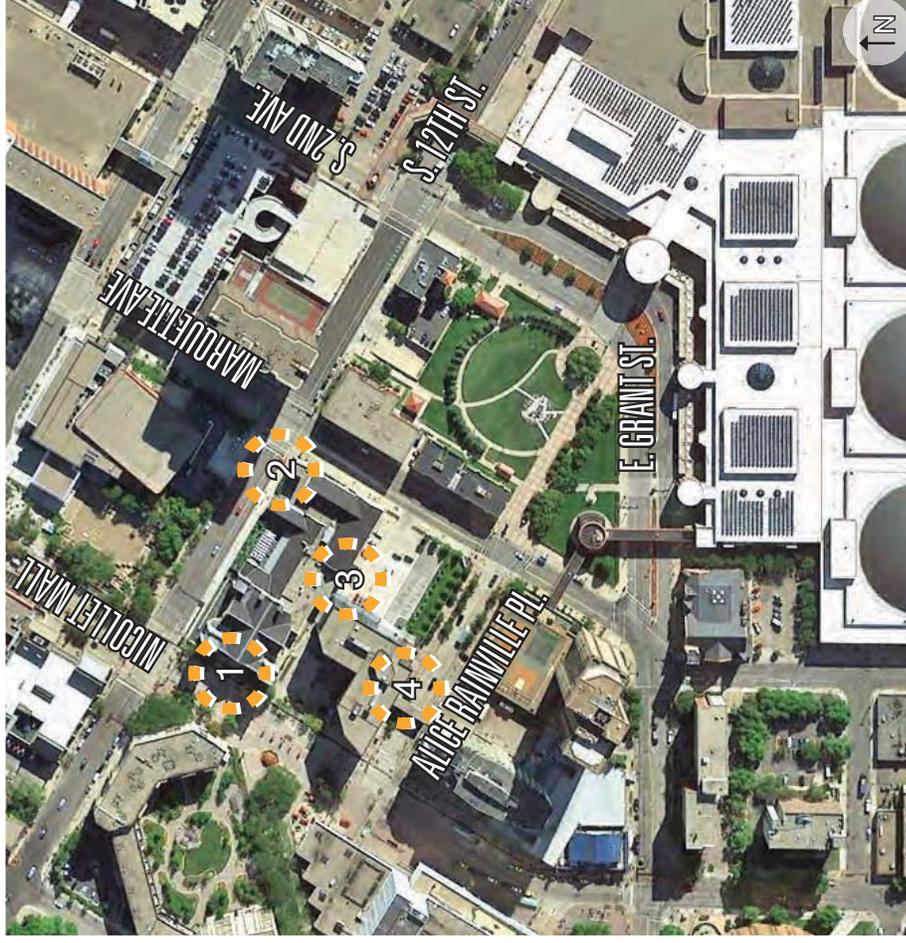
The primary access locations for the existing Westminster building are located as shown in Figure 3 and listed below.

- Access 1 – West side (Nicollet Mall)
- Access 2 – NE Corner (Marquette Avenue and 12<sup>th</sup> Street)
- Access 3 – South side (Existing Surface Lot)
- Access 4 – Underground Parking Garage (below church and 1221 Nicollet Building)

Church services occur regularly on Sunday mornings as follows:

- Service 1: 8:30 AM – 9:15 AM
- Service 2: 10:30 AM – 11:30 AM

Between 9:15 AM and 10:15 AM the church offers adult education and Church School for children. Following the 10:30 service, the church attendees may participate in fellowship activities reducing the effects of departure for a more even dispersion of people departing the building. The church hosts movies of the month once per month following Service 2, and also hosts a town hall forum eight times per year.



Key:



BUILDING ACCESS  
WESTMINSTER CHURCH TDMP

FIGURE 3

The church sanctuary holds 1,500 people with an average Sunday attendance of approximately 800 people. The church congregation size is approximately 3,100 and the current number of employees who come to Westminster throughout the course of a week is 74, including 30 full time employees.

There is one on-site surface parking lot and one underground parking garage accessed from Alice Rainville Place, below the church and 1221 Nicollet Tower. The church also utilizes the Orchestra Hall Parking ramp at the intersection of 11<sup>th</sup> Street and Marquette Avenue. Parking in this ramp is provided to church visitors free of charge. The church pays the ramp operator 0.75 per vehicle. Data from two typical Sundays, including March 13, 2016 (the date count data were obtained), show the following parking in the Orchestra Hall ramp:

- February 28, 2016 – 172 parked vehicles
- March 13, 2016 – 184 parked vehicles

The peak period of traffic generation for the church occurs on Sundays between 9:30AM and 12:15PM. Based on count data obtained, the church's peak hours of arrival and departure occur between services (Arrival) and following Service 2 (Departure). The times of these peak hours occur are:

- Arrival: 9:30 AM – 10:30 AM
- Departure: 11:15 AM – 12:15 PM

### 3.2 Future Site Access and Operations

The proposed addition and renovation project will maintain the existing building access locations, with access 3 (from Figure 3 – Building Access) opening onto a plaza and drop off zone rather than into the surface parking lot, which is eliminated with the project. The drop off zone at the Marquette Avenue Plaza will accommodate school buses, coach buses and general delivery.

The projected total number of employees for Westminster in 2025 is 82, with no increase in full time employees. The addition and renovation project will improve the church's functionality and operational capacity. Significant new programming and schedule modifications are not currently planned as part of the project, and the sanctuary is not being expanded.

The project provides 21<sup>st</sup> century gathering and learning spaces, improves classrooms and support spaces, and prepares the building for the future. The *Open Doors Open Futures* project is the most ambitious Westminster building project since the historic church was built in 1897. This project uses world-class building and open space design to provide a meaningful physical transformation of Westminster while dramatically improving their operational capacity to meet the needs of the church and surrounding community.

The project includes elimination of the on-site surface parking lot and construction of a new underground parking garage in place of the existing 1221 Nicollet Tower garage, which will be accessed from Alice Rainville Place. The parking spaces below the church will be maintained. The church will continue to utilize the Orchestra Hall Parking ramp at the intersection of 11<sup>th</sup> Street and Marquette Avenue.

Westminster projections show an increase in the congregation size from 3,100 to 3,800 by 2025.

### 3.2.1 Planned Site Driveways

The planned drop off area on the new Marquette Avenue Plaza will include an inbound (westbound) entrance driveway from Marquette Avenue and an outbound (southbound) driveway to Alice Rainville Place. There will also be a driveway to the underground parking garage on Alice Rainville Place as shown in Figure 2.

The driveway radii and widths are shown in the Figure 4. Concrete drive entrances include 10-foot radii and are designed per City of Minneapolis requirements.

The turning radius of three vehicle types was reviewed for the driveways to and from the drop off area to ensure that they will accommodate the anticipated user mix and travel patterns. Figure 4 shows the passenger vehicle movement may be readily executed without tracking into the adjacent or opposing lanes. The turning radius for motorists potentially accessing the underground garage was tested as well to ensure that this movement could be completed without issue during a Sunday service period with minimal traffic unrelated to the church traveling along Alice Rainville Place.

Turning radii were also reviewed for a school bus and overland bus. Figures 5 and 6 illustrate that these maneuvers may be executed without tracking into the adjacent or opposing lanes.

Figure 4 – Passenger Vehicle Turning Analysis

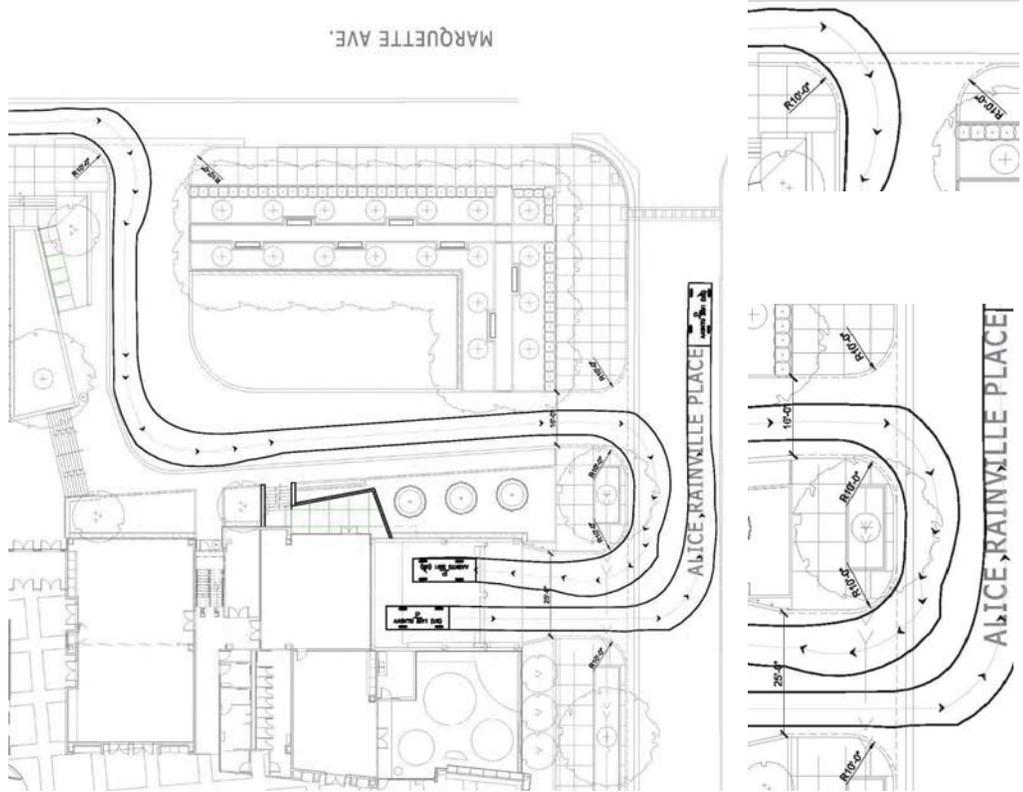


Figure 5 – School Bus Turning Analysis

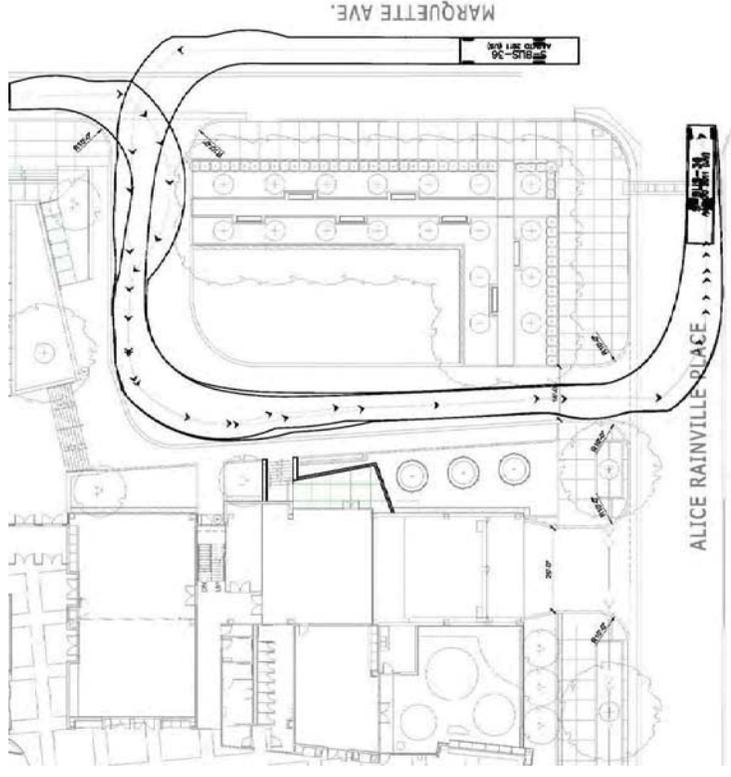
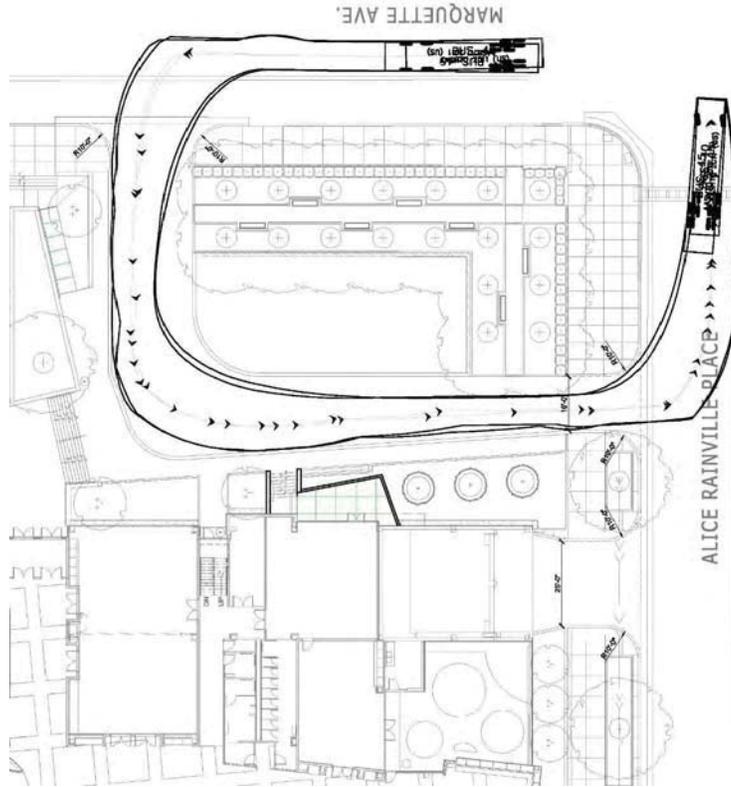


Figure 6 – Overland Bus Turning Analysis



### 3.3 Circulation

The arrival and departure peak hours were observed on March 13, 2016 on a "typical" church day, without a special event at the church, to gain understanding of the circulation patterns around Westminster Presbyterian Church. While the church did not have an event this date, the nearby Minneapolis Convention Center was hosting the Twin Cities Auto Show which likely resulted in increased traffic volumes during the latter part of the morning count period.

In addition to observations of circulation patterns, volume data for motorists and pedestrians were collected using video cameras and manual counting of the footage. The collected volume data is documented in Figures 7 and 8 for two peak hours, generally called "arrival" and "departure".

The peak "arrival" period identified occurred between church services, during the hour when people are departing the first church service and arriving for the second church service. The peak departure hour occurred while people departed the second church service.

#### 3.3.1 Peak Arrival Period

##### 3.3.1.1 Motorists

Motorists used Marquette Avenue to approach and depart the church and adjacent parking. Motorists also used 12<sup>th</sup> Street, then Marquette Avenue, to access nearby parking at the Orchestra Hall parking ramp and along 11<sup>th</sup> Street. Motorists were observed parking in the following locations:

- Surface lot on the church site with access to Marquette Avenue
- Orchestra Hall parking ramp with access to 11<sup>th</sup> Street and Marquette Avenue
- Underground parking garage at the 1221 Nicollet Tower

Approximately 26 vehicles entered the surface lot and 38 vehicles exited. Approximately 57 vehicles entered the underground parking garage at 1221 Nicollet Tower and 26 vehicles exited during the peak arrival period (Figure 7).

All motorists tended to arrive and disperse relatively evenly to the parking facilities from the adjacent street network.

##### 3.3.1.2 Pedestrians

The pedestrians observed during data collection primarily arrived in platoons traveling to and from the Orchestra Hall parking ramp to Westminster, crossing both Marquette Avenue and 12<sup>th</sup> Street. Approximately 90 pedestrians were recorded crossing the north and west leg of the intersection of Marquette Ave and 12<sup>th</sup> Street S. The other pedestrians observed at this intersection were headed toward the Twin Cities Auto Show at the Convention Center. The other three intersections at which data were collected had much less pedestrian activity, as shown in Figure 8.

#### 3.3.1.3 Drop Off and Pick Up

Observations showed a small number of motorists engaged in drop off and pick-up activity. Approximately 10 vehicles dropped off or picked visitors up at the location between 9:30 AM and 10:30 AM. A couple of these motorists were there to pick up a fruit order that the church parishioners had ordered as part of a fundraising activity.

Per church operations staff, the observed drop off and pick up activity correlated with their typical observations. Drop-off activities observed were orderly.

### 3.3.1.4 Bicyclists

During the morning arrival period, a small number of bicyclists were observed riding on the street traveling north along Marquette Avenue and east along 12<sup>th</sup> Street.

### 3.3.2 Departure

#### 3.3.2.1 Motorists

During the departure period, a higher number of motorists were observed at the intersections for which data were collected due to the 10:30 AM church service being the busiest service of the day. Generally, traffic leaving the church was able to disperse the immediate area with no significant delay at the study intersections or queuing on the streets or into parking facilities. All motorists tended to disperse relatively evenly on the adjacent street network. Similar to the arrival period, motorists used Marquette Avenue to depart the church and adjacent parking and they also used 11<sup>th</sup> Street and Marquette Avenue to depart the Orchestra Hall parking ramp.

The volume data shown on Figure 7 show that during the departure period there were approximately 88 vehicles exiting from the 1221 Nicollet Tower Parking garage and approximately 44 vehicles exiting from the surface lot driveway during the peak departure period.

#### 3.3.2.2 Pedestrians

As with the conclusion of any event, the peak departure period of pedestrian activity included greater volume of pedestrians than during arrival. Immediately following the conclusion of the last church service, pedestrian activity was more concentrated than during the AM arrival.

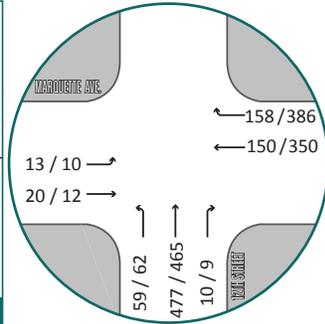
The majority of pedestrians were observed traveling to the Orchestra Hall parking ramp. At the intersection of Marquette Avenue at 12<sup>th</sup> Street, a total of 115 pedestrians were observed crossing the west leg, 152 crossing the south leg, 88 crossing the north leg and 152 crossing the east leg. Pedestrians were also observed accessing vehicles in the adjacent surface lot with far fewer traversing through the other three intersections that were counted for the project.

#### 3.3.2.3 Drop Off and Pick Up

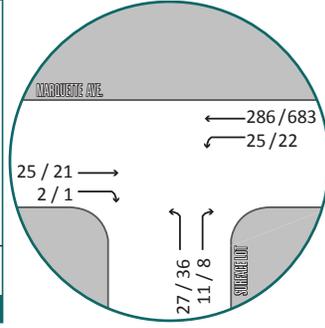
Observations showed a small number of motorists engaged in drop off and pick-up activity. Approximately 14 vehicles dropped off or picked visitors up at the location between 11:15 AM and 12:15 PM. A couple of these motorists were there to pick up a fruit order that the church parishioners had ordered as part of a fundraising activity.

Per church operations staff the observed drop off and pick up activity correlated with their typical observations. Drop-off activities observed were orderly.

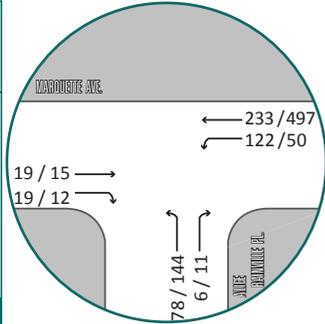
1 12th St. and Marquette Ave.



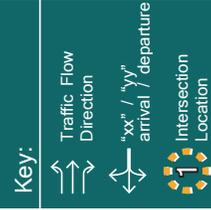
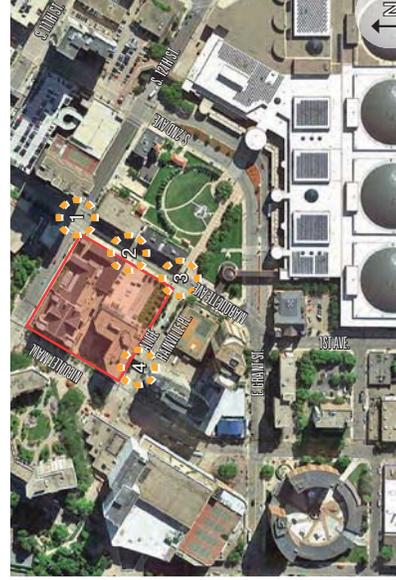
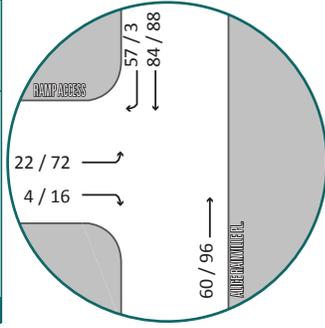
2 Marquette Ave. and Surface Lot



3 Alice Rainville Pl. and Marquette Ave.

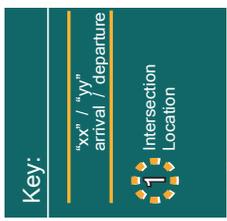
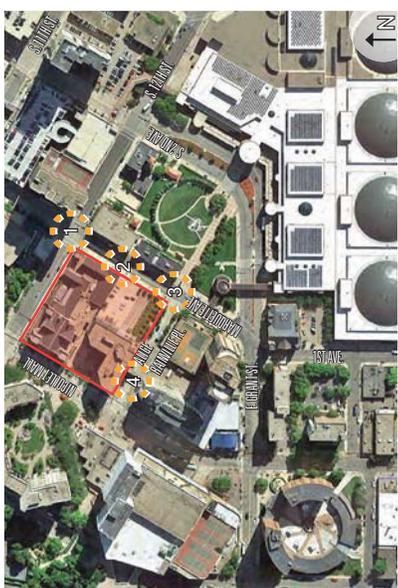
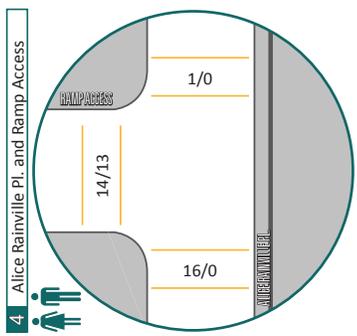
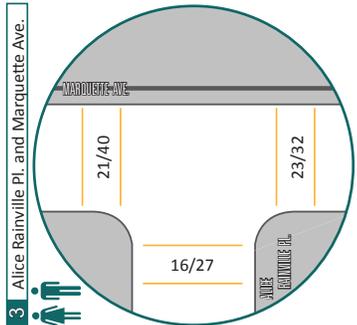
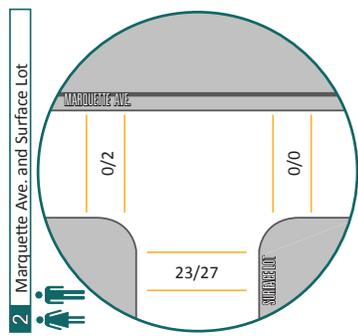
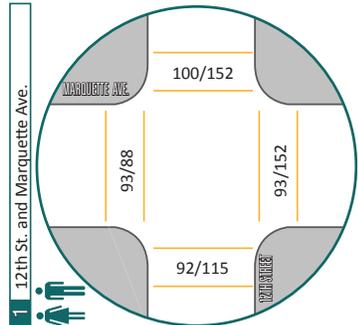


4 Alice Rainville Pl. and Ramp Access



EXISTING PEAK HOUR TURNING MOVEMENTS  
Sunday Arrival (9:30am-10:30am) & Departure (11:15am-12:15pm)  
WESTMINSTER CHURCH TDMP MARCH 2016





