



CPED STAFF REPORT

Prepared for the City Planning Commission

CPC Agenda Item #9
 April 13, 2015
 BZZ-7012

LAND USE APPLICATION SUMMARY

Property Location: 701 7th Street North & 618 7th Avenue North
Project Name: The Lock Up Self Storage
Prepared By: [Becca Farrar-Hughes](#), Senior City Planner, (612) 673-3594
Applicant: Lock Up Minneapolis 701, LLC
Project Contact: Beck Law Office, Attn: Peter Beck
Request: To allow a new 4-story self-service storage facility.
Required Applications:

Conditional Use Permit	To allow an accessory surface parking lot in the DP (Downtown Parking) Overlay District. Both properties are zoned I2 (Medium Industrial) District and are located in the DP (Downtown Parking) Overlay District;
Variance	To reduce the minimum off-street parking requirement;
Variance	To reduce the minimum off-street loading requirement;
Site Plan Review	To allow a new 4-story self-service storage facility.

SITE DATA

Existing Zoning	I2 (Medium) District DP (Downtown Parking) Overlay District
Lot Area	37,088 square feet / .85 acres
Ward(s)	5
Neighborhood(s)	North Loop Neighborhood Association
Designated Future Land Use	Transitional Industrial
Land Use Features	The site is located in Downtown which is a designated Growth Center. There are no other designated land use features immediately adjacent or that include the subject site. The property is located within one block of the future Bottineau LRT line and approximately two blocks from the future Southwest LRT line.
Small Area Plan(s)	North Loop Small Area Plan

BACKGROUND

SITE DESCRIPTION AND PRESENT USE. The subject parcels are occupied by a vacant single-story industrial building constructed in 1961 that is approximately 17,500 square feet in size, with 30 accessory surface parking spaces. The property has frontage on three public streets including 7th Street North, Oak Lake Avenue North and 7th Avenue North.

Date Application Deemed Complete	March 11, 2015	Date Extension Letter Sent	N/A
End of 60-Day Decision Period	May 10, 2015	End of 120-Day Decision Period	N/A

SURROUNDING PROPERTIES AND NEIGHBORHOOD. The properties surrounding the site are predominantly zoned I2 with the exception being the triangular-shaped Wells Fargo Brank property located to the south east of the subject site which is zoned C2. The uses within the immediate area are primarily industrial and commercial uses. To the west of the site across Interstate 94, the properties in Heritage Park are primarily residential with residential zonings.

PROJECT DESCRIPTION. The applicant proposes to demolish the existing building in order to construct a new 4-story or 53 foot tall, 95,640 square foot, self-service storage facility that includes 11 off-street parking spaces on the properties located at 701 7th Street North and 618 7th Avenue North. The properties are both zoned I2 (Medium Industrial) District and are located in the DP (Downtown Parking) Overlay District. The proposed use as a self-service storage facility is permitted in all industrial districts. The 11-space accessory surface parking lot requires a conditional use permit in the DP Overlay District. Further, the applicant proposes to vary both the off-street parking requirement and off-street loading requirement on the site. The proposed building triggers site plan review.

The site would be accessed off of 7th Avenue North and the principal entrance to the facility would be oriented towards 7th Street North. The exterior materials proposed on the building include precast concrete panels, precast brick panels, various types of metal panels, metal elements, overhead doors as well as clear and spandrel glass. The office for the proposed use would total approximately 700 square feet and the retail/café area that may be developed in the future is shown at 1,600 square feet. The applicant has indicated that the retail/café area would be used as extension of the office area until the demand develops for a retail or café tenant. The applicant has further stated that if they are unable to find a retail/café tenant to fill the space after five or six years, that they would probably convert the space to storage, but continue to market it to retail/café users.

The applicant has attended two City Planning Commission Committee of the Whole meetings. The project was originally discussed in December of 2014. The applicant was encouraged to completely redesign the site based on CPED Staff and Planning Commission concerns. The applicant made slight alterations and attended a second meeting in January of 2015. At that meeting, both CPED Staff and the Planning Commission continued to have strong concerns regarding the overall design and placement of the proposed building. Since that meeting the applicant has eliminated several applications and reworked the site plan to access the site off of 7th Street North as required by Public Works and further modified the elevations of the proposed building.

