

LAND USE APPLICATION SUMMARY

Property Location: 3120 and 3147 24th Avenue South
Project Name: Corcoran Triangle
Prepared By: Hilary Dvorak, Principal Planner, (612) 673-2639
Applicant: Wellington Management
Project Contact: David Wellington
Request: To construct a new 4-story, 135-unit residential building.
Required Applications:

Variance	Of the maximum building setback from East 32 nd Street in the PO Pedestrian Oriented Overlay District from 20 feet to approximately 95 feet.
Variance	To allow surface parking that is located in the required side yard and not located in the rear 40 feet or 20 percent of the lot.
Variance	To reduce the minimum width of a two-way drive aisle from 22 feet to 19 feet.
Site Plan Review	For a new 4-story, 135-unit residential building.
Preliminary and Final Plat	Preliminary and final plat.
Vacation	Of 24 th Avenue South from East 32 nd Street to the Metro Transit LRT tracks.

SITE DATA

Existing Zoning	R5 Multiple-family District TSA Transit Station Area PO Pedestrian Oriented Overlay District
Lot Area	112,931 square feet / 2.59 acres
Ward(s)	9
Neighborhood(s)	Corcoran; adjacent to Longfellow
Designated Future Land Use	Urban Neighborhood
Land Use Features	Transit Station Area (Lake Street/Midtown)
Small Area Plan(s)	<u>Hiawatha/Lake Station Area Master Plan</u> (2001) <u>Corcoran Midtown Revival Plan</u> (2002)

Date Application Deemed Complete	June 19, 2015	Date Extension Letter Sent	July 9, 2015
End of 60-Day Decision Period	August 18, 2015	End of 120-Day Decision Period	October 17, 2015

BACKGROUND

SITE DESCRIPTION AND PRESENT USE. The property is located on the northwest corner of East 32nd Street and Hiawatha Avenue. The two properties are triangular-shaped properties that have been vacant since Highway 55 was constructed. A fragment of 24th Avenue South currently runs between the two properties. This fragment of the street will be vacated as part of this development.

SURROUNDING PROPERTIES AND NEIGHBORHOOD. The site is surrounded by residential properties to the south and west. North of East 31st Street there will be a mixed-use development constructed including multiple-family residential, office and commercial. East of Hiawatha Avenue there is a mixture of commercial and industrial businesses. The site is located in the Corcoran neighborhood.

PROJECT DESCRIPTION. The applicant is proposing to construct a new 4-story, 135-unit residential building on the property. There will be a total of 151 Parking spaces provided on the site; 100 will be located in one level of underground parking and 51 will be located in a series of surface parking areas.

In place of the public street that terminates in the middle of the property, the applicant is proposing to construct a private road that will connect East 32nd Street to East 31st Street. The applicant is also proposing to construct an off-street bike trail that will run along the east side of the former 24th Avenue South street alignment and then parallel with the Green Line LRT tracks. Both the road and the bike trail will be open to the public to use.

Underneath the current alignment of 24th Avenue South is an existing water main, sanitary sewer line, storm sewer line and a gas main. None of these utilities can be moved without huge expense. Through discussions with Public Works it has been recommended that no public structures be constructed over the street alignment.

There are two single-family homes south of the development site that will remain. These homes are located at 3146 24th Avenue South and 2318 East 32nd Street. The applicant has tried to purchase these homes to no avail. Although the home located at 3146 24th Avenue South faces 24th Avenue South it is a reverse corner lot and will maintain access on East 32nd Street. The garage associated with this house is accessed from East 32nd Street.

RELATED APPROVALS. Not applicable.

PUBLIC COMMENTS. A letter of support from the Corcoran Neighborhood Organization is included with the report. Any additional correspondence received prior to the public meeting will be forwarded on to the Planning Commission for consideration.

ANALYSIS

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance of the maximum building setback from East 32nd Street in the PO Pedestrian Oriented Overlay District from 20 feet to approximately 95 feet based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

In the TSA Transit Station Area PO Pedestrian Oriented Overlay District there is a maximum setback from the front property line of 8 feet, except where a greater yard is required by the zoning ordinance. The homes located at 3146 24th Avenue South and 2318 East 32nd Street have a front yard along East 32nd Street so the required front yard along this street is 20 feet.

Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. Given the location of the existing underground utilities, the building cannot be built over the current street alignment which requires the building to be set back more than eight feet from the front property line.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The applicant is proposing to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan. The intent of yard controls are established to provide for the orderly development and use of land and to minimize conflicts among land uses by regulating the dimension and use of yards in order to provide adequate light, air, open space and separation of uses.

The applicant is designing the site with a private road between East 32nd Street and East 31st Street. It will be built so it aligns with the location of 24th Avenue South, south of East 32nd Street. Although the road will be private it will be open to the public to use. Even though the true front property line is along East 32nd Street, the private road will function more like the front of the property based on how the site is being designed.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Granting of this variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. In addition, granting of the variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties. The proposed alignment of the building, facing the private drive which runs north-south, is characteristic of residential development in Minneapolis. The building will be set back between 15 and 20 feet from the curb line of the new private road.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance to allow surface parking that is located in the required side yard and not located in the rear 40 feet or 20 percent of the lot based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

The surface parking areas located on the east and south sides of the site are located within a required interior side yard and are not located in the rear 40 feet or 20 percent of the lot. The parking area on the south side of the site is adjacent to the two single-family homes on the block. It

should be noted that this parking area depresses as the grade of the site slopes down to enter the underground parking garage.

Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. Given the location of the existing underground utilities, the building cannot be built over the current street alignment which then dictates where the parking can be located on the site.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The applicant is proposing to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan. Parking regulations are established to recognize the parking needs of uses and structures, to enhance the compatibility between parking areas and their surroundings, and to regulate the number, design, maintenance, use and location of off-street parking spaces and the driveways and aisles that provide access and maneuvering space.

The minimum parking requirement for multiple-family dwellings in the TSA Transit Station Area PO Pedestrian Oriented Overlay District is .9 spaces per dwelling unit. The minimum parking requirement for 135 dwelling units is 122 spaces. There is no maximum parking requirement for multiple-family dwellings except when the spaces are not provided within an enclosed structure. In this case there shall be no more than 2 spaces provided per dwelling unit. The applicant is proposing to have 51 parking spaces located in the two surface parking areas; this number does not exceed the maximum.

There is one parking space located in the northeast corner of the site that is not buffered from East 31st Street. CPED is recommending that this parking space be eliminated and that landscaping be provided in this location. Eliminating this parking space will also reduce conflicts as cars enter and exit the site from East 31st Street.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Granting of this variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. In addition, granting of the variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

The applicant is designing the site with a private road between East 32nd Street and East 31st Street. It will be built so it aligns with the location of 24th Avenue South, south of East 32nd Street. The parking area along this private road will look like street parking on a typical Minneapolis street. The parking area on the south side of the site will be screened from the adjacent residential structure by a decorative fence and landscaping. The applicant has indicated that two of these spaces will be reserved for the property owner of the house located at 3146 24th Avenue South.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance to reduce the minimum width of a two-way drive aisle from 22 feet to 19 feet based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

The parking area on the east side of the site is designed like a street. To meet the zoning code standards, the parallel parking spaces need to be 8 feet 6 inches wide and the two-way drive aisle needs to be 22 feet wide. Parking space dimensions cannot be varied. The applicant is proposing a two-way drive aisle that is 19 feet wide.

Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. Given the location of the existing underground utilities, the building cannot be built over the current street alignment which then dictates where the parking can be located on the site. In addition, the east property line is angled as it follows the alignment of the Green Line LRT tracks and Hiawatha Avenue. The angled property line creates more of a triangular-shaped lot than a rectangular-shaped lot.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The applicant is proposing to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan. Parking regulations are established to recognize the parking needs of uses and structures, to enhance the compatibility between parking areas and their surroundings, and to regulate the number, design, maintenance, use and location of off-street parking spaces and the driveways and aisles that provide access and maneuvering space.

If the drive aisle were designed to be one-way the width would only need to be 20 feet wide; one foot wider than what is being proposed. A 19-foot wide drive aisle will still allow two vehicles to pass one another.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Granting of this variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. In addition, granting of the variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties. As stated above, a 19-foot wide drive aisle will still allow two vehicles to pass one another.

SITE PLAN REVIEW

The Department of Community Planning and Economic Development has analyzed the application based on the required [findings](#) and [applicable standards](#) in the site plan review chapter:

I. Conformance to all applicable standards of Chapter 530, Site Plan Review.

Building Placement and Design – Requires alternative compliance

- The building will not reinforce the street wall along East 32nd Street as it will be set back 95 feet from the front property line. However, the building will reinforce the street wall along East 31st Street, maximize natural surveillance and visibility and facilitate pedestrian access and circulation. There will be a principal entrance at street level that can be accessed by residents and guests and there will be large windows on all sides of the building that maximize the opportunities for people to observe adjacent spaces. In addition, a five-foot wide walkway will be constructed from East 32nd

Street to East 31st Street. This walkway will connect the building entrance to the public sidewalks. Alternative compliance is needed.

- The building will be set back 95 feet from the front property line along East 31st Street. Alternative compliance is needed.
- The area between the building and the front property line contains landscaping, walkways, a bike trail, drive aisle and surface parking.
- The principal entrance to the building faces the east interior property line. Alternative compliance is needed.
- There will be a total of 151 parking spaces provided on-site. One hundred of the parking spaces will be provided in one level of below-ground parking and the remaining 51 parking spaces will be provided in two surface parking areas. One surface parking area will be located on the east side of the site and the other will be located on the south side of the site. Given the site configuration, all of the parking is located to the interior side of the building.
- The building wall facing the east property line is approximately 455 feet in length. The applicant has used recesses and projections in the building wall, punches of bright colors, a stepped back fourth floor and canopies over the ground level entrances to break the facade up into smaller sections.
- There are no areas of the building that are over 25 feet in length and void of windows, entries, recesses or projections, or other architectural elements.
- The exterior materials of the building will include brick, metal, cement board and rockface concrete block. The sides and rear of the building will be similar to and compatible with the front of the building. It should be noted that there will be no more than three primary materials on any single building elevation.
- Plain face concrete block is not being proposed as an exterior building material.
- The windows in the building are vertical in nature and are spaced across the building walls. The required minimum window percentages are being met on all sides of the building except the first floor facing the south parking area. See **Table I**. Alternative compliance is needed.
- The entire ground floor of the building facing East 31st Street and Hiawatha Avenue contains active functions.
- The roof pitch of the building will be flat. There is a mixture of both pitched and flat roofed buildings in the area. The predominate roofline of the multiple-family dwellings in the area are flat.

Table I. Percentage of Windows Required for Elevations Facing a Public Street, Sidewalk, Pathway, or On-Site Parking

	Code Requirement		Proposed	
Residential Uses				
1st Floor facing East 31 st Street	20% minimum	163 sq. ft.	26%	211 sq. ft.
2nd Floor and Above facing East 31 st Street	10% minimum	102 sq. ft.	At least 10 percent	
1st Floor facing Hiawatha Avenue	20% minimum	728 sq. ft.	26%	930sq. ft.
2nd Floor and Above facing Hiawatha Avenue	10% minimum	455 sq. ft.	At least 10 percent	
1st Floor facing the south parking lot	20% minimum	266 sq. ft.	12%	156 sq. ft.
2nd Floor and Above facing the south parking lot	10% minimum	166 sq. ft.	At least 10 percent	

Access and Circulation – Meets requirements

- A five-foot walkway will be constructed from East 32nd Street to East 31st Street. This walkway will connect the building entrance to the public sidewalks. The walkway crosses a driveway on the west side of the property. CPED is recommending that the walkway have a consistent surface through the driveway. The applicant is also proposing to construct an off-street bike trail that will run along the east side of the former 24th Avenue South street alignment and then parallel with the Green Line LRT tracks.
- There is no transit shelter proposed as part of this development.
- There will be a total of 151 parking spaces provided on-site. One hundred of the parking spaces will be provided in one level of below-ground parking and the remaining 51 parking spaces will be provided in two surface parking areas. One surface parking area will be located on the east side of the site and the other will be located on the south side of the site. There will be two access points into the site; one will be located along East 32nd Street and the other will be located along East 31st Street.
- Although the property is adjacent to a public alley there will not be vehicular access to it from the site.
- The maximum impervious surface requirement in the R5 District is 85 percent. According to the materials submitted by the applicant 32 percent of the site will be impervious.

Landscaping and Screening – Requires alternative compliance

- The zoning code requires that at least 20 percent of the site not occupied by the building be landscaped. The lot area of the site is 112,931 square feet. The footprint of the building is 35,679 square feet. When you subtract the footprint from the lot size the resulting number is 77,252 square feet. Twenty percent of this number is 15,450 square feet. According to the applicant’s landscaping plan there will be 38,825 square feet of landscaping on the site or approximately 50 percent of the site.
- The zoning code requires at least 1 canopy tree for each 500 square feet of required green space and at least 1 shrub for each 100 square feet of required green space be planted on the site. The

tree and shrub requirement for this site is 31 and 155 respectively. The applicant is providing a total of 33 canopy trees and 289 shrubs on the site. In addition, the applicant is proposing to provide a total of nine ornamental trees and 1,139 perennials on the site.

- A seven-foot wide landscaped yard is required when a parking or loading facility is fronting along a public street, public sidewalk or public pathway. A landscaped yard is required along East 31st Street. The way that the parking lot has been designed does not provide for a seven-foot wide landscaped yard on either side of the drive aisle. CPED is recommending that the parking lot be designed so there is a minimum of seven feet of landscaped yard on each side of the drive aisle.
- Screening that is three feet in height and not less than 60 percent opaque is required when a parking or loading facility is fronting along a public street, public sidewalk or public pathway. As shown, the applicant is proposing to plant Little Bluestem Blue Heaven grasses in the landscaped yard along East 31st Street. This plant material will grow to a height of at least four feet. CPED is recommending that the final landscaping plan provide screening that is three feet in height and not less than 60 percent opaque on each side of the drive aisle.
- A seven-foot wide landscaped yard is required when a parking or loading facility is abutting or across an alley from a residence or office residence district, or any permitted or conditional residential use. A landscaped yard is required along the south property line. There is a seven-foot wide landscaped yard along the south property line.
- Screening that is six feet in height and not less than 95 percent opaque is required when a parking or loading facility is abutting or across an alley from a residence or office residence district, or any permitted or conditional residential use. The applicant is proposing to plant Little Bluestem Blue Heaven grasses in the landscaped yard along East 31st Street. This plant material will grow to a height of at least four feet.
- Not less than one tree shall be provided for every 25 linear feet of parking lot frontage. The parking lot has 36 feet of frontage along East 31st Street. Two trees are required. There will be one tree planted between the parking lot and the property line along East 31st Street. CPED is recommending that an additional tree be planted between the parking lot and the property line along East 31st Street.
- In parking lots of 10 spaces or more, no parking space shall be located more than 50 feet from an on-site deciduous tree. Some of the parking spaces on the south side of the property are not located within 50 feet of an on-site deciduous tree. Alternative compliance is needed.
- Tree islands in parking lots must have a minimum width of seven feet in any direction. There are no tree islands in the parking lot.

Table 2. Landscaping and Screening Requirements

	Code Requirement	Proposed
Lot Area	--	112,931 sq. ft.
Building footprint	--	35,679 sq. ft.
Remaining Lot Area	--	77,252 sq. ft.
Landscaping Required	15,450 sq. ft.	38,825 sq. ft.
Canopy Trees (1: 500 sq. ft.)	31 trees	33 trees
Shrubs (1: 100 sq. ft.)	155 shrubs	289 shrubs

Additional Standards – Meets requirements

- The parking areas will be defined with six-inch by six-inch concrete curbing. The applicant is incorporating on-site retention and filtration. Stormwater runoff will be directed to an

underground containment basin located beneath the landscape area in the southeast corner of the property.

- The proposed building will not block views of any landmark buildings, significant open spaces or water bodies.
- The proposed building will most likely cast shadows on the residential properties to the west; however, the shadowing impacts should not be significant. The building will not cast shadows on adjacent public spaces.
- The building has been designed with recesses and projections along the building walls, a stepped back fourth floor and canopies over the ground level entrances which should help minimize wind effects on the surrounding area.
- The site plan complies with crime prevention design elements as walkways direct people to and from the building entrances, there will be large windows on all sides of the building that maximize the opportunities for people to observe adjacent spaces and there will be lights located near all of the building entrances.
- The site is neither historically designated nor located in a historic district.

2. Conformance with all applicable regulations of the zoning ordinance.

The proposed use is *permitted* in the R5 District.

Off-street Parking and Loading – Meets requirements

- As of the writing of this staff report, the minimum parking requirement for the development is 122 spaces, or .9 spaces per dwelling unit. The off-street parking requirement for this project will be 68 once the recently adopted changes to Chapter 54I, Off-Street Parking and Loading, are in effect on August 1, 2015.

Table 3. Vehicle Parking Requirements Per Use (Chapter 54I)

	Minimum Vehicle Parking	Applicable Reductions	Total Minimum Requirement	Maximum Parking Allowed	Proposed
Residential dwellings	135	Transit Incentives (13)	122	0, except when the spaces are not provided within an enclosed structure. In this case there shall be no more than 2 spaces provided per dwelling unit.	100 underground 51 surface parking spaces
Total	135	(13)	122	270 outdoor spaces	151

Table 4. Bicycle Parking and Loading Requirements (Chapter 54I)

	Minimum Bicycle Parking	Minimum Short-Term	Minimum Long-Term	Proposed	Loading Requirement	Proposed
Residential dwellings	68	--	Not less than 90%	68 Long-term 24 Short-term	1 Small	1 Small
Total	68	--	61	92	1 Small	1 Small

Building Bulk and Height – Meets requirements

Table 5. Building Bulk and Height Requirements

	Code Requirement	Proposed
Lot Area	--	112,931 sq. ft. / 2.59 acres
Gross Floor Area (GFA)	--	140,886 sq. ft.
Minimum Floor Area Ratio (GFA/Lot Area)	Not applicable	Not applicable
Maximum Floor Area Ratio (GFA/Lot Area)	2.0	1.25
Maximum Building Height	4 stories or 56 ft., whichever is less	4 stories or 46 ft.

Lot Requirements – Meets requirements

Table I. Lot Requirements Summary

	Code Requirement	Proposed
Dwelling Units (DU)	--	135 DUs
Density (DU/acre)	--	52 DU/acre
Minimum Lot Area	5,000 sq. ft.	112,931 sq. ft.
Maximum Impervious Surface Area	85%	32%
Maximum Lot Coverage	70%	66%
Minimum Lot Width	40 ft.	128 ft.

Yard Requirements – Requires variance(s)

- The applicant has applied for a variance of the maximum building setback from East 32nd Street in the PO Pedestrian Oriented Overlay District from 20 feet to approximately 95 feet.

Table 2. Minimum Yard Requirements

	Zoning District	Overriding Regulations	Total Requirement	Proposed
Front (South)	15 ft.	8 ft. in the TSA Transit Station Area PO Pedestrian Oriented Overlay District, except where a greater yard is required by the zoning ordinance. The homes located at 3146 24 th Avenue South and 2318 East 32 nd Street have a front yard along East 32 nd Street so the required front yard along this street is 20 ft.	20 ft.	95 ft.
Interior Side (East)	11 ft.	--	11 ft.	60 ft.
Interior Side (South)	11 ft.	--	11 ft.	16 ft.
Corner Side (North)	14 ft.	--	14 ft.	15 ft.
Rear (west)	11 ft.	--	11 ft.	11 ft.

Signs – Meets requirements

Table 3. Signage Summary

	Number Allowed/ Lot	Proposed Number	Maximum Area Per Sign	Proposed Area	Maximum Allowed Height	Proposed Height
Monument	1	0	32 sq. ft.	0 sq. ft.	8 ft.	0 ft.
Attached	2	1	48 sq. ft.	32 sq. ft.	14 ft.	12 ft.

Refuse Screening – Meets requirements

- Trash and recycling containers will be kept inside the building except on pick-up day.

Screening of Mechanical Equipment – Meets requirements with Conditions of Approval

- There will be an electrical transformer located in the southwest corner of the site. It is proposed to be screened with landscaping. The plans that were submitted do not show any additional mechanical equipment on the ground or on the roof. CPED is recommending that the transformer and any other proposed mechanical equipment be screened per the requirements of Chapter 535, Regulations of General Applicability.

Lighting – Meets requirements with Conditions of Approval

- A lighting plan showing footcandles was not submitted as part of the application materials. CPED is recommending that the final lighting plan conform to the standards of Chapter 535, Regulations of General Applicability.

Specific Development Standards – Not applicable

PO Overlay District Standards – Requires variance(s)

- The applicant has applied for a variance of the maximum building setback from East 32nd Street in the PO Pedestrian Oriented Overlay District from 20 feet to approximately 95 feet.

3. Conformance with the applicable policies of *The Minneapolis Plan for Sustainable Growth*.

The Minneapolis Plan for Sustainable Growth does not identify a land use for this site. However, it is surrounded by Mixed-Use to the north, Industrial to the east and Urban Neighborhood to the south and west. The Lake Street LRT Station Activity Center stops at East 31st Street. Given the surroundings, staff would assume that this site should be designated as Urban Neighborhood. The site is also located in the Lake Street/Midtown Transit Station Area. The proposed development is consistent with the following principles and policies outlined in the comprehensive plan:

Urban Design Policy 10.4: Support the development of residential dwellings that are of high quality design and compatible with surrounding development.

- 10.4.1 Maintain and strengthen the architectural character of the city's various residential neighborhoods.
- 10.4.2 Promote the development of new housing that is compatible with existing development in the area and the best of the city's existing housing stock.

Urban Design Policy 10.5: Support the development of multi-family residential dwellings of appropriate form and scale.

Urban Design Policy 10.6: New multi-family development or renovation should be designed in terms of traditional urban building form with pedestrian scale design features at the street level.