**PEAK HOUR PEDESTRIAN CROSSWALK COUNTS**  
 Sunday Arrival (9:30am-10:30am) & Departure (11:15am-12:15pm)

**WESTMINSTER CHURCH TDMP** MARCH 2016

FIGURE 8



**3.3.2.4 Bicyclists**

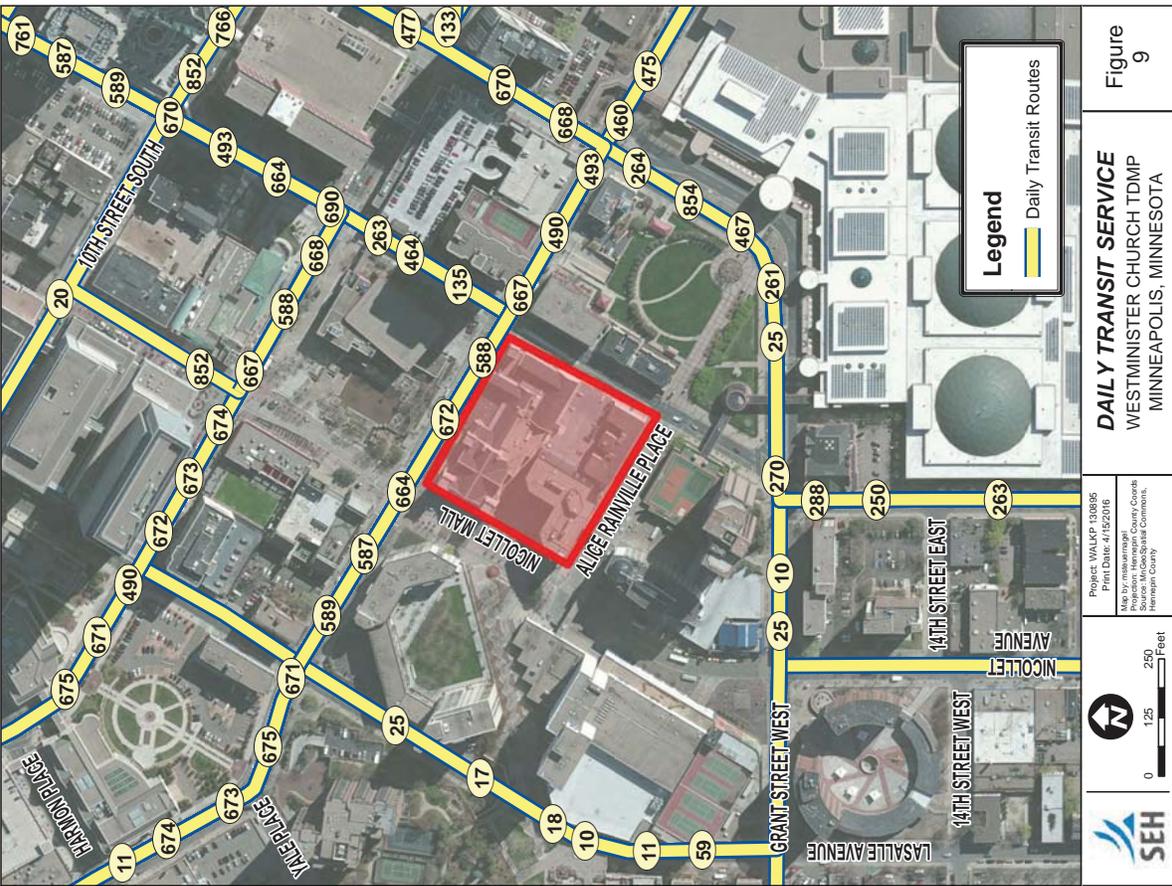
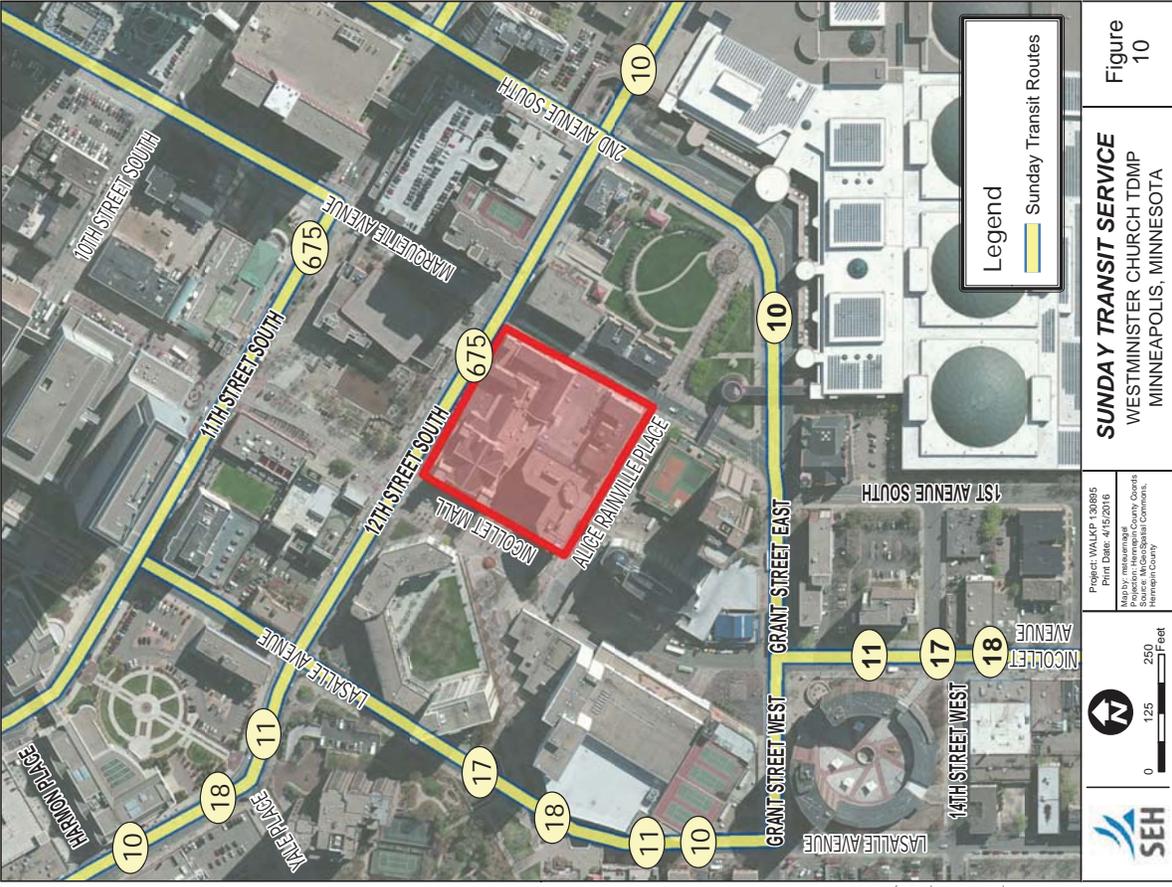
During the peak departure period, a small number of bicyclists were observed riding on the street traveling north along Marquette Avenue.

**3.3.3 Transit**

Westminster has access to several transit routes within a two-block area during weekdays and on Sundays. These routes are shown on Figures 9 and 10.

The route frequency on Sunday is as follows:

- Route 10 - 4:30 AM - 2:30 AM (every 15 -30 minutes)
- Route 11 - 4:30 AM - 1:30 AM (every 30-60 minutes)
- Route 17 - 5:00 AM - 2:00 AM (every 30 minutes)
- Route 18 - 4:30 AM - 2:00 AM (every 10-15minutes)
- Route 675 - 6:00 AM - 10:00 PM (once an hour)



#### 4.0 Parking

The current parking on-site at Westminster includes a 31 space surface lot and 160 spaces of underground parking, including 27 spaces under the existing church building. Observations conducted showed the two lots to be full between 10:30 AM and 11:30 AM.

The proposed addition and renovation project will eliminate the surface parking lot and construct a new underground parking garage connected to the existing underground parking below the church, which will continue to offer 27 spaces.

The proposed underground parking garage will be accessed on Alice Rainville Place and provide a total of 264 parking spaces for Westminster Church, all below ground.

Table 1  
Existing and Proposed Westminster Parking

Item	Units	Existing	Proposed Increase	Total
Surface Parking Lot	Spaces	31	-31	0
Underground Parking - Below Church	Spaces	27	0	27
Underground Parking - Other	Spaces	102	135	237
Total On-Site Parking Supply	Spaces	160	104	264
Building Above Ground	SQ FT	94,254	49,244	143,498
Parking Allowed by Downtown District Parking Ordinance*	Spaces			144
Parking Supply +/-	Spaces			120

Notes: Code 541.170 Institutional Land Use, Table 541-2 Specific Off-Street Parking Requirements - Downtown Districts. 1 space per 1,000 sq. ft. of GFA.

The parking supply was compared to the City of Minneapolis Zoning Code for the Downtown District in Zone B4S. The comparison to zoning code shows that the onsite parking supply exceeds the code by 120 parking spaces.

A variance request is being submitted to allow for the proposed number of parking spaces. All proposed parking is located in an underground garage and no surface parking is proposed, enhancing the compatibility of parking with the building's surroundings. The maximum parking amounts which govern the only available property for church expansion represent a unique circumstance as they are below the minimum parking demands associated with a church use.

Among the demands that the requested variance address is accommodating the minimum amount of on-site parking necessary to ensure current and long term church access. The church will also continue to utilize off-site ramp parking. However, off-site parking is often and unpredictably compromised by large events at surrounding institutions. Unique demands also include accommodating parking for a place of assembly, which is not addressed in the governing District table. Tables governing other Districts identify *Religious Institutions* as a specific use, recognizing the unique demands at places of assembly, and provide far greater parking maximums than those in the Downtown Service District table. The proposed

variance, well below what other District tables permit, accounts for the other transportation and parking options available in the Downtown District while providing parking amounts appropriate for this unique assembly use.

The following section provides analysis of future traffic operations based on the proposed number of parking spaces provided in the underground parking garage and current parking patterns and demonstrates that the adjacent streets and intersections will operate acceptably with the increased supply on-site.

#### 5.0 Traffic Operations

Traffic operations were evaluated using Synchro/SimTraffic software. The City of Minneapolis Central Business District traffic model was obtained and traffic signal timing data were confirmed based on provided timing sheets. The model was reduced to include only the pertinent study area intersections to replicate arrival patterns. To best simulate the arrival and departure peaks that occur with events such as a church service, the turning movement volumes were entered and simulated in 15-minute intervals. Conflicting pedestrian movements were also included.

Reporting was completed using SimTraffic results for an average of five runs. Traffic operations were reviewed for existing and future conditions (2025).

#### 5.1 Existing Conditions

All intersections operate at a LOS B or better in both the peak arrival and departure period as shown in Table 1 below. The worst movements at any intersection operate at a LOS C or better.

Table 2  
Existing Sunday Traffic Operations

Intersection	Approach	Delay (s/veh)						LOS By Approach		LOS By Intersection	
		L		T		R		Delay (S/Veh)	LOS	Delay (S/Veh)	LOS
		LOS	T	LOS	R	LOS	R	LOS	LOS	Delay (S/Veh)	LOS
12th Street South at Marquette Ave (Signal)	NB	0.0	A	8.6	A	5.7	A	7.2	A	5.9	A
	SB	13.9	B	7.8	A	0.0	A	9.5	A		
	EB	9.2	A	4.5	A	3.2	A	4.9	A		
	WB	0.0	A	0.0	A	0.0	A	0.0	A		
	NB	1.9	A	0.7	A	0.0	A	0.8	A		
	SB	0.0	A	0.3	A	0.6	A	0.3	A	1.1	A
Marquette Ave at Surface Lot Driveway	EB	6.0	A	0.0	A	3.3	A	5.2	A		
	WB	0.0	A	0.0	A	0.0	A	0.0	A		
	NB	5.6	A	5.0	A	0.0	A	5.2	A		
	SB	0.0	A	2.5	A	1.0	A	1.8	A	8.8	A
	EB	30.6	C	0.9	A	15.5	B	26.5	C		
	WB	0.0	A	0.0	A	0.0	A	0.0	A		
Marquette Ave at Alice Rainville Pl (Signal)	NB	0.0	A	0.0	A	0.0	A	0.0	A		
	SB	4.7	A	0.1	A	2.9	A	4.3	A	0.8	A
	EB	0.0	A	0.1	A	0.0	A	0.1	A		
	WB	0.0	A	0.6	A	0.3	A	0.5	A		
	NB	0.0	A	9.7	A	7.6	A	8.6	A		
	SB	16.8	B	7.7	A	0.0	A	11.5	B	6.4	A
12th Street South at Marquette Ave (Signal)	EB	8.7	A	2.8	A	3.9	A	3.5	A		
	WB	0.0	A	0.0	A	0.0	A	0.0	A		
	NB	1.6	A	1.2	A	0.0	A	1.2	A		
	SB	0.0	A	0.3	A	0.2	A	0.3	A	1.6	A
	EB	9.0	A	0.0	A	3.3	A	8.2	A		
	WB	0.0	A	0.0	A	0.0	A	0.0	A		
Marquette Ave at Surface Lot Driveway	NB	6.6	A	5.6	A	0.0	A	5.7	A		
	SB	0.0	A	1.7	A	1.5	A	1.6	A	10.0	B
	EB	31.0	C	3.1	A	14.2	B	26.0	C		
	WB	0.0	A	0.0	A	0.0	A	0.0	A		
	NB	6.7	A	0.0	A	4.8	A	6.3	A	2.3	A
	SB	0.0	A	0.8	A	0.0	A	0.8	A		
Alice Rainville Pl at Nicollet Tower Driveway	EB	0.0	A	0.4	A	0.2	A	0.4	A		
	WB	0.0	A	0.4	A	0.2	A	0.4	A		
	NB	0.0	A	0.0	A	0.0	A	0.0	A		
	SB	0.0	A	0.0	A	0.0	A	0.0	A		
	EB	0.0	A	0.0	A	0.0	A	0.0	A		
	WB	0.0	A	0.0	A	0.0	A	0.0	A		
Alice Rainville Pl at Nicollet Tower Driveway	NB	0.0	A	0.0	A	0.0	A	0.0	A		
	SB	0.0	A	0.0	A	0.0	A	0.0	A		
	EB	0.0	A	0.0	A	0.0	A	0.0	A		
	WB	0.0	A	0.0	A	0.0	A	0.0	A		
	NB	0.0	A	0.0	A	0.0	A	0.0	A		
	SB	0.0	A	0.0	A	0.0	A	0.0	A		

5.2 Future Conditions (2025) – Traffic Volume

The planned project includes removal of the surface parking lot and reconstruction of the parking garage to include a total of 264 parking spaces. This results in a shift in how vehicles access parking on the site. Additionally, with the increased parking supply available, it is assumed that more visitors will elect to park closer to the building, thus filling the underground parking garage before accessing the Orchestra Hall parking ramp two blocks to the north.

As a result of this travel pattern shift, trips were redistributed to the parking garage driveway at Alice Rainville Place. In addition, the increased trips due to the increased parking supply were added to reflect full conditions at the underground garage.

To determine the appropriate number of trips to add to reflect full or maximum parking at the new underground garage, existing data were used. It was assumed that the future arrival and departure peak period parking activity would be similar to that occurring today. Thus the proportions of inbound and outbound autos to the on-site parking supply (number of spaces)

during existing conditions was used along with the future supply to estimate future inbound and outbound trips.

Table 3  
Westminster Underground Parking Garage – Existing and Future Trips

Period	Scenario	Parking Supply (Spaces)	Inbound Trips (Vehicles)	Outbound Trips (Vehicles)	TOTAL Trips (Vehicles)
Arrival Peak Hour	Existing	160	84	66	150
	Proposed	264	139	109	248
	Increase	104	55	43	98
Departure Peak Hour	Existing	160	26	132	158
	Proposed	264	43	218	261
	Increase	104	17	86	103

The increased trips shown above were added to the network, routed to enter and exit the underground garage. To both simplify the analysis and be conservative, no trips were subtracted from Marquette Avenue to reflect any rerouting of through traffic to the Westminster underground garage. In other words, at the intersection of Alice Rainville Place and Marquette Avenue, no trips were subtracted from northbound through trips though they were added to the northbound left. It was surmised that this approach also reasonably accounts for an increase in area trips due to the projected 2025 increase in congregation size (approximately 22%).

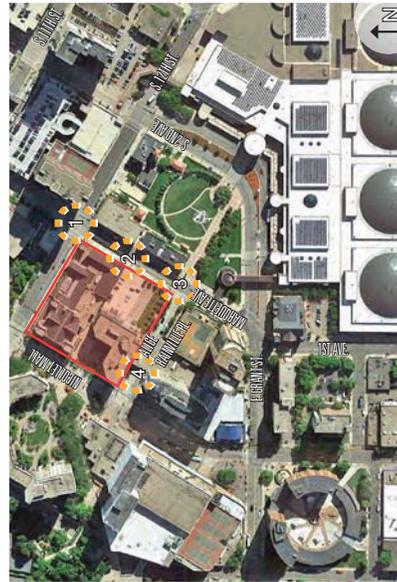
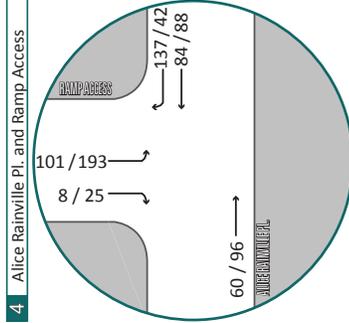
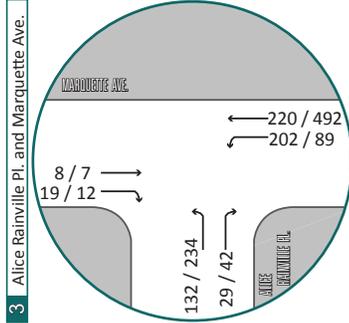
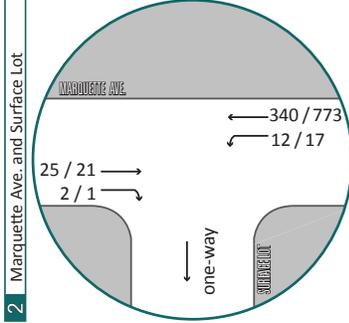
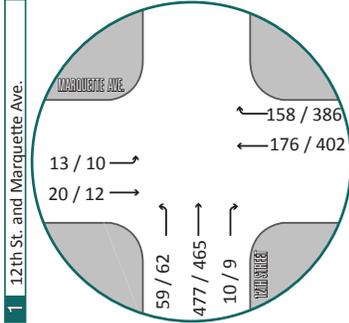
To validate this approach, another proportion was used to determine if the decision not to subtract trips would adequately account for the projected increase in traffic due to the increased congregation size projected for 2025. Table 4 shows the estimation of the potential new trips that would be split between the arrival and departure peak hours due to the projected trip increase.

Table 4  
Westminster 2025 Trip Estimation

Year	Congregation Size (People)	Average Sunday Attendance (People)	Increase (People)	Assumed People per Vehicle	New Inbound Trips* (Vehicles)	New Outbound Trips* (Vehicles)
Existing	3,100	800	-	-	-	-
2025	3,800	981	181	2	91	91

The volume of 91 trips in and out between both peak periods is more than accounted for by no subtraction of the through trips to reroute them to the expanded underground parking garage. Further, the presence of event traffic at the convention center (Twin Cities Auto Show) in the existing data, also provides a measure of conservatism to the analysis.

The estimated 2025 turning movements are shown on Figure 11.



**2025 PEAK HOUR TURNING MOVEMENTS**  
 Sunday Arrival (9:30am-10:30am) & Departure (11:15am-12:15pm)  
**WESTMINSTER CHURCH TDMP**

**Key:**

- Traffic Flow Direction (indicated by arrows)
- "xx" / "yy" arrival / departure
- Intersection Location (indicated by a '1' in a square)



FIGURE 11

**5.2.1 Future Conditions (2025) – Traffic Operations**

The model was updated to reflect the estimated 2025 traffic volumes and closure of the surface parking lot. Under future conditions, all intersections operate at a LOS B or better in both the peak arrival and departure period as shown in Table 2 below. The worst movements at any intersection operate at a LOS C or better.

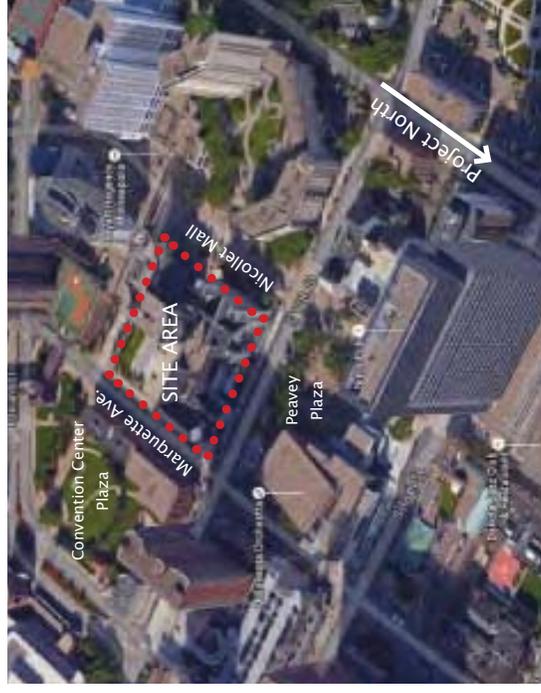
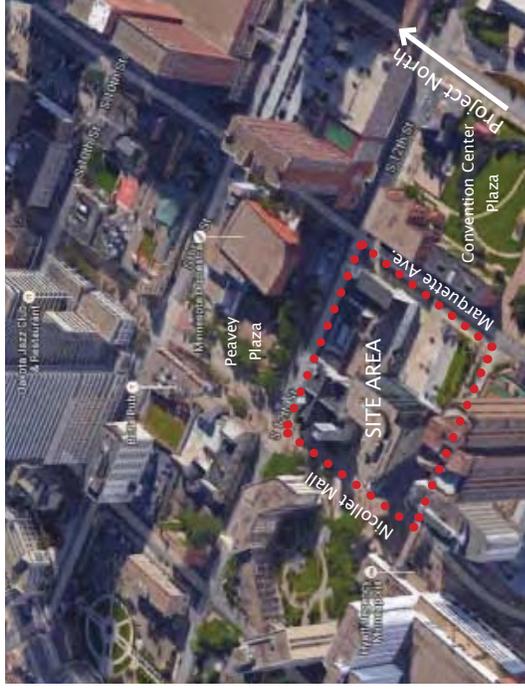
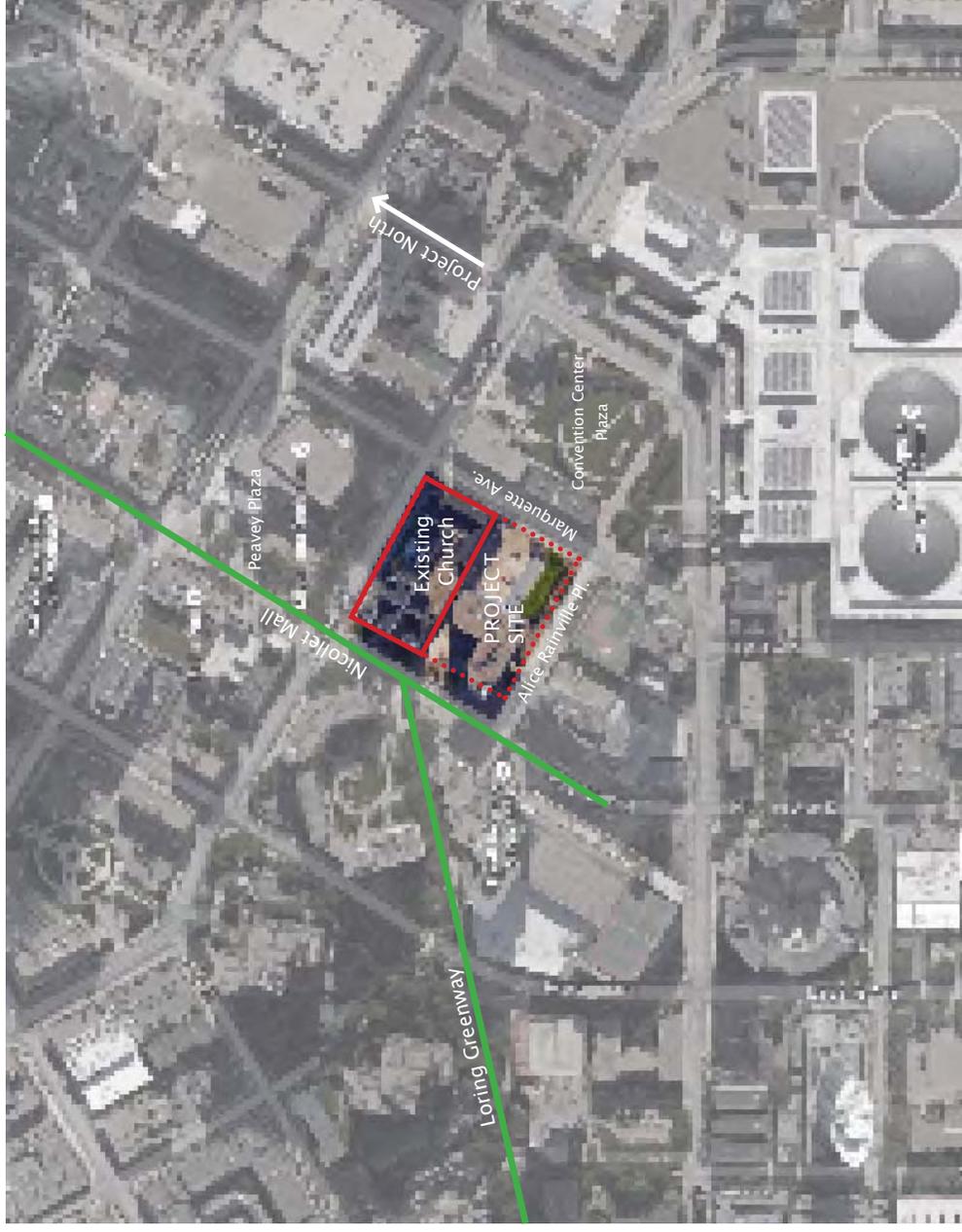
**Table 5**  
**2025 Sunday Traffic Operations**