RELATED APPROVALS. Not applicable.

PUBLIC COMMENTS. Staff has received official correspondence from the North Loop Neighborhood Association which is attached for reference. No additional correspondence was received prior to the printing of this report. Any additional correspondence received before the public meeting will be forwarded on to the Planning Commission for consideration and included as part of the public record.

ANALYSIS

CONDITIONAL USE PERMIT

The Department of Community Planning and Economic Development has analyzed the application to allow an 11 space accessory surface parking lot in the DP Overlay District based on the following findings:

1. *The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

Allowing an 11-space accessory surface parking lot in the DP Overlay District would not be detrimental to or endanger the public health, safety, comfort or general welfare. As previously noted, there are currently 30 off-street parking spaces on site that are accessory to the existing single-story building. While Staff would prefer that no surface parking be provided on site, the demand for the use is such that providing structured parking would not be practical in this circumstance and further, the applicant is not proposing to exceed the 20 space limitation that exists in the DP Overlay District despite the fact that technically the Zoning Code requires more spaces than what are being proposed.

2. *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

Allowing an 11-space accessory surface parking lot on the premises to serve the use would not be expected to be injurious to the use and enjoyment of other property in the vicinity nor would it impede on possible future development if landscaped to the standards of Chapter 530. With the exception of one off-street parking space, none of the proposed parking immediately fronts along a public street, and as previously noted, the applicant is attempting to minimize the off-street parking provided on site based on the demand they experience at their other facilities.

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

Currently the site has several access points. The applicant had originally proposed to access the site off of 7th Street North but Public Works would not allow the access point given that there are numerous multi-modal transit oriented projects planned for 7th Street North in the near future including Arterial Bus Rapid Transit (ABRT), associated bus stations, protected bike lanes and other potential Metro Transit related projects. As such the applicant is proposing to access the site via one curb cut off of 7th Avenue North.

The Public Works Department has reviewed the preliminary plan and will review the final plan for compliance with standards related to access and circulation, drainage, and sewer/water connections. The applicant would be required to work closely with the Public Works Department, the Plan Review Section of CPED and the various utility companies during the duration of the development should the plan be approved. This would be required to ensure that all procedures are followed in order to comply with city and other applicable requirements.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

Chapter 541, Off-Street Parking and Loading would typically require one off-street parking space per 500 square feet of gross floor area of office, sales, or display area in excess of 4,000 square feet (minimum of 4 spaces) plus one space per 3,000 square feet of gross floor area of warehousing up to 30,000 square feet plus one space per 5,000 square feet of gross floor area of warehousing over 30,000 square feet. However, the property is located in the DP Overlay District and per Section 551.765 all other uses not specifically identified are required to provide not less than one parking space for each 4,000 square feet of gross floor in excess of 4,000 square feet. The applicant is proposing to construct a 95,640 square foot building on the premises; as such 23 off-street parking spaces would be required. The applicant is proposing a total of 11 off street parking spaces which requires a variance. See the variance findings listed below. Based on Staff's understanding of the parking demand and operations at similar facilities, measures have been provided in regard to

minimizing traffic congestion from a parking perspective as the applicant would be providing adequate off-street parking for the proposed development.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

According to The Minneapolis Plan for Sustainable Growth, the subject parcel is located within an area designated as transitional industrial. Industrial areas located outside of Industrial Employment Districts are labeled “transitional” since they may eventually evolve to other uses compatible with surrounding development. Although they may remain industrial for some time, they do not have the same level of policy protection as areas within industrial districts. Industrial building design in the City is expected to adhere to the same principles as other development in having adequate windows, quality materials, architectural features and green space. Also, pedestrian connections between the industrial building and the sidewalk via walkways, and entrances must be oriented to the street.