- 10.6.1 Design buildings to fulfill light, privacy, and view requirements for the subject building as well as for adjacent properties by building within required setbacks.
- 10.6.2 Promote the preservation and enhancement of view corridors that focus attention on natural or built features, such as the Downtown skyline, landmark buildings, significant open spaces or bodies of water.
- 10.6.3 Provide appropriate physical transition and separation using green space, setbacks or orientation, stepped down height, or ornamental fencing to improve the compatibility between higher density and lower density residential uses.
- 10.6.4 Orient buildings and building entrances to the street with pedestrian amenities like wider sidewalks and green spaces.
- 10.6.5 Street-level building walls should include an adequate distribution of windows and architectural features in order to create visual interest at the pedestrian level.
- 10.6.6 Integrate transit facilities and bicycle parking amenities into the site design.

Urban Design Policy 10.18: Reduce the visual impact of automobile parking facilities.

- 10.18.1 Require that parking lots meet or exceed the landscaping and screening requirements of the zoning code, especially along transit corridors, adjacent to residential areas, and areas of transition between land uses.
- 10.18.2 Parking lots should maintain the existing street face in developed areas and establish them in undeveloped areas through the use of fencing, walls, landscaping or a combination thereof along property lines.

10.18.3 Locate parking lots to the rear or interior of the site.

10.18.4 Provide walkways within parking lots in order to guide pedestrians through the site.

4. Conformance with applicable development plans or objectives adopted by the City Council.

The site is located within the boundaries of two small area plans; the *Hiawatha/Lake Station Area Master Plan*, which was adopted by the City Council in 2001, and the *Corcoran Midtown Revival Plan*, which was adopted by the City Council in 2002. In both of these plans, townhome development is called out as the future land use on this site. While the development is a multiple-family building the building has been designed to look like walk-up townhome units from the street.

5. Alternative compliance.

The Planning Commission or zoning administrator may approve alternatives to any site plan review requirement upon finding that the project meets one of three criteria required for alternative compliance. Alternative compliance is requested for the following requirements:

- **Reinforce the Street Wall.** The building will not reinforce the street wall along East 32nd Street as it will be set back 95 feet from the front property line. Given the location of the existing underground utilities, the building cannot be built over the current street alignment which requires the building to be set back from the front property line. CPED is recommending that the City Planning Commission grant alternative compliance.
- **Setback from Front Property Line.** The building will be set back 95 feet from the front property line along East 31st Street. Given the location of the existing underground utilities, the building cannot be built over the current street alignment which requires the building to be set back more than eight feet from the front property line. CPED is recommending that the City Planning Commission grant alternative compliance.
- **Entrance Facing the Street.** The principal entrance to the building faces the east interior property line. Given the location of the existing underground utilities, the building cannot be built over the current street alignment which requires the building to be set back from the front property line. CPED is recommending that the City Planning Commission grant alternative compliance.
- **Window Requirements.** The minimum window requirement on the ground floor facing the south parking area is 20 percent. There will be 12 percent windows in this location. The access to the underground parking is located on this side of the building. Given this, the percentage of windows is reduced because not all of the windows are located between two and ten feet. CPED is recommending that the City Planning Commission grant alternative compliance.
- **Distance to Trees.** In parking lots of 10 spaces or more, no parking space shall be located more than 50 feet from an on-site deciduous tree. Some of the parking spaces on the south side of the property are not located within 50 feet of an on-site deciduous tree. The access to the underground parking is located on this side of the building. Given this, the parking area depresses as the grade of the site slopes down to enter the underground parking garage. Because this area will be shaded by the retaining wall, it would be impractical to plant additional trees so every parking space is located within 50 feet of an on-site deciduous tree. In addition, the applicant has proposed an exceptional landscaping plan that exceeds the minimum number of trees and shrubs. CPED is recommending that the City Planning Commission grant alternative compliance.

PRELIMINARY AND FINAL PLAT

The Department of Community Planning and Economic Development has analyzed the application for a preliminary and final plat based on the following findings:

1. *The subdivision is in conformance with these land subdivision regulations, the applicable regulations of the zoning ordinance and policies of the comprehensive plan.*

The applicant is proposing to replat several lots and right-of-way that is proposed to be vacated into one new lot. The subdivision is in conformance with the design requirements of the land subdivision regulations except for Section 598.240 (2b), which requires that lots for multiple family dwellings have no more than five sides. The proposed lot has 10 sides. In order to be in conformance with the land subdivision regulations, a variance of Section 598.240 (2b) is required to allow the lot to have 10 sides. While variances from the zoning code require a separate application, variances from the subdivision standards are done as a part of the subdivision application subject to the standards listed below.

598.310. Variances. Where the planning commission finds that hardships or practical difficulties may result from strict compliance with these regulations, or that the purposes of these regulations may be served to a greater extent by an alternative proposal, it may approve variances to any or all of the provisions of this chapter. In approving variances, the planning commission may require such conditions as it deems reasonable and necessary to secure substantially the objectives of the standards or requirements of these regulations. No variance shall be granted unless the planning commission makes the following findings:

- (1) There are special circumstances or conditions affecting the specific property such that the strict application of the provisions of this chapter would deprive the applicant of the reasonable use of land.
- (2) The granting of the variance will not be detrimental to the public welfare or injurious to other property in the area in which the property is located.

The two properties currently have more than five sides. Given the shape of the existing lots it would be difficult to design the lot with less than five sides. CPED recommends varying the number of sides to 10.

Zoning Ordinance:

The subdivision is in conformance with the design requirements of the zoning code.

Comprehensive Plan:

The subdivision is in conformance with the policies of the comprehensive plan.

2. *The subdivision will not be injurious to the use and enjoyment of other property in the immediate vicinity, nor be detrimental to present and potential surrounding land uses, nor add substantially to congestion in the public streets.*

The applicant is proposing to replat several lots and right-of-way that is proposed to be vacated into one new lot.

3. *All land intended for building sites can be used safely without endangering the residents or users of the subdivision or the surrounding area because of flooding, erosion, high water table, soil conditions, improper drainage, steep slopes, rock formations, utility easements or other hazard.*

The land intended for building can be used safely without endangering the residents or users of the subdivision or the surrounding area because of flooding, erosion, high water table, soil conditions, improper drainage, steep slopes, rock formations, utility easements or other hazard. The site is relatively flat.

4. *The lot arrangement is such that there will be no foreseeable difficulties, for reasons of topography or other conditions, in securing building permits and in providing driveway access to buildings on such lots from an*

approved street. Each lot created through subdivision is suitable in its natural state for the proposed use with minimal alteration.

The lot created by this application presents no foreseeable difficulties for this development. No significant alterations to the land appear necessary.

5. *The subdivision makes adequate provision for stormwater runoff, and temporary and permanent erosion control in accordance with the rules, regulations and standards of the city engineer and the requirements of these land subdivision regulations. To the extent practicable, the amount of stormwater runoff from the site after development will not exceed the amount occurring prior to development.*

The Surface Water and Sewers Division of the Public Works Department will review and approve the drainage and sanitary system plans for this development prior to building permits being issued.

VACATION

The applicant is proposing to vacate a portion of 24th Avenue South from East 32nd Street to the Metro Transit LRT tracks. As noted above, underneath the current alignment of 24th Avenue South is an existing water main, sanitary sewer line, storm sewer line and a gas main. None of these utilities can be moved without huge expense. Through discussions with Public Works it has been recommended that no public structures be constructed over the street alignment.

RESPONSES FROM UTILITIES AND AFFECTED PROPERTY OWNERS.

Of the utilities and/or affected property owners that have responded the City of Minneapolis and Xcel Energy have requested an easement over the area to be vacated.

The Department of Public Works and the Department of Community Planning and Economic Development find that the area proposed for vacation is not needed for any public purpose, and it is not part of a public transportation corridor, and that it can be vacated if any easements requested are granted by the petitioner.

RECOMMENDATIONS

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt staff findings for the application(s) by Wellington Management for the properties located at 3120 and 3147 24th Avenue South:

A. Variance of the maximum building setback.

Recommended motion: **Approve** the application for a variance of the maximum building setback from East 32nd Street in the PO Pedestrian Oriented Overlay District from 20 feet to approximately 95 feet.

B. Variance of the surface parking location.

Recommended motion: **Approve** the application for a variance to allow surface parking that is located in the required side yard and not located in the rear 40 feet or 20 percent of the lot, subject to the following conditions:

- I. The parking space located in the northeast corner of the site shall be eliminated and landscaping shall be provided in this location.

C. Variance of the drive aisle width.

Recommended motion: **Approve** the application for a variance to reduce the minimum width of a two-way drive aisle from 22 feet to 19 feet.

D. Site Plan Review for a new residential building.

Recommended motion: **Approve** the application for a new 4-story, 135-unit residential building, subject to the following conditions:

1. Approval of the final site plan, landscaping plan, elevations and lighting plan by the Department of Community Planning and Economic Development.
2. All site improvements shall be completed by August 21, 2017, unless extended by the Zoning Administrator, or the permit may be revoked for non-compliance.
3. The five-foot walkway that will connect East 32nd Street to East 31st Street shall have a consistent surface through the driveway.
4. The parking lot shall be designed so there is a minimum of seven feet of landscaped yard on each side of the drive aisle along East 31st Street as required by section 530.170 of the zoning code.
5. The final landscaping plan shall provide screening that is three feet in height and not less than 60 percent opaque on each side of the drive aisle along East 31st Street as required by section 530.170 of the zoning code.
6. An additional tree shall be planted between the parking lot and the property line along East 31st Street as required by section 530.170 of the zoning code.
7. The transformer and any other proposed mechanical equipment shall be screened per the requirements of Chapter 535, Regulations of General Applicability.
8. The final lighting plan shall conform to the standards of Chapter 535, Regulations of General Applicability.

E. Preliminary and Final Plat.

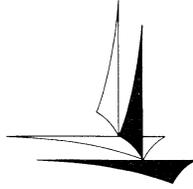
Recommended motion: **Approve** the preliminary and final plat.

F. Vacation of a portion of 24th Avenue South from East 32nd Street to the Metro Transit LRT tracks.

Recommended motion: **Approve** the vacation of a portion of 24th Avenue South from East 32nd Street to the Metro Transit LRT tracks subject to the retention of easements by the City of Minneapolis and Xcel Energy.

ATTACHMENTS

1. PDR report from June 2, 2015
2. Project description and responses to variance findings
3. Responses to subdivision findings and preliminary and final plat
4. Vacation information
5. Zoning map
6. Civil drawings
7. Landscape plans
8. Floor plans
9. Elevations
10. Materials breakdown
11. Renderings
12. Photos
13. Correspondence



Minneapolis Development Review
250 South 4th Street
Room 300
Minneapolis, MN 55415

Preliminary Development Review Report

Development Coordinator Assigned: **MATTHEW JAMES**
(612) 673-2547
matthew.james@minneapolismn.gov

Status *

Tracking Number:	PDR 1001314
Applicant:	WELLINGTON MANAGEMENT 1625 ENERGY PARK DR. SUITE #100 ST. PAUL, MN 55108
Site Address:	3120 24TH AVE S
Date Submitted:	26-MAY-2015
Date Reviewed:	03-JUNE-2015

Purpose

The purpose of the Preliminary Development Review (PDR) is to provide Customers with comments about their proposed development. City personnel, who specialize in various disciplines, review site plans to identify issues and provide feedback to the Customers to assist them in developing their final site plans.

The City of Minneapolis encourages the use of green building techniques. For additional information please check out our green building web page at: http://www.ci.minneapolis.mn.us/mdr/GreenBuildingOptions_home.asp.

DISCLAIMER: *The information in this review is based solely on the preliminary site plan submitted. The comments contained in this report are preliminary ONLY and are subject to modification.*

Project Scope

135 unit apartment complex of affordable housing with underground parking.

Review Findings (by Discipline)

Historical Preservation Committee

- There is no HPC flag on this property. HPC review is not required at this time.

Business Licensing

- There is no Minneapolis Business Licensing review required for the proposed project.

***Approved:** You may continue to the next phase of developing your project.

***Resubmission Required:** You cannot move forward or obtain permits until your plans have been resubmitted and approved.

□ Addressing

- Per City of Minneapolis Street Naming and Address Standard V1.22, the City of Minneapolis holds authority for assignment of all addresses, verification, change, and/or additions. Each assigned address number uses the street that provides the best/direct access for life safety equipment and best/direct access to the occupants.
- The address for the proposed Corcoran Triangle project will be 3120 24th Ave S. This address meets the City of Minneapolis Street Naming and Address Standard requirements.
- When assigning suite sequences the following guidelines are as follows:
 - The first one to two digits of the suite sequence number will designate the floor number of the site.
 - The last two digits of the suite sequence number will designate the unique ID for the unit (condo, suite, unit, or apartment).
 - Suite sequence digit numbers will be assigned to dwelling, commercial and retail units, not common areas. For example, laundry rooms, saunas, workout rooms, etc., would not be assigned numbers.
 - Please provide each condo, suite, unit or apartment number.

□ Parks - Forestry

- Contact Craig Pinkalla (612-499-9233 cpinkalla@minneapolisparks.org) regarding any questions related to planting, removal or the process for protection of trees during construction in the city right of way. Also, contact him regarding the question of exiting trees in the right of way.
- Effective January 1, 2014, the City of Minneapolis and the Minneapolis Park and Recreation Board adopted an update to the existing Parkland Dedication Ordinance. The adopted City of Minneapolis Parkland Dedication ordinance is located in Section 598.340 of the City's Land Subdivision ordinance:
 - <http://library.municode.com/index.aspx?clientId=11490>
- As adopted, the fee in lieu of dedication for new residential units is \$1,521 per unit (affordable units excluded per ordinance) and for commercial and industrial development it is \$202.80 per development employee (as defined in ordinance). Any dedication fee (if required) must be paid at the time of building permit issuance.
- There is also an administration fee that is 5% of the calculated park dedication fee.
- As proposed, there will be no Park Dedication Fee collected for the project as it is affordable housing.

□ Zoning - Planning

- Land Use Applications identified so far:
- Variance of the maximum setback in the PO Overlay from 20 feet to approximately 95 feet.
- Variance to allow surface parking that is located in the required side yard and not located in the rear 40 feet or 20 percent of the lot.
- Variance to reduce the minimum width of a two-way drive aisle from 22 feet to 20 feet.
- Variance to reduce the minimum width of parallel parking spaces from 8 feet 6 inches to 8 feet.
- Site plan review.
- Preliminary and final plat.
- Vacation of 24th Avenue South from East 32nd Street to the Metro Transit LRT tracks.

□ Right of Way

- An encroachment permit shall be required for all streetscape elements in the Public right-of-way such as: plants & shrubs, planters, tree grates and other landscaping elements, sidewalk furniture (including bike racks and bollards), and sidewalk elements other than standard concrete walkways such as pavers, stairs, raised landings, retaining walls, access ramps, and railings (NOTE: railings may not extend into the sidewalk pedestrian area). Please contact Bob Boblett at (612) 673-2428 for further information.
- Note to the Applicant: Any elements of an earth retention system and related operations (such as construction crane boom swings) that fall within the Public right-of-way will require an encroachment permit application. If there are to be any earth retention systems which will extend outside the property line of the development then a plan must be submitted showing details of the system. All such elements shall be removed from the Public right-of-way following construction with the exception of tie-backs which may remain but must be uncoupled and de-tensioned. Please contact Bob Boblett at (612) 673-2428 for further information.

- In addition, any elements of an earth retention system and related excavations that fall within the Public right-of-way will require a "Right-of-Way Excavation Permit". This permit is typically issued to the General Contractor just prior to the start of construction. However, it is the Applicant's responsibility to insure that all required permits have been acquired by its consultants, contractors, sub-contractor's prior to the start of work.
- The site plan indicates a vacation of existing Public right-of-way; the Applicant shall continue to work with Bob Boblett at (612) 673-2428 throughout the vacation process.
- Note that the site plan does not include a "demolition plan", consequently it is difficult to determine the exact extents of the Project or provide a complete review related to the areas of disturbance. It is recommended that a "demolition plan" be provided.
- The site plan indicates encroachment of the proposed bike path onto Metro Transit property. The Applicant shall provide documentation from Metro Transit that the proposed encroachments have been approved.
- The site plan indicates an "easement" and "improvements" on the adjacent property locate 3146 E. 32nd St.,; however no additional details are provided. The Applicant shall provide documentation that the proposed easement and improvements have been approved by the Owner of 3146 E. 32nd St.

❑ Street Design

- The plan as submitted meets the requirements of the Public Works Street Design Division.

❑ Sidewalk

- Any concrete infrastructure in the public right of way that is currently defective, or that is damaged during site development, must be removed and replaced.
- ADA compliant pedestrian ramps are required at each crosswalk at the entrance point at E. 32nd St. Construct two (2) ADA compliant pedestrian ramps at this location. Include the appropriate details and standard plates in the site plan, refer Mn/DOT Standard Plan 5-297.250 Pedestrian Curb Ramp Details at: <http://standardplans.dot.state.mn.us/stdplan.aspx>

❑ Traffic and Parking

- The nature of the proposed development is such that traffic impacts will be an issue; please contact Allan Klugman at (612) 673-2743 to discuss the requirements of a Travel Demand Management Plan (TDMP).
- Current ordinance states that all maneuvers associated with loading, parking or sanitation pick up for a private development shall occur on private property. Please provide a narrative explaining the trash removal operations and show turning maneuvers for all truck type vehicles that will be using the loading dock/parking entrance areas.
- Per City Ordinance the Applicant shall provide for (and identify) a solid waste collection point (SWCP) on the site plans. The location of the SWCP is subject to the review and approval of the Public Works Department.
- Please contact Bill Prince at (612) 673-3901 regarding existing and proposed street lighting. All street lighting (existing and proposed) shall be shown clearly on the site plan.
- Note to the Applicant: Please add the following notes to the site plan:
 - Street lighting installed as part of the Project shall be inspected by the City. Contractors shall arrange for inspections with the Traffic Department, please contact Dave Prehall at (612) 673-5759 for further information. Any lighting installations not meeting City specifications will be required to be reinstalled at Owner expense.
 - An obstruction permit is required anytime construction work is performed in the Public right-of-way. Please contact Scott Kramer at (612) 673-2383 regarding details of sidewalk and lane closures. Log on to <http://minneapolis.mn.roway.net/> for a permit.
 - Contact Allan Klugman at (612) 673-2743 prior to construction for the temporary removal/temporary relocation of any City of Minneapolis signal system that may be in the way of construction.
 - All costs for relocation and/or repair of City Traffic facilities shall be borne by the Contractor and/or Property Owner.

- Contact Doug Maday at (612) 673-5755 prior to construction for the removal of any City of Minneapolis right of way signs that may be in the way of construction.
- Remove the painted stripping from the plans from within the cul-de-sac at E. 31st St. as the City will determine how to mark bike infrastructure within the Public right-of-way.

□ Water

- The current site plan does not include a "demolition plan"; all existing and proposed underground Public Utilities (water, sanitary sewer, and storm drain) shall be shown on the site plan with corresponding pipe sizes and types. For Public watermain infrastructure records call (612) 673-2865. Any existing connections not in use shall be noted on the plans for removal, and shall be removed per the requirements of the Utility Connections Department, call (612) 673-2451 for more information.
- The water service connections should be designed to avoid bends and shall be no further than 2' outside the line of the building; it is recommended to run water service lines straight from the main into the proposed building to the meter location. Note to the Applicant: PVC is not an acceptable material for water services in Minneapolis.
- Please contact Rock Rogers at (612) 673-2286, to review domestic and fire service design, connections, and sizes.
- The Applicant shall explain the need for the proposed hydrant. Private hydrants, if required, need to be tapped and drawn after the general supply meter. If hydrants are for Public (City owned) use, the Developer will need to fund and coordinate with Water Treatment and Distribution Division for installments; please contact Marie Asgian at (612) 673-5682 for further information.

□ Sewer Design

- Stormwater Management: The project is subject to the stormwater treatment requirements of Chapter 54 of the Minneapolis Code of Ordinances. Please provide the necessary documentation and analysis demonstrating compliance with Chapter 54, including stormwater calculations, BMP maintenance plans, etc.
- An operations and maintenance plan is required for the stormwater treatment devices. The O&M plan shall define the maintenance regimen, including type and interval of maintenance and party to conduct such maintenance. Please provide a copy of the O&M Plan.
- Please provide details for the proposed stormwater infiltration system.
- Please provide a copy of any geotechnical reports for the site demonstrating infiltration is appropriate for the site.
- Contact Jeremy Strehlo concerning water treatment issues around the use of roof water drainage for lawn irrigation.
- Utility: Ownership of the existing City sanitary sewer main in the 24th Ave S right-of-way, north of 32nd St E, should be transferred to the property owner as the ROW is proposed to be vacated. It should be verified that the only property served by the sewer is the project property.
- Please provide a profile view of the proposed City storm sewer relocation.
- The storm sewer at the NW corner of 32nd St E and Hiawatha is a Mn/DOT storm sewer. Any removals, as proposed would require the approval of Mn/DOT. Please provide documentation that Mn/DOT has reviewed and approved the plan.
- Please note, the abandoned sanitary sewer crossing the property was filled with flowable fill and left in place.
- Utility Connections: If ownership of the City sanitary sewer main in 24th Ave S is transferred to the property owner, the sanitary sewer from the building to the City sanitary sewer manhole in the intersection of 24th Ave S and 32nd St E will be considered the private connection to the property. This will be the responsibility of the property owner for maintenance and repair.
- Assuming the sanitary sewer is private within the property, connecting directly to the existing sanitary manhole should be considered as opposed to cutting in a new wye on the pipe.
- The private storm sewer system should be contained within the property boundaries. The private storm sewer manhole and pipes proposed within the right-of-way of 31st St E should be relocated to within the property.