Intersection	Approach	Delay (s/veh)					LOS By Approach			LOS By Intersection	
		L	T	R	LOS	LOS	Delay (S/Veh)	LOS	LOS		
AM Peak Hour	12th Street South at Marquette Ave (Signal)	NB	0.0	A	6.2	A	4.5	A	5.4	A	8.2
		SB	12.0	B	4.9	A	0.0	A	7.0	A	
		EB	16.1	B	9.5	A	4.9	A	10.1	B	
		WB	0.0	A	0.0	A	0.0	A	0.0	A	
		NB	1.5	A	0.5	A	0.0	A	0.5	A	
		SB	0.0	A	0.3	A	0.1	A	0.3	A	
	Marquette Ave at Surface Lot Driveway	EB	0.0	A	0.0	A	0.0	A	0.0	A	0.5
		WB	0.0	A	0.0	A	0.0	A	0.0	A	
		NB	4.3	A	3.1	A	0.0	A	3.7	A	
		SB	0.0	A	1.6	A	1.4	A	1.5	A	
		EB	29.0	C	2.8	A	17.1	B	24.5	C	
		WB	0.0	A	0.0	A	0.0	A	0.0	A	
PM Peak Hour	12th Street South at Marquette Ave (Signal)	NB	0.0	A	4.8	A	5.6	A	5.2	A	7.6
		SB	10.8	B	3.7	A	0.0	A	6.3	A	
		EB	19.0	B	10.2	B	7.3	A	11.2	B	
		WB	0.0	A	0.0	A	0.0	A	0.0	A	
		NB	1.6	A	0.8	A	0.0	A	0.8	A	
		SB	0.0	A	0.5	A	0.0	A	0.5	A	
	Marquette Ave at Surface Lot Driveway	EB	0.0	A	0.0	A	0.0	A	0.0	A	0.8
		WB	0.0	A	0.0	A	0.0	A	0.0	A	
		NB	6.1	A	4.5	A	0.0	A	4.7	A	
		SB	0.0	A	4.7	A	2.2	A	3.3	A	
		EB	26.6	C	5.0	A	17.4	B	22.1	C	
		WB	0.0	A	0.0	A	0.0	A	0.0	A	
Alice Rainville Pl at Nicollet Tower Driveway	NB	0.0	A	0.0	A	0.0	A	0.0	A	10.7	
	SB	20.9	C	0.0	A	21.8	C	21.0	C		
	EB	0.0	A	2.3	A	0.0	A	2.3	A		
	WB	0.0	A	0.6	A	0.4	A	0.5	A		
	NB	0.0	A	0.0	A	0.0	A	0.0	A		
	SB	0.0	A	0.0	A	0.0	A	0.0	A		

## **SECTION 6**

- PHOTOS OF EXISTING PROPERTY AND PROJECT RENDERINGS

EXISTING CONTEXT



Existing Aerial Views of Site Context



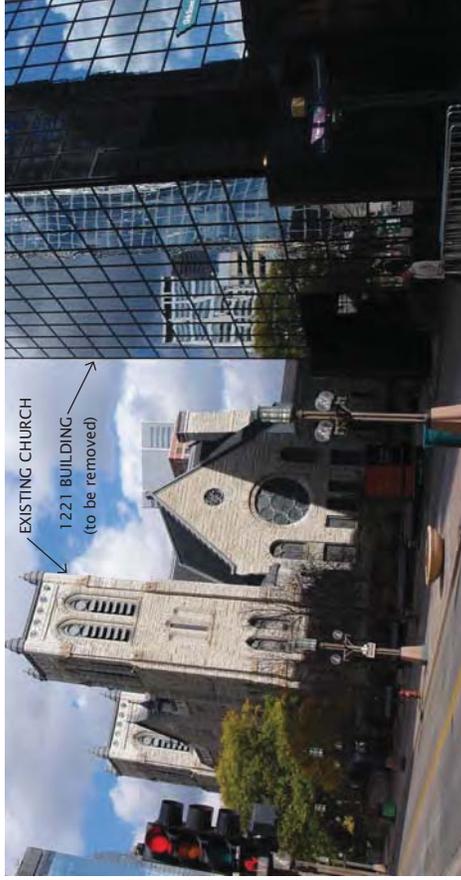
WESTMINSTER PRESBYTERIAN CHURCH  
 ISSUED FOR GENERAL LAND USE APPLICATION AND SITE PLAN REVIEW  
 April 20, 2016

James dayton design

NICOLLET MALL EXISTING SITE CONDITIONS



View Looking South From Nicollet Mall and 12th Street



View Looking North Along Nicollet Mall



Mid-Block View of Site From Nicollet Mall



Mid-Block View of Site From Nicollet Mall



james dayton design

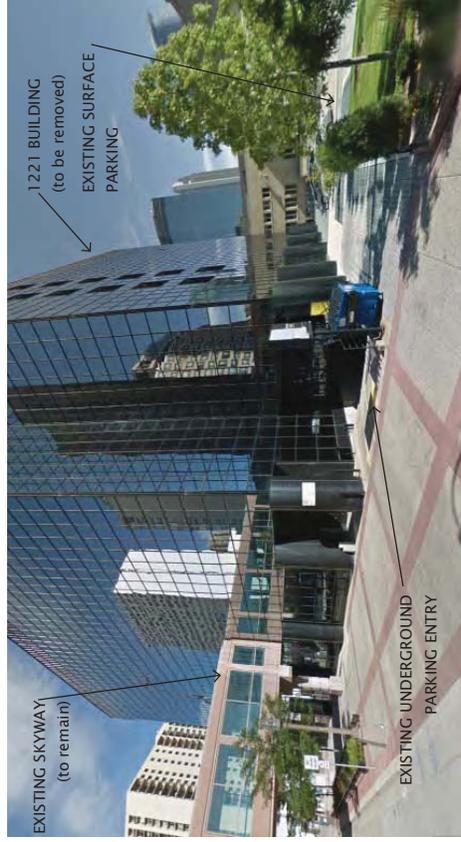
# ALICE RAINVILLE PLACE EXISTING SITE CONDITIONS



View From Nicollet Mall and Alice Rainville Place Looking East



View From Marquette Ave. and Alice Rainville Place Looking West



Mid-Block View of Site From Alice Rainville Place



james dayton design

MARQUETTE AVENUE EXISTING SITE CONDITIONS



View From Marquette Ave. and Alice Rainville Place



View From Marquette Ave. and 12th Street Looking South



Mid-Block View of Site From Marquette Ave.



Detail View of Surface Parking Accessed From Marquette Ave.



james dayton design



New Plaza  
Drop-off L

ALICE RAINVILLE PLACE

Existing Skyway

New Lower Plaza

Entry Plaza

## Aerial View of Addition Looking East





ALICE RAINVILLE PLACE

New Underground Parking  
Entry

New Plaza with Drop-off Drive  
Lane

MARQUETTE AVE,

## Aerial View of Addition Looking West



Existing



Existing Church from Corner of Nicollet Mall and 12th St With Addition Beyond

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Existing



View of Addition and Plaza From Nicollet Mall

WESTMINSTER PRESBYTERIAN CHURCH  
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 April 20, 2016



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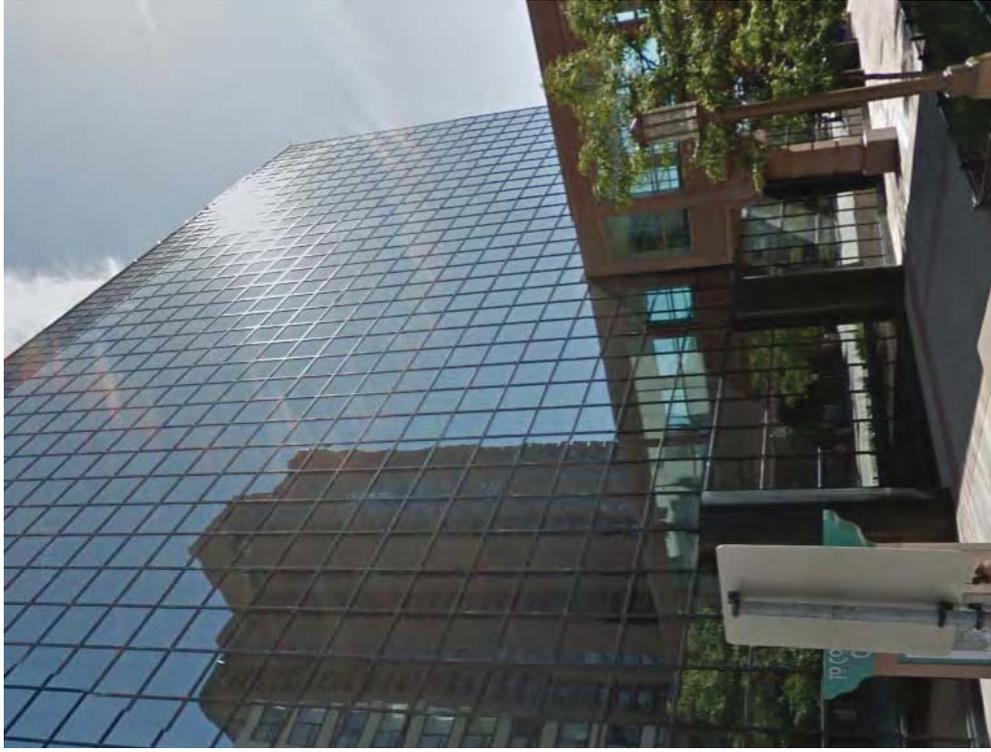
Tower with Bell Carillon  
Lower Plaza  
Upper Plaza Along Building  
Window Into Main Hall

New Lower Plaza Adjacent To Nicollet Mall

WESTMINSTER PRESBYTERIAN CHURCH  
Issued for General Land Use Application and Site Plan Review  
April 20, 2016



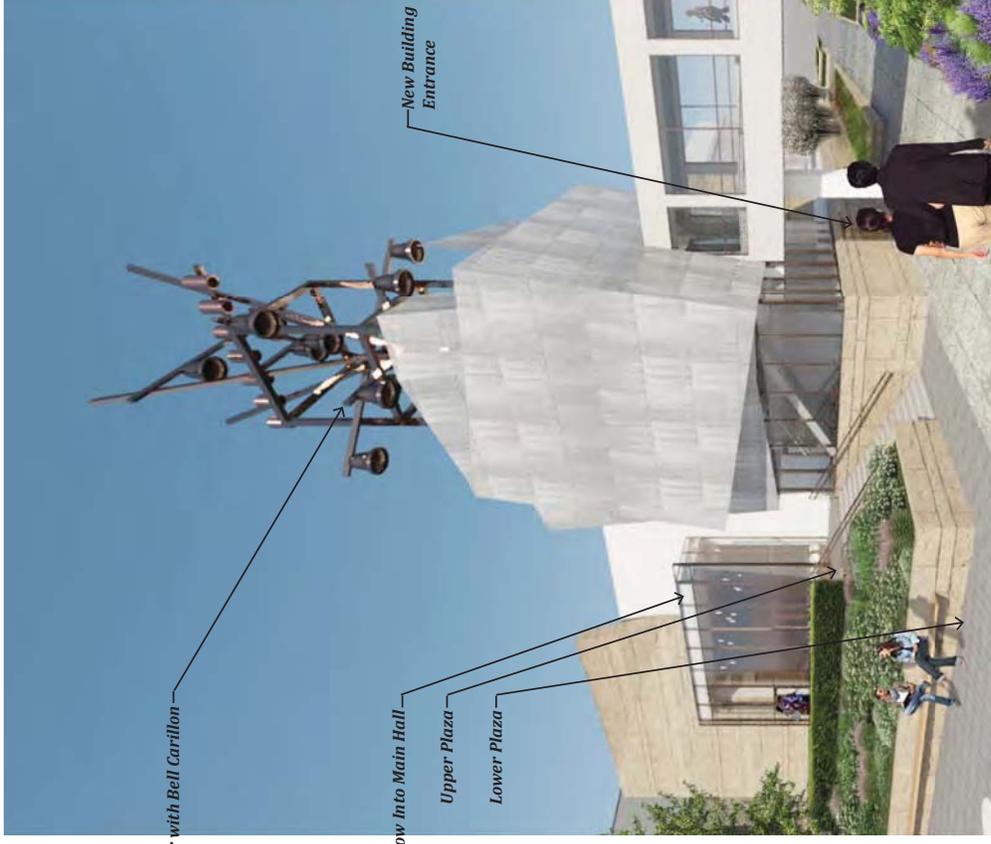
James dayton design



Existing View Of Skyway On Alice Rainville Place



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Tower with Bell Carillon

Window Into Main Hall

Upper Plaza

Lower Plaza

New Building Entrance

New Skyway Entrance on Alice Rainville Place

WESTMINSTER PRESBYTERIAN CHURCH

Issued for General Land Use Application and Site Plan Review

April 20, 2016



Tower with Bell Carillon  
Existing Skyway to Remain

New Streetscaping  
Meditation Garden  
Entry Into Underground Parking

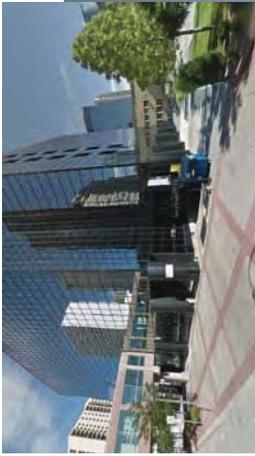
New Building Entrance

### New Facade and Landscaping on Alice Rainville Place

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 April 20, 2016



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Existing



New Parking Garage Entrance and Facade on Alice Rainville Place

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April 20, 2016



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New Building Entry

New Drop-Off Drive Lane

New Lawn

New Plaza Adjacent to Marquette Ave.

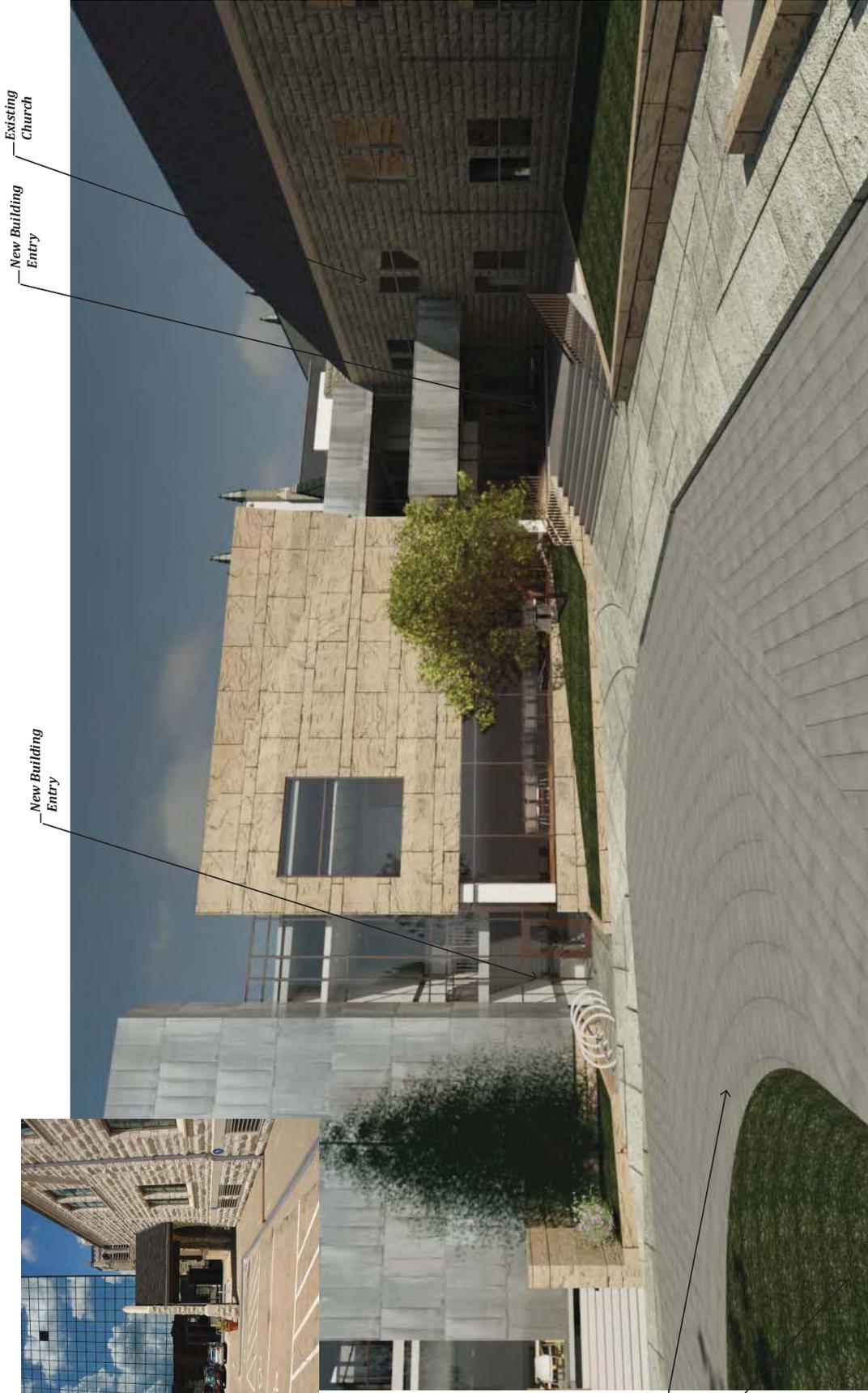


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WESTMINSTER PRESBYTERIAN CHURCH  
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 April 20, 2016



Existing



Existing Church

New Building Entry

New Building Entry

New Drop-Off Drive Lane

New Lawn

New Entrance Off of Marquette Ave

WESTMINSTER PRESBYTERIAN CHURCH  
 Issued for General Land Use Application and Site Plan Review  
 April 20, 2016



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Existing

Bell Carillon Beyond  
 New Landscaping  
 New Drop-Off Drive Lane  
 New Building Entry



New Plaza and Entrance Off of Marquette Ave



James dayton design

WESTMINSTER PRESBYTERIAN CHURCH  
 Issued for General Land Use Application and Site Plan Review  
 April 20, 2016



New Landscaping at Corner of Alice Rainville Place and Marquette Ave

WESTMINSTER PRESBYTERIAN CHURCH  
Issued for General Land Use Application and Site Plan Review  
April 20, 2016



james dayton design

## **SECTION 7**

- PROJECT DRAWINGS

WALL  
DSCAPE PLAN

SEE LANDSCAPE PLAN

SITE/PLANTER WALLS  
SEE LANDSCAPE PLAN &  
STRUCTURAL PLAN

ACCESSIBLE RAMP

ACCESSIBLE BUILDING ENTRY

# ED ADDITION

= 856.58  
P1=836.58  
P2=826.58

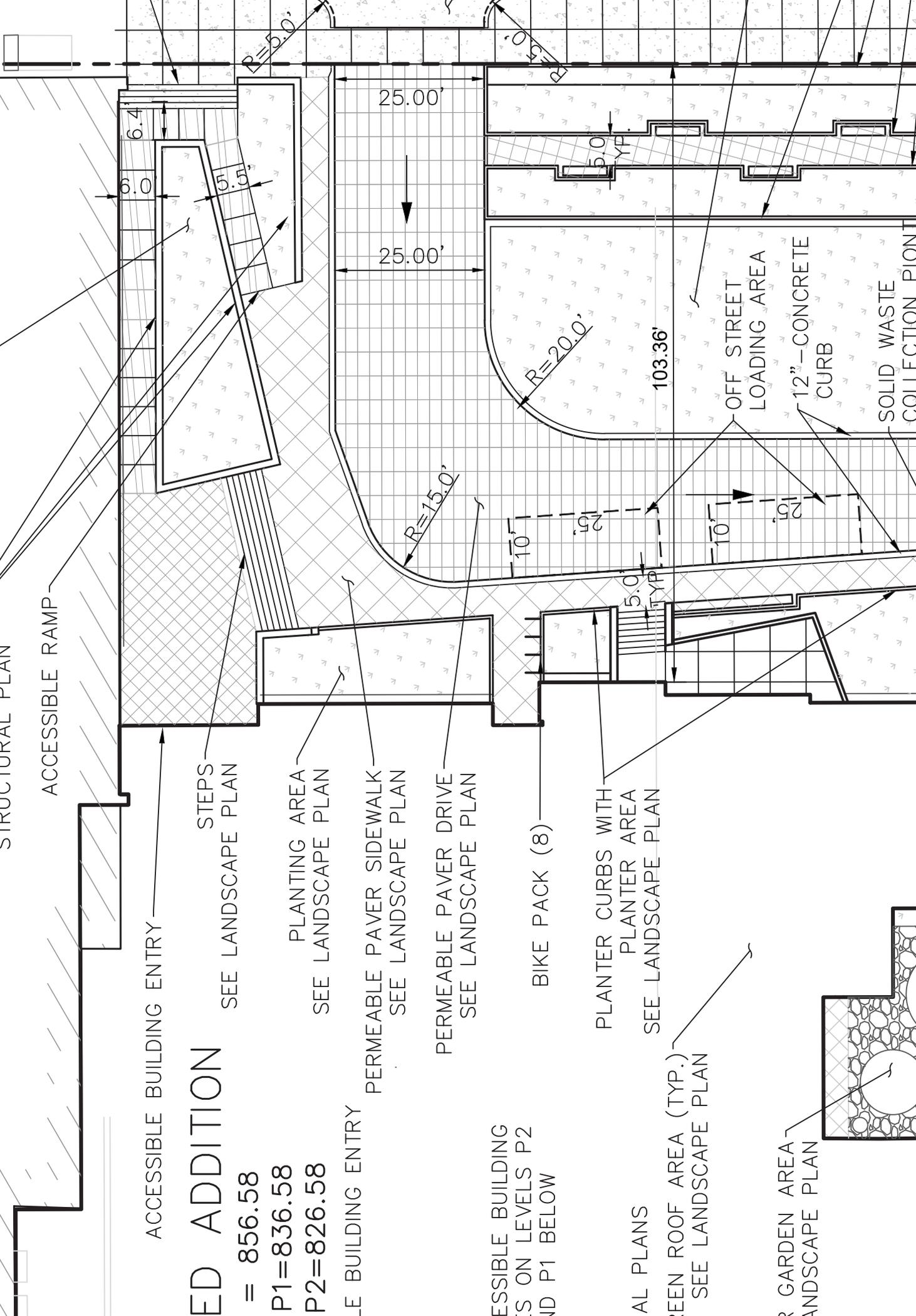
LE BUILDING ENTRY

CESSIBLE BUILDING  
S ON LEVELS P2  
ND P1 BELOW

AL PLANS

REEN ROOF AREA (TYP.)  
SEE LANDSCAPE PLAN

R GARDEN AREA  
LANDSCAPE PLAN



STEPS  
SEE LANDSCAPE PLAN

PLANTING AREA  
SEE LANDSCAPE PLAN

PERMEABLE PAVER SIDEWALK  
SEE LANDSCAPE PLAN

PERMEABLE PAVER DRIVE  
SEE LANDSCAPE PLAN

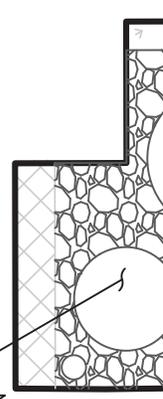
BIKE PACK (8)