The site is located in Downtown which is a designated Growth Center. There are no other designated land use features immediately adjacent or that include the subject site. However, the property is located within one block of the future Bottineau LRT line and approximately two blocks from the future Southwest LRT line. The properties surrounding the site are predominantly zoned I2 with the exception being the triangular-shaped Wells Fargo Bank property located to the south east of the subject site which is zoned C2. The uses within the immediate area are primarily industrial and commercial uses. To the west of the site across Interstate 94, the properties in Heritage Park are primarily residential with residential zonings. The proposed development including the proposed I1-space surface parking lot in the DP Overlay District would be consistent with the following general land use policies of *The Minneapolis Plan for Sustainable Growth* provided the conditions of approval are implemented into the final design of the project:

- **Land Use Policy 1.1:** “Establish land use regulations to achieve the highest possible development standards, enhance the environment, protect public health, support a vital mix of land uses, and promote flexible approaches to carry out the comprehensive plan.”

(1.1.5) “Ensure that land use regulations continue to promote development that is compatible with nearby properties, neighborhood character, and natural features; minimizes pedestrian and vehicular conflict; promotes street life and activity; reinforces public spaces; and visually enhances development.”

- **Land Use Policy 1.3:** Ensure that development plans incorporate appropriate transportation access and facilities, particularly for bicycle, pedestrian, and transit.

(1.3.1) “Require safe, convenient, and direct pedestrian connections between principal building entrances and the public right-of-way in all new development and, where practical, in conjunction with renovation and expansion of existing buildings.

- **Urban Design Policy 10.12:** Design industrial uses with appropriate transitions and other design features which minimize negative impacts on surrounding residential uses.

(10.12.1) “Provide appropriate physical transition and separation using green space, fencing, setbacks or orientation between industrial uses and other surrounding uses.

(10.12.2) “Encourage site planning for new developments that orients the “back” of proposed buildings to the “back” of existing development.

(10.12.5) “Promote quality design and building orientation of industrial development that is appropriate with the surrounding neighborhoods.

(10.12.6) Use the site plan review process to ensure that lighting and signage associated with industrial uses do not create negative impacts for residential properties.

- **Urban Design Policy 10.22:** Use Crime Prevention Through Environmental Design (CPTED) principles when designing all projects that impact the public realm, including open spaces and parks, on publicly owned and private land.

(10.22.3) “Design the site, landscaping, and buildings to promote natural observation and maximize the opportunities for people to observe adjacent spaces and public sidewalks.”

(10.22.4) “Provide on-site lighting at all building entrances and along walkways that maintains a minimum acceptable level of security while not creating glare or excessive lighting of the site.”

As previously noted, the proposal is in conformance with the above noted policies and implementation steps of the Comprehensive Plan provided the conditions of approval are implemented into the final design of the project.

There is one additional plan that must be considered when evaluating the proposal. The *North Loop Small Area Plan* was approved in 2010 and covers the subject property. It is officially an update to the *Downtown East/North Loop Master Plan*, adopted by the City in 2003, and builds upon the policy direction of *The Minneapolis Plan for Sustainable Growth*, the City’s comprehensive plan. The update is meant to encapsulate the remainder of the North Loop neighborhood that has not had small area planning. It is meant to articulate a vision for the neighborhood based on existing City policy and input from community stakeholders – both public and private - throughout the planning process.

As noted in the plan, the parcel is designated as Transitional Industrial, and is located in the area designated as Lower Mix, in District B (appropriate building types include housing, commercial, and industrial structures of two to ten stories. This type is generally consistent with the bulk and height of historic warehouse structures in the neighborhood). The Transitional Industrial designation encompasses the entire Lower North Loop west of 7th Street North and east of I-94 with additional parcels on the far west side of the neighborhood. As previously noted, industrial areas located outside of Industrial Employment Districts are labeled “transitional” since they may eventually evolve to other uses compatible with surrounding development. Although they may remain industrial for some time, they will not have the same level of policy protection as areas within industrial districts. Private property in the Lower North Loop is more suburban in nature, with industrial development that is set back from the street with few windows. The development guidelines dictate that any new buildings along should include pedestrian-oriented design features and other plan recommendations to improve connections through the neighborhood.