- Non Stormwater Discharges: Detail all mechanical and non-stormwater discharges. Non-stormwater discharges are not permitted unless approved by the City of Minneapolis. Non-stormwater discharges not declared and approved will not be permitted. If there currently are none and nothing is proposed declare this status on the plans.
- For comments or questions on Public Works Surface Water & Sewers Division related requirements please contact Jeremy Strehlo, (Professional Engineer) at (612) 673-3973, or jeremy.strehlo@minneapolismn.gov

❑ Construction Code Services

- Clarify/identify accessible parking and accessible route to accessible entrance.
- A Service Availability Charge (SAC) determination will need to be submitted to the Metropolitan Council for the proposed project. Please refer to this link for more information or Contact Karon Cappaert at 651-602-1118 or karon.cappaert@metc.state.mn.us .
http://www.ci.minneapolis.mn.us/mdr/docs/sac_availability_charge.pdf

❑ Fire Safety

- Provide required fire suppression system throughout building.
- Fire department connection must be on address side of building and within 150 feet of a fire hydrant.
- Provide fire alarm system throughout building.
- Provide and maintain fire apparatus access at all times.
- For questions regarding fire safety issues contact Mike Raeker at (612) 673-2624 or michael.raeker@minneapolismn.gov.

❑ Environmental Health

- This project is in the boundaries of the US EPA South Minneapolis Residential Soil Contamination Superfund Site, <http://www.epa.gov/region5/cleanup/cmcheartland/index.htm> . The project developer needs to contact Greg Hanson, Project Manager (651-201-6681, Greg.Hanson@state.mn.us) from the Minnesota Department of Agriculture for site review of arsenic impacts prior to beginning work on site. If impacted soil is encountered the developer should enter the MDA Voluntary Investigation and Clean-up and request a letter of no association. The developer will address contamination encountered during construction as part of this development.
- If dewatering is required during site construction see below for city permit requirements. The scope of work calls for underground parking. The highest groundwater level expected for this site should be determined and used in establishing the lowest level for underground parking. The underground parking and other subgrade structures should be designed to prevent infiltration of groundwater without the need for a permanent dewatering system being installed. If a continuously operating permanent dewatering system is needed it must be approved as part of the sanitary sewer and storm drain site plan approval prior to construction beginning.
- No construction, demolition or commercial power maintenance equipment shall be operated within the city between the hours of 6:00 p.m. and 7:00 a.m. on weekdays or during any hours on Saturdays, Sundays and state and federal holidays, except under permit. Contact Environmental Services at (612) 673-3867 for permit information.
- Permits and approval are required from Environmental Services for the following activities: Temporary storage of impacted soils on site prior to disposal or reuse; Reuse of impacted soils on site; Dewatering and discharge of accumulated storm water or ground water, underground or aboveground tank installation or removal, well construction or sealing. Contact Tom Frame at (612) 673-5807 for permit applications and approvals.
- A review of the project, permits issued and an inspection from Environmental Service for identification of equipment and site operations that require annual registration with the City of Minneapolis will occur for this project.

END OF REPORT



June 2, 2015

RE: Land Use Application:

Corcoran Triangle Apartments Project Description:

The Corcoran Triangle project is a four story affordable apartment building with one story of underground parking. There are 135 units in a mix of one, two and three bedroom units. All the units are either tax credit units with some additional 'homeless' units incorporated throughout and incorporate private funds as well as many sources of public funding. Project amenities include a fourth floor community room with exterior deck, a first story community room that is available to the public with an exterior patio and a first floor office. There is a fenced in courtyard on the west side of the building which includes a playground and a large outdoor garden/seating area on the east side of the building. A bike trail runs along the east side of the site connecting the neighborhood to the south to the commercial and transportation opportunities to the north. Exterior bike racks will be provided for 24 public bikes, and a large enclosed area inside the garage will provide storage for private bikes.

Site and Building Design:

The site is a triangular shaped site directly adjacent to the light rail line. The site is within 200' of the light rail station and one block from an important intersection of Lake Street with a multitude of goods and services accessible by foot. The site was designed to re-connect the urban street grid by connecting 24th Avenue to 31st Street. This road connection was clearly identified in the neighborhood small area plan, and drove the design decisions. This connection is designed to look and act like a typical Minneapolis Street with two-way traffic and parking on each side. Although a private road, this will remain open and accessible to the neighborhood pedestrian, bikes and cars. The project incorporates a bike trail, as well as direct sidewalk connections. Because of the new road connection there is a small portion of the site on the southeast corner that is split off from the rest of the project. This area is designated as a useable open space for residents and the neighborhood.

The building has been designed to front this road as the main street. This orientation also parallels Hiawatha Avenue. To enhance this street there are a series of front doors, and individual stoops with direct sidewalk access to this street. The building entrance faces the new street on the east, just north of the community room/outdoor patio on the southeast corner of the building and framed by two three story bays sitting on a brick plinth and lighted entrance canopy. The location of the front door provides clear visibility to the new road, to the entrance on 32nd Street. The design incorporates brick, corrugated metal panel and cement board siding. The east elevation faces a new north/south street and sidewalk which parallels the light rail train tracks and includes a series of nine bays with first floor walk-up units and porches, creating a traditional residential street.

Trash: The trash is handled with an internal trash chute that is collected in the garage in a dumpster. The dumpster is located in an enclosed room. On trash collection days (estimated at twice per week).

Snow: The open space on the southeast is available for snow storage, and will be utilized as such.

Loading: There is a designated loading stall on the new street for deliveries, and move-in / move-out.

Unit Mix: The building is a mixture of one, two and three bedroom units. The units range in size from 631 gross square feet to 1346 gross square feet. There are a total of:

60 one bedroom units/one bath

54 two bedroom units/two baths

21 three bedroom units/two baths

Height and area: The building is four stories and forty-six (46') in height. The fourth floor is stepped back along the south and east with the bays being three stories in height. Building area is as follows:

First floor	35,679 gsf
Second floor	35,649 gsf
Third floor	35,649 gsf
Fourth floor	33,909 gsf
Total above grade	140,886 gsf
Garage (below grade)	35,814 gsf

Parking: Parking is located in two locations - below grade in a garage and exterior surface parking

	standard	compact	handicap	total
surface	49	0	2	51
garage	96	2	2	100
TOTAL	145	2	4	151
Bike Stalls				92

Variances:

The existing site is triangular in shape and because of the shape difficulties exist with fitting the building into the location. The alignment of the roadway defined much of the site. The zoning ordinances look at 32nd Street as the front, however, in look and in function the new road is the front and was designed as such.

Maximum setback variance:

The project did not have the opportunity to locate the building close to 32nd Street which is technically the front for zoning purposes. However, in look and in function, the new road acts as the front. The building was sited and designed to respond the new road as the front. The road was incorporated because it reinforces the existing urban grid, and enhances pedestrian connectivity. Additionally, this road is clearly identified in the small area plan as a key design element. The road needed to align with

the public road south. Based on this alignment there was no other practical site option on 32nd that was under project control, and thus the front door is beyond the maximum setback. This is a condition of the triangular site, and the road connection. The building has been located as reasonably close this corner as possible while respecting the single family home to the south. The front door has been located very close to the Southeast corner, and is clearly identified with the building massing and design.

Side-yard setback variance:

Parking has been located in side-yard on the south. This area is also the access to the underground parking. Because of this access the parking area is lower than the street from 2-9', and is hardly visible from 32nd Street. Parking has been added in this location as it is directly adjacent to an existing drive aisle, and will minimize drive area. If the building is pushed further to the north it greatly inhibits the internal courtyard on the west side. This project is intended for family use with many two and three bedrooms and needs a safe play area. The internal courtyard provides this area, and any reduction of its size is problematic to the residential use. Although there are good transit connections here, the owner and management company believe this is the minimum number of acceptable parking stalls to meet the needs of the project and the residents in a building with many larger units. This site is isolated along the rail line, and by Lake Street to the north. The larger reservoir of on-street parking is not readily available in this location.

Street width variance:

The roadway alignment in the north south direction was directly related to the alignment of the road to the south of 32nd and defined the location and access of the road. The triangular nature of the site proved problematic with the shape of the building as it moves toward the north because of the diminishing width of the site. There was always the desire to provide a bike lane for a community bike connection to the light rail. The reduction in width is requested to provide additional landscape area, and setback from the front of the building. This is intended to be a pedestrian friendly environment and the reduction of width will aid in slowing traffic as well as providing additional green space, and area for pedestrians.

The variances requested on the project are necessary to increase the functionality of the building as intended, and are design considerations to most effectively solve a difficult triangular site.

Thank you for consideration.

Sincerely,

COLLAGE ARCHITECTS

A handwritten signature in black ink, appearing to read 'Pete Keely', written over the printed name below.

Pete Keely, A.I.A.



June 16, 2015

**RE: Subdivision application:
Corcoran Triangle Apartments Project Description:**

The Corcoran Triangle project is a four story affordable apartment building with one story of underground parking. There are 135 units in a mix of one, two and three bedroom units. All the units are either tax credit units with some additional 'homeless' units incorporated throughout and incorporate private funds as well as many sources of public funding. Project amenities include a fourth floor community room with exterior deck, a first story community room that is available to the public with an exterior patio and a first floor office. There is a fenced in courtyard on the west side of the building which includes a playground and a large outdoor garden/seating area on the east side of the building. A bike trail runs along the east side of the site connecting the neighborhood to the south to the commercial and transportation opportunities to the north. Exterior bike racks will be provided for 24 public bikes, and a large enclosed area inside the garage will provide storage for private bikes.

Findings:

The site and the subdivision have been designed with particular attention to the location of streets, sidewalks, and bike trails to promote public welfare and use-ability of the site. Surface drainage has also been considered in the overall layout and design. A portion of the site on the southeast corner has been left for open space as to help with drainage, but also to provide better visibility, access and overall public safety at a congested intersection.

1. The subdivision conforms to the subdivision regulations, and to the zoning ordinance except as outlined in the variances below.
2. The subdivision is not injurious to others in the immediate vicinity. The subdivision will provide additional street, pedestrian and bike connections making helping the surrounding uses and easing congestion. The additional use on the site does not substantially add congestion.
3. The subdivision does not harm other building sites as all can be used safely, and accessed safely.
4. The subdivision does not create any foreseeable difficulties in access, and does provide access to approved streets.
5. The subdivision has and/ or will provide adequate provisions for storm water management, erosion controls and will be in accordance with the requirements of the city's engineering department, and the watershed district.

Site and Building Design:

The site is a triangular shaped site directly adjacent to the light rail line. The site is within 200' of the light rail station and one block from an important intersection of Lake Street with a multitude of goods

and services accessible by foot. The site was designed to re-connect the urban street grid by connecting 24th Avenue to 31st Street. This road connection was clearly identified in the neighborhood small area plan, and drove the design decisions. This connection is designed to look and act like a typical Minneapolis Street with two-way traffic and parking on each side. Although a private road, this will remain open and accessible to the neighborhood pedestrian, bikes and cars. The project incorporates a bike trail, as well as direct sidewalk connections. Because of the new road connection there is a small portion of the site on the southeast corner that is split off from the rest of the project. This area is designated as a useable open space for residents and the neighborhood.

The building has been designed to front this road as the main street. This orientation also parallels Hiawatha Avenue. To enhance this street there are a series of front doors, and individual stoops with direct sidewalk access to this street. The building entrance faces the new street on the east, just north of the community room/outdoor patio on the southeast corner of the building and framed by two three story bays sitting on a brick plinth and lighted entrance canopy. The location of the front door provides clear visibility to the new road, to the entrance on 32nd Street. The design incorporates brick, corrugated metal panel and cement board siding. The east elevation faces a new north/south street and sidewalk which parallels the light rail train tracks and includes a series of nine bays with first floor walk-up units and porches, creating a traditional residential street.

Trash: The trash is handled with an internal trash chute that is collected in the garage in a dumpster. The dumpster is located in an enclosed room. On trash collection days (estimated at twice per week).

Snow: The open space on the southeast is available for snow storage, and will be utilized as such.

Loading: There is a designated loading stall on the new street for deliveries, and move-in / move-out.

Unit Mix: The building is a mixture of one, two and three bedroom units. The units range in size from 631 gross square feet to 1346 gross square feet. There are a total of:

60 one bedroom units/one bath

54 two bedroom units/two baths

21 three bedroom units/two baths

Height and area: The building is four stories and forty-six (46') in height. The fourth floor is stepped back along the south and east with the bays being three stories in height. Building area is as follows:

First floor	35,679 gsf
Second floor	35,649 gsf
Third floor	35,649 gsf
Fourth floor	33,909 gsf
Total above grade	140,886 gsf
Garage (below grade)	35,814 gsf

Parking: Parking is located in two locations - below grade in a garage and exterior surface parking

	standard	compact	handicap	total

surface	49	0	2	51
garage	96	2	2	100
TOTAL	145	2	4	151
Bike Stalls				92

Variances:

The existing site is triangular in shape and because of the shape difficulties exist with fitting the building into the location. The alignment of the roadway defined much of the site. The zoning ordinances look at 32nd Street as the front, however, in look and in function the new road is the front and was designed as such.

1. The unique nature of the triangular site, and the location of the access points would make the site difficult to develop in accordance with the neighborhood plan if strict application of the zoning code were followed.
2. The variance is not detrimental to public welfare or other properties, and actually helps the circulation of the site and the neighborhood and would be beneficial.
3. The variances are within the intent of ordinances as it follows the intent of the area plan.

Front door setback variance:

The project did not have the opportunity to locate the building close to 32nd Street which is technically the front for zoning purposes. However, in look and in function, the new road acts as the front. The building was sited and designed to respond the new road as the front. The road was incorporated because it reinforces the existing urban grid, and enhances pedestrian connectivity. Additionally, this road is clearly identified in the small area plan as a key design element. The road needed to align with the public road south. Based on this alignment there was no other practical site option on 32nd that was under project control, and thus the front door is beyond the maximum setback. This is a condition of the triangular site, and the road connection. The front door has been located very close to the Southeast corner, and is clearly identified with the building massing and design.

Side-yard setback variance:

Parking has been located in side-yard on the south. This area is also the access to the underground parking. Because of this access the parking area is lower than the street from 2-9', and is hardly visible from 32nd Street. Parking has been added in this location as it is directly adjacent to an existing drive aisle, and will minimize drive area. If the building is pushed further to the north it greatly inhibits the internal courtyard on the west side. This project is intended for family use with many two and three bedrooms and needs a safe play area. The internal courtyard provides this area, and any reduction of its size is problematic to the residential use. Although there are good transit connections here, the owner and management company believe this is the minimum number of acceptable parking stalls to meet the needs of the project and the residents in a building with many larger units. This site is isolated

along the rail line, and by Lake Street to the north. The larger reservoir of on-street parking is not readily available in this location.

Street width variance:

The roadway alignment in the north south direction was directly related to the alignment of the road to the south of 32nd and defined the location and access of the road. The triangular nature of the site proved problematic with the shape of the building as it moves toward the north because of the diminishing width of the site. There was always the desire to provide a bike lane for a community bike connection to the light rail. The reduction in width is requested to provide additional landscape area, and setback from the front of the building. This is intended to be a pedestrian friendly environment and the reduction of width will aid in slowing traffic as well as providing additional green space, and area for pedestrians.

The variances requested on the project are necessary to increase the functionality of the building as intended, and are design considerations to most effectively solve a difficult triangular site.

Thank you for consideration.

Sincerely,

COLLAGE ARCHITECTS

A handwritten signature in black ink, appearing to read 'Pete Keely', written in a cursive, stylized script.

Pete Keely, A.I.A.

VACATION OF E. 24th AVE. SOUTH
Corridor Plaza LLC

Description of right-of-way to be vacated:

The right of way that is proposed to be vacated is as follows:

a. Undeveloped ROW:

Corridor Plaza LLC is requesting the vacation of the unimproved area of E. 24th Avenue S. which lies adjacent to Lots 17, 18, 19, 20, 21, (a portion of) 22, Block 6, Griswold's 3rd Addition to Minneapolis lying northeasterly of Line 1, as described below:

Line 1:

Commencing at Right of Way Boundary Corner B303, as shown on Minnesota Department of Transportation Right of Way Plat No. 27-110, recorded with Hennepin County; thence Westerly along the most Southerly line of said plat, on an assumed azimuth of 269 degrees 25 minutes 19 seconds a distance of 221.07 feet to the point of beginning of said Line 1; thence on an azimuth of 152 degrees 22 minutes 10 seconds a distance of 1800.00 feet and there terminating.

Corridor Plaza LLC is further requesting the vacation of the unimproved area of E. 24th Avenue S. which lies adjacent to those parts of Lots 11, 12, 13 and 14 Griswold's Rearrangement of part of Block Five (5) Griswold's 3rd Addition to Minneapolis, lying Southwesterly of Line 1 (defined above).

Comments Regarding Vacation of the Undeveloped ROW:

- **Current Improvements.**
The above-described areas are unimproved, vacant land. No road improvements exist on this land and the reserved roadway dead-ends without outlet to any other roads, into the LRT system installed along the Hiawatha corridor.
- **Accruals.**
Those areas described as accruing to Lots 13 and 14 Griswold's Rearrangement of part of Block Five (5) Griswold's 3rd Addition to Minneapolis, lying Southwesterly of Line 1 and a portion of Lot 12 will accrue to Metropolitan Council, the area created from such accrual is approximately 1,567 sf. We have discussed the requested vacation with Met Council staff and they are supportive of the vacation and proposed developed of this undeveloped parcel of land for transit-oriented affordable housing that would be supported by the vacation.
- **Reason for Vacation and Future Use.**
The vacation would allow for the construction and development of affordable multi-family housing and installation of a private road to access planned improvements including underground parking on the western side of the property, as well as provide for street-level access to ground -level eastern facing units in the development and would further create a neighborhood corridor linking the walkways and roadways of 31st Street and 32nd Street through the development to the services available at Lake and Hiawatha including the LRT Hiawatha transit station, Hi-Lake Shopping Center, the proposed L&H Station and other retail and services along the Lake and Hiawatha corridor. The linkage of 31st Street to 32nd Street will allow the city to eliminate one dead end street as well as set the ground work for the future elimination of the cul-de-sac at 31st St. East. The applicant's site plan will include street and side walk connections through the development to the public right of way at 31st Street East and 32nd Street East.

b. Cul de Sac:

Corridor Plaza LLC is requesting the vacation of the cul-de-sac installed and identified as E. 24th Avenue S. which lies adjacent to a portion of Lots 22, 23, 24 and 25 Block 6, Griswold's 3rd Addition to Minneapolis lying northeasterly of Line 1.

Corridor Plaza LLC is further requesting the vacation of the cul-de-sac installed and identified as E. 24th Avenue S. which lies adjacent to Lots 8, 9, 10 and 11 Griswold's Rearrangement of part of Block Five (5) Griswold's 3rd Addition to Minneapolis, lying Southwesterly of Line 1.

Comments Regarding Vacation of the Undeveloped ROW:

- **Current Improvements.**

The above described area has installed on it a cul-de-sac which provides dead-end access to unimproved, vacant land. The vacation would allow for the installation of a private road to access planned multi-family improvements including underground parking on the western side of the property, as well as provide for street-level access to the individual units in the planned development creating a neighborhood corridor through the development to the services available at Lake and Hiawatha including the LRT Hiawatha transit station, Hi-Lake Shopping Center, the proposed L&H Station and other retail and services along the Lake and Hiawatha corridor.

- **Accruals.**

The land that would accrue to Corridor Plaza LLC by virtue of the vacation would put approximately 19,500 square feet onto City tax rolls that is currently undeveloped or under-utilized roadway providing access to vacant undeveloped land.

- **Reason for Vacation and Future Use.**

The vacation would allow for the construction and development of affordable multi-family housing and installation of a private road to access planned improvements including underground parking on the western side of the property, as well as provide for street-level access to ground -level eastern facing units in the development and would further create a neighborhood corridor linking the walkways and roadways of 31st Street and 32nd Street through the development to the services available at Lake and Hiawatha including the LRT Hiawatha transit station, Hi-Lake Shopping Center, the proposed L&H Station and other retail and services along the Lake and Hiawatha corridor. The linkage of 31st Street to 32nd Street will allow the city to eliminate one dead end street as well as set the ground work for the future elimination of the cul-de-sac at 31st St. East. The applicant's site plan will include street and side walk connections through the development to the public right of way at 31st Street East and 32nd Street East.

LEGAL DESCRIPTION OF ADJACENT PROPERTY OWNERS
(#5 on application)

CORRIDOR PLAZA LLC (3210 & 3147 – 24TH Avenue South)

PARCEL 1:

Those part of Lots 8, 9, 10, 11 and 12, Griswold's Rearrangement of part of Block Five (5) Griswold's 3rd Addition to Minneapolis, and Lot 8, Block 5, Griswold's 3rd Addition to Minneapolis, according to the plats thereof on file and of record in the office of the County Recorder in and for Hennepin County, Minnesota; lying Southwesterly of Line 1, as described below;

Line 1:

Commencing at Right of Way Boundary Corner B303, as shown on Minnesota Department of Transportation Right of Way Plat No. 27-110, recorded with Hennepin County; thence Westerly along the most Southerly line of said plat, on an assumed azimuth of 269 degrees 25 minutes 19 seconds a distance of 245.86 feet, thence on an azimuth of 152 degrees 44 minutes 23 seconds a distance of 1505.67 feet and there terminating.

PARCEL 2:

Lots 18, 19, 20, 21, 22 and 23, Block 6, Griswold's 3rd Addition to Minneapolis, according to the plat thereof on file and of record in the office of the County Recorder in and for Hennepin County, Minnesota;

Together with:

The North 38 feet of the Easterly 122 feet of Lot 24, Block 6, said addition, said plat;

Together with:

That part of Lots 15, 16 and 17, Block 6 said addition, said plat, lying Southwesterly of Line 1, as described below;

Line 1:

Commencing at Right of Way Boundary Corner B303, as shown on Minnesota Department of Transportation Right of Way Plat No. 27-110, recorded with Hennepin County; thence Westerly along the most Southerly line of said plat, on an assumed azimuth of 269 degrees 25 minutes 19 seconds a distance of 245.86 feet, thence on an azimuth of 152 degrees 44 minutes 23 seconds a distance of 1505.67 feet and there terminating.