PLANTER CURBS WITH  
PLANTER AREA  
SEE LANDSCAPE PLAN

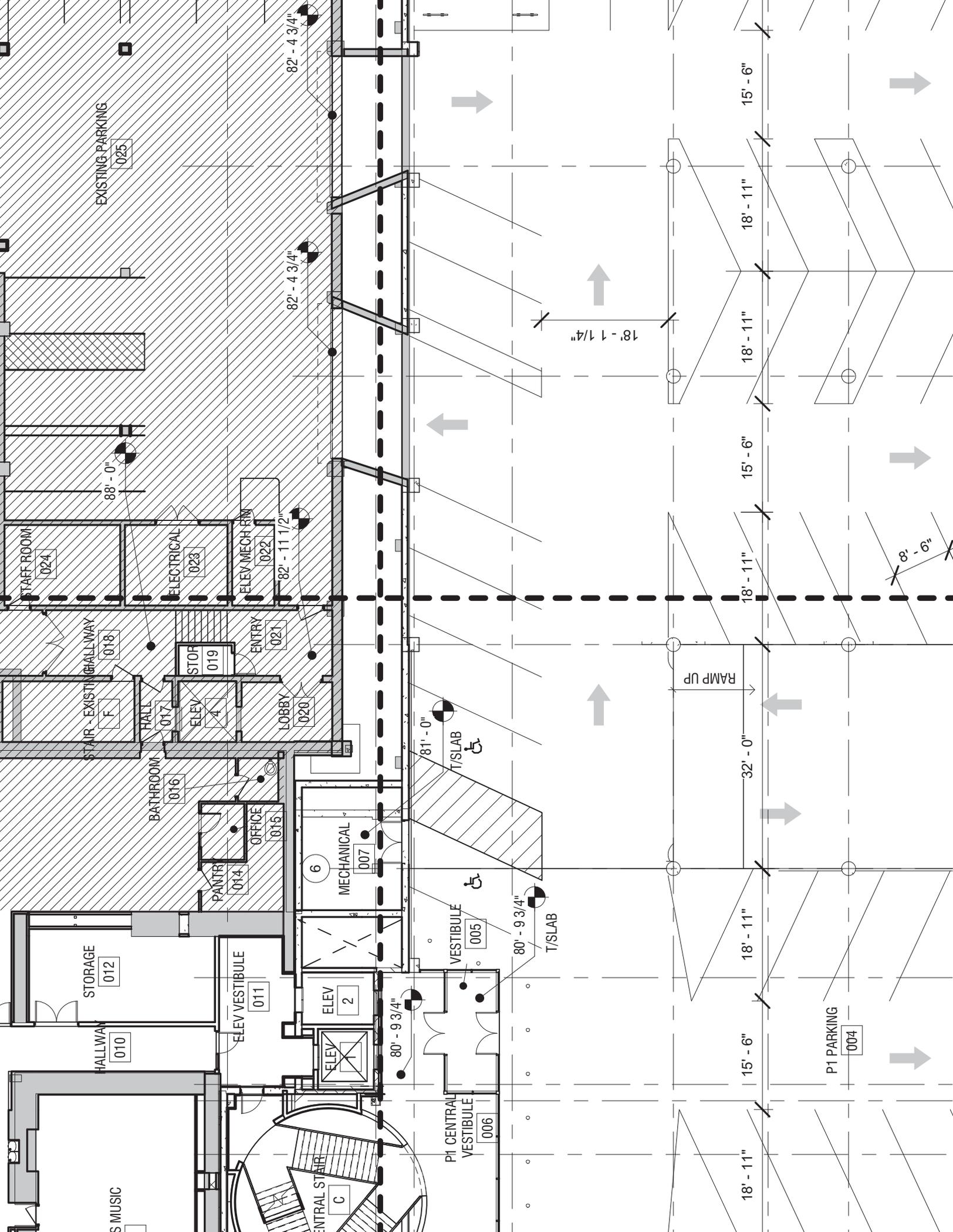
OFF STREET  
LOADING AREA

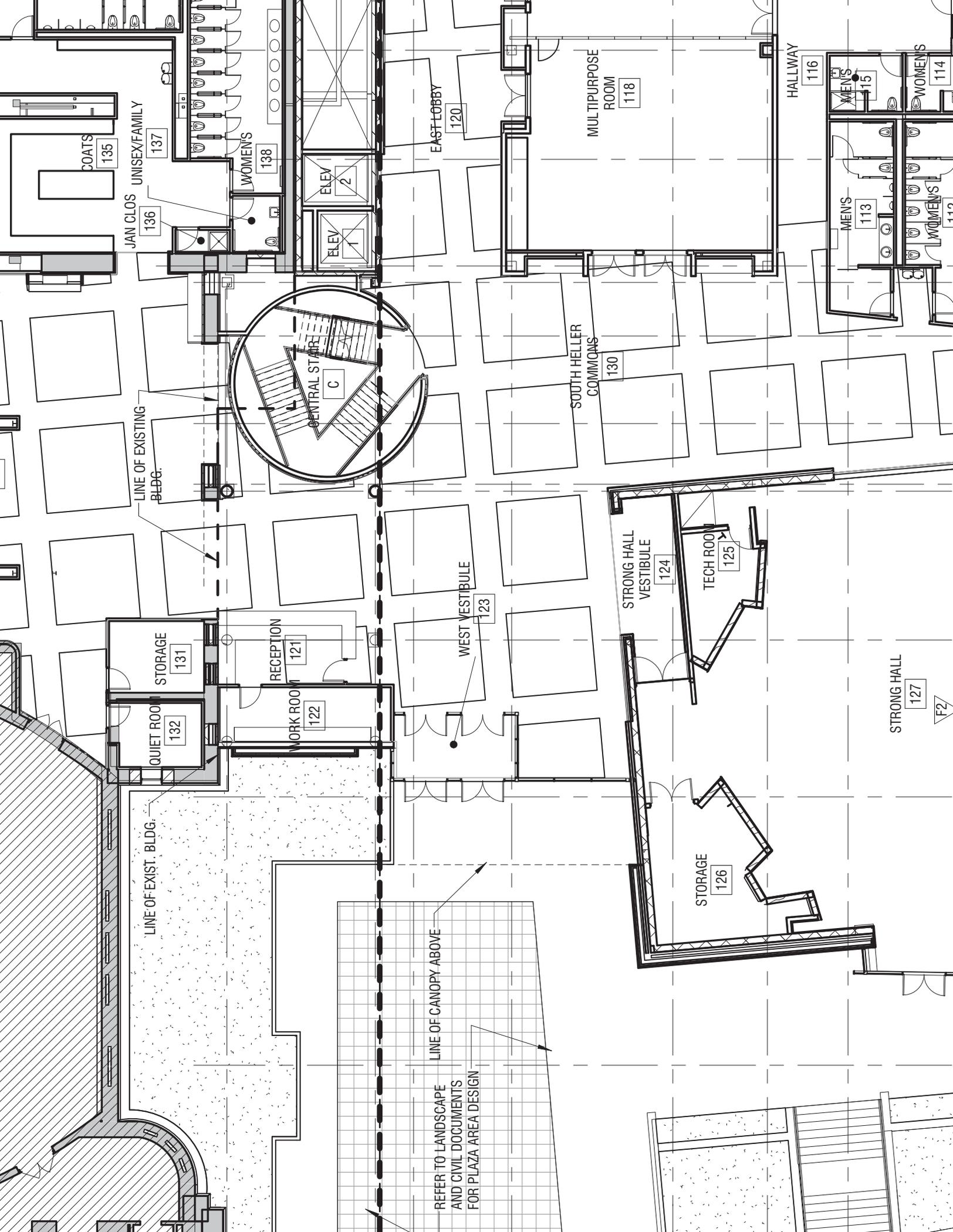
12" - CONCRETE  
CURB

SOLID WASTE  
COLLECTION POINT









COATS  
135

JAN CLOS  
136

UNISEX/FAMILY  
137

WOMEN'S  
138

ELEV  
2

ELEV  
1

CENTRAL STAIR  
C

EAST LOBBY  
120

SOUTH HELLER  
COMMONS  
130

WEST VESTIBULE  
123

STORAGE  
131

QUIET ROOM  
132

RECEPTION  
121

WORK ROOM  
122

STRONG HALL  
VESTIBULE  
124

TECH ROOM  
125

STORAGE  
126

STRONG HALL  
127

MULTIPURPOSE  
ROOM  
118

HALLWAY  
116

MEN'S  
115

WOMEN'S  
114

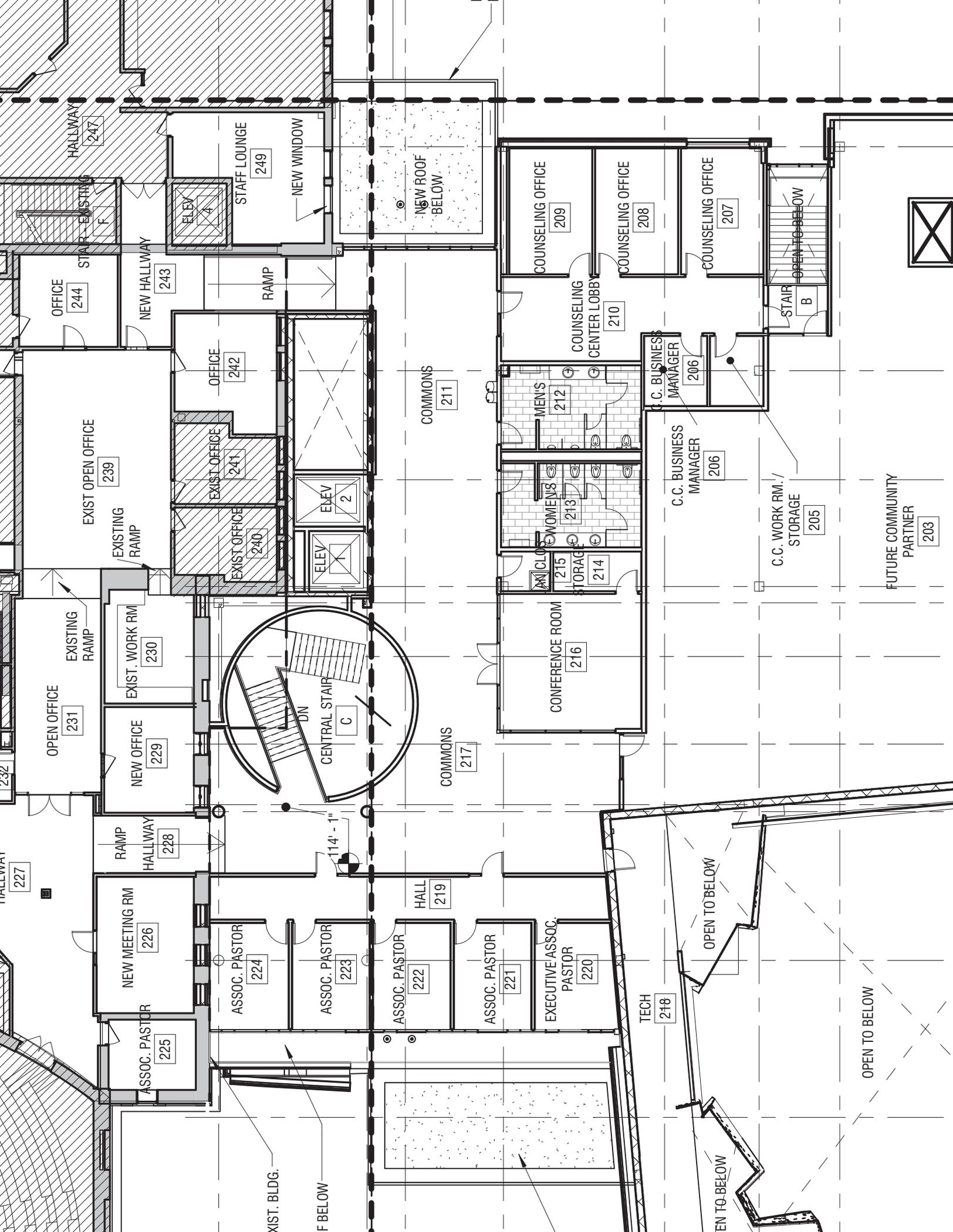
MEN'S  
113

WOMEN'S  
117

LINE OF EXIST. BLDG.

LINE OF CANOPY ABOVE

REFER TO LANDSCAPE  
AND CIVIL DOCUMENTS  
FOR PLAZA AREA DESIGN



HALLWAY 247

STAFF LOUNGE 249

NEW WINDOW

NEW ROOF BELOW

EXIST. BLDG.

F BELOW

EXIST. RAMP

NEW HALLWAY 243

OFFICE 244

EXIST. OPEN OFFICE 239

EXIST. OFFICE 242

EXIST. OFFICE 241

EXIST. OFFICE 240

COMMONS 211

CONFERENCE ROOM 216

WOMEN'S 213

MEN'S 212

COUNSELING CENTER LOBBY 210

C.C. BUSINESS MANAGER 206

COUNSELING OFFICE 209

COUNSELING OFFICE 208

COUNSELING OFFICE 207

STAIR B

OFFICE 244

NEW HALLWAY 243

RAMP

OFFICE 242

EXIST. OFFICE 241

EXIST. OFFICE 240

ELEV 2

ELEV 2

COMMONS 211

CONFERENCE ROOM 216

WOMEN'S 213

MEN'S 212

COUNSELING CENTER LOBBY 210

C.C. BUSINESS MANAGER 206

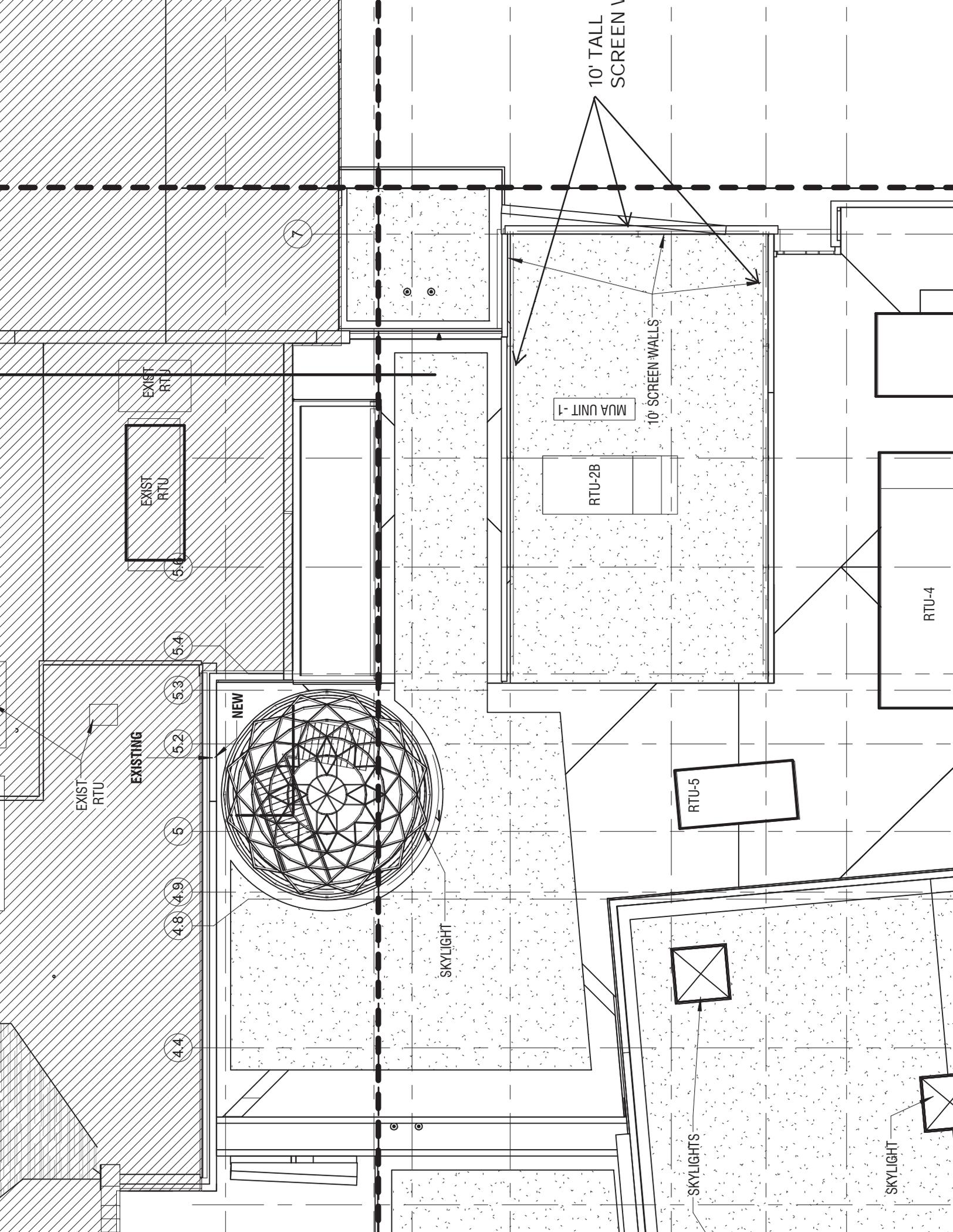
COUNSELING OFFICE 209

COUNSELING OFFICE 208

COUNSELING OFFICE 207

STAIR B

EXIST. RAMP



10' TALL  
SCREEN WALLS

MUA UNIT -1

10' SCREEN WALLS

RTU-2B

RTU-4

RTU-5

EXIST  
RTU

EXIST  
RTU

EXIST  
RTU

EXISTING

NEW

SKYLIGHT

SKYLIGHTS

SKYLIGHT

4.4

4.8

4.9

5

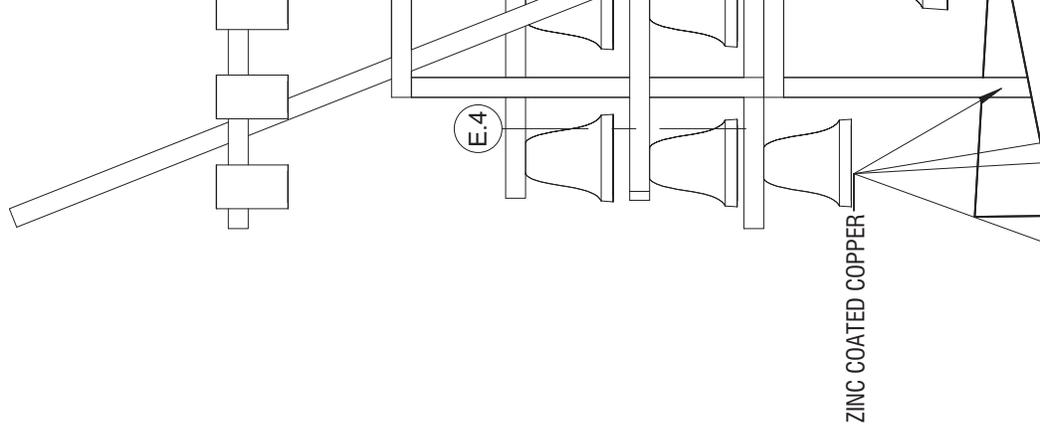
5.2

5.3

5.4

5.6

7



E ————

D.8 ————

D ————

C.7 ————

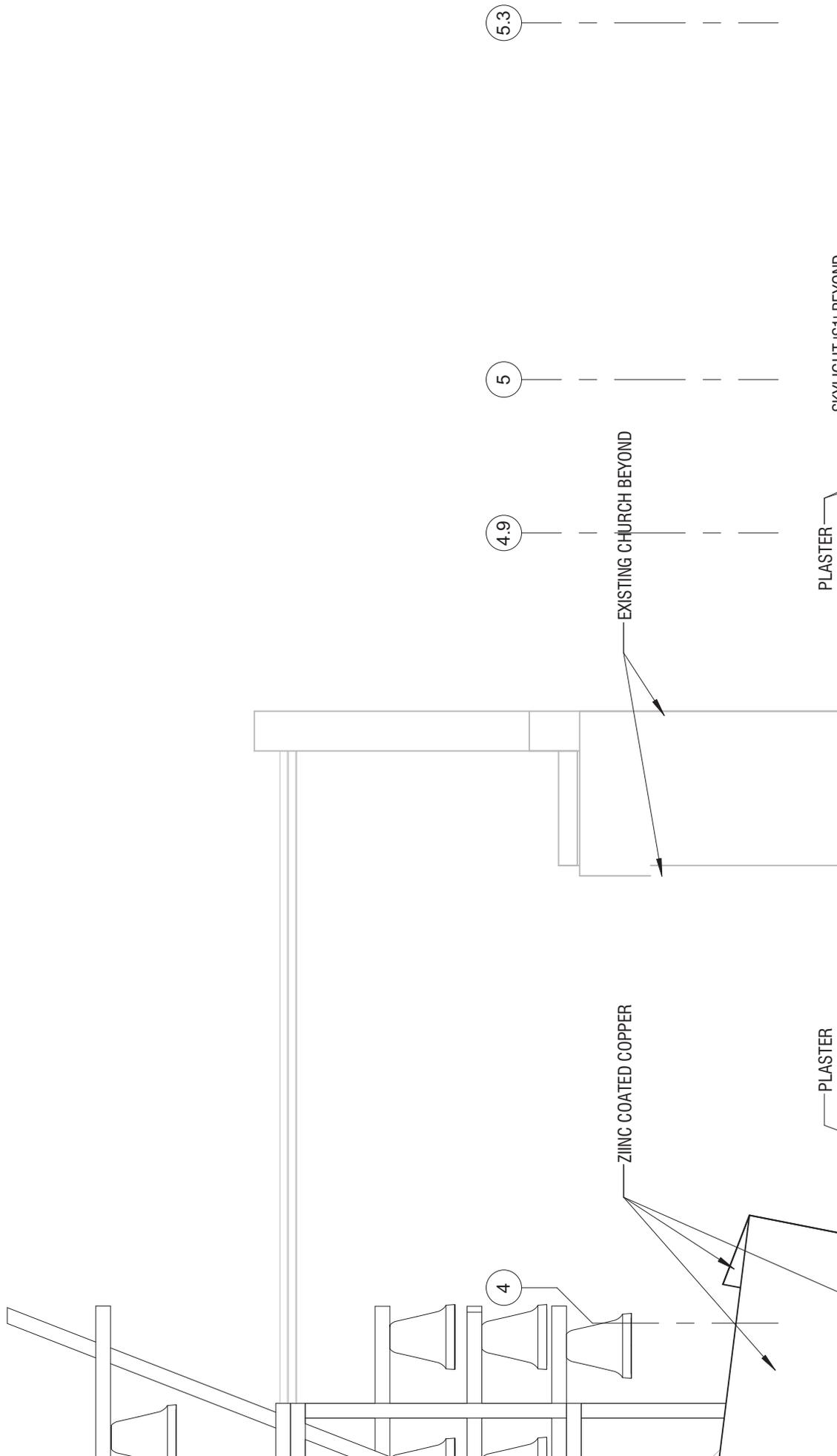
C ————

B.6 ————

# EXTERIOR ELEVATION - SOUTH

SCALE: 1/8" = 1'-0"