The Lower Mix district currently is an eclectic mix of Public Works properties, the Minneapolis Farmers Market, large warehouses, private industrial and commercial businesses, and social service housing providers. This plan proposes to continue this diversity of uses. There are numerous development guidelines as outlined in the plan. They reinforce and encourage the site plan review standards pertaining to building form, public realm, etc. The plan calls for the following elements:

- All new buildings to be at least two stories in height.
- In general, buildings should have an urban frontage, which means the building is immediately accessible to the public sidewalk and in some cases have active ground floor uses.
- New industrial buildings should include a principal entrance facing the sidewalk without the impediment of a surface parking lot. Parking for new structures should be underground or hidden behind structures.
- Edges of parking areas need to be clearly distinguished from the public right of way with a combination of fencing and landscaping. Facades should include prominent front entrances and abundant window glass.
- Principal entrances of buildings – commercial, industrial, residential - should face the street. They should be clearly defined and emphasized through the use of architectural design features.
- In the neighborhood broadly, emphasis for streetscape should initially be placed on streets that connect through and to adjacent areas.

It is CPED Staff's position that should the recommended conditions of approval be implemented, that the proposed development that includes the proposed 11-space surface parking lot in the DP Overlay District would be in conformance with the adopted *North Loop Small Area Plan*.

6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

If the requested land use/zoning applications are approved, and the conditions of approval implemented, the proposal would comply with the applicable provisions of the I2 District and DP Overlay District.

VARIANCES

The Department of Community Planning and Economic Development has analyzed the following variance applications: (1) To reduce the minimum off-street parking requirement; (2) To reduce the minimum off-street loading requirement; based on the following [findings](#):

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

Minimum off-street parking requirement: The applicant is proposing to construct a 95,640 square foot self-service storage building on the premises; as such 23 off-street parking spaces would be required. The applicant is proposing a total of 11 off-street parking spaces which requires a variance. Practical difficulties exist in complying with the ordinance because of the demonstrated parking demand and operations at similar facilities which do not necessitate the provision of additional off-street parking spaces. Further, the location of the site within the DP Overlay District encourages the minimization of surface parking on the premises. The applicant would be providing adequate off-street parking for the proposed development.

Minimum off-street loading requirement: The applicant is proposing to construct a 95,640 square foot self-service storage building on the premises. Self-service storage has a high loading requirement and between 60,001 square feet and 100,000 square feet the loading requirement is 3 large loading spaces which dimensionally are 12 feet in width by 50 feet in length. The applicant is proposing to incorporate 3 fully enclosed small loading spaces on the premises in lieu of the 3 large loading spaces. This is due in part because large trucks would not be able to maneuver on the

premises given the layout and access to the site. The applicant has stated that access to the site would be limited to trucks that are 30 feet in length. The driveway entrance to the facility would be signed to that effect and the customer contracts would include that notice. At 30 feet, the trucks would still be fully located within the building.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

Minimum off-street parking requirement: Allowing a reduction in the accessory off-street parking requirement for the proposed development is a reasonable request and is keeping with the spirit and intent of the ordinance and comprehensive plan. The applicant is providing adequate parking based on their experience and demand at other similar facilities. Further, reducing the provision of surface off-street parking in this location is consistent with the objectives of the DP Overlay District.

Minimum off-street loading requirement: Allowing a reduction in the off-street loading requirement from 3 large spaces to 3 small spaces is a reasonable request and is keeping with the spirit and intent of the ordinance and comprehensive plan. The applicant has committed to limiting access to the site so that no trucks greater than 30 feet in length can access the site due to maneuvering restrictions on the premises. Given this commitment, allowing 3 small spaces in lieu of 3 large spaces is appropriate in this specific circumstance.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Minimum off-street parking requirement: The proposal to reduce the minimum off-street parking requirement on the premises would not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. The reduction in off-street parking would further not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties. The 11 spaces that are proposed are adequate based on the information provided by the applicant regarding the operation and functionality of the facility.

Minimum off-street loading requirement: The proposal to reduce the minimum off-street loading requirement on the premises would not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. The reduction in off-street loading would further not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties. The provision of 3 small spaces in lieu of the 3 large spaces are adequate based on the information provided by the applicant regarding the operation and functionality of the facility as well as the restrictions that would be imposed regarding the types of trucks that can service and deliver to the site.