The following being Torrens property:

Lots 18 and 19, Block 6, Griswold's 3rd Addition to Minneapolis

Hennepin County, Minnesota
Abstract Property
Torrens Property
Torrens Certificate No. 1230869

SIMON SANCHEZ, SR. (3146 – 24TH Avenue South)

The south 12 feet of Lot 24, except the rear 50 feet thereof and Lot 25, except that part taken for Street and except the rear 50 feet thereof, Block 6, Griswold's 3rd Addition to Minneapolis.

Torrens Property

Torrens Certificate No. 859134

**Petition to Vacate
City Of Minneapolis Street, Alley or Utility Easement**

Petitioner: Corridor Plaza LLC
Address: 1625 Energy Park Dr., Ste. 100
St. Paul, MN 55108
Contact Person: Stephen B. Wellington, Jr. (651) 999-5526

Vacation File No.
1646
Page 1 of 2

Description of Easement to be vacated: 24th Ave. S., from 32nd St. E., north to Metro Transit light rail tracks – adjacent to Block 6, Griswold’s 3rd Addition to Minneapolis.

Review and Comment

Utility easements cannot be reserved unless there is an existing facility within the area to be vacated. If there is an existing utility, please enclose a map showing its location.

Name of Reviewing Agency _____

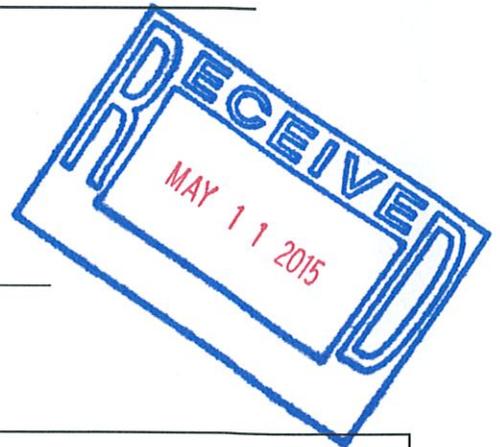
Phone: _____ e-Mail: _____

Approve petition as requested

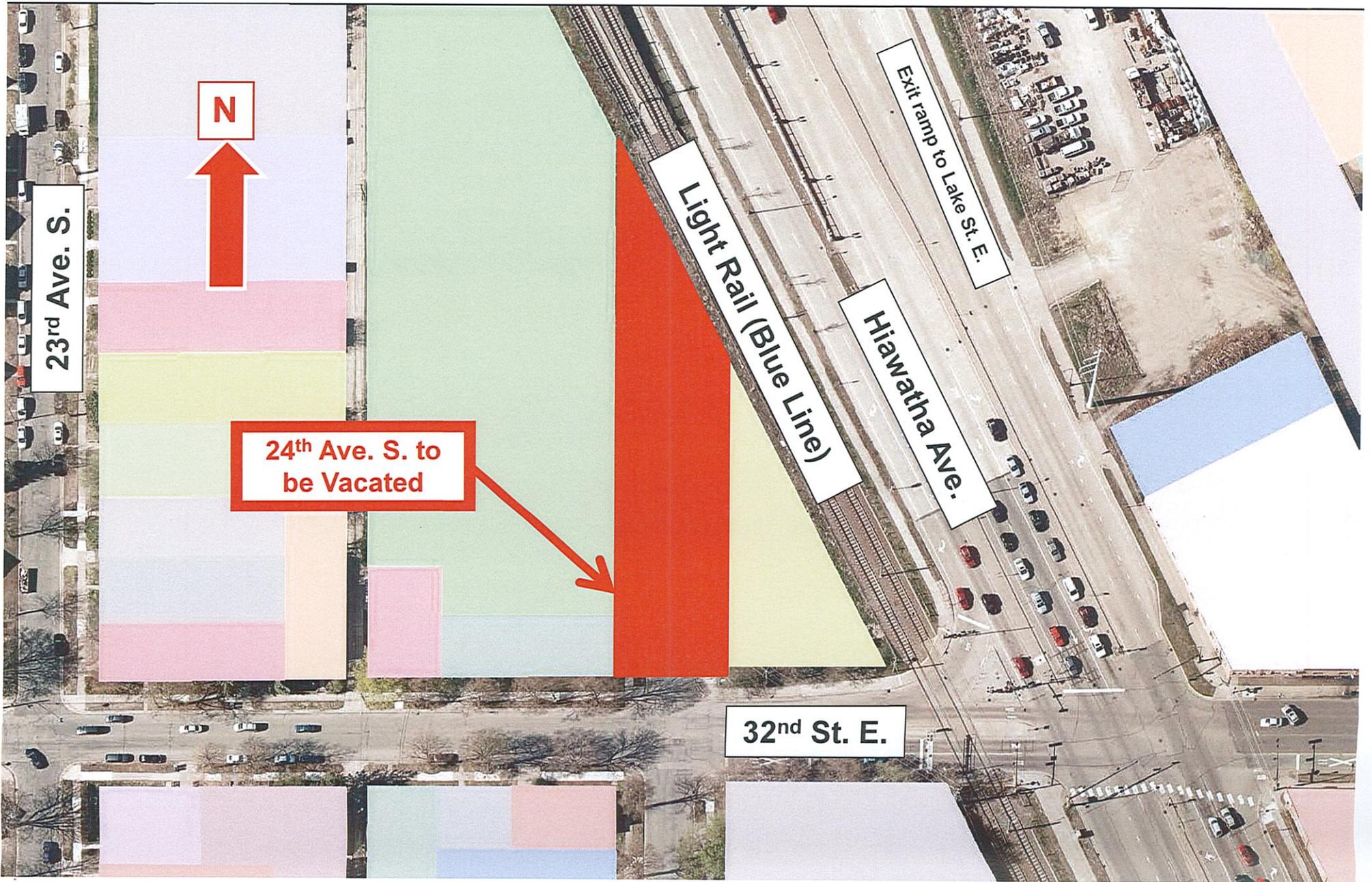
Deny petition (provide explanation)

Reserve Easements (provide description)

By: _____ Date _____



Comments:





**Petition to Vacate
City Of Minneapolis Street, Alley or Utility Easement**

Petitioner: Corridor Plaza LLC
Address: 1625 Energy Park Dr., Ste. 100
St. Paul, MN 55108
Contact Person: Stephen B. Wellington, Jr. (651) 999-5526

Vacation File No.

1646

Page 1 of 2

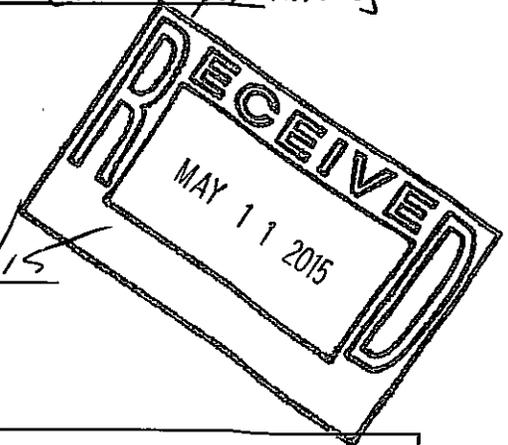
Description of Easement to be vacated: 24th Ave. S., from 32nd St. E., north to Metro Transit light rail tracks – adjacent to Block 6, Griswold's 3rd Addition to Minneapolis.

Review and Comment

Utility easements cannot be reserved unless there is an existing facility within the area to be vacated. If there is an existing utility, please enclose a map showing its location.

Name of Reviewing Agency Mpls Fire Dept.
Phone: 370-3836 e-Mail: Todd.White@Minneapolismn.gov
Approve petition as requested
Deny petition (provide explanation)
Reserve Easements (provide description)

By: Deputy Chief White Date 5/21/15



Comments:

[Empty box for comments]

**Petition to Vacate
City Of Minneapolis Street, Alley or Utility Easement**

Petitioner: Corridor Plaza LLC
Address: 1625 Energy Park Dr., Ste. 100
St. Paul, MN 55108
Contact Person: Stephen B. Wellington, Jr. (651) 999-5526

Vacation File No.
1646
Page 1 of 2

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Review and Comment

Utility easements cannot be reserved unless there is an existing facility within the area to be vacated. If there is an existing utility, please enclose a map showing its location.

Name of Reviewing Agency Center Point Energy

Phone: 612-321-5381

e-Mail: Charles.Mayers@CenterPointenergy.com

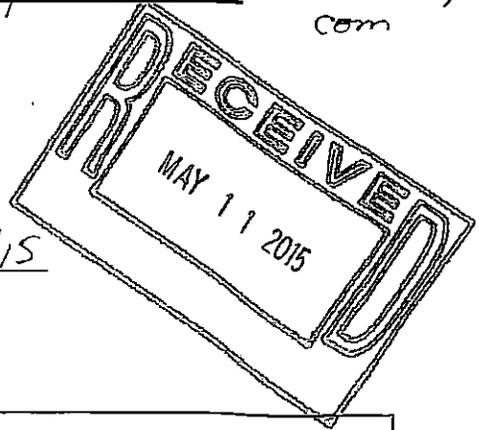
Approve petition as requested

Deny petition (provide explanation)

Reserve Easements (provide description)

By: Chuck Mayer

Date 5/21/15



Comments: No Interest.



OSP National Support /
Investigations
2400 North Glenville
Richardson, TX 75082

MCI Communications Services, Inc.

05/22/2015

**CITY OF MINNEAPOLIS PLANNING AND DEVELOPMENT
HILARY DVORAK
250 S. 4TH STREET, ROOM 300
MINNEAPOLIS, MN 55415**

**RE: FILE NO. 1646 – EASEMENT VACATION REQUEST
– AREA OF 24TH AVENUE S. FROM 32ND STREET E.
TO METRO TRANSIT LIGHT RAIL.–MINNEAPOLIS, MN.**

Verizon Business ID: 4227-2015

Dear Sir or Madam:

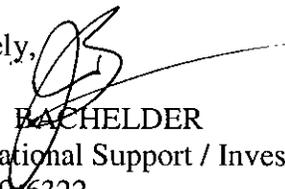
MCI has been notified by your office regarding the above referenced project.

For your records, in reviewing the area in question, it has been determined that MCI does not have facilities within your project area.

You should address correspondence concerning any future projects to the attention of **OSP National Support/Investigations** at the above address.

If you need further assistance with this project, please do not hesitate to call me.

Sincerely,


JOHN BACHELDER
OSP National Support / Investigations
972-729-6322

**Petition to Vacate
City Of Minneapolis Street, Alley or Utility Easement**

Petitioner: Corridor Plaza LLC
Address: 1625 Energy Park Dr., Ste. 100
St. Paul, MN 55108
Contact Person: Stephen B. Wellington, Jr. (651) 999-5526

Vacation File No.
1646
Page 1 of 2

Description of Easement to be vacated: 24th Ave. S., from 32nd St. E., north to Metro Transit light rail tracks – adjacent to Block 6, Griswold's 3rd Addition to Minneapolis.

Review and Comment

Utility easements cannot be reserved unless there is an existing facility within the area to be vacated. If there is an existing utility, please enclose a map showing its location.

Name of Reviewing Agency XCEL ENERGY

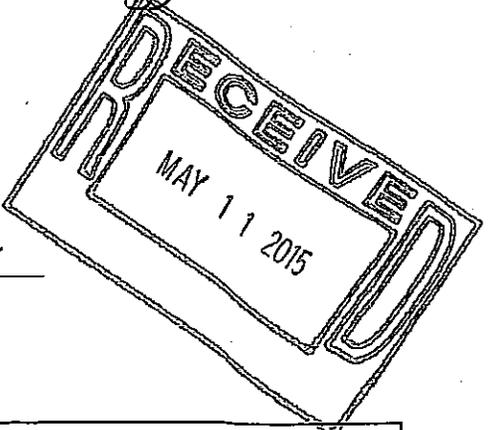
Phone: 612-330-1956 e-Mail: Sean.w.lawler@xcelenergy.com

Approve petition as requested

Deny petition (provide explanation)

Reserve Easements (provide description)

By: SEAN LAWLER Date 5/26/15



Comments:
XCEL ENERGY REQUESTS AN EASEMENT TO BE RESERVED OVER THE AREA BEING VACATED AND DESCRIBED ABOVE FOR EXISTING ELECTRIC FACILITIES. THE PETITIONER HAS BEEN PROVIDED WITH A CONTRACT AND QUOTE TO RELOCATE THESE FACILITIES BY XCEL. ONCE THE CONTRACT IS SIGNED & PAYMENT RECEIVED THE RESERVED EASEMENT CAN BE

RELEASED.

**Petition to Vacate
City Of Minneapolis Street, Alley or Utility Easement**

Petitioner: Corridor Plaza LLC
Address: 1625 Energy Park Dr., Ste. 100
St. Paul, MN 55108
Contact Person: Stephen B. Wellington, Jr. (651) 999-5526

Vacation File No.

1646

Page 1 of 2

Description of Easement to be vacated: 24th Ave. S., from 32nd St. E., north to Metro Transit light rail tracks – adjacent to Block 6, Griswold's 3rd Addition to Minneapolis.

Review and Comment

Utility easements cannot be reserved unless there is an existing facility within the area to be vacated. If there is an existing utility, please enclose a map showing its location.

Name of Reviewing Agency COMCAST TOM NIEDZIELSKI

Phone: 651-493-5407 e-Mail: Thomas.Niedzielski@comcast.com

Approve petition as requested

Deny petition (provide explanation)

Reserve Easements (provide description)

By: Duane Carlson

Date 5-28-15

Comments:

Reply to Vacation Inquiry File # 1646 24 th Ave. S., from 32 nd St. E., north to Metro Transit rail tracks.	To Lisa Baldwin	From: Tom Niedzielski
	City of Minneapolis Planning Commission	Comcast
	Phone # 612-673-5342	Phone # 612-490-7750
	Fax # 612-673-2526	Fax #651-493-5116

Description of public right-of way proposed to be vacated:

See attached legal description

This section to be completed ONLY by City Depts

- We have no objections to this vacation
- We have no objections to the vacation, subject to conditions stated below
- We object to the vacation for the reasons stated below

Conditions/Reasons:

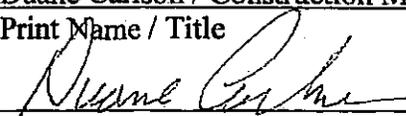
This section to be completed ONLY by Utilities

- We do NOT have facilities in the proposed vacated area, and we therefore RELEASE our utility easement rights, subject to any conditions or exceptions stated below
- We do NOT have facilities in the proposed vacated area, but we wish to RETAIN our easement rights, subject to any conditions or exceptions stated below
- We DO have facilities in the proposed vacated area, and we therefore RETAIN our easement rights, subject to any conditions or exceptions stated below

Conditions/Exceptions: *We can release these rights provided the proposed improvements do not interfere with its existing facilities or interfere with Comcast's access to the said facilities to maintain, repair, or upgrade them. Should the said improvements in anyway interfere with or prevent access to the said facilities, the petitioner or its successor shall provide new easements as required and the petitioner or its successor shall assume all costs of relocating facilities to the new easements..*

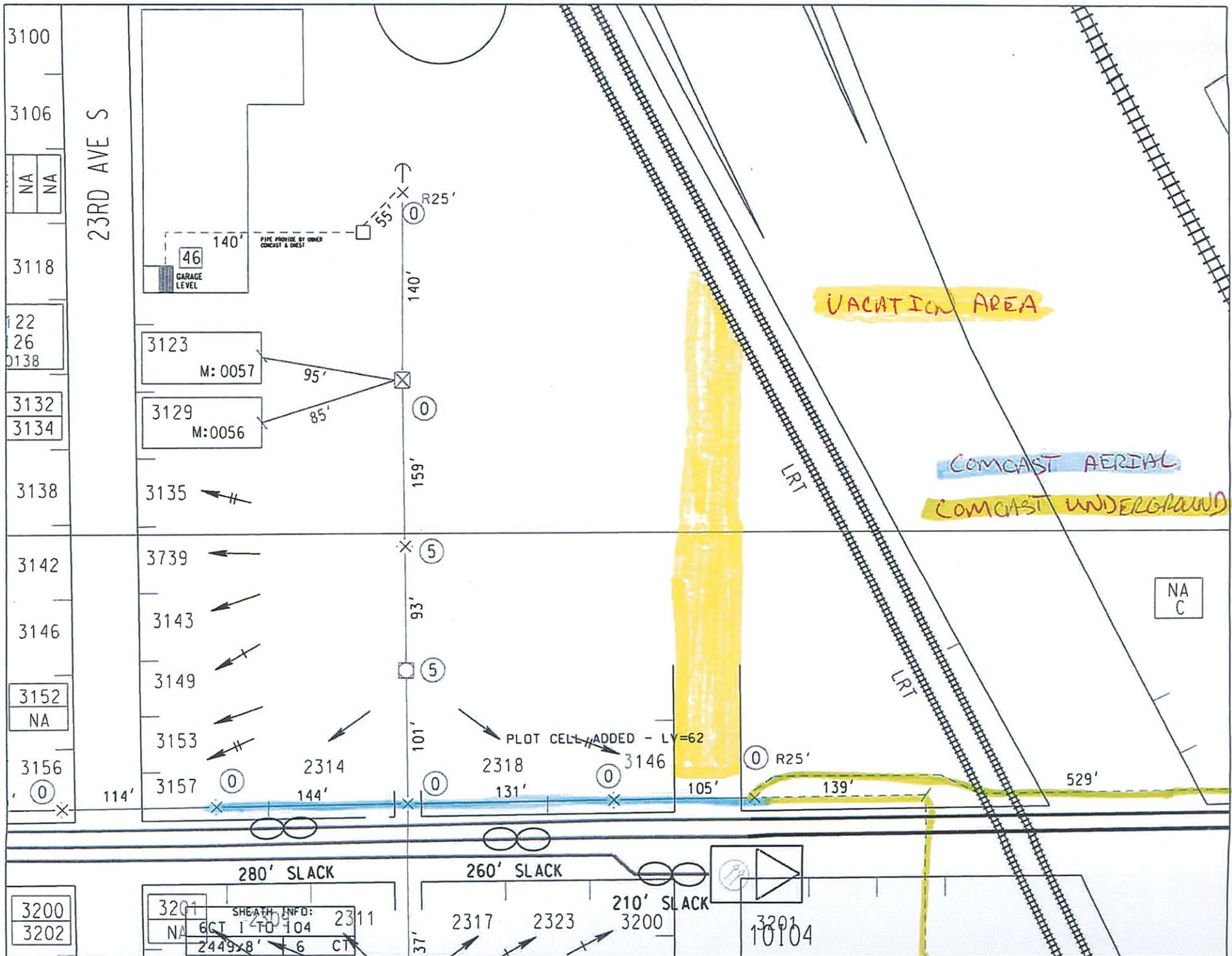
Duly authorized representative:

Duane Carlson / Construction Manager
Print Name / Title


Signature

Comcast
Company Name

5/18/15
Date



**Petition to Vacate
City Of Minneapolis Street, Alley or Utility Easement**

Petitioner: Corridor Plaza LLC
Address: 1625 Energy Park Dr., Ste. 100
St. Paul, MN 55108
Contact Person: Stephen B. Wellington, Jr. (651) 999-5526

Vacation File No.

1646

Page 1 of 2

Description of Easement to be vacated: 24th Ave. S., from 32nd St. E., north to Metro Transit light rail tracks – adjacent to Block 6, Griswold's 3rd Addition to Minneapolis.

Review and Comment

Utility easements cannot be reserved unless there is an existing facility within the area to be vacated. If there is an existing utility, please enclose a map showing its location.