2  
A2.1



B

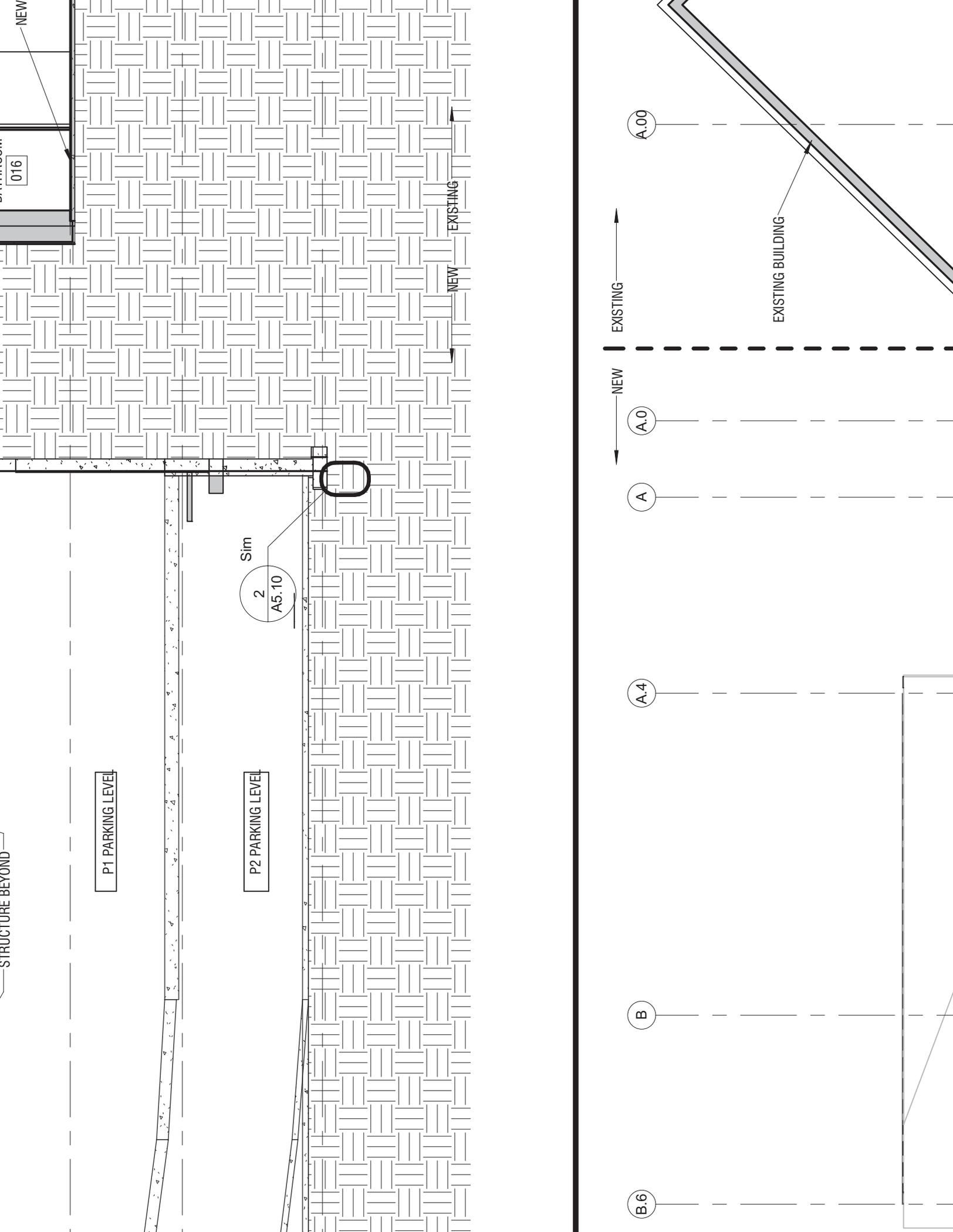
B.6

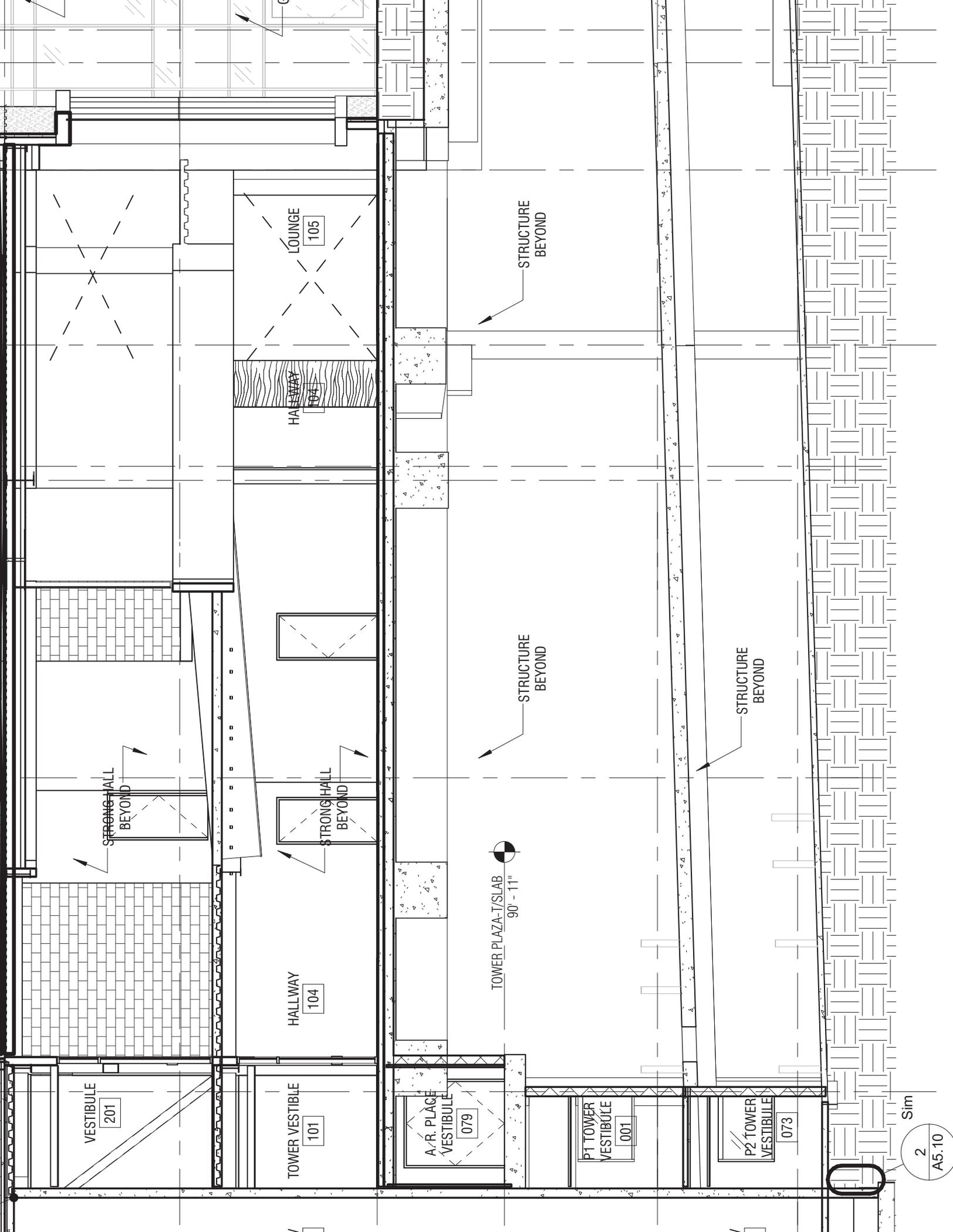
C

C.7

D

D.8





VESTIBULE  
201

TOWER VESTIBULE  
101

HALLWAY  
104

STRONG HALL  
BEYOND

STRONG HALL  
BEYOND

HALLWAY  
104

LOUNGE  
105

A.R. PLACE  
VESTIBULE  
079

P1 TOWER  
VESTIBULE  
001

P2 TOWER  
VESTIBULE  
073

TOWER PLAZA-T/SLAB  
90' - 11"

STRUCTURE  
BEYOND

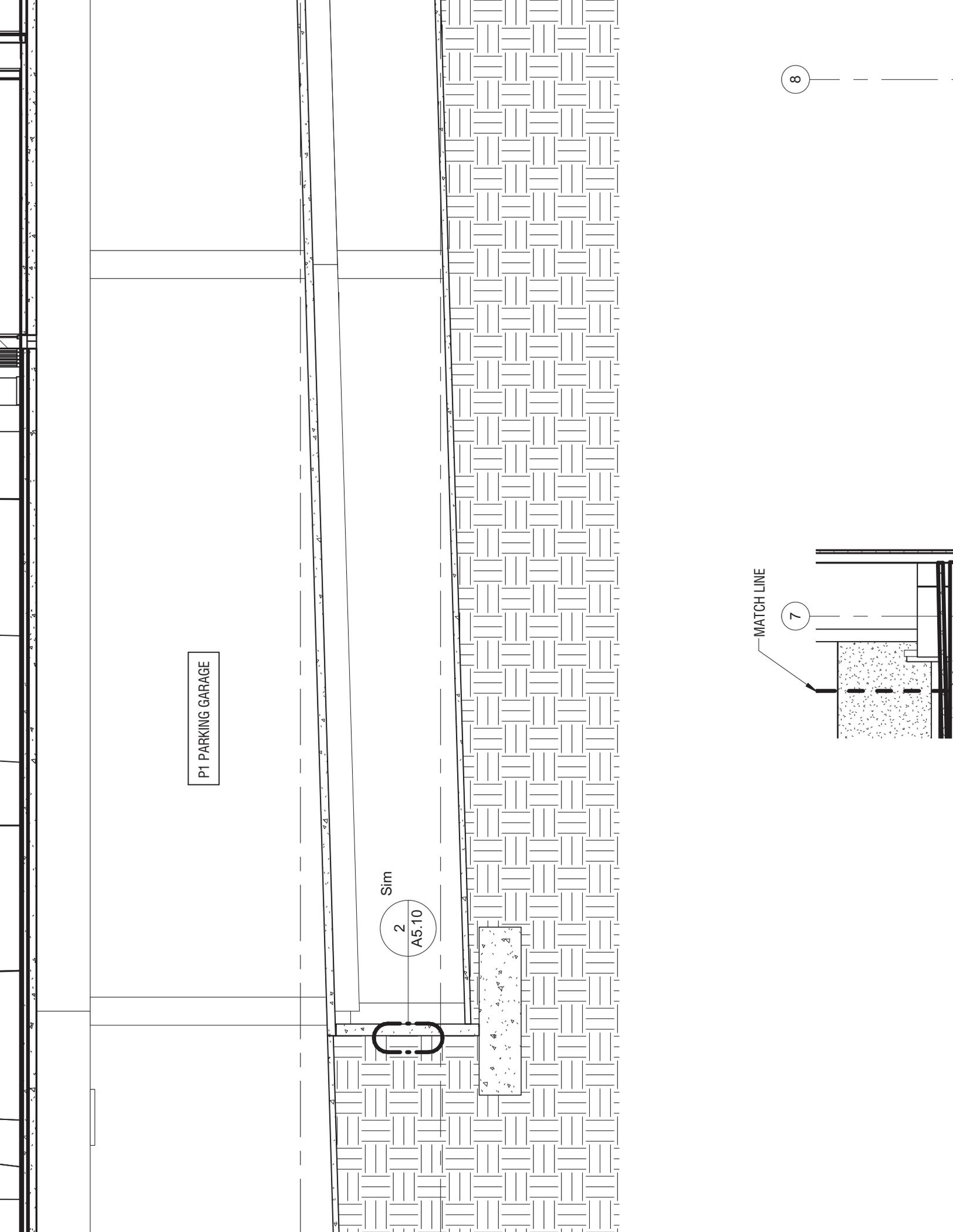
STRUCTURE  
BEYOND

STRUCTURE  
BEYOND

Sim

2

A5.10



P1 PARKING GARAGE

2  
A5.10

Sim

7

MATCH LINE

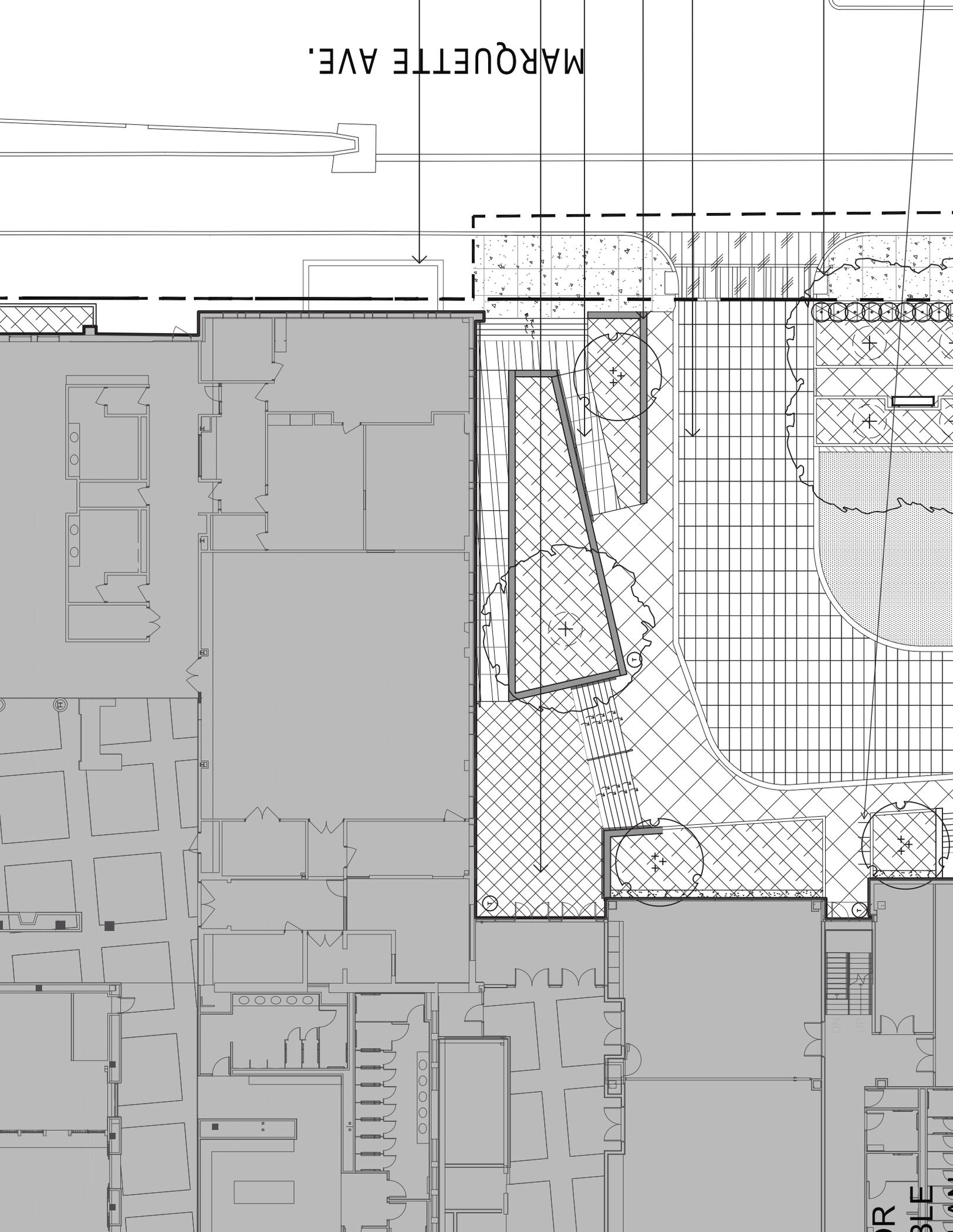
8

	Nicollet Plaza (SF)	Roof Area (Not including Existing Building) (SF)	Memory Garden (SF)	Westminster Green (SF)	Total (SF)	Total (ac.)
Impervious - Surface/Other (Remaining Areas)	11897	0	730	4419	17047	0.391
Impervious - Roofs	0	11697	0	0	11697	0.269
Pervious - Green Roof*	0	13297	0	0	13297	0.305
Pervious - Plaza Permeable Pavers**	4548	0	0	0	4548	0.104
Pervious - Driveway Permeable Pavers**	0	0	0	4113	4113	0.094
Pervious - Sidewalk Permeable Pavers**	0	0	0	1736	1736	0.040
Pervious - Green Space	4795	0	663	7325	12782	0.293
Pervious - Tree Trenches, Shrubs	524	0	0	0	524	0.012
<b>Total</b>	<b>21763</b>	<b>24994</b>	<b>1393</b>	<b>17593</b>	<b>65743</b>	<b>1.509</b>

\*Modeled in MIDS as Impervious Area  
 \*\*Modeled in MIDS and HydroCAD as Impervious Area

	Area (SF)	Area (ac.)
<b>Impervious/Pervious Areas - Green Roof and Permeable Pavers counted as Pervious</b>		
Impervious	28744	0.660
Pervious	36999	0.849
<b>Total Disturbed Area</b>	<b>65743</b>	<b>1.509</b>
<b>HydroCAD Model Areas - Green Roof counted as Pervious, Permeable Pavers counted as Impervious</b>		
Impervious	39140	0.898
Pervious	26603	0.611
<b>Total Disturbed Area</b>	<b>65743</b>	<b>1.509</b>
<b>MIDS Model Areas - Green Roof and Permeable Pavers counted as Impervious</b>		
Impervious	52437	1.204
Pervious	13306	0.305
<b>Total Disturbed Area</b>	<b>65743</b>	<b>1.509</b>

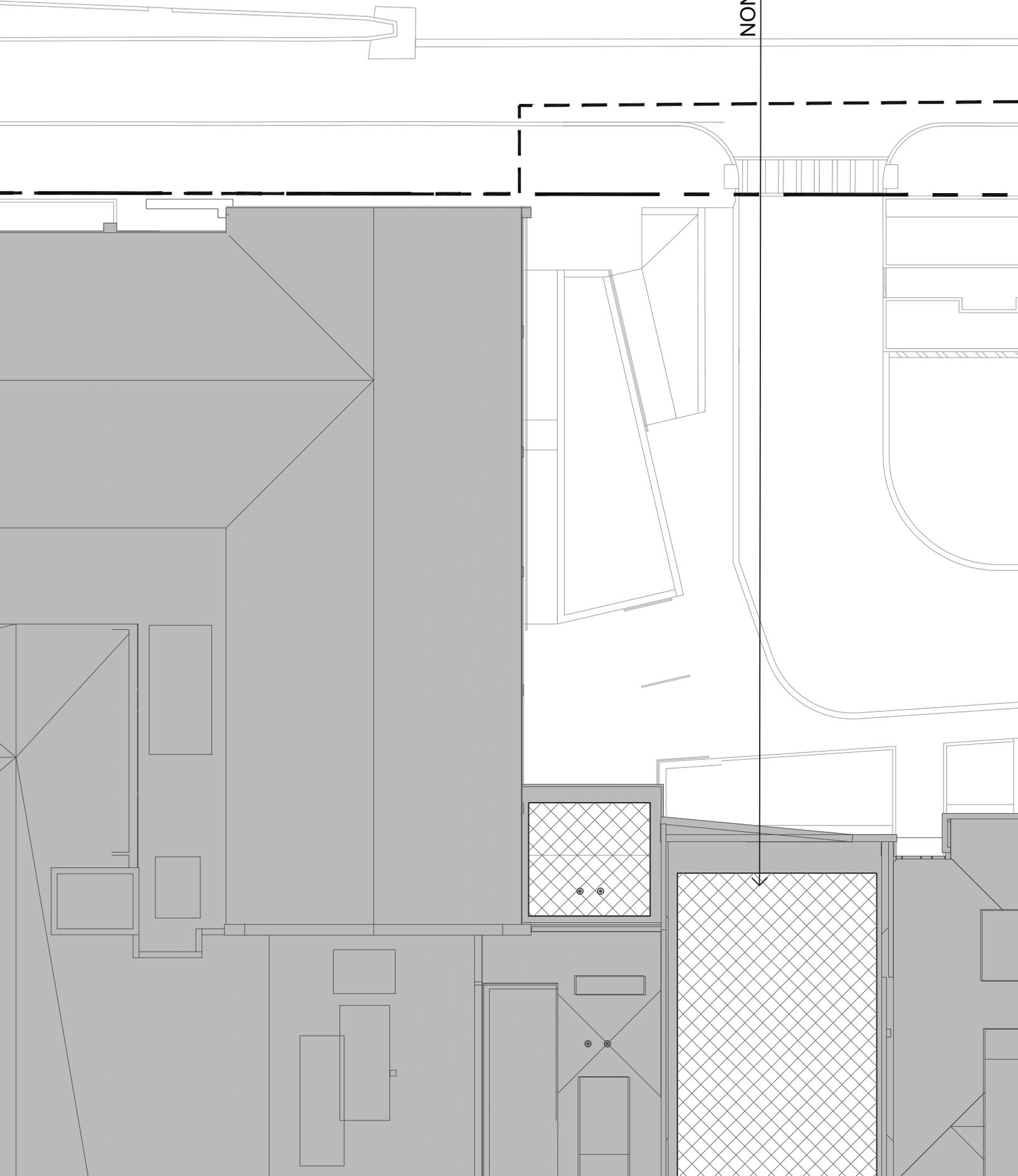
MARQUETTE AVE.



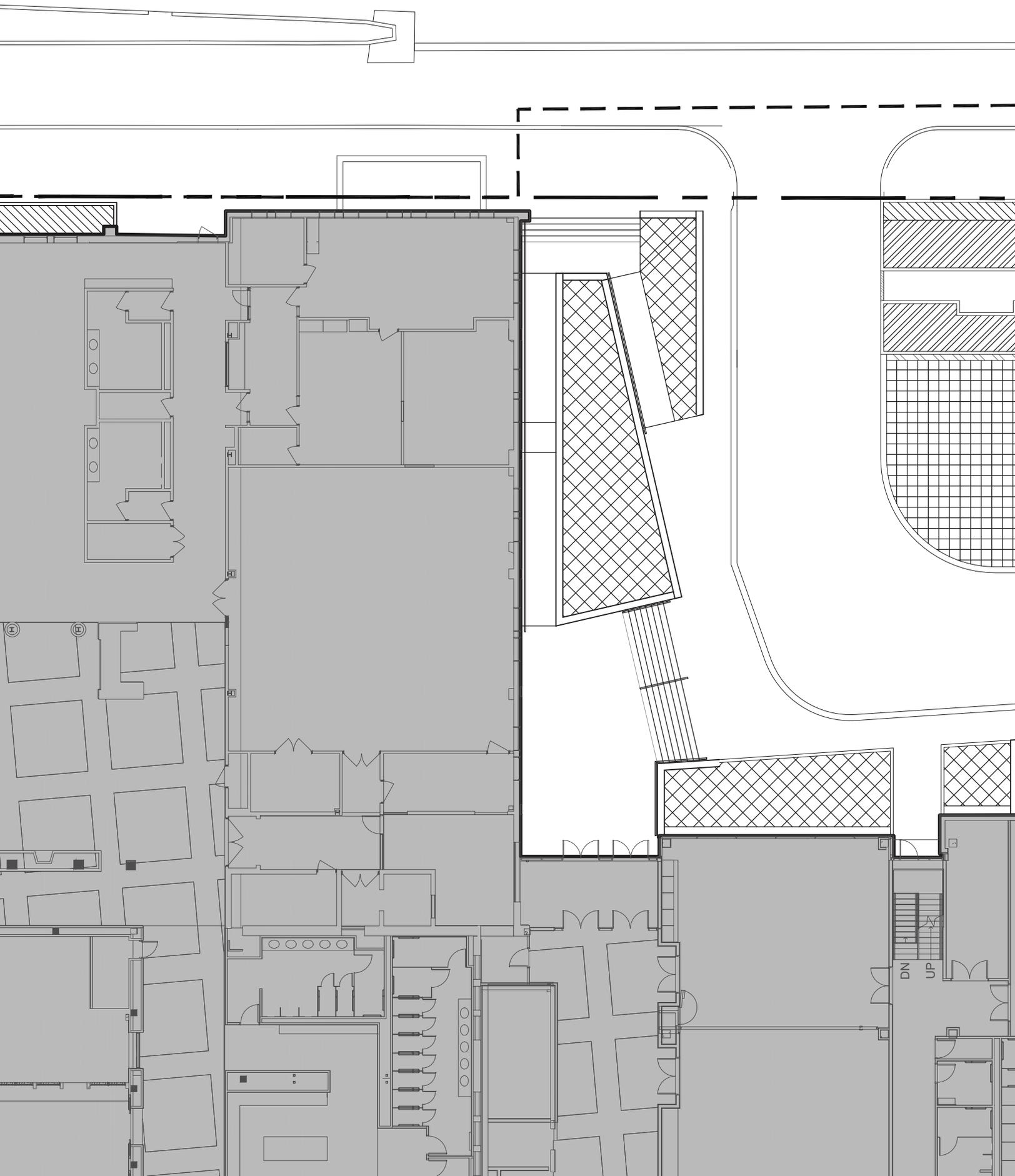
OR  
BLE

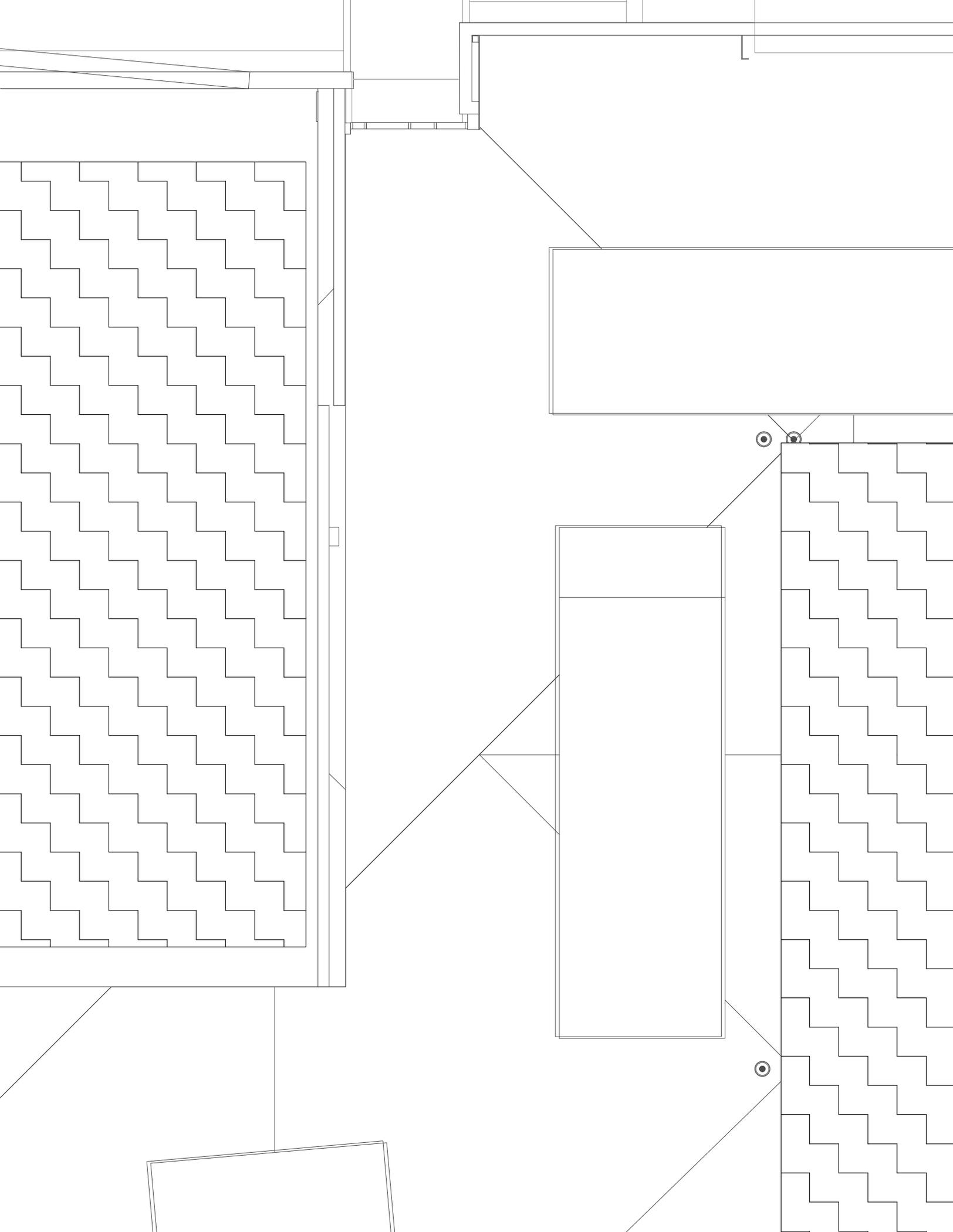
MARQUETTE AVE.