SITE PLAN REVIEW

The Department of Community Planning and Economic Development has analyzed the application based on the required [findings](#) and [applicable standards](#) in the site plan review chapter:

I. Conformance to all applicable standards of Chapter 530, Site Plan Review.**Building Placement and Design – Requires alternative compliance**

- The proposed building has frontage on three public streets – 7th Street North, Oak Lake Avenue and 7th Avenue North. The building is configured somewhat awkwardly with the rear of the building located within approximately 1 foot to 6 feet from the property line along 7th Avenue North, approximately 16 feet to 36 feet from the property line along Oak Lake Avenue to accommodate an approximately 1,800 square foot potential outdoor seating area for a future retail/café tenant and only a small corner of the building is located within 8 feet of 7th Street North. Alternative compliance is necessary for building placement along two of the three street frontages. CPED Staff would recommend that the Planning Commission grant alternative compliance along 7th Street North, and adjacent to Oak Lake Avenue provided the potential outdoor seating area is reduced in size. The building could be redesigned to comply with the building placement provision along 7th Street North specifically by locating the loading and office area directly adjacent to the public street, but it would require that the site be entirely reconfigured. The loading and office areas could be designed to have transparency through the site. As previously noted, the size of the outdoor space adjacent to the public street is too large, and without an identified tenant and the possibility that the retail/café space would never be filled, the area would be ultimately be inactive and no seating ever installed. Staff would recommend that the Planning Commission reduce the area proposed at 1,800 square feet to half of that amount at 900 square feet.
- The area between the building and lot lines along all three street frontages includes various amenities – landscaping and potential seating should there ever be a café/retail tenant along Oak Lake Avenue and 7th Avenue North, and parking along 7th Street North. Alternative compliance would be necessary along 7th Street North. Staff would recommend that the Planning Commission grant alternative compliance in this specific circumstance provided the landscaping and screening adjacent to 7th Street North is enhanced and that a decorative fence be installed within the area.
- The principal entrance to the building would face 7th Street North but is recessed approximately 22 to 30 feet from the property line. Further the configuration of the doors where a shared vestibule is used for each space technically results in the building being defined as a shopping center which triggers a Conditional Use Permit (CUP). As noted in the conditions of approval, the office space and retail/café space each require a separate principal entrance facing the public street.
- The on-site accessory surface parking lot is primarily located to the interior of the site but due to the configuration of the building, the accessible parking space is located between the building and the lot line along 7th Street North. Alternative compliance would be necessary. Staff would recommend that the Planning Commission grant alternative compliance in this specific circumstance provided the landscaping and screening adjacent to 7th Street North is enhanced and that a decorative metal fence be installed within the area.
- Provided there is a minimum of a two inch reveal between the panels proposed on the exterior building walls, there would be no blank, uninterrupted walls that exceed 25 feet in length on the building elevations.
- The exterior materials proposed on the building are durable and include precast concrete panels, precast brick panels, various types of metal panels, metal elements, overhead doors as well as clear and spandrel glass.
- The materials and the appearance of the rear and side walls are similar to and compatible with the front of the building.
- No plain face concrete block is proposed.