Name of Reviewing Agency CENTURYLINK

Phone: 651-312-7018

e-Mail: nicholas.reynolds@centurylink.com

Approve petition as requested

Deny petition (provide explanation)

Reserve Easements (provide description)

By: NICK REYNOLDS

Date 6/1/15

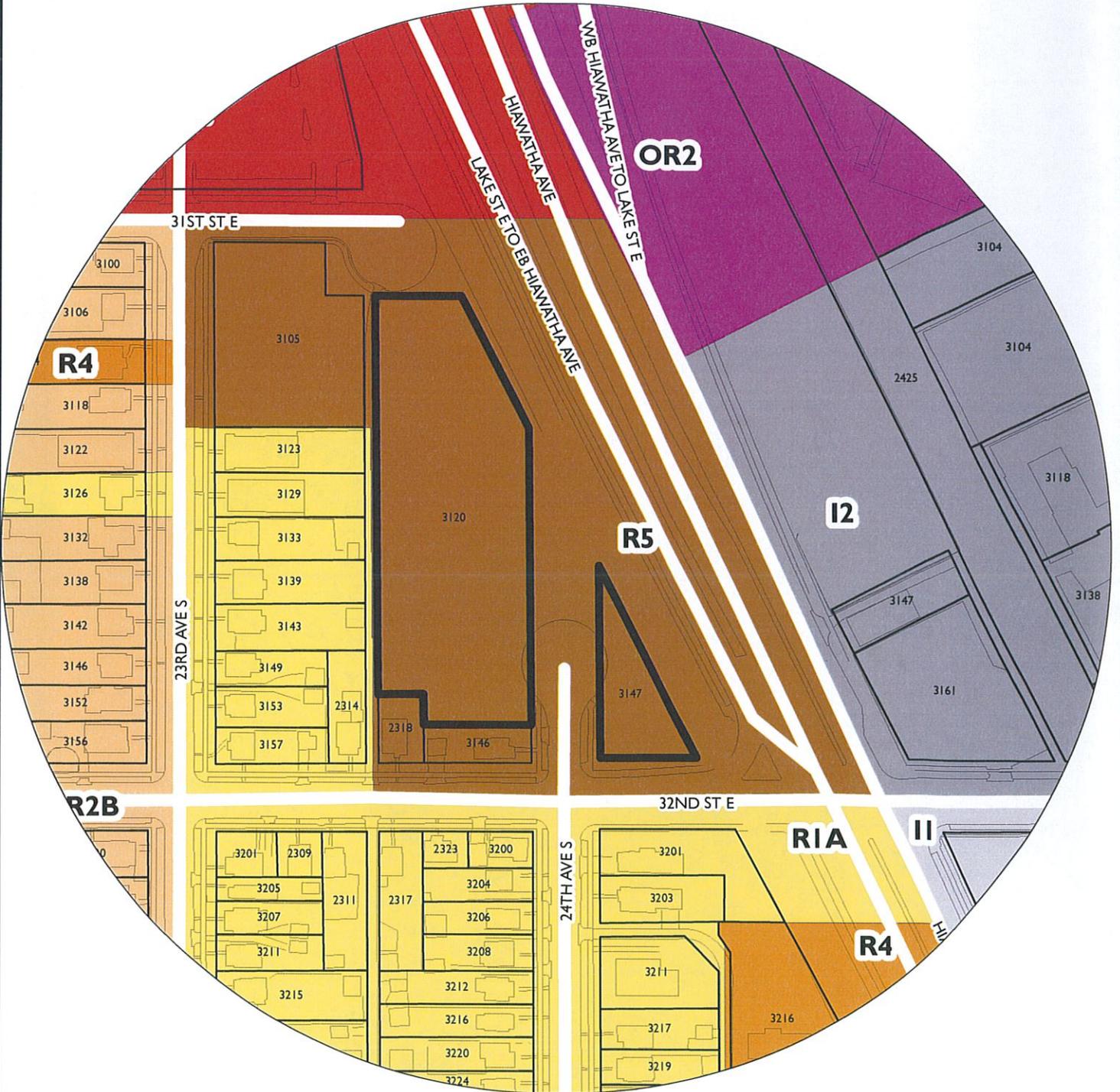
Comments: CENTURYLINK HAS NO FACILITIES IN THE SHADED AREA AS DESCRIBED. THE ONLY PLANT THAT IS NEAR IS A DSL CABINET AND A RELATED POWER BOX LOCATED TO THE EAST OF AN EXISTING XCEL POWER POLE LOCATED EAST OF 24TH AND NORTH OF 32ND JUST BEHIND SIDEWALK.

Corcoran Triangle

9th

NAME OF APPLICANT

WARD



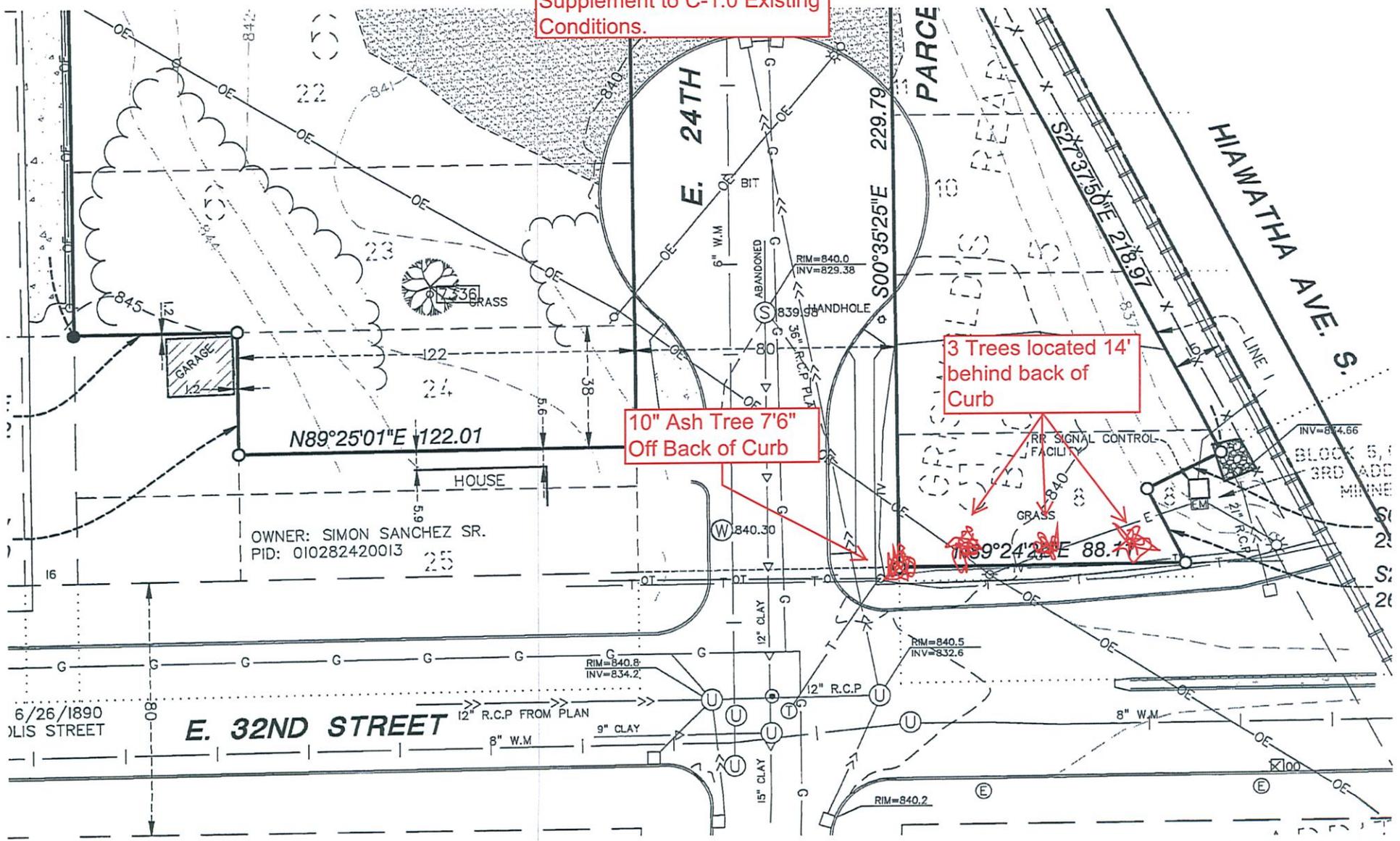
PROPERTY ADDRESS

3120 and 3147 24th Avenue South

FILE NUMBER

BZZ-7234

Supplement to C-1.0 Existing Conditions.



10" Ash Tree 7'6"
Off Back of Curb

3 Trees located 14'
behind back of
Curb

6/26/1890
OLIS STREET

E. 32ND STREET

E. 24TH

PARCE

HIAWATHA AVE. S.

OWNER: SIMON SANCHEZ SR.
PID: 010282420013

RIM=840.8
INV=834.2

RIM=840.5
INV=832.6

INV=884.66

BLOCK 5, 1
3RD ADD
MINNE

RIM=840.2

RIM=840.0
INV=829.38

W 840.30

N 89°24'22"E 88.7'

GARAGE

HOUSE

6" W.M.

ABANDONED

35" R.C.P. 11/12

839.98 HANDHOLE

RR SIGNAL CONTROL
FACILITY

GRASS

8" W.M.

15" CLAY

12" R.C.P.

12" CLAY

DT

W 840.30

6" W.M.

Drawing name: S:\2008\060203\Gen & spec\Construction Document\060203-0101-1516.dwg, Jun 03, 2013 - 10:43am

The drawing set includes the following sheets:

- ME-0101-1516-01**: Section through wall and window.
- ME-0101-1516-02**: Section through wall and window.
- ME-0101-1516-03**: Section through wall and window.
- ME-0101-1516-04**: Section through wall and window.
- ME-0101-1516-05**: Section through wall and window.
- ME-0101-1516-06**: Section through wall and window.
- ME-0101-1516-07**: Section through wall and window.
- ME-0101-1516-08**: Section through wall and window.
- ME-0101-1516-09**: Section through wall and window.
- ME-0101-1516-10**: Section through wall and window.
- ME-0101-1516-11**: Section through wall and window.
- ME-0101-1516-12**: Section through wall and window.
- ME-0101-1516-13**: Section through wall and window.
- ME-0101-1516-14**: Section through wall and window.
- ME-0101-1516-15**: Section through wall and window.
- ME-0101-1516-16**: Section through wall and window.
- ME-0101-1516-17**: Section through wall and window.
- ME-0101-1516-18**: Section through wall and window.
- ME-0101-1516-19**: Section through wall and window.
- ME-0101-1516-20**: Section through wall and window.
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- ME-0101-1516-25**: Section through wall and window.
- ME-0101-1516-26**: Section through wall and window.
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- ME-0101-1516-28**: Section through wall and window.
- ME-0101-1516-29**: Section through wall and window.
- ME-0101-1516-30**: Section through wall and window.
- ME-0101-1516-31**: Section through wall and window.
- ME-0101-1516-32**: Section through wall and window.
- ME-0101-1516-33**: Section through wall and window.
- ME-0101-1516-34**: Section through wall and window.
- ME-0101-1516-35**: Section through wall and window.
- ME-0101-1516-36**: Section through wall and window.
- ME-0101-1516-37**: Section through wall and window.
- ME-0101-1516-38**: Section through wall and window.
- ME-0101-1516-39**: Section through wall and window.
- ME-0101-1516-40**: Section through wall and window.
- ME-0101-1516-41**: Section through wall and window.
- ME-0101-1516-42**: Section through wall and window.
- ME-0101-1516-43**: Section through wall and window.
- ME-0101-1516-44**: Section through wall and window.
- ME-0101-1516-45**: Section through wall and window.
- ME-0101-1516-46**: Section through wall and window.
- ME-0101-1516-47**: Section through wall and window.
- ME-0101-1516-48**: Section through wall and window.
- ME-0101-1516-49**: Section through wall and window.
- ME-0101-1516-50**: Section through wall and window.
- ME-0101-1516-51**: Section through wall and window.
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- ME-0101-1516-98**: Section through wall and window.
- ME-0101-1516-99**: Section through wall and window.
- ME-0101-1516-100**: Section through wall and window.

Collage
ARCHITECTURE

ALLIANT
CONSTRUCTION

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Minneapolis, MN 55415
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612.736.3050 FAX
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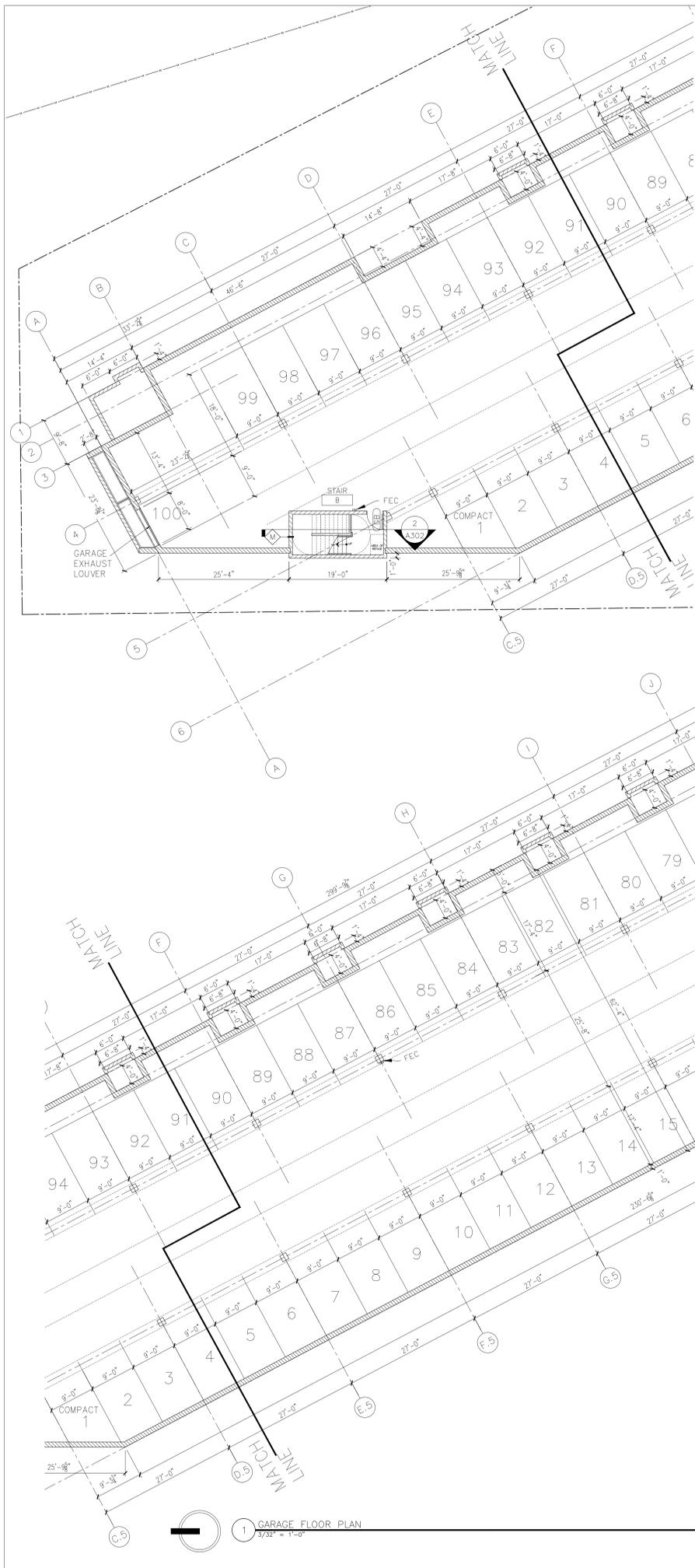
CORCORAN TRIANGLE APARTMENTS
E. 32ND STREET
MINNEAPOLIS, MN
PRELIMINARY REVIEW
DETAILS

DATE: 05/10/2012
TIME: 10:43 AM
PROJECT: CORCORAN TRIANGLE APARTMENTS
DRAWN BY: J. H. HARRIS
CHECKED BY: J. H. HARRIS
DATE: 05/10/2012
TIME: 10:43 AM
PROJECT: CORCORAN TRIANGLE APARTMENTS
DRAWN BY: J. H. HARRIS
CHECKED BY: J. H. HARRIS

FOR REVIEW ONLY
PRELIMINARY
NOT FOR CONSTRUCTION

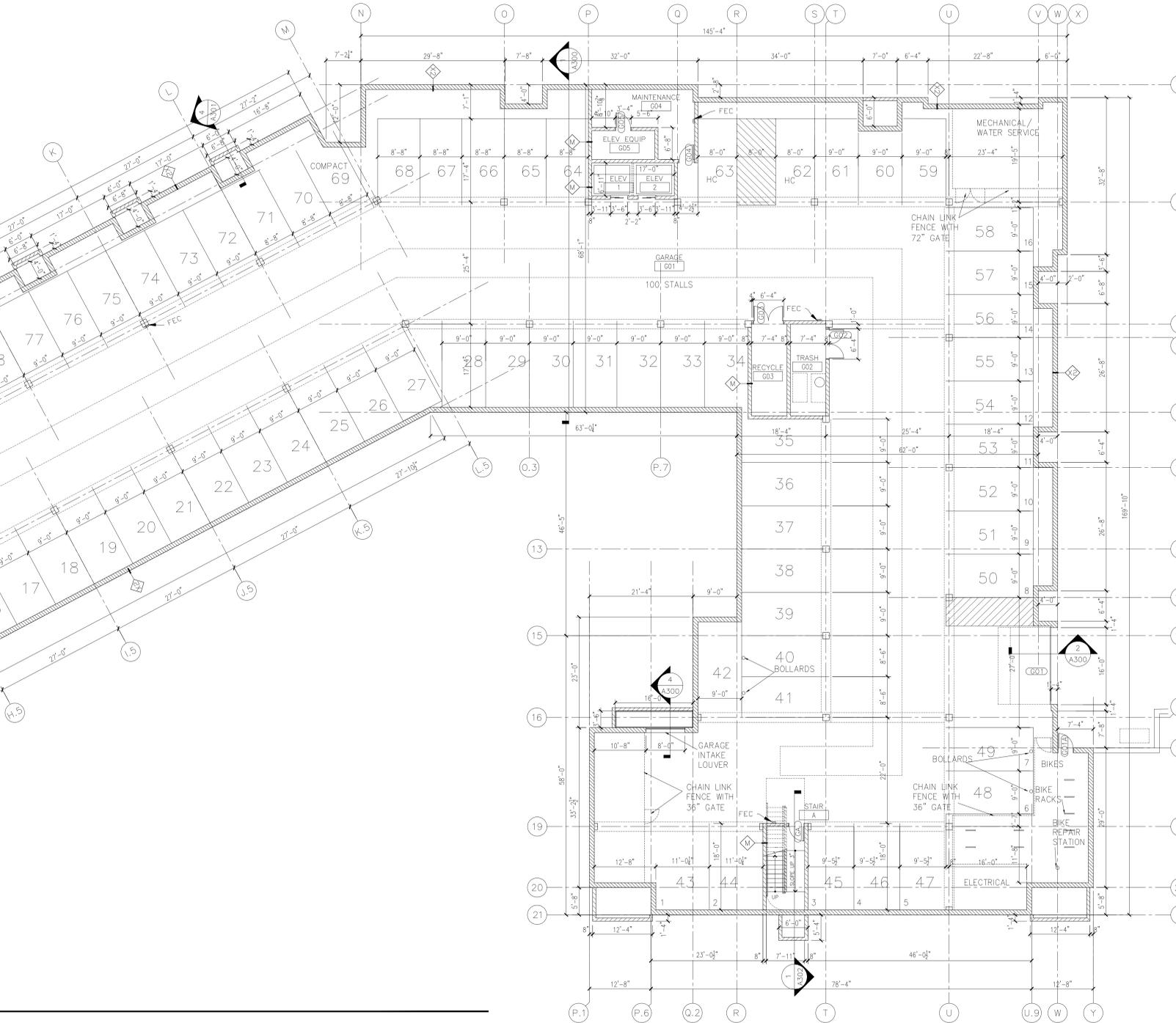
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CHECKED BY: J. H. HARRIS

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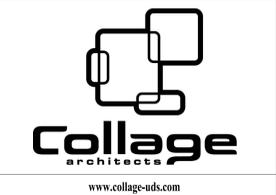


- GENERAL NOTES
- ALL EXTERIOR DIMENSIONS ARE TO OUTSIDE FACE OF SHEATHING OR OUTSIDE FACE OF MASONRY UNLESS OTHERWISE NOTED.
 - VERIFY SLAB EDGE LOCATIONS, BRICK AND MASONRY FACE WITH ARCHITECTURAL AND STRUCTURAL DETAILS.
 - INTERIOR WALL DIMENSIONS ARE TO CENTERLINE OF WALLS UNLESS NOTED OTHERWISE. WHERE CLEAR DIMENSIONS ARE INDICATED, THESE DIMENSIONS SHALL BE MAINTAINED.
 - ROUGH OPENING DIMENSIONS OF WINDOWS, DOORS AND OTHER COMPONENTS SHALL BE VERIFIED WITH ACTUAL DIMENSIONS OF SUPPLIED COMPONENTS.
 - DO NOT SCALE DRAWINGS. IF DIMENSION IS NOT CLEAR, NOT GIVEN, OR PRESENTS A CONFLICT, NOTIFY THE ARCHITECT IMMEDIATELY.
 - NOTIFY ARCHITECT IMMEDIATELY, IF DIMENSIONAL DISCREPANCIES OCCUR.
 - PROVIDE CONTROL JOINTS AT FLOORS AND WALLS PER MANUFACTURER'S RECOMMENDATIONS AND SPECIFICATIONS.
 - INSTALL ALL ELEMENTS AND MATERIALS TO MEET MANUFACTURER'S RECOMMENDATIONS. IF ARCHITECTURAL DRAWINGS CONFLICT WITH MANUFACTURER'S RECOMMENDATIONS, NOTIFY ARCHITECT IMMEDIATELY.
 - PROVIDE SEALED JOINTS AT MOISTURE CONTROL MEMBRANE.
 - PROTECT MATERIALS ON-SITE DURING CONSTRUCTION FROM MOISTURE AND OTHER DAMAGE.
 - INSULATE GAPS AT WOOD FRAMING BETWEEN WINDOWS, DOORS AND OTHER MATERIALS WITH INSULATION.
 - WOOD IN CONTACT WITH CONCRETE TO BE TREATED WOOD.
 - PROVIDE WOOD BLOCKING BEHIND ALL WALL HUNG CASEWORK, MILLWORK, SHELVING, FIXTURES AND ELSEWHERE AS NOTED OR AS NECESSARY BASED ON MANUFACTURER'S RECOMMENDATIONS.
 - GENERAL CONTRACTOR TO VERIFY WITH MECHANICAL, ELECTRICAL AND PLUMBING SUBCONTRACTORS AS TO LOCATIONS FOR EQUIPMENT, FIXTURES, METERS, DUCTWORK, ETC. NOTIFY ARCHITECT IMMEDIATELY OF ANY CONFLICTS BETWEEN ARCHITECTURAL DRAWINGS AND M.E.P. SUBCONTRACTORS.
 - CONSTRUCT RATED ASSEMBLIES AT ALL LOCATIONS INDICATED IN CODE REVIEW AND PER DRAWINGS, INCLUDING ALL CEILING AND FLOOR ASSEMBLIES. PROVIDE RATED ENCLOSURE WHERE FIXTURES OR EQUIPMENT PENETRATES THE ASSEMBLY.
 - PROVIDE FIRE STOPPING AT ALL PENETRATIONS OF RATED ASSEMBLIES. ANY DISTURBANCE OF ASSEMBLY SHALL BE REPAIRED.
 - FIRE-BLOCK ALL CONCEALED WALL SPACES, INCLUDING 10'-0" VERTICALLY AND HORIZONTALLY.
 - DO NOT PENETRATE STAIR OR ELEVATOR ENCLOSURE UNLESS SERVING STAIR OR ELEVATOR.
 - FINISH FLOOR ELEVATION CHANGE AT DOORS OR MATERIAL TRANSITIONS NOT TO EXCEED 1/2".
 - SEE SHEET A001 FOR ALL WALL TYPES.