NON ACCESSIBILE



MARQUETTE AVE.





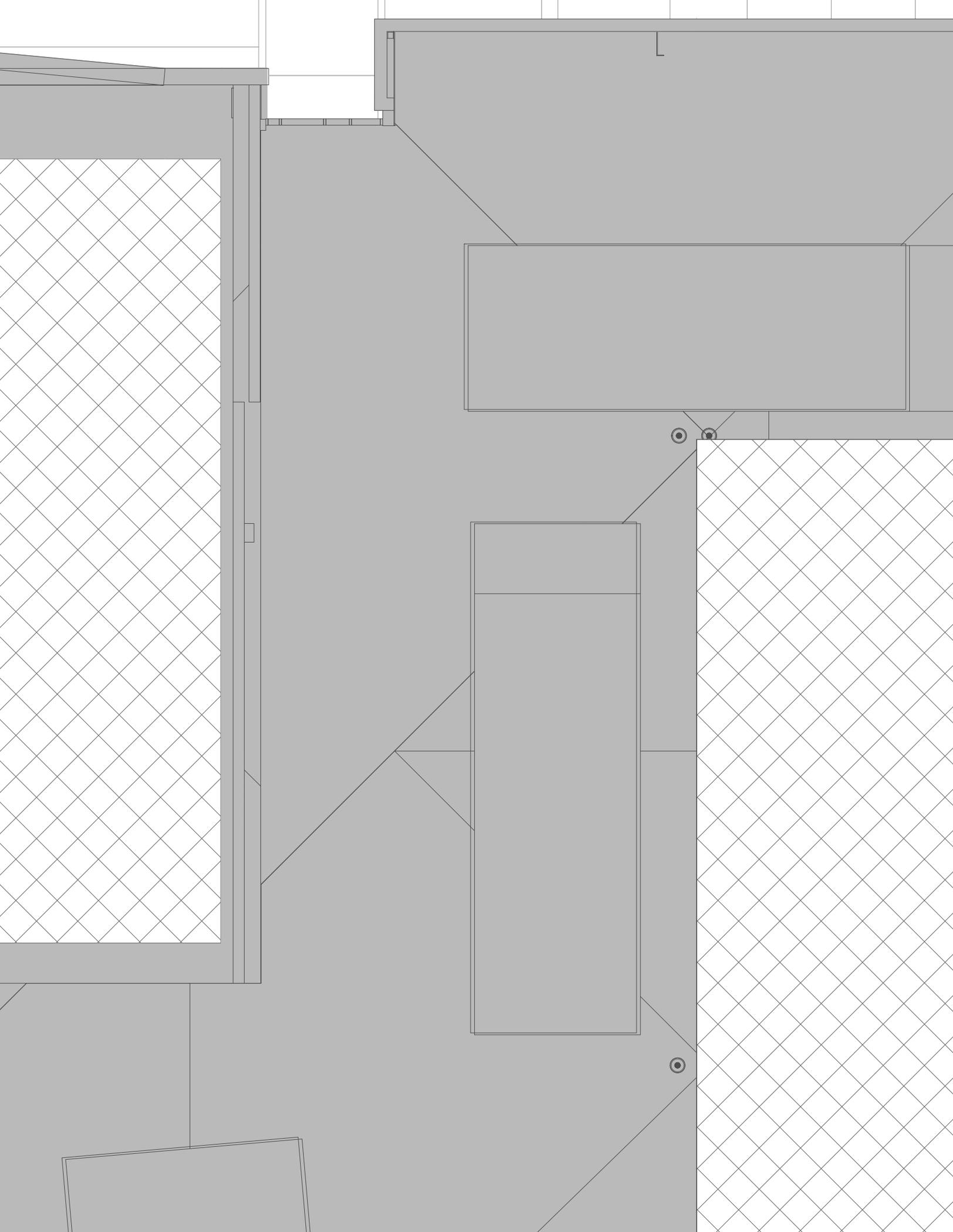
C, K/L-7.09	Refer to civil drawings for specifications
E/L-7.09	
C, K/L-7.09	
-----	
-----	Refer to civil drawings
B/L-7.08	
C/L-7.09	
A/L-7.13	
	Refer to civil

KEY	CODE	PRODUCT	NAME / MODEL NUMBER / MATERIAL	F
		Bench	Bancal, Backless, No Arms, IPE	Col
		Bench	Bancal, Backed, with Arms, IPE	Col
		Cantilevered Bench	Custom IPE seat SS Frame	
		Seating Platform	Custom IPE Decking	

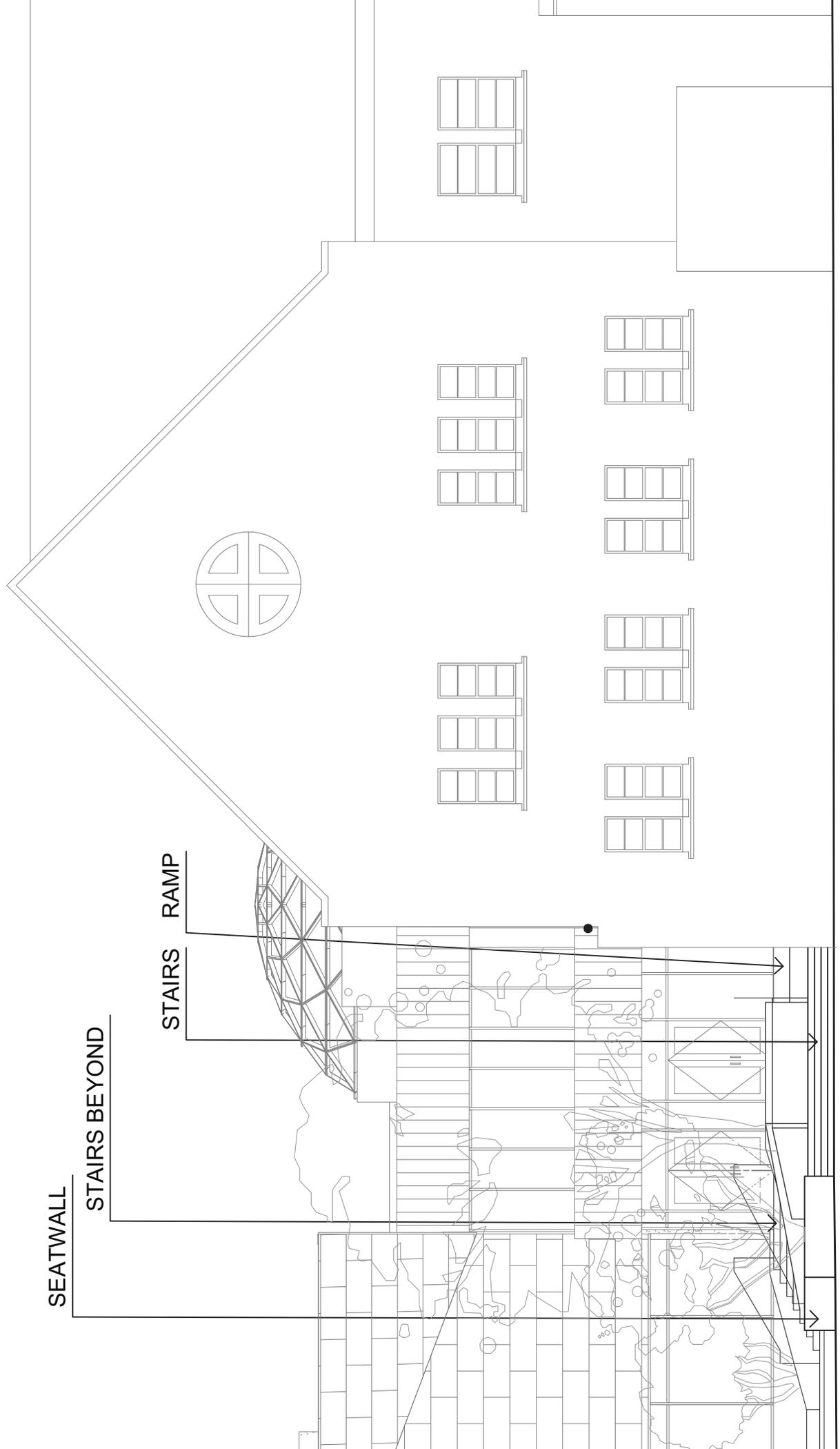
ARQUETTE AVE.

(1) ULM  
MO  
M





KEY	SCIENTIFIC NAME	COMMON NAME	QUANTITY	SIZE	SPACING	NOTE
	Calamintha nepeta 'Montrose White'	Monrose White Dwarf Calamint	92	1G 15" O.C.		
	Dicentra spectabilis	Bleeding Heart	40	1G 18" O.C.		
	Eupatorium perfoliatum	American Boneset	104	1G 18" O.C.		
	Euphorbia 'Bonfire'	Bonfire Spurge	30	1G 18" O.C.		
	Hemerocallis 'Catherine Woodbury'	Daylily	170	1G 18" O.C.		
	Hemerocallis 'Joylene Nichole'	Daylily	170	1G 18" O.C.		
	Hemerocallis 'Siloam Double Classic'	Daylily	170	1G 18" O.C.		
	Hosta 'Big Daddy'	Hosta	20	5G or Field Grown, 18" O.C.		
	Iris 'Breakers'	Iris	67	1G 15" O.C.		
	Nepeta 'faassenii' 'Walker's Low'	Nepeta	122	1G 18" O.C.		
	Paeonia 'Sarah Bernhardt'	Peony	10	5G 30" O.C.		
	Penstemon 'Dark Towers'	Penstemon	30	1G 18" O.C.		
	Salvia nemerosa 'Snow Hill'	Wood Sage	68	1G 15" O.C.		
	Sedum spectabile 'Autumn Joy'	Autumn stonecrop	68	1G 15" O.C.		
	Stachys 'Helen von Stein'	Lamb's Ear	30	1G 18" O.C.		
	Stachys officinalis 'Hummelo'	Betony	96	1G 15" O.C.		
	Vernonia Gigantea	Giant Ironweed	70	1G 18" O.C.		
	Veronicastrum 'Fascination'	Culver's Root	70	1G 18" O.C.		
<b>BULBS</b>						
KEY	SCIENTIFIC NAME	COMMON NAME	QUANTITY	SIZE	SPACING	NOTE
	Allium 'Glohemaster'	Allium	275	bulb, 12cm		

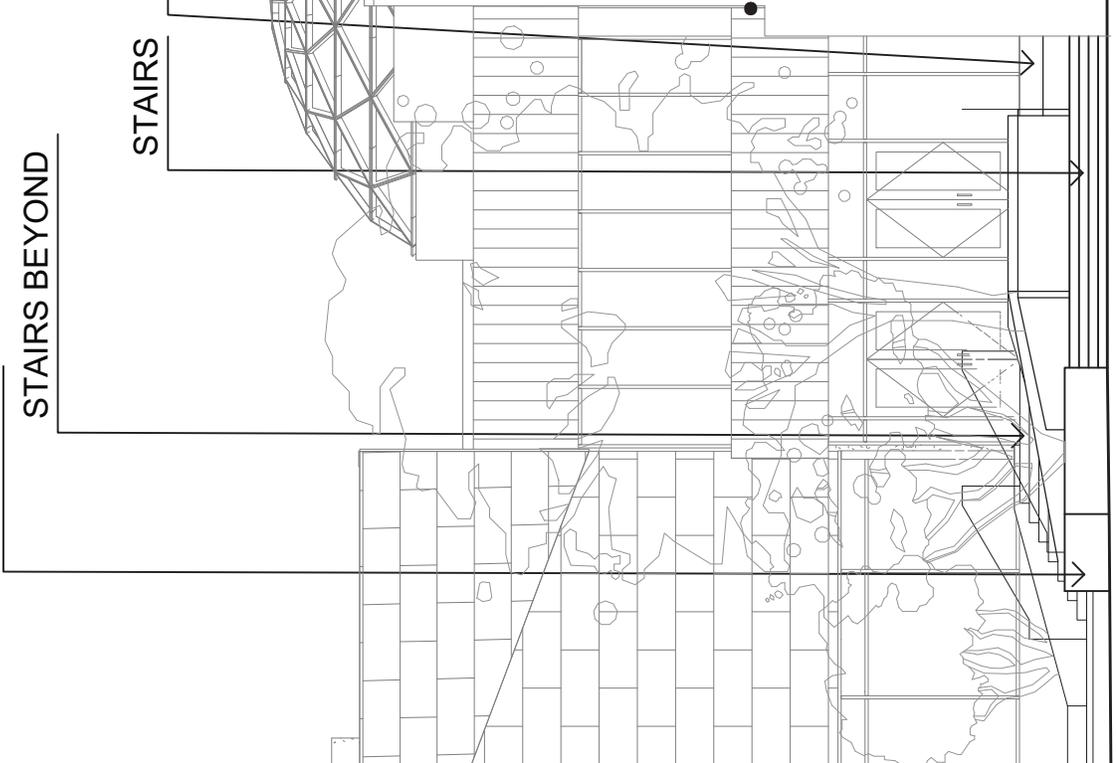


SEATWALL

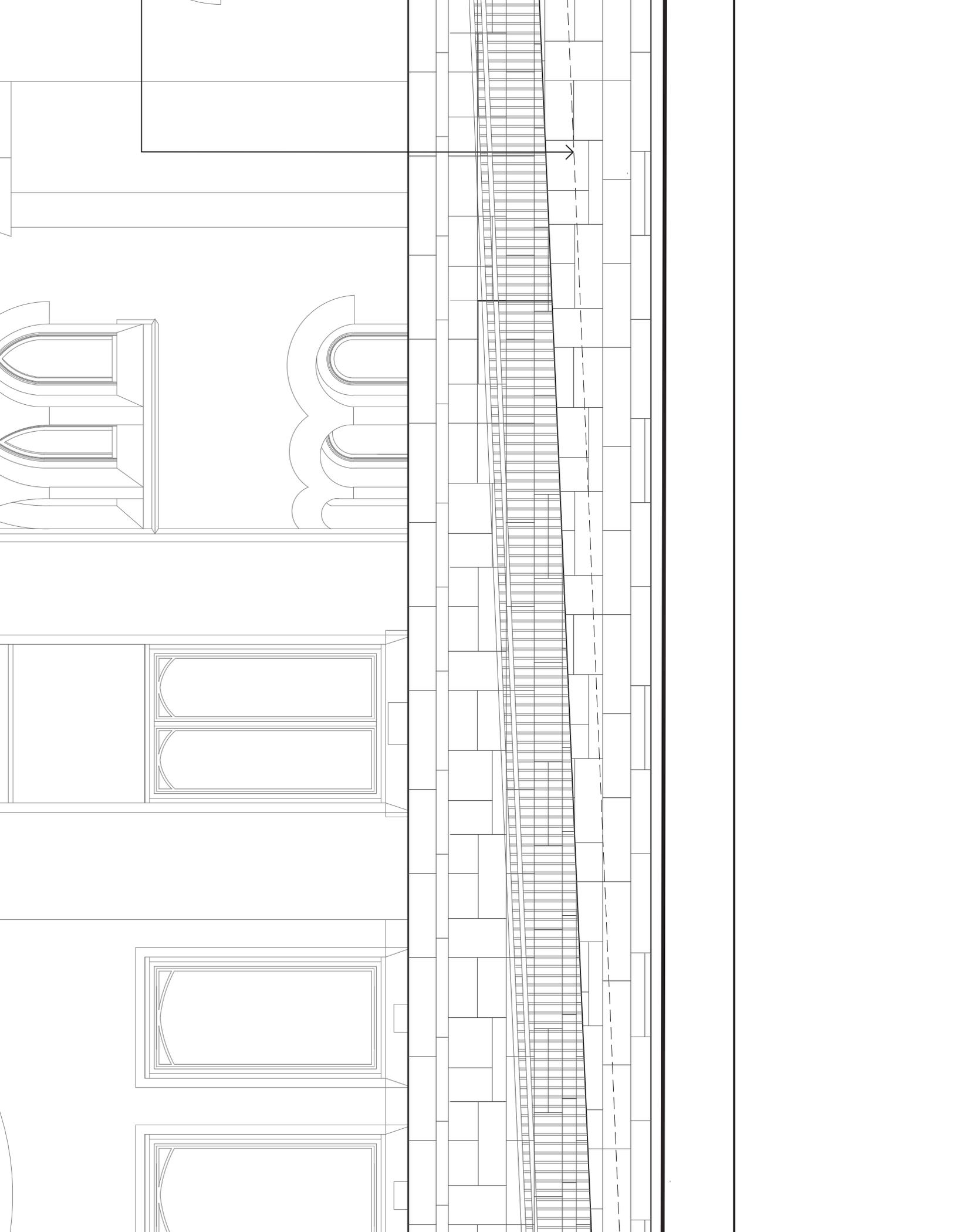
STAIRS BEYOND

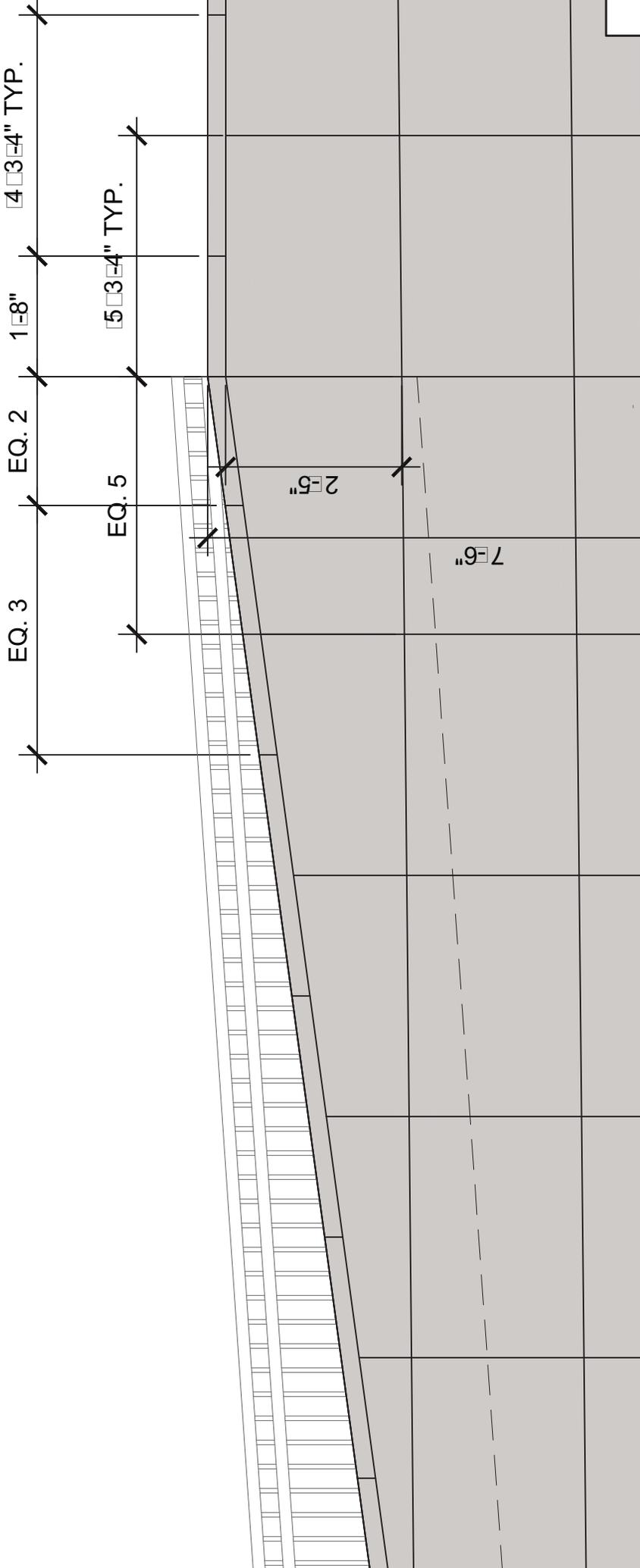
STAIRS

RAMP









4 #3-4" TYP.

1-8"

5 #3-4" TYP.