- Due to this provision: Industrial uses in Table 550-1, Principal Uses in the Industrial Districts, may provide less than thirty (30) percent windows on the walls that face an on-site parking lot, provided the parking lot is not located between the building and a public street, public sidewalk or public pathway; the only elevation that is not meeting the applicable window provisions on the ground floor of the building is the south elevation of the proposed structure. The above listed provision impacts a portion of the north elevation and the west elevation of the building both of which have substantially less windows than required (0% and 8%).
- The south elevation of the proposed structure is not meeting the window requirement as 30% are required and 0% is being provided. This is due to the fact that the applicant is proposing clear glass that looks into display windows, which does not meet the requirements of the site plan review ordinance. Ground-floor windows facing the street must allow views into and out of the building. Shelving or other fixtures cannot block such views. Display windows do not allow views into and out of a building. Alternative compliance is necessary. Staff would recommend that the Planning Commission require compliance as it would be practical to include windows that meet the 30% window requirement on this elevation especially given that much of the north elevation and the west elevation are exempt from the window requirements. For example, the floor plan could be modified to include a corridor along the entire length of the elevation where intermittent activity (customers accessing units, custodial and office workers, etc.) would occur. Further, this is a 218 foot long building wall adjacent to a public street and public sidewalk with no windows that allow views into the building. Where windows (both those that meeting the requirements and those that do not) are located on the ground floor of the building, they are more or less distributed in an even manner and are also somewhat vertical in their proportion. See **Table I**.
- Regarding the floors above the first floor of the building (floors 2 through 4), there are some floors on each elevation that exceed the 10% requirement and others that do not, specifically on the north and west elevations of the building. The windows are even in their distribution and vertical in their proportion. Alternative compliance would be necessary for the 10% window requirement. Staff would recommend that the Planning Commission require compliance with the provision on all elevations, floors 2-4, as applicable in order to provide breaks in the massing that is provided on the exterior building walls. See **Table I**.
- Except for industrial uses in Table 550-1, Principal Industrial Uses in the Industrial Districts, the first floor or ground level of buildings shall be designed to accommodate active functions by ensuring that parking, loading, storage, or mechanical equipment rooms are limited to no more than thirty (30) percent of the linear building frontage along each wall facing a public street, public sidewalk, or public pathway. This standard is not applicable in this circumstance as the use is industrial.
- With a flat roof, the proposed form and pitch of the roof line would be considered compatible.
- There is no parking garage proposed. All on-site parking is located in a surface parking lot.

Table I. Percentage of Windows Required for Elevations Facing a Public Street, Sidewalk, Pathway, or On-Site Parking

	Code Requirement	Proposed
Oak Lake Ave (East) -1 st Floor	30% minimum / 230 sq. ft.	47% / 364 sq. ft.
7th Street North facing public street (74 feet of North elevation)-1 st Floor	30% minimum / 178 sq. ft.	48% / 287 sq. ft.
Facing on-site parking lot (119 feet of North elevation)– 1 st Floor	Less than 30% minimum / 30% minimum = 286 sq. ft.	8% / 24 sq. ft.
Facing on-site parking lot (West)– 1 st Floor	Less than 30% minimum / 30% minimum = 247 sq. ft.	0% / 0 sq. ft.
7 th Avenue North (South) – 1 st Floor	30% minimum / 523 sq. ft.	0% / 0 sq. ft.
Oak Lake Ave (East) – 2 nd thru 4 th floors	10% minimum	> 10% on 2 nd thru 4 th floors
7th Street North facing public street (74 feet of North elevation)- 2 nd thru 4 th floors	10% minimum	> 10% on 2 nd thru 4 th floors
Facing on-site parking lot (119 feet of North elevation)– 2 nd thru 4 th floors	10% minimum	> 10% on 2 nd floor < 10% on 3 rd and 4 th floors
Facing on-site parking lot (West)– 2 nd thru 4 th floors	10% minimum	< 10% on 2 nd floor > 10% on 3 rd and 4 th floors
7 th Avenue North (South) – 2 nd thru 4 th floors	10% minimum	> 10% on 2 nd thru 4 th

Access and Circulation – Meets requirements with Conditions of Approval

- A well-lit walkway at least 4 feet in width does not connect the buildings and the adjacent public sidewalks to the on-site parking. The only parking space that is connected to the building and the sidewalk via a walkway is the single accessible parking space provided adjacent to 7th Street North. Alternative compliance would be necessary. Staff would recommend that the Planning Commission require compliance with this provision as it would be practical to expect the provision of a walkway to allow customers to have a safe and direct connection from all on-site parking spaces to the principal building entrance.
- No transit shelters are included in the development; however, there is a bus stop located in front of the building along 7th Street North. Further, the site is within walking distance of Target Field Station and Downtown Minneapolis and would be located within one block of the future Bottineau LRT line and approximately two blocks from the future Southwest LRT line.
- There are no surrounding residential uses; however, the development has been designed to minimize conflicts with pedestrian traffic as one curb cut off of 7th Avenue North serves the subject site.
- There is no public alley adjacent to the site as it was previously vacated. The site would be accessed off of 7th Avenue North via a single curb cut.