- GENERAL UNIT PLAN NOTES
- ALL INTERIOR WALL ARE TYPE 'A', UNLESS OTHERWISE NOTED.
 - NOTIFY ARCHITECT IMMEDIATELY IF DIMENSIONAL DISCREPANCIES OCCUR.
 - REFER TO OVERALL BUILDING PLANS FOR THE FOLLOWING:
 - EXTERIOR WALL TYPES
 - EXTERIOR DOORS & WINDOW SIZES & LOCATIONS
 - EXTERIOR WALL DIMENSIONS
 - INTERIOR DEMISING WALL TYPES
 - PROVIDE ROO & SHELF AT ALL CLOSETS UNLESS NOTED OTHERWISE.
 - PROVIDE BASE AND FLOOR FINISHES BEHIND REFRIGERATOR AND AT REMOVABLE CABINETS.
 - CAULK/SEAL VANITY COUNTERS TO WALL AND ALL PLUMBING FIXTURES TO WALL, COUNTER AND FLOOR.
 - MAINTAIN 40" CLEAR IN FRONT OF COUNTER TO ADJACENT SURFACE OR OBSTRUCTION.
 - PROVIDE WOOD BLOCKING BEHIND ALL WALL HUNG CASEWORK, MILLWORK, SHELVING, FIXTURES & ELSEWHERE AS NOTED OR AS NECESSARY BASED ON MANUFACTURER'S RECOMMENDATIONS.
 - GENERAL CONTRACTOR TO VERIFY WITH MECHANICAL SUBCONTRACTORS AS TO LOCATIONS FOR EQUIPMENT, ETC. NOTIFY ARCHITECT OF ANY NECESSARY MODIFICATIONS.
 - VERIFY LOCATIONS OF ALL MECHANICAL AND PLUMBING, IF CONFLICTS ARISE, NOTIFY ARCHITECT IMMEDIATELY.
 - VERIFY ALL FIXTURES AND EQUIPMENT FIT IN PLANS PRIOR TO INSTALLATION AND THAT FIXTURES MAINTAIN REQUIRED CLEARANCES AND CLEAR AREA AS INDICATED WITH DIMENSIONS OR
 - CONSTRUCT RATED ASSEMBLIES AT ALL LOCATIONS INDICATED IN CODE REVIEW & PER DRAWINGS INCLUDING ALL CEILING & FLOOR ASSEMBLIES.
 - PROVIDE FIRE STOPPING AT ALL PENETRATIONS OF RATED ASSEMBLIES. ANY DISTURBANCE OF ASSEMBLY SHALL BE REPAIRED.
 - SEE SHEET A001 FOR ALL WALL TYPES.
 - FIELD VERIFY ALL DIMENSIONS & ADJUST CABINET SIZING TO FIT FIELD CONDITIONS. PROVIDE FINISHED END PANELS TO MATCH CABINERY AT ALL EXPOSED LOCATIONS. PROVIDE FILLERS WHERE NECESSARY TO ALLOW PROPER CABINET CLEARANCES & TO FILL OPENING COMPLETELY.
 - PROVIDE CABINET MANUFACTURER'S STANDARD TOE KICK AT CABINETS. STAIN TO MATCH CABINET.
 - RSF = REMOVABLE SINK FRONT.
 - ALL DWELLING UNITS TO HAVE CONCEALED WALL BLOCKING PROVIDED FOR FUTURE GRAB BARS. SEE DETAIL X1000.
 - PAINT ELECTRICAL BOX TO MATCH WALL.
 - CONTRACTOR TO VERIFY CLEARANCES TO ALL COMBUSTIBLE MATERIALS FROM FIREPLACES OR MECHANICAL EQUIPMENT AND MEET MANUFACTURER'S RECOMMENDATIONS.



1 GARAGE FLOOR PLAN
3/32" = 1'-0"



Corcoran Triangle
3120 24th Avenue South
MINNEAPOLIS MINNESOTA

OWNER **Corcoran Triangle, LLC**

DATE:
I HEREBY CERTIFY THAT THIS PLAN SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A REGISTERED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.

PETER KEELY
REGISTRATION No: C-2779

Collage | architects
Architect
Pete Keely
651.472.0050
705 Raymond Avenue
St. Paul, Minnesota 55114

Ericksen Rod & Associates
Structural Engineer
651.251.7970
2550 University Ave Suite 201-S
St. Paul, Minnesota 55114

KEY PLAN
DATE: 06.05.2015

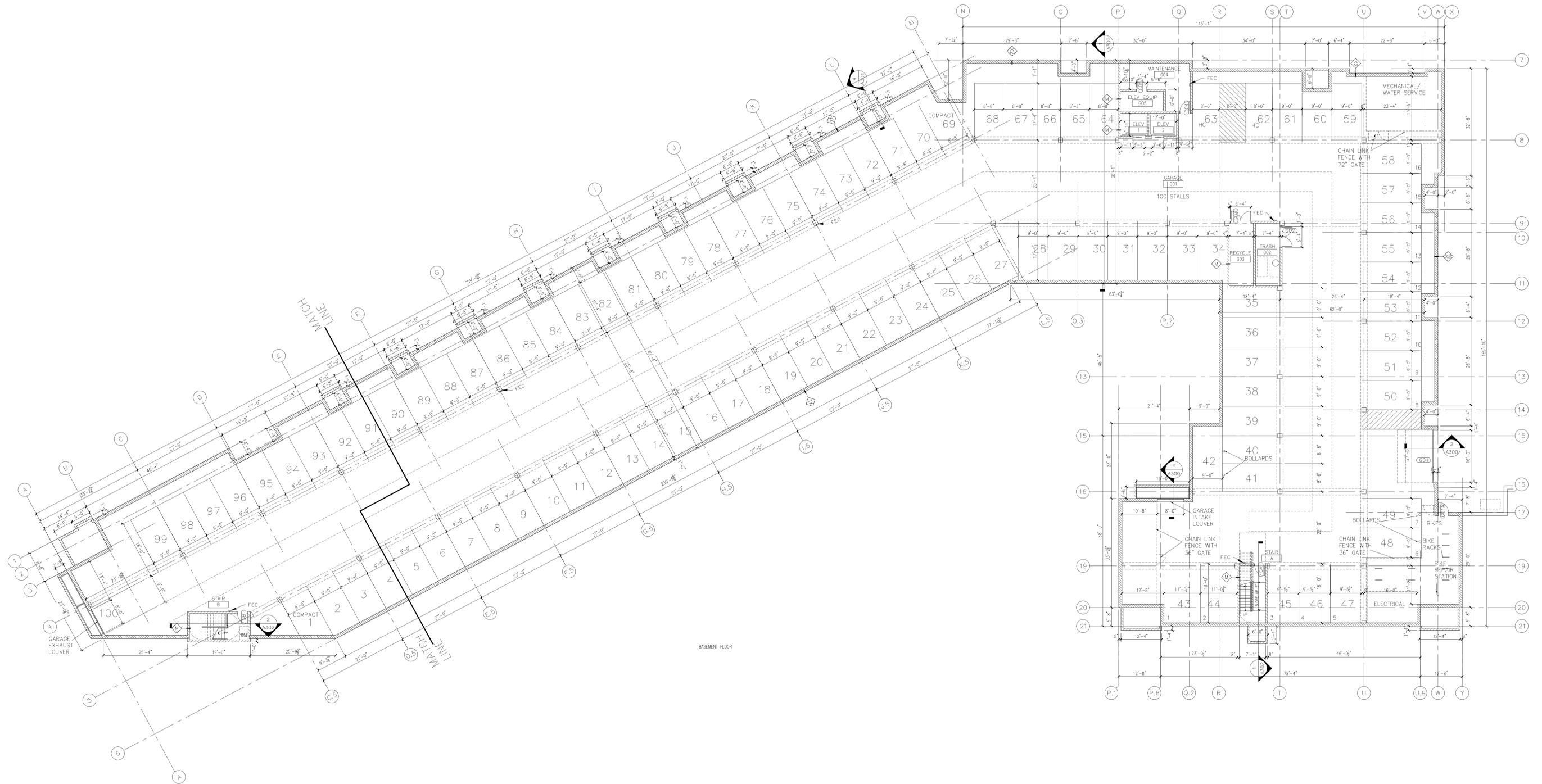
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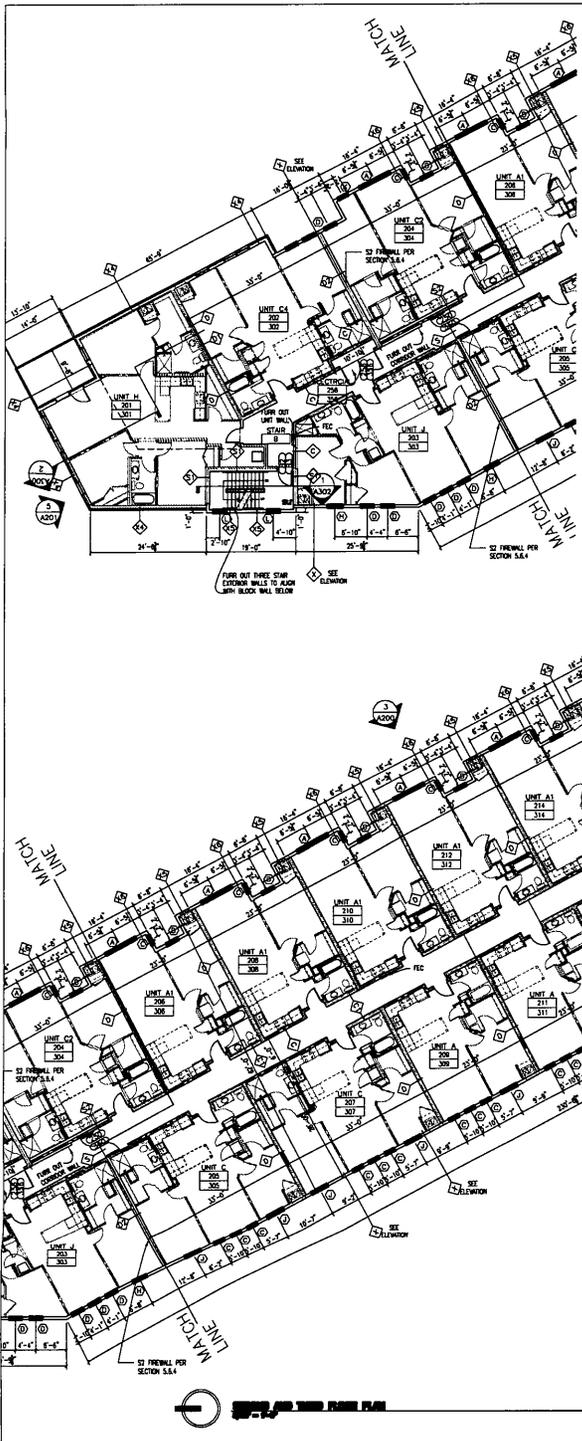
PROJECT NO:
CAD DWG FILE: A-100 LOWER LEVELS
DRAWN BY: S.B. J.D. C.N.
CHK'D BY:
COPYRIGHT:

SHEET TITLE
GARAGE FLOOR PLAN

A-100



BASEMENT FLOOR



GENERAL NOTES

1. ALL EXTERIOR DIMENSIONS ARE TO OUTSIDE FACE OF SHEATHING OR OUTSIDE FACE OF MASONRY UNLESS OTHERWISE NOTED.
2. VERIFY GLASS EDEL LOCATIONS, BRICK AND MASONRY FACE WITH ARCHITECTURAL AND STRUCTURAL DETAILS.
3. INTERIOR WALL DIMENSIONS ARE TO CENTERLINE OF WALLS UNLESS NOTED OTHERWISE. WHERE CLEAR DIMENSIONS ARE INDICATED, THESE DIMENSIONS SHALL BE MAINTAINED.
4. ROUND OPENING DIMENSIONS OF WINDOWS, DOORS AND OTHER COMPONENTS SHALL BE VERIFIED WITH ACTUAL DIMENSIONS OF SUPPLIED COMPONENTS.
5. DO NOT SCALE DRAWINGS. IF DIMENSIONS ARE NOT CLEAR, NOT DRAW OR PRESENT A CONFLICT. NOTIFY THE ARCHITECT IMMEDIATELY.
6. NOTIFY ARCHITECT IMMEDIATELY IF DIMENSIONAL DISCREPANCIES OCCUR.
7. PROVIDE CONTROL JOINTS AT FLOORS AND WALLS PER MANUFACTURER'S RECOMMENDATIONS AND SPECIFICATIONS.
8. INSTALL ALL ELEMENTS AND MATERIALS TO MEET MANUFACTURER'S RECOMMENDATIONS. IF ARCHITECTURAL DRAWINGS CONFLICT WITH MANUFACTURER'S RECOMMENDATIONS, NOTIFY ARCHITECT IMMEDIATELY.
9. PROVIDE SEALED JOINTS AT MORTURE CONTROL LINING.
10. PROTECT MATERIALS ON-SITE DURING CONSTRUCTION FROM MOISTURE AND OTHER DAMAGE.
11. INSULATE GAPS AT WOOD FRAMING BETWEEN WINDOWS, DOORS AND OTHER MATERIALS WITH INSULATION.
12. WOOD IN CONTACT WITH CONCRETE TO BE TREATED WOOD.
13. PROVIDE WOOD BLOCKING BEHIND ALL WALL HUNG CASEWORK, MILLWORK, SHELVING, FIXTURES AND ESSENTIALS AS NOTED OR AS NECESSARY BASED ON MANUFACTURER'S RECOMMENDATIONS.
14. GENERAL CONTRACTOR TO VERIFY WITH MECHANICAL, ELECTRICAL AND PLUMBING SUBCONTRACTORS AS TO LOCATIONS FOR EQUIPMENT, PIPES, METERS, SUCTORS, ETC. NOTIFY ARCHITECT IMMEDIATELY OF ANY CONFLICTS BETWEEN ARCHITECTURAL DRAWINGS AND ALL SUBCONTRACTORS.
15. CORRECTLY RATED ASSEMBLIES AT ALL LOCATIONS INDICATED IN CODE REVIEW AND PER DRAWINGS. INCLUDING ALL CEILING AND FLOOR ASSEMBLIES. PROVIDE RATED ENCLOSURE WHERE FIXTURES OR EQUIPMENT PENETRATES THE ASSEMBLY.
16. PROVIDE FIRE STOPPING AT ALL PENETRATIONS OF RATED ASSEMBLIES. ANY DISTURBANCE OF ASSEMBLY SHALL BE REPAIRED.
17. FRESH AIR ALL CONCEALED WALL SPACES, INCLUDING 10" VERTICALLY AND HORIZONTALLY.
18. DO NOT PENETRATE STAIR OR ELEVATOR ENCLOSURE UNLESS SERVING STAIR OR ELEVATOR.
19. FRESH FLOOR ELEVATION CHANGE AT DOORS OR MATERIAL TRANSITIONS NOT TO EXCEED 1/4".
20. SEE SHEET A01 FOR ALL WALL TYPES.

GENERAL UNIT PLAN NOTES

1. ALL INTERIOR WALLS ARE TYPE 'K' UNLESS OTHERWISE NOTED.
2. NOTIFY ARCHITECT IMMEDIATELY IF DIMENSIONAL DISCREPANCIES OCCUR.
3. REFER TO QUOTE/ALL BUILDING PLANS FOR THE FOLLOWING:
 - EXTERIOR WALL TYPES
 - EXTERIOR DOORS & WINDOW SIZES & LOCATIONS
 - EXTERIOR WALL, DOOR, WINDOW INTERIOR FINISHING WALL TYPES
4. PROVIDE ROOF & SHELF AT ALL CLOSETS UNLESS NOTED OTHERWISE.
5. PROVIDE BASE AND FLOOR FINISHES BEHIND REFRIGERATOR AND AT REMOVABLE CABINETS.
6. CALCULABLE VANITY COUNTERS TO WALL AND ALL PLUMBING FIXTURES TO WALL, COUNTER AND FLOOR.
7. MAINTAIN 4" CLEAR IN FRONT OF COUNTER TO ADJACENT SURFACE OR OBSTRUCTION.
8. PROVIDE WOOD BLOCKING BEHIND ALL WALL HUNG CASEWORK, MILLWORK, SHELVING, FIXTURES & ESSENTIALS AS NOTED OR AS NECESSARY BASED ON MANUFACTURER'S RECOMMENDATIONS.
9. GENERAL CONTRACTOR TO VERIFY WITH MECHANICAL, ELECTRICAL AND PLUMBING SUBCONTRACTORS AS TO LOCATIONS FOR EQUIPMENT, ETC. NOTIFY ARCHITECT OF ANY NECESSARY MODIFICATIONS.
10. VERIFY LOCATIONS OF ALL MECHANICAL AND PLUMBING. IF CONFLICTS ARISE, NOTIFY ARCHITECT IMMEDIATELY.
11. VERIFY ALL FIXTURES AND EQUIPMENT FIT IN PLACE PRIOR TO INSTALLATION AND THAT FIXTURES MAINTAIN REQUIRED CLEARANCES AND CLEARANCE AS INDICATED WITH DIMENSIONS OR :
12. CONSTRUCT RATED ASSEMBLIES AT ALL LOCATIONS INDICATED IN CODE REVIEW & PER DRAWINGS INCLUDING ALL CEILING FLOOR ASSEMBLIES.
13. PROVIDE FIRE STOPPING AT ALL PENETRATIONS OF RATED ASSEMBLIES. ANY DISTURBANCE OF ASSEMBLY SHALL BE REPAIRED.
14. SEE SHEET A01 FOR ALL WALL TYPES.
15. FIELD VERIFY ALL DIMENSIONS & ADJUST CABINET SIZES TO FIT FIELD CONDITIONS. PROVIDE FINISHES AND PANELS TO MATCH CABINETS AT ALL EXPOSED LOCATIONS. PROVIDE FINISHES WHERE NECESSARY TO ALLOW PROPER CABINET CLEARANCES & TO FIT OPENING COMPLETELY.
16. PROVIDE CABINET MANUFACTURER'S STANDARD TOE KICK AT CABINETS. STAIN TO MATCH CABINET.
17. RST - REMOVABLE SINK FRONT.
18. ALL DWELLING UNITS TO HAVE CONCEALED WALL BLDGING PROVIDED FOR FUTURE GRANITE SINKS. SEE DETAIL B3013.
19. PAINT ELECTRICAL BOX TO MATCH WALL.
20. CONTRACTOR TO VERIFY CLEARANCES TO ALL CONCEALED MATERIALS FROM PREPARATION OF MECHANICAL EQUIPMENT AND ADJUST MANUFACTURER'S RECOMMENDATIONS.

SEE ELEVATIONS FOR PLACEMENT OF EXTERIOR MATERIAL. MATERIALS CHANGE FROM CEMENT SIDING TO METAL SIDING.

Collage
ARCHITECTS

www.collage-arch.com

Corcoran Triangle
MINNEAPOLIS, MINNESOTA

Corcoran Triangle, LLC

DATE:

I HEREBY CERTIFY THAT THIS PLAN SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.

PROJECT NO.: PROJECT 1701 C-279

Collage | architects
Architect
Paul Brady
Collage Architects
700 University Avenue
St. Paul, Minnesota 55114

Evans Reed & Associates
Structural Engineer
645 LEXINGTON
3000 University Ave Suite 200-4
St. Paul, Minnesota 55114

KEY PLAN

DATE: 11.14.18

PROJECT NO.

ON THE FILE: PROJECT 1701 C-279

DRAWN BY: S.R. J.D.

CHECKED BY:

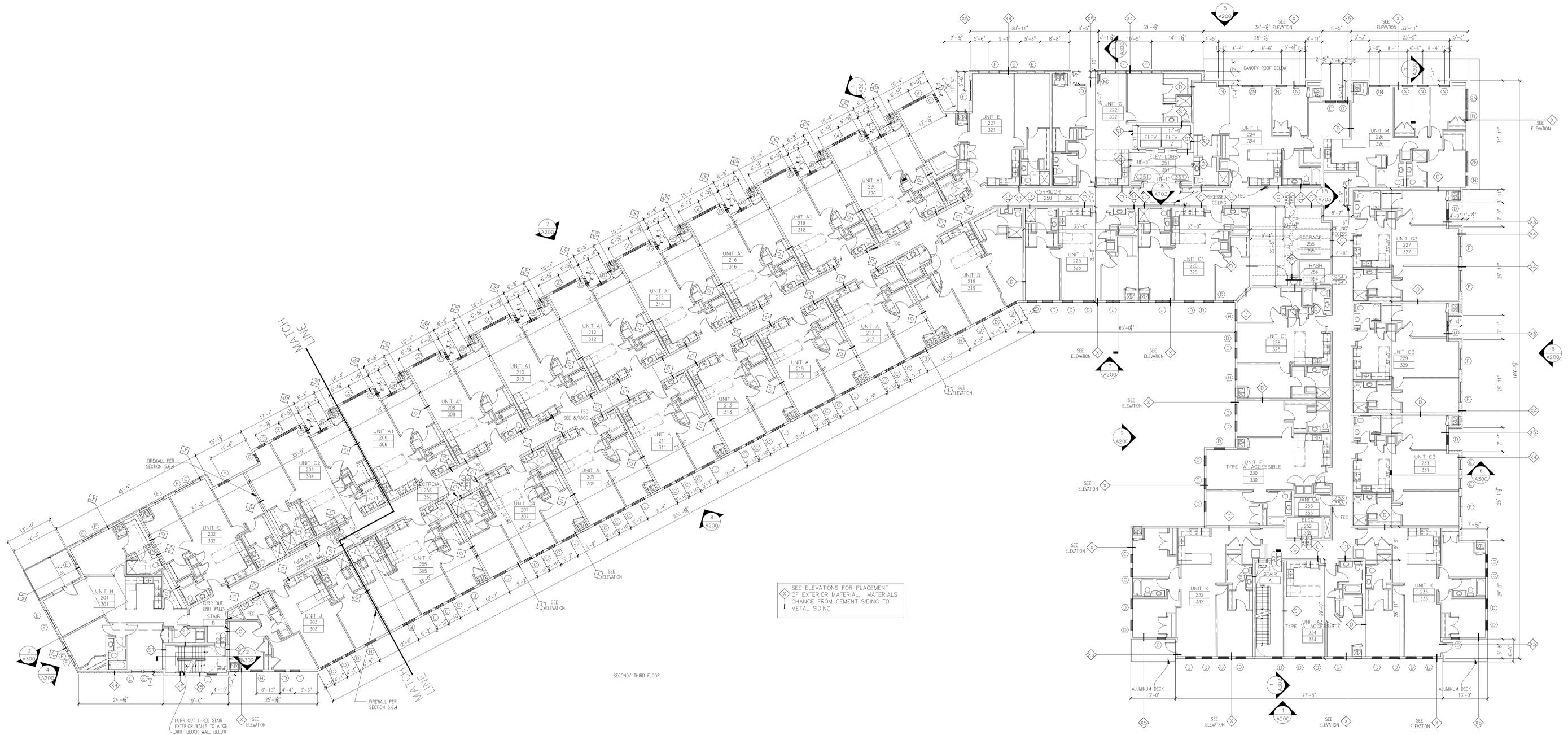
DATE:

PROJECT NO.