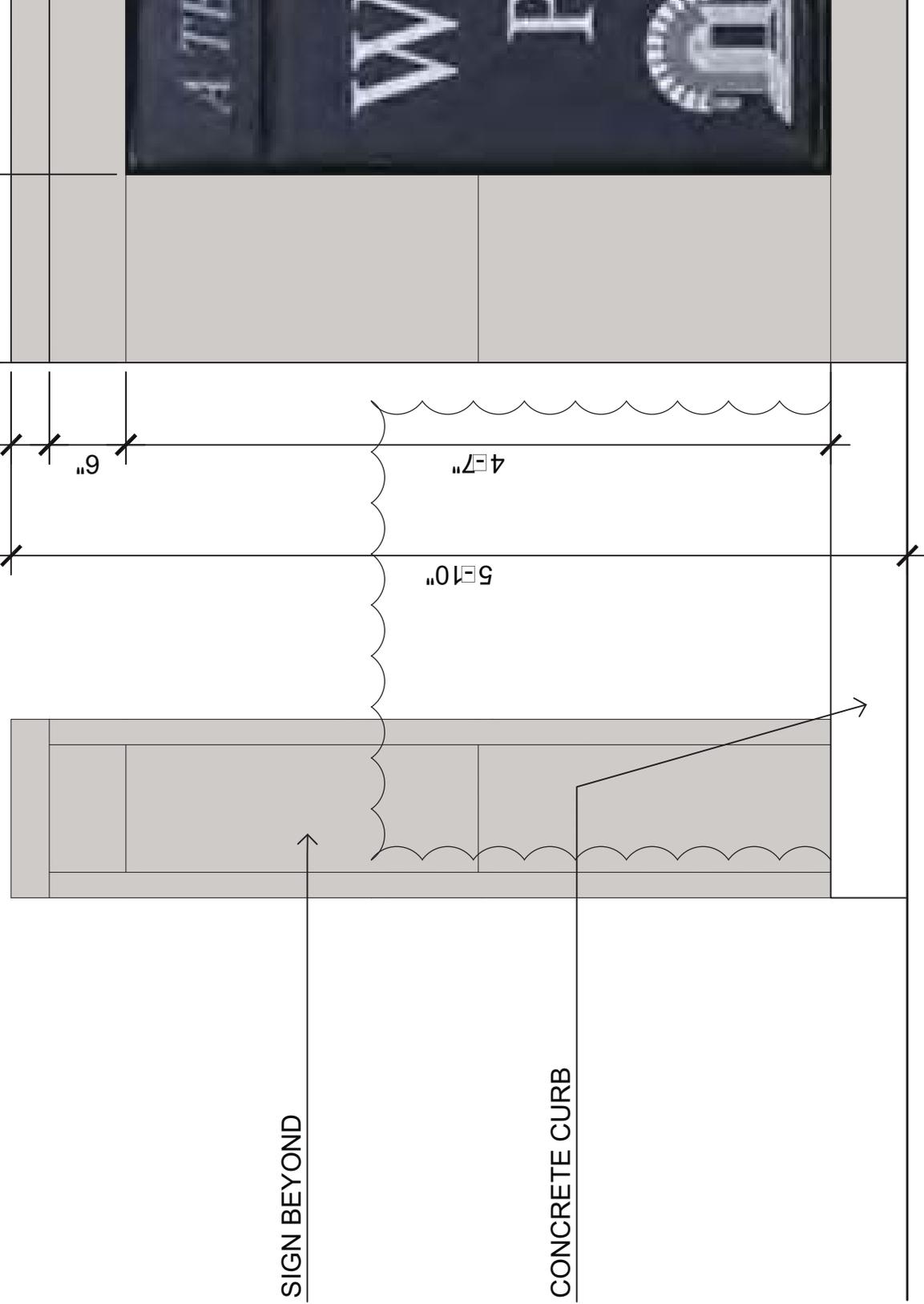
EQ. 2

EQ. 5

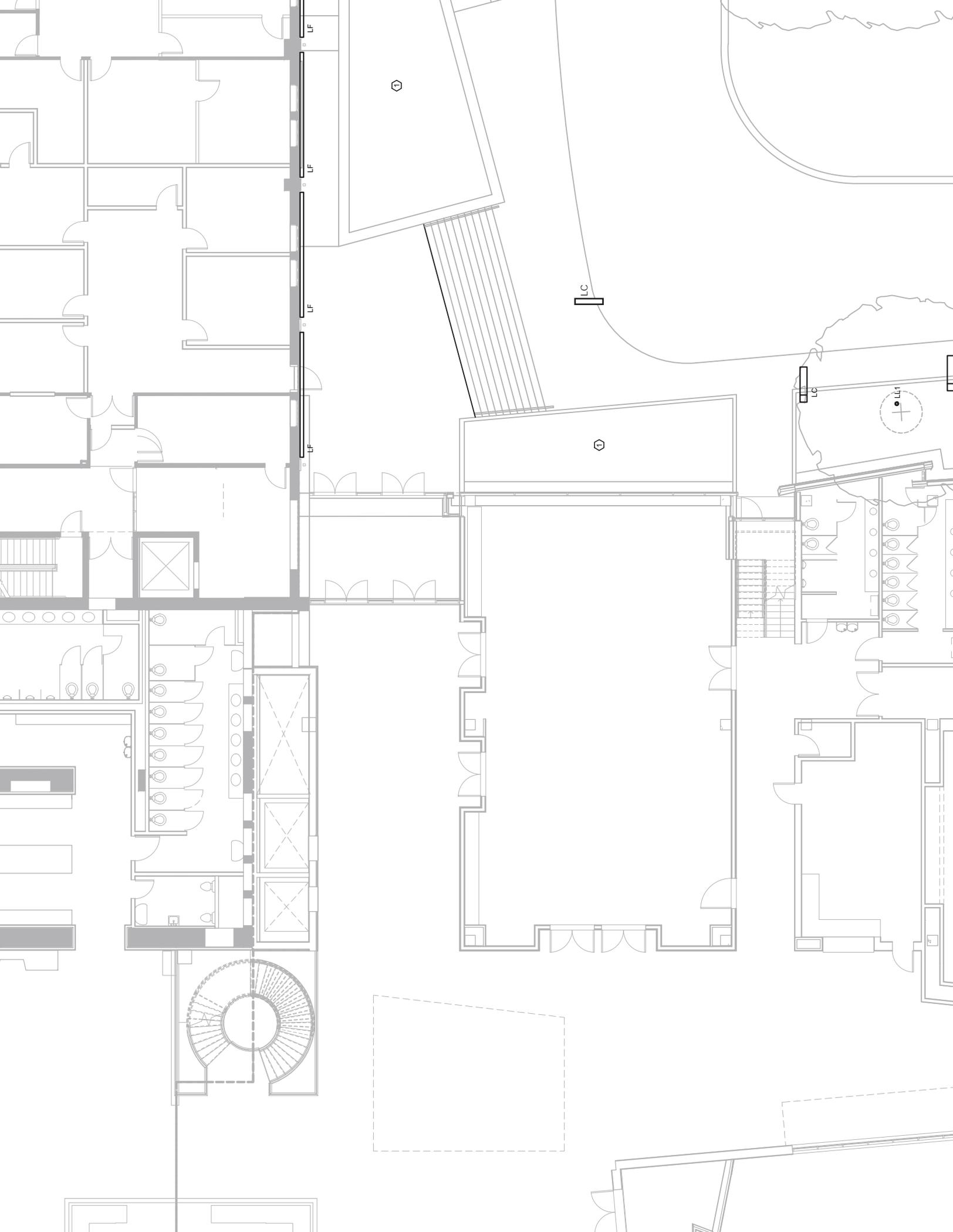
2-5"

EQ. 3

7-6"



**B** SIGN WALL AT MARQUETTE  
 SCALE 1" = 1'-0"







LOTTE STREET  
BLACK BUILDING  
= 43360 SQ. FT.

LANDSCAPE  
BENCHES, SIDEWALK, WALLS, ETC.

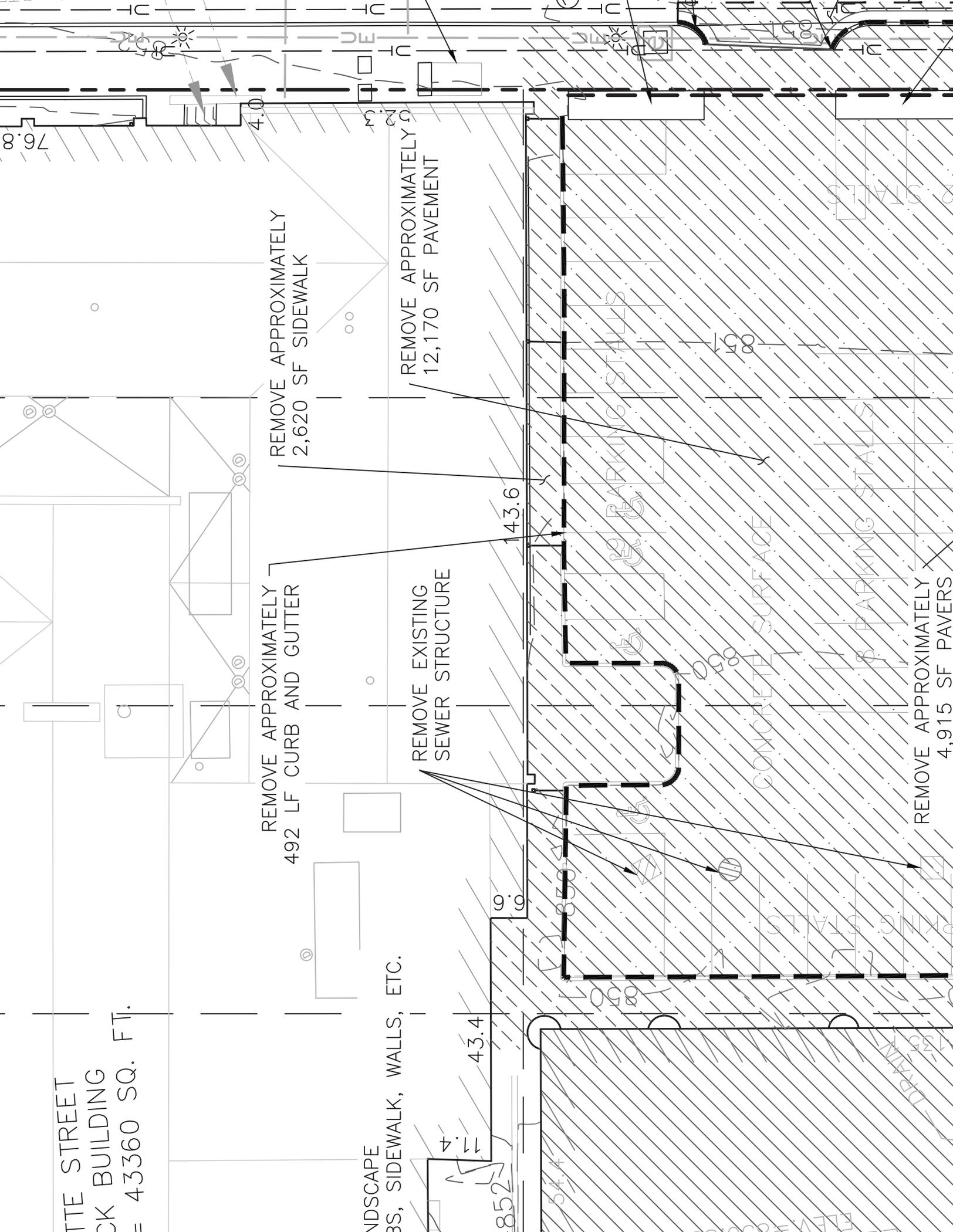
REMOVE APPROXIMATELY  
492 LF CURB AND GUTTER

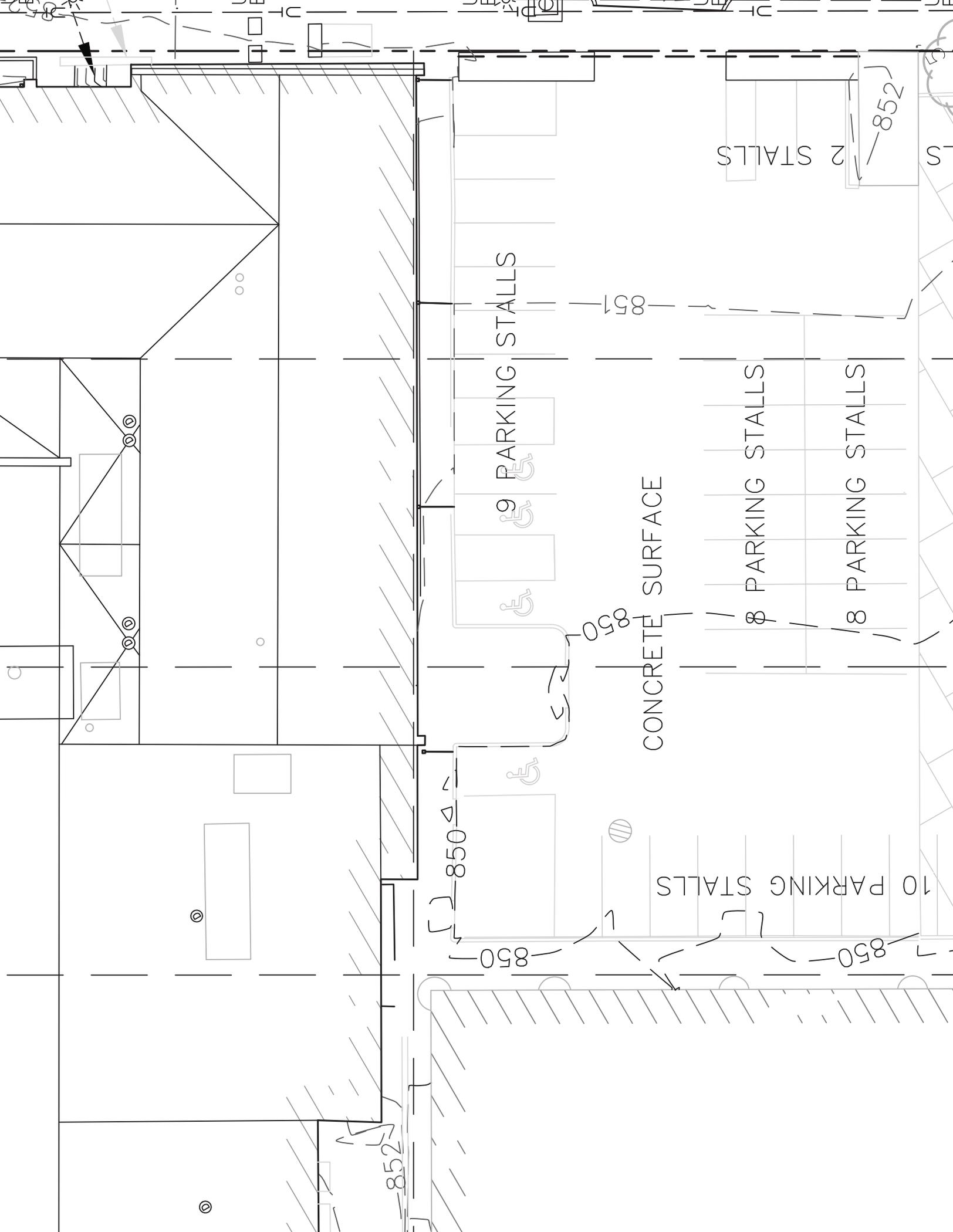
REMOVE APPROXIMATELY  
2,620 SF SIDEWALK