- The site has been somewhat designed to minimize the use of impervious surfaces as the proposal is close to meeting the 20% landscaping requirement. Much of the site is covered by the proposed building as well as surface parking, driveways and drive-aisles.

Landscaping and Screening – *Requires alternative compliance*

- In general, the composition and location of landscaped areas complement the scale of development and surroundings.
- A 7-foot wide landscaped yard is required between the parking lot and the public street. This would apply for the accessible parking space located adjacent to North 7th Street and for the two parallel spaces on the west side of the building off of 7th Avenue North. The screening requirement is 3 feet in height and 60% opaque and not less than one tree shall be provided for each 25 linear feet of parking lot frontage. Two canopy trees would be required adjacent to 7th Street North and one adjacent to 7th Avenue North. There is a landscaped yard adjacent to 7th Street North that varies in width from 1 foot to 23 feet and an 11 foot landscaped yard adjacent to 7th Avenue North. Proposed screening along both frontages would not meet the requirements. Alternative compliance would be necessary. CPED Staff would recommend granting partial alternative compliance along 7th Street North for the width of the landscaped yard but that the Planning Commission require full compliance with the screening provisions, linear canopy tree provision, as well as install a decorative metal fence as an alternative compliance measure.
- The Zoning Code requires that at least 20 percent of the site not occupied by buildings be landscaped. The total site area is 37,088 square feet. The proposed building footprint is 19,325 square feet. A total of 3,553 square feet of landscaping would be necessary to meet the 20 percent requirement. The applicant is providing 3,250 square feet of landscaping, or 18% of the area not covered by buildings. Alternative compliance would be necessary. Staff would recommend that the Planning Commission require compliance with this provision as it would be practical to landscape the area to the north of the parking lot. This area is currently being used to access the adjacent property which will no longer be permitted. This approximate 8 foot area is not being landscaped.
- The Zoning Code requires that at least 7 canopy trees and 36 shrubs be provided. The applicant is proposing to install 4 canopy trees and 3 ornamental trees in addition to 115 shrubs. Alternative compliance would be necessary for the number of trees. Staff would recommend that the Planning Commission require compliance with the provision that requires 7 canopy trees as there are locations that are devoid of trees on the property. Further, Staff would recommend that the Planning Commission require that the applicant provide a combination of deciduous and evergreen shrubs on the property in order to provide a mix of plantings.
- The corners of the on-site parking lot are landscaped.
- The surface parking lot consists of 11 spaces. Of the 11 parking spaces, 9 of these spaces within the surface parking lot are more than 50 feet from an on-site deciduous tree. Alternative compliance is necessary. Staff would recommend that the Planning Commission require compliance with this provision as it would be practical to plant trees in the applicable landscape areas to meet this requirement.
- No tree islands are proposed.
- There are areas not occupied by buildings, etc., that are not covered by landscaping. Specifically in the area to the north of the parking lot. This area is currently being used to access the adjacent property which will no longer be permitted. This approximate 8 foot area is not being landscaped. Alternative compliance would be necessary. Staff would recommend that the Planning Commission require that this area be landscaped.
- The installation and maintenance of all landscape materials shall comply with 530.210

Table 2. Landscaping and Screening Requirements

	Code Requirement	Proposed
Lot Area	N/A	37,088 sq. ft. / .85 acres
Building footprint	N/A	19,325 sq. ft.
Remaining Lot Area	N/A	17,763 sq. ft.
Landscaping Required	3,553 sq. ft.	3,250 sq. ft.
Canopy Trees (1: 500 sq. ft.)	7 trees	4 canopy and 3 ornamental trees
Shrubs (1: 100 sq. ft.)	36 shrubs	115 shrubs