DATE:

SECOND & THIRD FLOOR PLAN

A-102



SEE ELEVATIONS FOR PLACEMENT OF EXTERIOR MATERIAL MATERIALS CHANGE FROM CEMENT SIDING TO METAL SIDING.

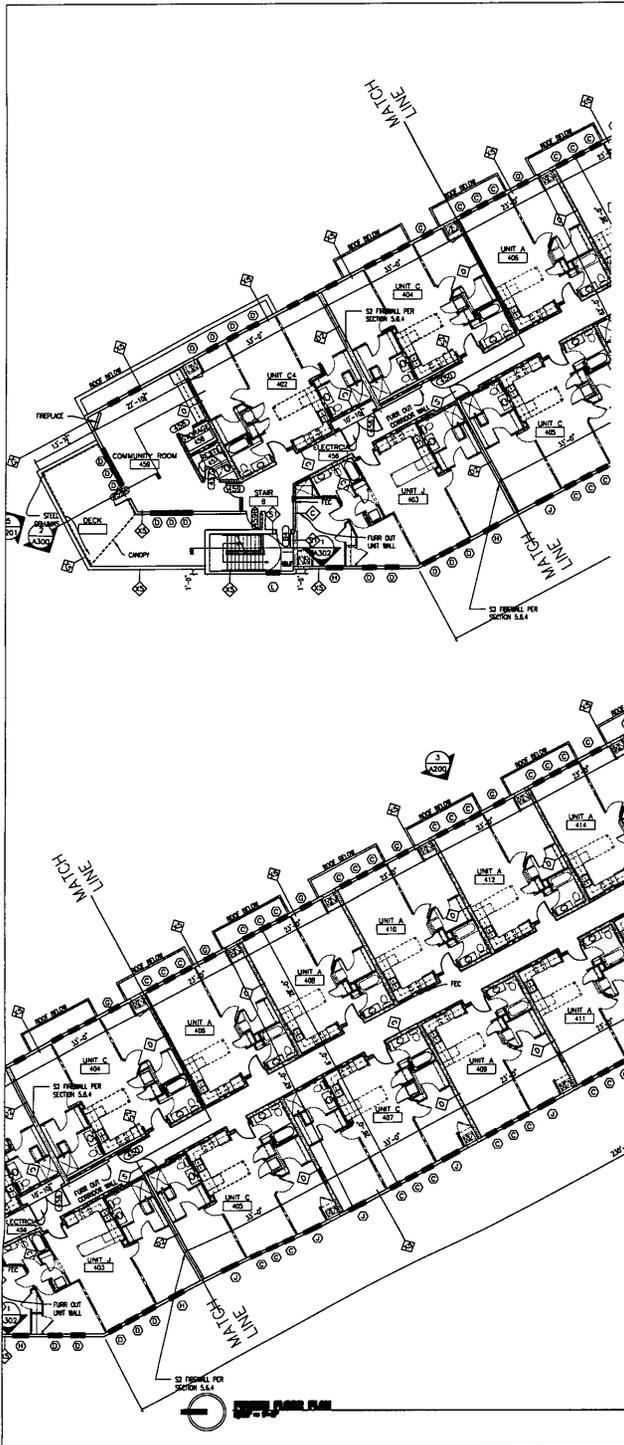
SECOND/THIRD FLOOR

FURR OUT THREE STAIR EXTERIOR WALLS TO ALIGN WITH BLOCK WALL BELOW

FIREWALL PER SECTION 5.6.4

ALUMINUM DECK 13'-0"

ALUMINUM DECK 13'-0"



- GENERAL NOTES
1. ALL EXTERIOR DIMENSIONS ARE TO OUTSIDE FACE OF SHEATHING OR OUTSIDE FACE OF MASONRY UNLESS OTHERWISE NOTED.
 2. VERIFY SLAB EDGE LOCATIONS, BRICK AND MASONRY FACE WITH ARCHITECTURAL AND STRUCTURAL DETAILS.
 3. INTERIOR WALL DIMENSIONS ARE TO CENTERLINE OF WALL UNLESS NOTED OTHERWISE. WHERE CLEAR DIMENSIONS ARE INDICATED, THESE DIMENSIONS SHALL BE MAINTAINED.
 4. ROOM OPENING DIMENSIONS OF WINDOWS, DOORS AND OTHER COMPONENTS SHALL BE VERIFIED WITH ACTUAL DIMENSIONS OF SUPPLIED COMPONENTS.
 5. DO NOT SCALE DRAWINGS IF DIMENSION NOT CLEAR, NOT GIVEN, OR PRESENTS A CONFLICT. NOTIFY ARCHITECT IMMEDIATELY.
 6. NOTIFY ARCHITECT IMMEDIATELY IF DIMENSIONAL DISCREPANCIES OCCUR.
 7. PROVIDE CONTROL JOINTS AT FLOOR AND WALLS PER MANUFACTURER'S RECOMMENDATIONS AND SPECIFICATIONS.
 8. INSTALL ALL ELEMENTS AND MATERIALS TO MEET MANUFACTURER'S RECOMMENDATIONS. IF ARCHITECTURAL DRAWINGS CONFLICT WITH MANUFACTURER'S RECOMMENDATIONS, NOTIFY ARCHITECT IMMEDIATELY.
 9. PROVIDE SEALED JOINTS AT MOISTURE CONTROL MEMBRANE.
 10. PROTECT MATERIALS ON-SITE DURING CONSTRUCTION FROM MOISTURE AND OTHER DAMAGE.
 11. INSULATE GAPS AT WOOD FRAMING BETWEEN WINDOWS, DOORS AND OTHER MATERIALS WITH INSULATION.
 12. WOOD IN CONTACT WITH CONCRETE TO BE TREATED WOOD.
 13. PROVIDE WOOD BLOCKING BEHIND ALL WALL NAILS CASEWORK, MILLWORK, SHELVING, FIXTURES AND ELEMENWARE AS NOTED OR AS NECESSARY BASED ON MANUFACTURER'S RECOMMENDATIONS.
 14. GENERAL CONTRACTOR TO VERIFY WITH MECHANICAL, ELECTRICAL AND PLUMBING SUBCONTRACTORS AS TO LOCATIONS FOR EQUIPMENT, FIXTURES, RISERS, SUCTORS, ETC. NOTIFY ARCHITECT IMMEDIATELY IF ANY CONFLICTS BETWEEN ARCHITECTURAL DRAWINGS AND M.E.P. SUBCONTRACTORS.
 15. CONTRACT RATED ASSEMBLIES AT ALL LOCATIONS INDICATED IN CODE REVIEW AND PER DRAWINGS, INCLUDING ALL CEILING AND FLOOR ASSEMBLIES. PROVIDE RATED ENCLOSURE WHERE FIXTURES OR EQUIPMENT PENETRATES THE ASSEMBLY.
 16. PROVIDE FIRE STOPPING AT ALL PENETRATIONS OF RATED ASSEMBLIES. ANY DISTURBANCE OF ASSEMBLY SHALL BE REPAIRED.
 17. FIRE-BLOCK ALL CONCEALED WALL SPACES, INCLUDING 10" VERTICALLY AND HORIZONTALLY.
 18. DO NOT PENETRATE STAIR OR ELEVATOR ENCLOSURE UNLESS DRYING STAIR OR ELEVATOR.
 19. FINISH FLOOR ELEVATION CHANGE AT DOORS OR MATERIAL TRANSITIONS NOT TO EXCEED 1/2".
 20. SEE SHEET A01 FOR ALL WALL TYPES.

- GENERAL UNIT PLAN NOTES
1. ALL INTERIOR WALL ARE TYPE 'K' UNLESS OTHERWISE NOTED.
 2. NOTIFY ARCHITECT IMMEDIATELY IF DIMENSIONAL DISCREPANCIES OCCUR.
 3. REFER TO GENERAL BUILDING CLASSES FOR THE FOLLOWING:
 - EXTERIOR WALL TYPES
 - EXTERIOR WALL CONNECTIONS
 - EXTERIOR CORNER WALL TYPES
 4. PROVIDE DOOR & SHELVE AT ALL CLOSETS UNLESS NOTED OTHERWISE.
 5. PROVIDE BASE AND FLOOR FINISHES BEHIND REFRIGERATION AND AT REASONABLE CABINETS.
 6. CALCULATE WAINUT COUNTERS TO WALL AND ALL PLUMBING FIXTURES TO WALL COUNTER AND FLOOR.
 7. MAINTAIN 4" CLEAR IN FRONT OF COUNTER TO ADJACENT SURFACE OR OBSTRUCTION.
 8. PROVIDE WOOD BLOCKING BEHIND ALL WALL NAILS CASEWORK, MILLWORK, SHELVING, FIXTURES & ELEMENWARE AS NOTED OR AS NECESSARY BASED ON MANUFACTURER'S RECOMMENDATIONS.
 9. GENERAL CONTRACTOR TO VERIFY WITH MECHANICAL, ELECTRICAL AND PLUMBING SUBCONTRACTORS AS TO LOCATIONS FOR EQUIPMENT, RISERS, SUCTORS, ETC. NOTIFY ARCHITECT IMMEDIATELY IF ANY CONFLICTS ARE.
 10. VERIFY LOCATIONS OF ALL MECHANICAL AND PLUMBING. IF CONFLICTS ARISE, NOTIFY ARCHITECT IMMEDIATELY.
 11. VERIFY ALL FIXTURES AND EQUIPMENT FIT IN PLANS PRIOR TO INSTALLATION AND THAT FIXTURES MAINTAIN REQUIRED CLEARANCES AND CLEAR AREA AS INDICATED WITH DIMENSIONS OF []
 12. CONTRACT RATED ASSEMBLIES AT ALL LOCATIONS INDICATED IN CODE REVIEW & PER DRAWINGS INCLUDING ALL CEILING & FLOOR ASSEMBLIES.
 13. PROVIDE FIRE STOPPING AT ALL PENETRATIONS OF RATED ASSEMBLIES. ANY DISTURBANCE OF ASSEMBLY SHALL BE REPAIRED.
 14. SEE SHEET A01 FOR ALL WALL TYPES.
 15. FIELD VERIFY ALL DIMENSIONS & ADJUST CABINET DESIGN TO FIT FIELD CONDITIONS. PROVIDE FINISHED END PANELS TO MATCH CABINETS AT ALL CONNECTIONS. PROVIDE ALLOWANCES WHERE NECESSARY TO ALLOW PROPER CABINET CLEARANCES & TO FILL OPENING COMPLETELY.
 16. PROVIDE CABINET MANUFACTURER'S STANDARD TOE KICK AT CABINETS, STAIN TO MATCH CABINET.
 17. RSP REMOVED SHIRTS/POET.
 18. ALL SHELLING UNITS TO HAVE CONDENSED WALL BLOCKING PROVIDED FOR FUTURE GRAIN BARS. SEE DETAIL SA009.
 19. PAINT ELECTRICAL BOX TO MATCH WALL.
 20. CONTRACTOR TO VERIFY CLEARANCES TO ALL COMBUSTIBLE MATERIALS FROM PENETRATIONS OF MECHANICAL EQUIPMENT AND MEET MANUFACTURER'S RECOMMENDATIONS.

SEE ELEVATIONS FOR PLACEMENT OF EXTERIOR MATERIAL. MATERIALS CHANGE FROM CEMENT SIDING TO METAL SIDING.



Collage
ARCHITECTURE
www.collage-arch.com

Corcoran Triangle
MINNEAPOLIS MINNESOTA

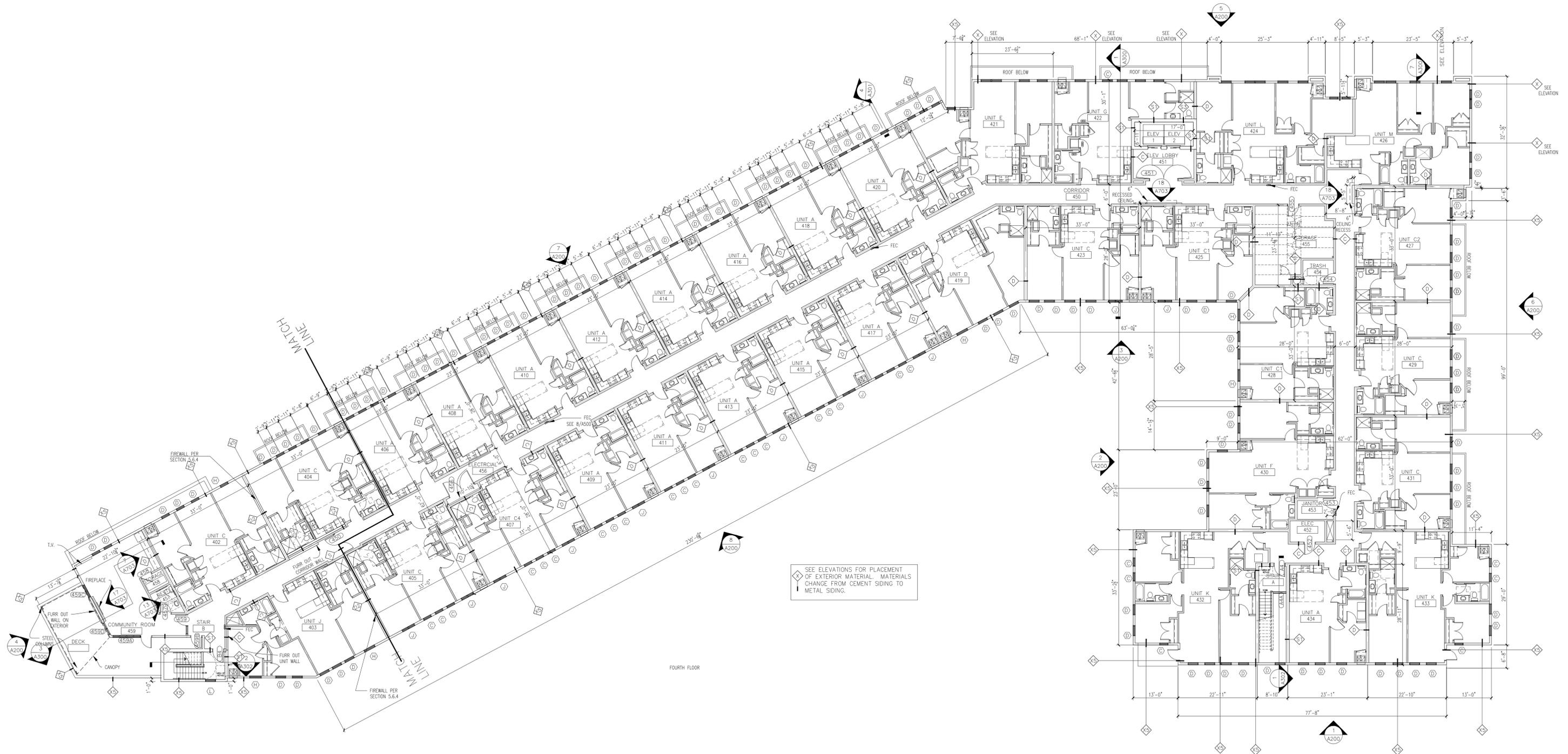
OWNER: Corcoran Triangle, LLC

DATE: 1/2018

DESIGNER: Collage Architecture
3700 University Ave Suite 200-4
St. Paul, Minnesota 55155

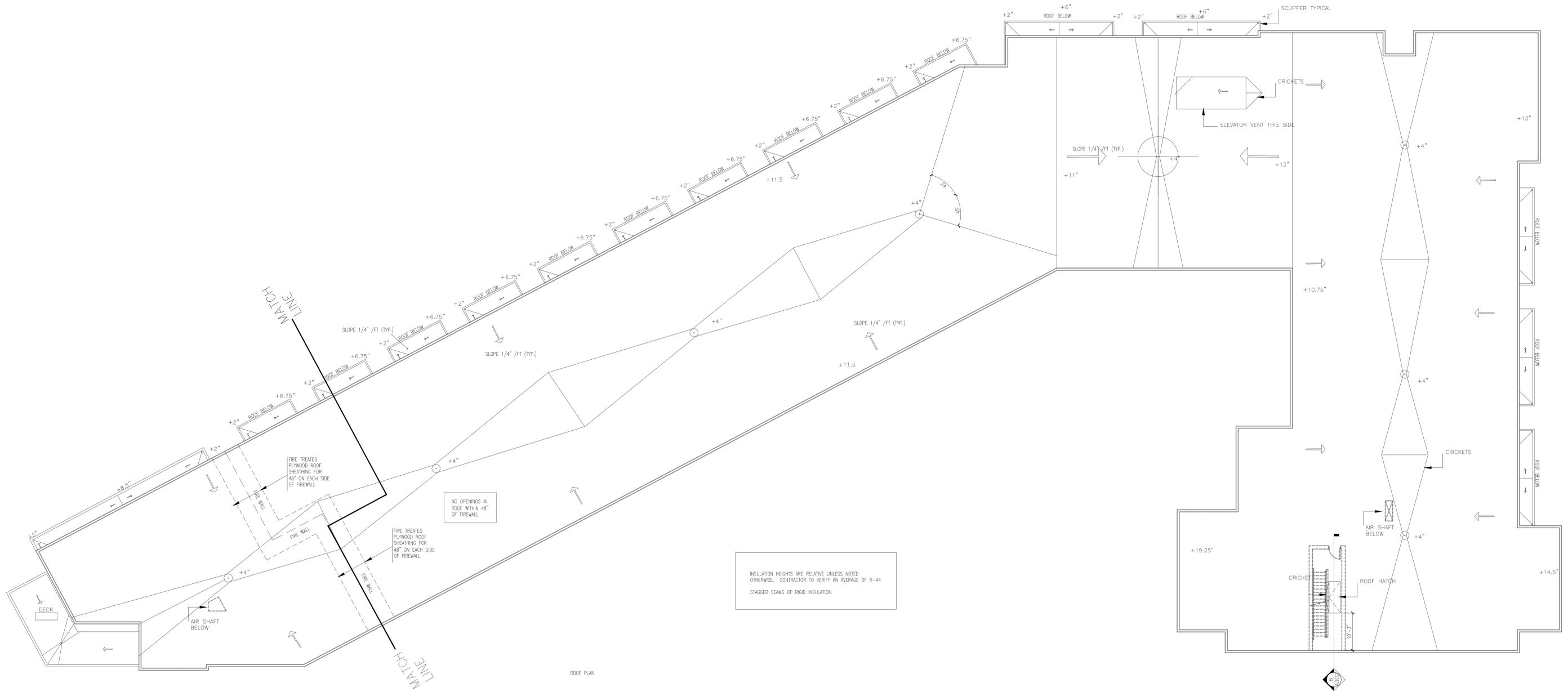
ARCHITECT: Collage Architecture
3700 University Ave Suite 200-4
St. Paul, Minnesota 55155

PROJECT NO.	02-2018-0001
CLIENT FILE	02-2018-0001
DESIGN BY	S.B. J.O.
CHECK BY	02/2018
DATE	02/2018
PROJECT NAME	
SHEET NO.	
FOURTH FLOOR PLAN	
A-103	



SEE ELEVATIONS FOR PLACEMENT OF EXTERIOR MATERIAL. MATERIALS CHANGE FROM CEMENT SIDING TO METAL SIDING.

FOURTH FLOOR



ROOF PLAN



Corcoran Triangle
3100 24th Avenue South
SPRINGFIELD, MISSOURI

OWNER: Corcoran Triangle, LLC

DATE: 11/15/11
1. GEORGE COUNTY HEALTH PLAN PRESENTATION AND BIDDING
PROPOSAL BY THE HEALTH PLAN COMMITTEE AND BOARD OF
HEALTH CARE AS A REQUIREMENT TO RE-EVALUATE THE CARE OF
MEMBERS.