REMOVE APPROXIMATELY  
12,170 SF PAVEMENT

REMOVE EXISTING  
SEWER STRUCTURE

REMOVE APPROXIMATELY  
4,915 SF PAVERS





2 STALLS

852

PARKING STALLS

851

9 PARKING STALLS



CONCRETE SURFACE

850

8 PARKING STALLS

8 PARKING STALLS

10 PARKING STALLS

850

850

850

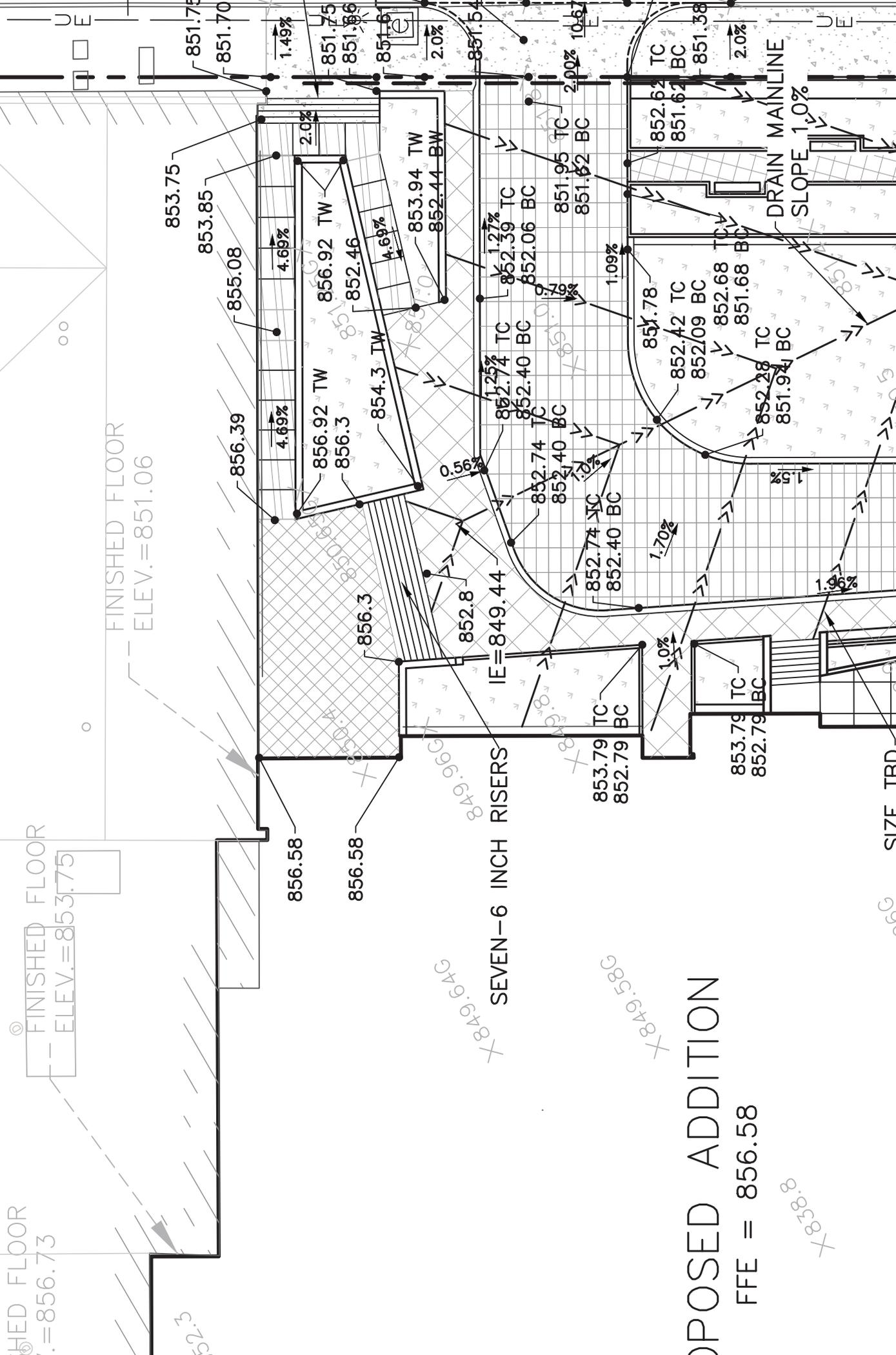
852

⊙

FINISHED FLOOR  
ELEV. = 856.73

FINISHED FLOOR  
ELEV. = 853.75

FINISHED FLOOR  
ELEV. = 851.06



856.58

856.58

856.3

852.8

IE = 849.44

853.79 TC  
852.79 BC

853.79 TC  
852.79 BC

853.75

853.85

856.39

856.92 TW  
856.3

854.3 TW

856.92 TW  
852.46

853.94 TW  
852.44 BW

852.74 TC  
852.40 BC

852.39 TC  
852.06 BC

851.78

852.42 TC  
852.09 BC

852.68 TC  
851.68 BC

852.28 TC  
851.94 BC

851.95 TC  
851.62 BC

852.62 TC  
851.62 BC

DRAIN MAINLINE  
SLOPE 1.0%

FINISHED FLOOR  
ELEV. = 856.73

FINISHED FLOOR  
ELEV. = 853.75

FINISHED FLOOR  
ELEV. = 851.06

856.58

856.58

856.3

852.8

IE = 849.44

853.79 TC  
852.79 BC

853.79 TC  
852.79 BC

FINISHED FLOOR  
ELEV. = 856.73

FINISHED FLOOR  
ELEV. = 853.75

FINISHED FLOOR  
ELEV. = 851.06

856.58

856.58

856.3

852.8

IE = 849.44

853.79 TC  
852.79 BC

853.79 TC  
852.79 BC

PROPOSED ADDITION  
FFE = 856.58

856.58

853.8

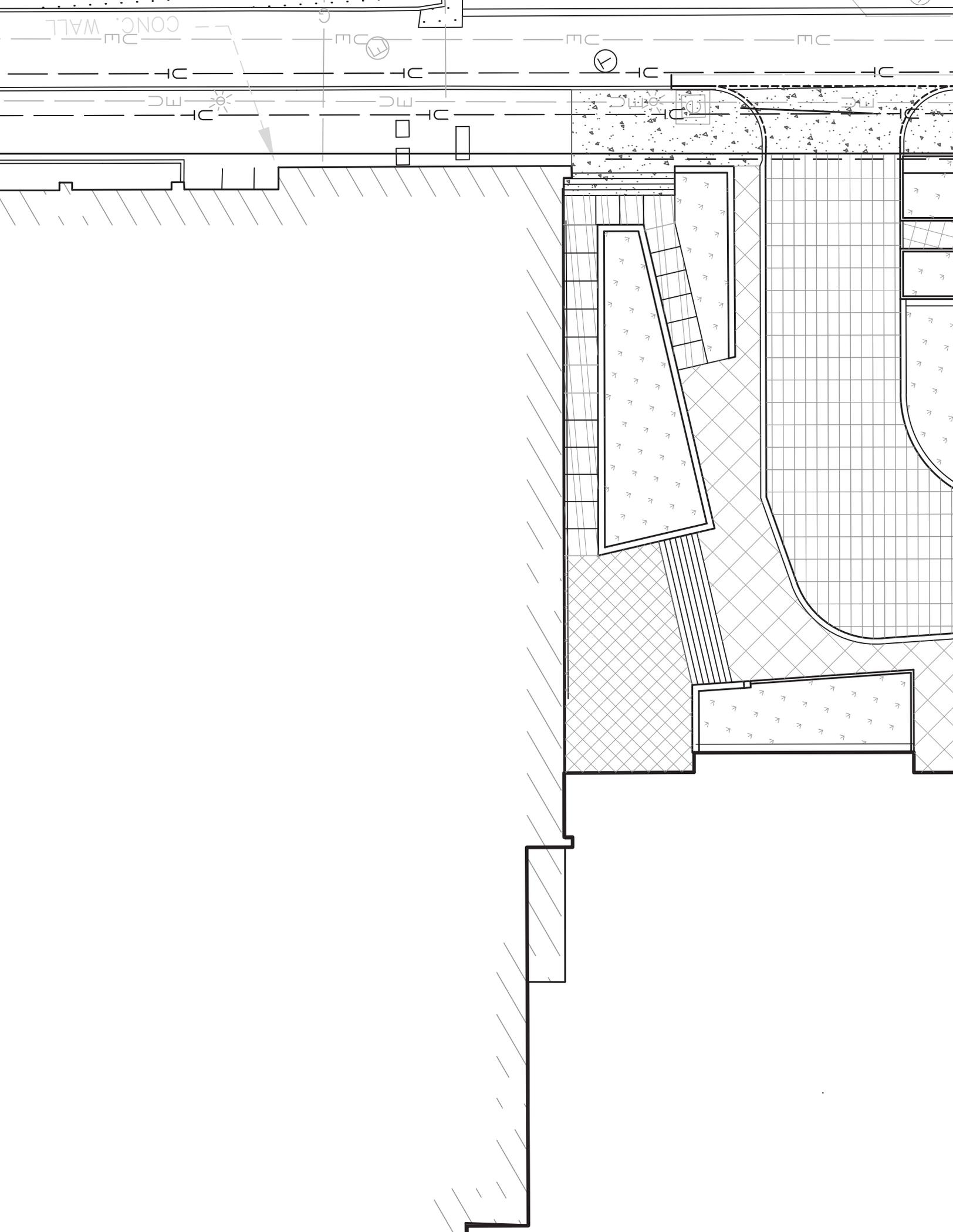
849.64G

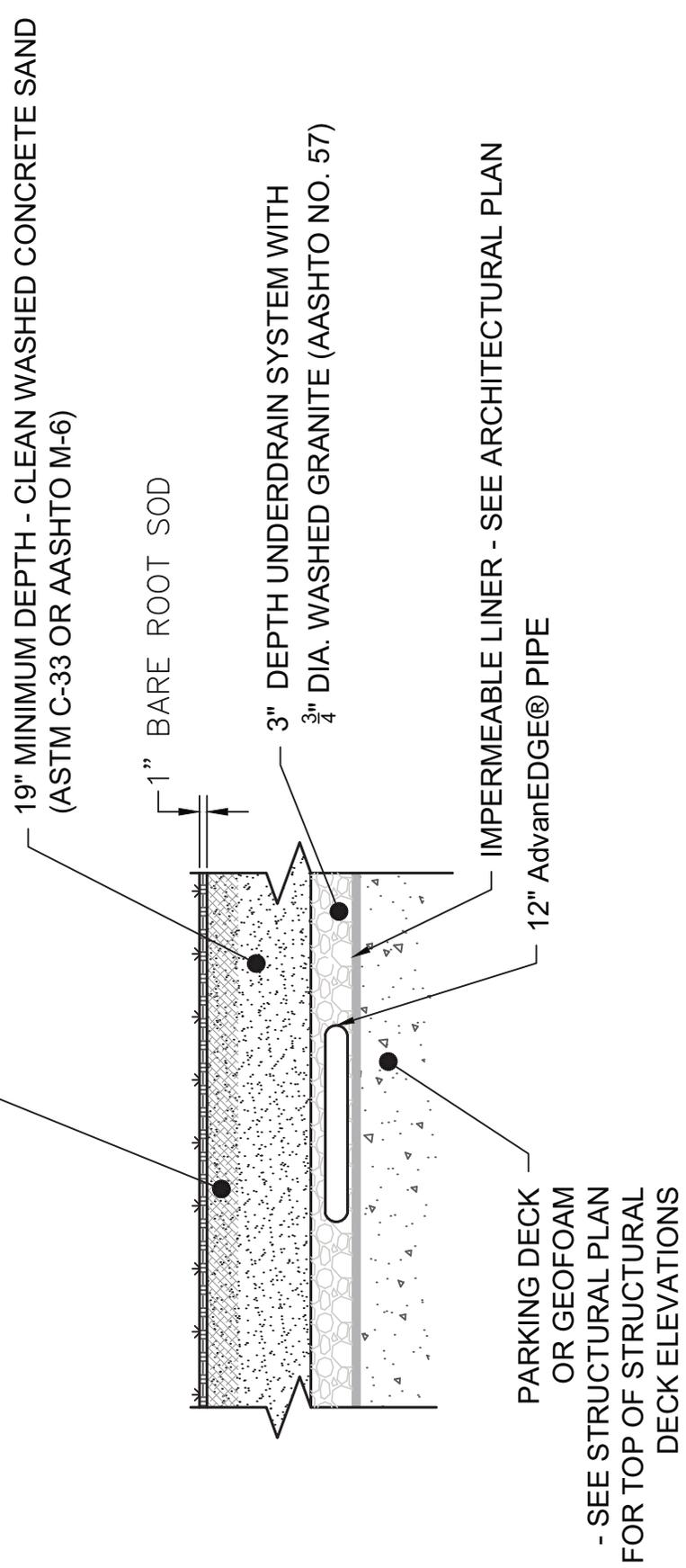
849.96G

849.58G

838.8

SIZE TRD





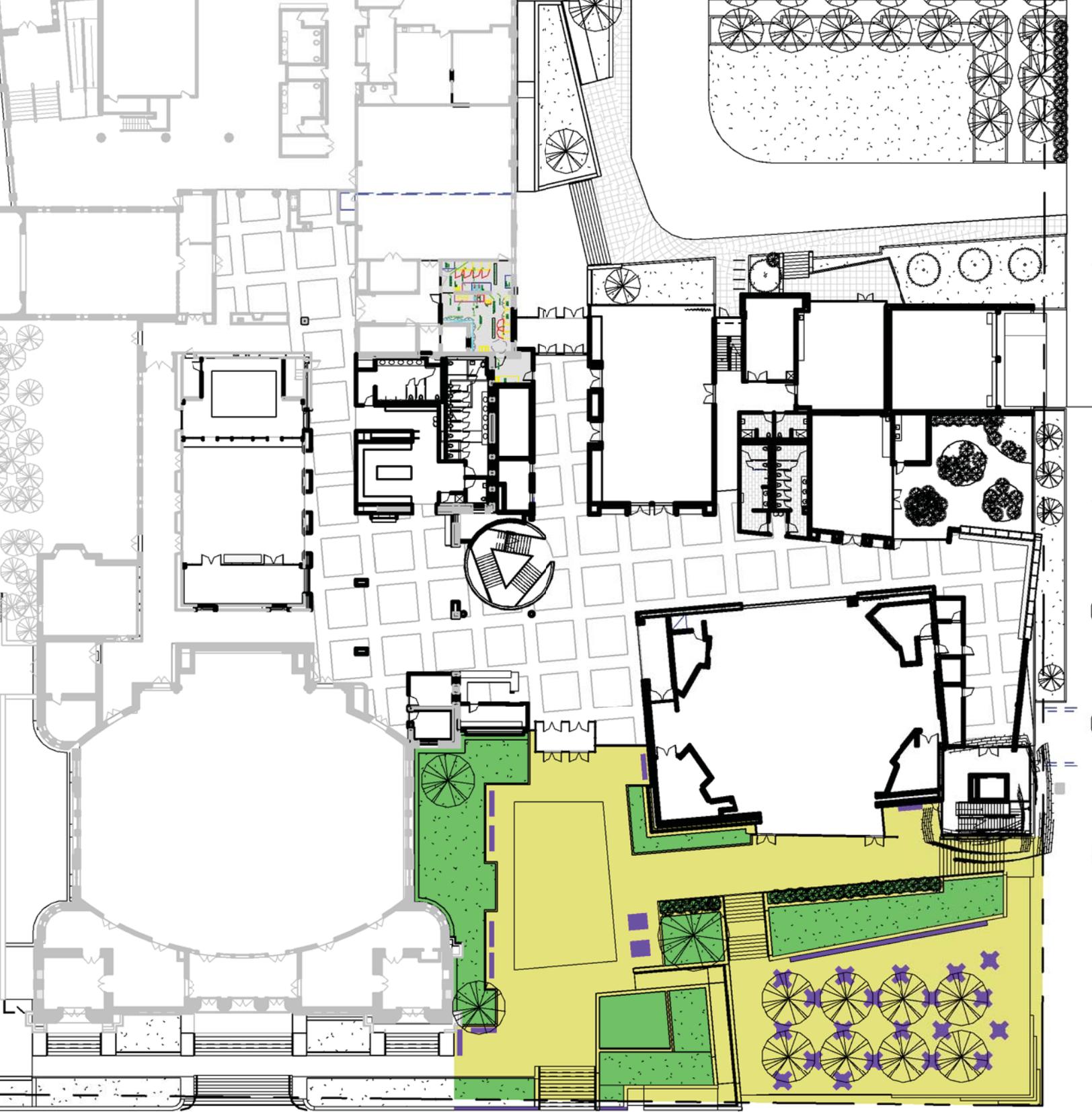
**NOTES:**  
 SAND MUST BE SILICA BASED - NO LIMESTONE BASED PRODUCTS MAYBE USED.  
 SAND MUST BE CLEAN. (UNWASHED DEPOSITS MAY NOT BE USED).  
 MANUFACTURED SAND OR STONE DUST IS NOT ACCEPTABLE.

2.0
C9.0

**GREENSPACE SECTION**

PERMEABLE INTERLOCKING PAVERS - SEE LANDSCAPE PLAN

2" DEPTH - 3/8" DIA. WASHED GRANITE (AASHTO NO. 8 STONE (MNDOT 3127-FA-3) BEDDING LAYER)



(EA)

S S

Shubs  
Shrubs

8 LF  
4 LF  
5 LF  
20%)  
2 LF  
0 LF  
9 LF  
8 LF

ad

F

E.4

E

D.8



Minneapolis Development Review  
250 South 4<sup>th</sup> Street  
Room 300  
Minneapolis, MN 55415

## Preliminary Development Review Report

**Development Coordinator Assigned:** **MATTHEW JAMES**  
**(612) 673-2547**  
**matthew.james@minneapolismn.gov**

<b>Status *</b>

<b>Tracking Number:</b>	PDR 1001427
<b>Applicant:</b>	WESTMINSTER PRESBYTERIAN CHURCH 1200 MARQUETTE AVE. MINNEAPOLIS, MN 55403
<b>Site Address:</b>	1200 MARQUETTE AVE 1226 MARQUETTE AVE 1221 NICOLLET MALL
<b>Date Submitted:</b>	19-APR-2016
<b>Date Reviewed:</b>	27-APR-2016

### Purpose

The purpose of the Preliminary Development Review (PDR) is to provide Customers with comments about their proposed development. City personnel, who specialize in various disciplines, review site plans to identify issues and provide feedback to the Customers to assist them in developing their final site plans.

The City of Minneapolis encourages the use of green building techniques. For additional information please check out our green building web page at: [http://www.ci.minneapolis.mn.us/mdr/GreenBuildingOptions\\_home.asp](http://www.ci.minneapolis.mn.us/mdr/GreenBuildingOptions_home.asp).

***DISCLAIMER: The information in this review is based solely on the preliminary site plan submitted. The comments contained in this report are preliminary ONLY and are subject to modification.***

### Project Scope

Two-story, 41,000 square foot addition housing a large worship area in addition to gathering spaces and multi-purposed rooms.

### Review Findings (by Discipline)

#### Historical Preservation Committee

- There is no HPC flag on this property. HPC review is not required at this time. HPC review is required for any wrecking permits pertaining to the removal of existing structures.

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\*Approved: You may continue to the next phase of developing your project.

\*Resubmission Required: You cannot move forward or obtain permits until your plans have been resubmitted and approved.

### ❑ Business Licensing

- There is no Minneapolis Business Licensing review required for the proposed project.

### ❑ Addressing

- Per City of Minneapolis Street Naming and Address Standard V1.22, the City of Minneapolis holds authority for assignment of all addresses, verification, change, and/or additions. Each assigned address number uses the street that provides the best/direct access for life safety equipment and best/direct access to the occupants.
- There will be no specific address for the proposed Westminster Presbyterian Church addition. It will retain the main address of 1200 Marquette Ave.
- When assigning suite sequences the following guidelines are as follows:
  - The first one to two digits of the suite sequence number will designate the floor number of the site.
  - The last two digits of the suite sequence number will designate the unique ID for the unit (condo, suite, unit, or apartment).
  - Suite sequence digit numbers will be assigned to dwelling, commercial and retail units, not common areas. For example, laundry rooms, saunas, workout rooms, etc., would not be assigned numbers.

### ❑ Parks - Forestry

- Contact Craig Pinkalla (612-499-9233 [cpinkalla@minneapolisparks.org](mailto:cpinkalla@minneapolisparks.org)) regarding any questions related to planting, removal or the process for protection of trees during construction in the city right of way:
  - Removal of (7) existing Blvd Honey Locust approved based on proposed landscape plan.
  - Bicolor Oak selection on Alice Raniville P. is approved.
  - Open planting space desing is consistent with Standards typology.
- Effective January 1, 2014, the City of Minneapolis and the Minneapolis Park and Recreation Board adopted an update to the existing Parkland Dedication Ordinance. The adopted City of Minneapolis Parkland Dedication ordinance is located in Section 598.340 of the City's Land Subdivision ordinance:
  - <http://library.municode.com/index.aspx?clientId=11490>
- As adopted, the fee in lieu of dedication for new residential units is \$1,521 per unit (affordable units excluded per ordinance) and for commercial and industrial development it is \$202.80 per development employee (as defined in ordinance). Any dedication fee (if required) must be paid at the time of building permit issuance.
- There is also an administration fee that is 5% of the calculated park dedication fee.
- As proposed, for your PROJECT, there will be no Park Dedication Fee assessed.
- This is a preliminary calculation based on your current proposal; a final calculation will be made at the time of building permit submittal.
- For further information, please contact Matthew D. James at (612) 673-2547.

### ❑ Zoning - Planning

- The proposed project requires site plan review and four variances (1. request to reduce minimum floor area ratio, 2. request to allow more parking than allowed. 3. request to reduce drive aisle width, and 4. request to allow more than one freestanding sign on a zoning lot).

### ❑ Right of Way

- The plans indicate that the existing transformer vaults currently located in the right-of-way will remain on-site. Please verify that these vaults will contain the needed electric utility transformers for the development. If other locations outside of the vaults are needed, it is recommended that the Applicant begin discussions with Xcel Energy as soon as possible in order to identify electric utility and transformer locations on the site plan. It should not be assumed that the City will approve any proposed transformer location in the Public right-of-way.
- An encroachment permit shall be required for all streetscape elements in the Public right-of-way such as: plants & shrubs, planters, tree grates and other landscaping elements, sidewalk furniture (including bike racks and bollards), and sidewalk elements other than standard concrete walkways such as pavers, stairs, raised

landings, retaining walls, access ramps, and railings (NOTE: railings may not extend into the sidewalk pedestrian area). Please contact Bob Boblett at (612) 673-2428 for further information.

- Numerous physical encroachments into the Public right-of-way are proposed along Nicollet Mall; please continue to coordinate with Rick Kreuser at (612) 673-3624, the Project Manager overseeing the reconstruction of Nicollet Mall to insure all encroachments meet with current Nicollet Mall plan requirements. Note that as a condition of final site plan approval, the plans must be coordinated with the most current plan for the Nicollet Mall reconstruction.
- Note to the Applicant: Any elements of an earth retention system and related operations (such as construction crane boom swings) that fall within the Public right-of-way will require an encroachment permit application. If there are to be any earth retention systems which will extend outside the property line of the development then a plan must be submitted showing details of the system. All such elements shall be removed from the Public right-of-way following construction with the exception of tie-backs which may remain but must be uncoupled and de-tensioned. Please contact Bob Boblett at (612) 673-2428 for further information.
- In addition, any elements of an earth retention system and related excavations that fall within the Public right-of-way will require a "Right-of-Way Excavation Permit". This permit is typically issued to the General Contractor just prior to the start of construction. However, it is the Applicant's responsibility to insure that all required permits have been acquired by its consultants, contractors, sub-contractor's prior to the start of work.
- The Project limits fall within the boundaries of the Downtown Improvement District (DID). Any improvements, modifications, and alterations to the streetscape are subject to the review and approval of the DID. Please contact Ben Shardlow at (612) 656-3830 for further information.
- Please continue to coordinate with Rick Kreuser at (612) 673-3624, the project manager overseeing the reconstruction of Nicollet Mall.
- Landscaping notes are not included in the submitted plan sets, please be aware that in addition to an encroachment permit application, any irrigation system in the Public right-of-way will require the Applicant to register as a private utility and to enroll in the Gopher State One-Call system, so that anyone digging in your area will be made aware that your lines are in the right-of-way.

#### □ Street Design

- Proposed driveway aprons are shown incorrectly and must be constructed to City standards with level sidewalks and 5' radii, not 10'radii. All driveway aprons shall be designed and constructed to City standards. All driveway aprons shall be shown graphically correct on all related plan sheets. The Minneapolis Standard Plate ROAD-2003 provided on Sheet C8.0 has been updated and shall be replaced with ROAD-2003-R1. Please refer to the following: [http://www.minneapolismn.gov/publicworks/plates/public-works\\_road](http://www.minneapolismn.gov/publicworks/plates/public-works_road).
- Note to the Applicant: Please continue to coordinate with Rick Kreuser at (612) 673-3624, the project manager overseeing the reconstruction of Nicollet Mall.

#### □ Sidewalk

- Note to the Applicant: Any currently defective sidewalks or other concrete infrastructure within the public right of way, or any concrete infrastructure damaged during construction, must be removed and replaced.
- The Project streetscape shall be designed in accordance with the Access Minneapolis design guidelines; see <http://www.ci.minneapolis.mn.us/publicworks/transplan/index.htm> for further information. The Minneapolis Convention Center (MCC) is currently in the midst of a large planning initiative that may include Alice Rainville Place; the Applicant is encouraged to coordinate streetscape design with the Convention Center; please contact Jeff Johnson for further information at (612) 335-6310.
- All proposed trees in the Public right-of-way are subject to the review and approval of the Minneapolis Park Board. Please contact Craig Pinkalla at (612) 499-9233 to discuss tree species selection, planting method, spacing and locations. Tree planting details shall be included in the plans. For all trees proposed in "hardscape environments" within the Public right-of-way, the Applicant shall provide engineered/structured soil in the form of a tree trench or tree pit for all proposed street trees. Sidewalk layouts and landscaping in the Public right-of-way shall follow established design standards; refer to the following: City of Minneapolis Urban Forest Policy ([http://www.minneapolismn.gov/www/groups/public/@cped/documents/webcontent/convert\\_282934.pdf](http://www.minneapolismn.gov/www/groups/public/@cped/documents/webcontent/convert_282934.pdf))

- ADA compliant pedestrian ramps are required at each proposed crosswalk. Construct two (2) ADA compliant pedestrian ramps at each proposed location. Include the appropriate details and standard plans in the site plan; refer Mn/DOT Standard Plan 5-297.250 Pedestrian Curb Ramp Details at: <http://standardplans.dot.state.mn.us/stdplan.aspx>. All pedestrian ramps will need to meet current ADA regulations with APS. Please contact Ryan Anderson at 612-673-3986 for more information about APS.

## □ Traffic and Parking

- The nature of the proposed development is such that traffic impacts will be an issue and a Travel Demand Management Plan (TDMP) will be required, contact Allan Klugman at (612) 673-2743 to discuss. Note that site circulation is considered an issue and that current City Ordinance states that all maneuvers associated with parking and "drop-off/pick-up" for a private development shall occur on private property. It is recommended that the Applicant consider alternatives to parking ramp access from the drop-off area and not Alice Rainville Place. The TDMP shall be used to assist justification for site circulation, access point locations, curb cuts, and in setting driveway opening sizes.
- Pavement markings at the intersection of Marquette Ave. S. and Alice Rainsville Place shall be removed from the plans.
- Current ordinance states that all maneuvers associated with loading, parking or sanitation pick up for a private development shall occur on private property. Please provide a narrative explaining the trash removal operations and show turning maneuvers for all truck type vehicles that will be using the loading dock/parking entrance areas. Per City Ordinance the Applicant shall provide for (and identify) a solid waste collection point (SWCP) on the site plans. The location of the SWCP is subject to the review and approval of the Public Works Department.
- There is lighting conduit in the boulevard that may conflict with the locations of the new drive aprons. Please contact Dave Prehall at (612) 673-5759 to discuss what actions may have to take place before construction of the new apron.
- All street lighting (existing and proposed) shall be shown clearly on the site plan. Prior to site plan approval, the Applicant shall contact Joseph Laurin at (612) 673-5987 to determine street lighting requirements. Note: If decorative street lighting exists on the proposed site it must be preserved or replaced at existing levels. Per the City of Minneapolis Street Lighting Policy, street lighting is required as the project falls within the Central Business District. Street lighting must be brought up to the Pedestrian Street Lighting Corridor standards with the disturbance of sidewalk and curb and photometric calculations must meet the standards outlined by the policy.
- The Minneapolis Street Light policy is available at the following: <http://www.minneapolismn.gov/publicworks/streetlighting/index.htm>. All street lights shall be designed and constructed to City standards. Please refer to the following: [http://www.ci.minneapolis.mn.us/publicworks/plates/public-works\\_traffic](http://www.ci.minneapolis.mn.us/publicworks/plates/public-works_traffic).
- The vehicle access point on Alice Rainville Place shall provide for an unobstructed 20' sight triangle and shall have less than a 4% slope for the first 20 feet from the property line. Given the limited sight distance exiting the parking garage, if these minimums cannot be attained, the Applicant shall provide mitigating measures, such as pedestrian warning devices, signage, and slip prevention measures on the ramp.
- All vehicle entrance and exit points shall be signed appropriately for the one-way flow through the drop-off lot. Please consider altering the drop-off to have defined spaces such as a drive lane and parking lane.
- Note to the Applicant: The construction of this development will likely require the use of Public right-of-way (roadway and sidewalks) for construction purposes. A request for an estimate of street use and obstruction permit fees can be made to the City's Traffic Department; please contact Scott Kramer at (612) 673-2383 for further information.
- Note to the Applicant: Please add the following notes to the site plan:
  - Street lighting installed as part of the Project shall be inspected by the City. Contractors shall arrange for inspections with the Traffic Department, please contact Dave Prehall at (612) 673-5759 for further information. Any lighting installations not meeting City specifications will be required to be reinstalled at Owner expense.

- An obstruction permit is required anytime construction work is performed in the Public right-of-way. Please contact Scott Kramer at (612) 673-2383 regarding details of sidewalk and lane closures. Log on to <http://minneapolis.mn.roway.net/> for a permit.
- Contact Allan Klugman at (612) 673-2743 prior to construction for the temporary removal/temporary relocation of any City of Minneapolis lighting or traffic signal system that may be in the way of construction.
- All costs for relocation and/or repair of City Traffic facilities shall be borne by the Contractor and/or Property Owner.
- Contact Doug Maday at (612) 673-5755 prior to construction for the removal of any City of Minneapolis right of way signs that may be in the way of construction.

#### ❑ Water

- Water taps need to be located no more than two feet outside the building line.

#### ❑ Sewer Design

- Groundwater: Please identify the lowest floor elevation on the Grading Plan and note on the plan if any groundwater discharges are proposed upon completion of construction in order to keep the below grade portions of the building dry.
- Stormwater Management: Please provide a table of the BMP input parameters used in the MIDS Calculator.
- An operations and maintenance plan is required for the stormwater treatment devices. The O&M plan shall define the maintenance regimen, including type and interval of maintenance and party to conduct such maintenance. Please provide a copy of the O&M Plan.
- Please add the following note to the appropriate plan sheet: The Contractor, property owner or responsible party shall contact Minneapolis Surface Waters and Sewers 48 hours prior to any excavation or construction related to or in the location of the proposed Stormwater Management BMP (Contact Paul Chellsen, (612) 673-2406 or [paul.chellsen@minneapolismn.gov](mailto:paul.chellsen@minneapolismn.gov)).
- Please add the following note to the appropriate plan sheet: Upon the project's completion the General Contractor, Property Owner or Responsible Party shall provide to the Department of Public Works a Final Stormwater Management Report including record drawings. This report will serve as a means of verification that the intent of the approved stormwater management design has been met. This final report shall substantiate that all aspects of the original design have been adequately provided for by the construction of the project.
- Please clarify if the Total Site, Impervious and Pervious Areas identified on Sheet C4.0 are the total site areas or disturbed areas. The total site appears to be larger than this. The areas based on disturbed area should still be provided, but clarified.
- Utility Connections: Please provide the relevant design information on the proposed storm and sanitary sewer services (inverts, pipe slopes, etc.)
- It appears that the proposed sanitary sewer service is located under the existing underground vault and transformer. It should be verified that the service is able to be constructed in this location.
- Non Stormwater Discharges: Detail all mechanical and non-stormwater discharges. Non-stormwater discharges are not permitted unless approved by the City of Minneapolis. Non-stormwater discharges not declared and approved will not be permitted. If there currently are none and nothing is proposed declare this status on the plans.
- For comments or questions on Public Works Surface Water & Sewers Division related requirements please contact Jeremy Strehlo, (Professional Engineer) at (612) 673-3973, or [jeremy.strehlo@minneapolismn.gov](mailto:jeremy.strehlo@minneapolismn.gov).

#### ❑ Construction Code Services

- Accessible route from Nicollet Mall side of addition shall be a maximum slope of 5%. 1 to 12 or 8.33% is shown on civil drawings and is not permitted by accessibility code.

**❑ Fire Safety**

- Provide fire suppression system in new addition.
- Provide fire alarm protection throughout addition.
- Maintain fire apparatus at all times.

**❑ Environmental Health**

- If impacted soil is encountered during site activities call the MN State Duty officer at (651) 649-5451.
- If dewatering is required during site construction see below for city permit requirements. Subgrade structures should be designed to prevent infiltration of groundwater without the need for a permanent dewatering system being installed. If a continuously operating permanent dewatering system is needed it must be approved as part of the sanitary sewer and storm drain site plan approval prior to construction beginning.
- No construction, demolition or commercial power maintenance equipment shall be operated within the city between the hours of 6:00 p.m. and 7:00 a.m. on weekdays or during any hours on Saturdays, Sundays and state and federal holidays, except under permit. Contact Environmental Services at (612) 673-3867 for permit information.
- Permits and approval are required from Environmental Services for the following activities: Temporary storage of impacted soils on site prior to disposal or reuse; Reuse of impacted soils on site; Dewatering and discharge of accumulated storm water or ground water, underground or aboveground tank installation or removal, well construction or sealing. Contact Tom Frame at (612) 673-5807 for permit applications and approvals.

END OF REPORT