PROJECT NO: 1104-0119

Collage | architects

Architect
400 South
WALTON
SPRINGFIELD, MISSOURI 65704

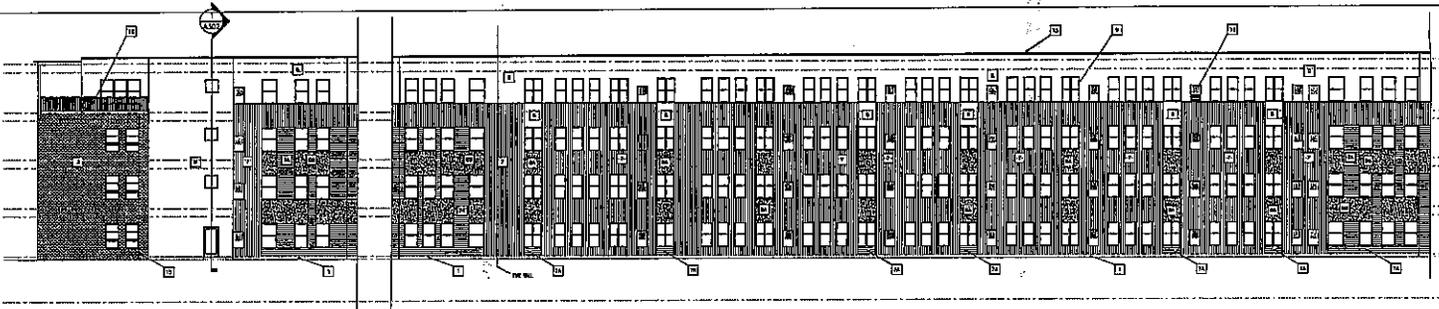
Engineer: Red & Associates
4141 LUTHER
SPRINGFIELD, MISSOURI 65711
A. Paul Edwards, P.E.

SHEET NO: 04.02.001

PROJECT NO.	1104-0119
DATE	11/15/11
DESIGNER	Collage Architects
DRAWN BY	SB, JG
CHECKED BY	
DATE	

ELEVATIONS

A-200



4 WEST ELEVATION
3/2" = 1'-0"

ROOF FINISH
TOP OF ROOF
EL. = 142'-5 1/2"

FOURTH FLOOR
TOP OF FINISH
EL. = 112'-0"

THIRD FLOOR
TOP OF FINISH
EL. = 102'-0"

SECOND FLOOR
TOP OF FINISH
EL. = 92'-0"

TOP OF PRECAST
FIRST FLOOR
EL. = 82'-0"

TOP OF SLAB
GARAGE LEVEL
EL. = 72'-0"



3 EAST ELEVATION
3/2" = 1'-0"

ROOF FINISH
TOP OF ROOF
EL. = 142'-5 1/2"

FOURTH FLOOR
TOP OF FINISH
EL. = 112'-0"

THIRD FLOOR
TOP OF FINISH
EL. = 102'-0"

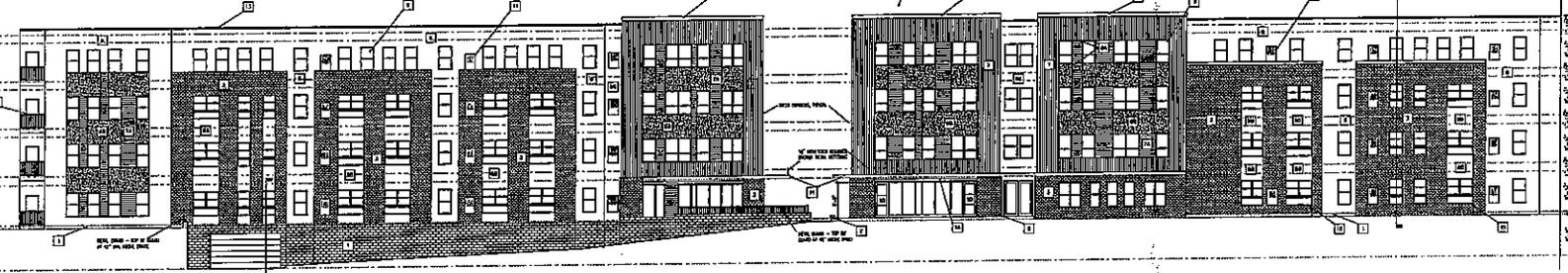
SECOND FLOOR
TOP OF FINISH
EL. = 92'-0"

TOP OF PRECAST
FIRST FLOOR
EL. = 82'-0"

TOP OF SLAB
GARAGE LEVEL
EL. = 72'-0"

MATERIAL INDEX

- 1 UNFINISHED BLOCK
- 2 SANDWICH PANEL
- 3 FACE BRICK
- 4 BUTYDOL
- 5 BRICK GILL
- 6 BRICK/GROUT/SEAL
- 7 METAL WORK
- 8 EXPOSED BRASS LAP JOINT
- 9 CONCRETE BOARD FLAT PANEL
- 10 STAINLESS STEEL
- 11 PAVED PAVEMENT
- 12 MECHANICAL DUCTWORK / CHILL
- 13 PREPARED METAL CLADDING
- 14 PREPARED METAL TRIM
- 15 METAL CANOPY
- 16 CONCRETE JOIST



1 SOUTH ELEVATION
3/2" = 1'-0"

2 SOUTHWEST ELEVATION
3/2" = 1'-0"

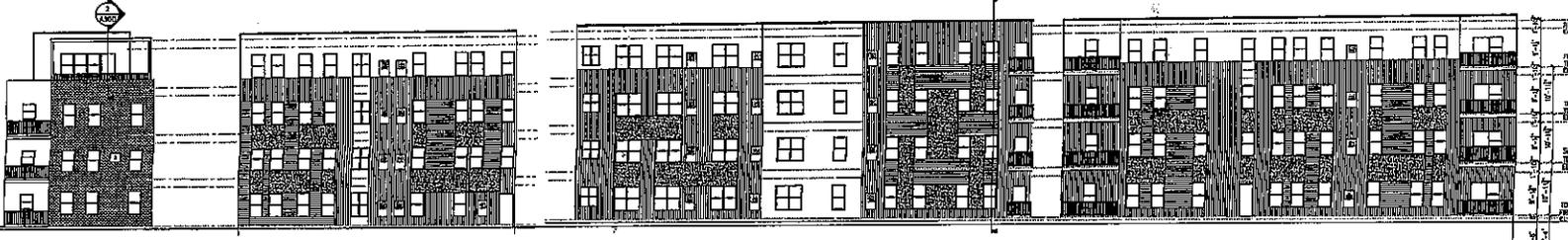
ROOF FINISH
TOP OF ROOF
EL. = 142'-5 1/2"

THIRD FLOOR
TOP OF FINISH
EL. = 102'-0"

SECOND FLOOR
TOP OF FINISH
EL. = 92'-0"

TOP OF PRECAST
FIRST FLOOR
EL. = 82'-0"

TOP OF SLAB
GARAGE LEVEL
EL. = 72'-0"



5 NORTH ELEVATION
3/2" = 1'-0"

6 SOUTH WEST ELEVATION
3/2" = 1'-0"

4 NORTH ELEVATION
3/2" = 1'-0"

3 SOUTH WEST ELEVATION
3/2" = 1'-0"

ROOF FINISH
TOP OF ROOF
EL. = 142'-5 1/2"

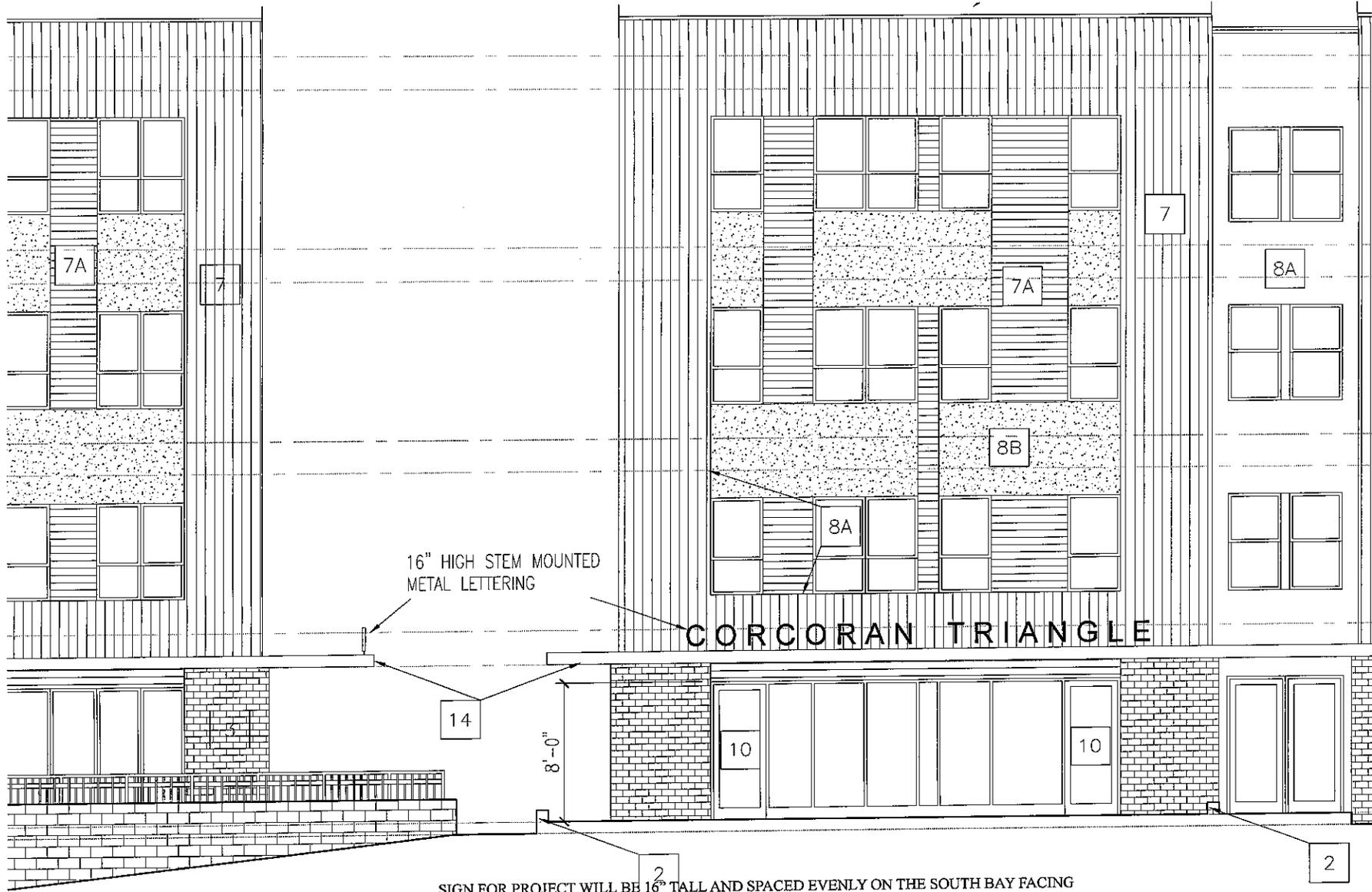
FOURTH FLOOR
TOP OF FINISH
EL. = 112'-0"

THIRD FLOOR
TOP OF FINISH
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TOP OF FINISH
EL. = 92'-0"

TOP OF PRECAST
FIRST FLOOR
EL. = 82'-0"

TOP OF SLAB
GARAGE LEVEL
EL. = 72'-0"



SIGN FOR PROJECT WILL BE 16" TALL AND SPACED EVENLY ON THE SOUTH BAY FACING THE LIGHT RAIL. SPECIFIC TEXT AND FONT MAY CHANGE.

Corcoran Triangle
 Minneapolis, MN



Please provide a materials break down (percentage) per elevation.

West Elevations - Courtyard: (elevations 8/A200 and 3/A200)

Cement board = 26%

Metal = 41.6%

Brick = 5.5%

West Elevation: (elevation 1/A200)

Cement board = 22.2%

Metal = 68.1%

Brick = 0%

East Elevations: (elevations 7/A200 and 5/A200)

Cement board = 34.0%

Metal = 24.7%

Brick = 16.4%

South Elevation: (elevation 6/A200)

Cement board = 27.5%

Metal = 18.7%

Brick = 19.8%

North Elevation: elevation 4/A200)

Cement Board = 13.6%

Metal = 0%

Brick = 62.1 %

North Elevation – Courtyard: (elevation 2/A200)

Cement Board = 29.9%

Metal = 47.5%

Brick = 0 %



EAST PERSPECTIVE



SOUTH EAST PERSPECTIVE



SOUTH CORNER OF PROJECT WALKING ALONG LIGHT RAIL



WEST PERSPECTIVE



SOUTH EAST PERSPECTIVE



www.collage-uds.com



Corcoran Triangle
MINNEAPOLIS MINNESOTA

OWNER Corcoran Triangle, LLC

DATE:
I HEREBY CERTIFY THAT THIS PLAN SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A REGISTERED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.

PETER KEELY
REGISTRATION NO: C-2179

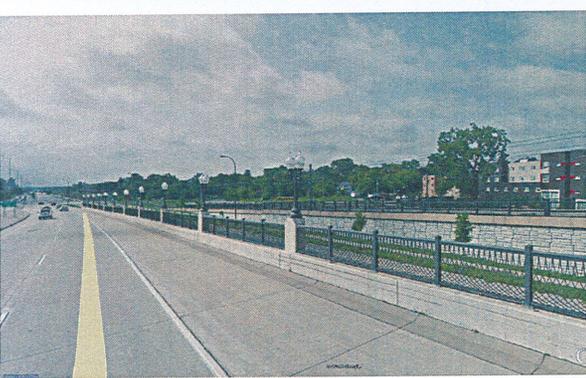
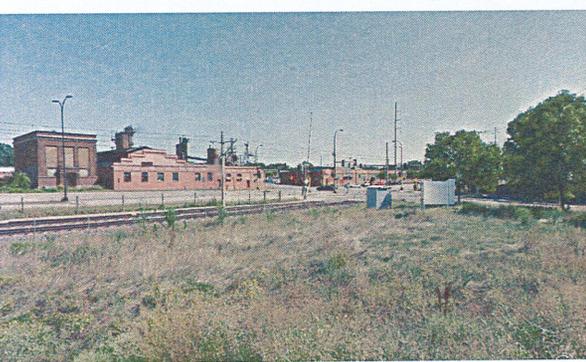
Collage | architects
Architect
Pete Keely
651.472.0050
705 Raymond Avenue
St. Paul, Minnesota 55114

Ericksen Roed & Associates
Structural Engineer
651.251.7570
2550 University Ave Suite 201-S
St. Paul, Minnesota 55114

KEY PLAN
DATE: 06.02.2015

REV	DATE	DESCRIPTION

PROJECT NO: 040
CDD FILE:
DRAWN BY: S.B. J.D.
CHECKED BY:
DATE:



Corcoran Triangle
Minneapolis, MN





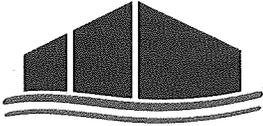












WELLINGTON
MANAGEMENT

Commercial Real Estate

June 18, 2015

Alondra Cano
Ninth Ward Minneapolis City Council Member
350 South 5th Street, Room 307
Minneapolis, MN 55415-1382

Re: Corcoran Triangle Project
3120 24th Avenue South & 3147 24th Avenue South, Minneapolis, MN

Dear Council Member Cano,

Thank you for your support of our Corcoran Triangle project. As a follow up, I wanted to provide you with the following information in connection with our submittal of a General Land Use Application and Subdivision Application.

Description of the Project

Corcoran Triangle will consist of a single, four-story building with 135 units of mixed-income affordable housing on a 2.78 acre urban infill parcel. The building's 140,000 square feet will include a community room, class room and management office. It will also provide valuable benefits to the surrounding community by transforming an empty parcel into a vibrant intersection of affordable housing, supportive services and direct access to multiple forms of transportation, including light rail, bus and bicycle routes. Corcoran Triangle will contain place making elements to connect the development with greater community, such as new public sidewalks, pedestrian-scale lighting, and pedestrian and bicycle connections to the Lake Street LRT Station, located just one block from the site.

Zoning

The developer is seeking front door, side-yard and street width variances to increase the functionality of the building and are design considerations to most effectively solve a difficult triangular shaped site.

Thank you for your continued support and cooperation. Please feel free to reach out to me if you have additional questions.

Sincerely,

Stephen B. Wellington, Jr.
President



CORCORAN neighborhood
Strengthening and protecting the unique character of our neighborhood

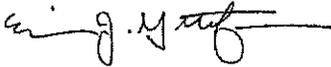
June 25, 2014

Noel Nix
Wellington Management
1625 Energy Park Dr #100
St Paul, MN 55108

Dear Noel,

At its June 19, 2014 meeting, the Board of Directors of the Corcoran Neighborhood Organization voted to support the Corcoran Triangle project at 3120 24th Ave S as presented by Wellington Management on June 5, 2014 as it is consistent with the Corcoran Midtown Revival Plan and neighborhood policies related to sustainability, parking, and public realm.

Sincerely,



Eric Gustafson
Executive Director
Corcoran Neighborhood Organization



November 6, 2014

Eric Gustafson, Executive Director
Corcoran Neighborhood Organization
34521 Cedar Avenue South
Minneapolis, MN 55407

Re: Corcoran Triangle Project

Dear Eric,

We are pleased to report that the City of Minneapolis has approved our request for \$415,000 in Affordable Housing Trust Fund dollars for Corcoran Triangle. In addition, last week the board of the Minnesota Housing Finance Agency approved funding of \$2.35 million for this project. This leaves us with a gap of \$2.5 million in the total development cost of this \$23 million 135 unit apartment development.

We plan to apply for approximately \$2,000,000 in additional financial assistance from the City of Minneapolis. This request will rely on tax increment financing (TIF). We also intend to apply to Hennepin County in the spring of 2015 for approximately \$500,000 in financial assistance from the county's Affordable Housing Incentive Fund. These are the last two pieces of funding that are needed to break ground in the spring of 2015. We have always intended to request TIF from the City once the other funding was in place.

Without the TIF, the project cannot happen. The City's decision to use TIF financing is directly informed by strong support from community organizations like CNO. We appreciate all the support that CNO has provided over the years. We need to ask for your support once again. We really feel that 2015 will be the year that this important project finally gets off the ground. Specifically, I would like to ask for your active support in advocating for the TIF funding with Councilmember Cano and with City staff.

As always, we stand ready to answer any questions and to address any concerns from you or other members of the Corcoran Neighborhood Organization.

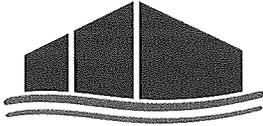
Thank you for cooperation and assistance.

Sincerely,

A handwritten signature in black ink that reads "SB Wellington".

Stephen B. Wellington, Jr.
President

Cc: Becky Landon, Alison Newton, David Bergstrom



WELLINGTON MANAGEMENT

Commercial Real Estate

June 18, 2015

Eric Gustafson, Executive Director
Corcoran Neighborhood Organization
34521 Cedar Avenue South
Minneapolis, MN 55407

Re: Corcoran Triangle Project
3120 24th Avenue South & 3147 24th Avenue South, Minneapolis, MN

Dear Eric,

Thank you for the support you have given the Corcoran Triangle Project. As a follow up to my letter dated November 6, 2014 outlining our Corcoran Triangle Project, I wanted to provide you with the following information in connection with our submittal of a General Land Use Application and Subdivision Application.

Description of the Project

Corcoran Triangle will consist of a single, four-story building with 135 units of mixed-income affordable housing on a 2.78 acre urban infill parcel. The building's 140,000 square feet will include a community room, class room and management office. It will also provide valuable benefits to the surrounding community by transforming an empty parcel into a vibrant intersection of affordable housing, supportive services and direct access to multiple forms of transportation, including light rail, bus and bicycle routes. Corcoran Triangle will contain place making elements to connect the development with the greater community, such as new public sidewalks, pedestrian-scale lighting, and pedestrian and bicycle connections to the Lake Street LRT Station, located just one block from the site.

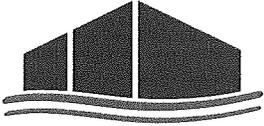
Zoning

The developer is seeking front door, side-yard and street width variances to increase the functionality of the building and are design considerations to most effectively solve a difficult triangular shaped site.

Thank you for your support and cooperation.

Sincerely,

Stephen B. Wellington, Jr.
President



WELLINGTON
MANAGEMENT

Commercial Real Estate

June 18, 2015

Melanie Majors, Executive Director
Joe Sturm, Housing and Environment Coordinator
Longfellow Community Council
2727 26th Avenue South
Minneapolis, MN 55406

Re: Corcoran Triangle Project
3120 24th Avenue South & 3147 24th Avenue South, Minneapolis, MN

Dear Ms. Majors and Mr. Sturm,

We are the developer for the newly opened affordable senior housing project – Lake Street Station located at 2220 E. Lake Street and for a current affordable housing development - Corcoran Triangle located in the Corcoran neighborhood near the border of the Longfellow neighborhood. We are writing to advise you of our project in connection with our submittal of the general land use and subdivision applications. At this time, no further action on this matter is required, unless you have any questions about the project.

The project consists of a single, four-story building with 135 units of mixed-income affordable housing on a 2.78 acre urban infill parcel located at 3120 24th Avenue South & 3147 24th Avenue South, Minneapolis. The building's 140,000 square feet will include a community room, class room and management office. It will also provide valuable benefits to the surrounding community by transforming an empty parcel into a vibrant intersection of affordable housing, supportive services and direct access to multiple forms of transportation, including light rail, bus and bicycle routes. Corcoran Triangle will contain place making elements to connect the development with greater community, such as new public sidewalks, pedestrian-scale lighting, and pedestrian and bicycle connections to the Lake Street LRT Station, located just one block from the site.

The developer is seeking front door, side-yard and street width variances to increase the functionality of the building and are design considerations to most effectively solve a difficult triangular shaped site.

If you have any questions regarding the Corcoran Triangle Project, please contact Dave Bergstrom at dbergstrom@wellingtonmgt.com or 651-292-9844.

Sincerely,

Stephen B. Wellington, Jr.
President