Additional Standards – Meets requirements with Conditions of Approval

- The on-site parking lot has been designed with segments of discontinuous curbing to provide some on-site retention and filtration of stormwater.
- The proposed structure would be considerably larger and taller than the single-story structure that is currently located on the site. However, it meets the underlying Zoning Code standards pertaining to height and bulk. Staff would not expect the proposal to result in the significant blocking of views. Further, the proposal would not be expected to have impacts on blocking views of important city elements. The structure would also not be expected to have significant shadowing impacts on adjacent properties nor result in significant impacts on light, wind and air in relation to the surrounding area.
- The proposal appears to comply with standards regarding crime prevention through environmental design including but not limited to surveillance, lighting, space delineation, natural access control, etc., with the exception being the entire south elevation of the building. This is due to the fact that the applicant is proposing clear glass that looks into display windows, which does not meet the requirements of the site plan review ordinance. Ground-floor windows facing the street must allow views into and out of the building. Shelving or other fixtures cannot block such views. Display windows do not allow views into and out of a building. Further, this is a 218 foot long building wall adjacent to a public street and public sidewalk with no windows that allow views into and out of the building. Alternative compliance would be necessary for this provision. Staff would recommend as previously noted that the Planning Commission require compliance with the 30% window requirement along the south elevation of the building.
- There are no designated or eligible historic structures on the subject property. The applicant proposes to demolish the existing building located on the property in order to construct the proposed building. The applicant elected not to request a historic review letter prior to submitting formal applications to the City.

2. Conformance with all applicable regulations of the zoning ordinance.

The proposed use is *permitted* in the I2 District. The proposed accessory surface parking that is proposed is *conditional* in the DP Overlay District.

Off-street Parking and Loading – *Requires a variance.*

- Should a retail use ever be included in the ground level of the building, bicycle parking requirements shall apply.

Table 3. Vehicle Parking Requirements Per Use (Chapter 541)

	Minimum Parking Requirement	Applicable Reductions	Total Minimum Requirement	Maximum Parking Allowed	Proposed
Self-service storage facility	1 space per 500 sq. ft. of GFA of office, sales, or display area in excess of 4,000 sq. ft. (min of 4 spaces) + 1 space per 3,000 sq. ft. of GFA of warehousing up to 30,000 sq. ft. + 1 space per 5,000 sq. ft. of GFA of warehousing over 30,000 sq. ft.	Section 551.765 - all other uses not specifically identified are required to provide not less than one parking space for each 4,000 sq. ft. of GFA in excess of 4,000 sq. ft.	23	1 space per 200 sq. ft. of GFA of office, sales, or display area + 1 space per 1,500 sq. ft. of GFA of warehousing up to 30,000 sq. ft. + 1 space per 2,500 sq. ft. of GFA of warehousing over 30,000 sq. ft.	11
Total	27	23	23	57	11

Table 4. Bicycle Parking and Loading Requirements (Chapter 541)

	Minimum Bicycle Parking	Minimum Short-Term	Minimum Long-Term	Proposed	Loading Requirement	Proposed
Self-service storage facility	N/A	N/A	N/A	0	3 large	3 small
Total	0	0	0	0	3 large	3 small

Building Bulk and Height – *Meets requirements.*

Table 5. Building Bulk and Height Requirements

	Code Requirement	Proposed
Lot Area	N/A	37,088 square feet / .85 acres
Gross Floor Area (GFA)	N/A	95,640 sq. ft.
Maximum Floor Area Ratio (GFA/Lot Area)	2.7	2.58
Maximum Building Height	4 stories or 56 feet, whichever is less	4 stories or 53 ft.

Residential Lot Requirements – *Not applicable*

Yard Requirements – *Not applicable.*

Signs – *Meets requirements*

- The sign standards in the industrial district allow the following: 1 square foot of signage per 1 foot of primary building is allowed wall if a freestanding or roof sign is on the same zoning lot. If there is no freestanding or roof sign on the same zoning lot, 1.5 square feet of signage for each 1 foot of primary building is allowed. Sign area based upon one primary building wall shall not be placed on any other wall. The minimum guaranteed sign area is 30 square feet, there is no maximum number of signs within size allocation and the maximum area per sign except projecting signs is 180 square feet. The maximum area per projecting sign is 20 square feet. No backlit signs are permitted and the maximum height of signs is 28 feet.
- The applicant is proposing one wall sign on the east elevation of the building that is 120 square feet in size (7 feet, 10 inches by 15 feet, 4 inches) and located at a height of 22 feet. The east building wall is approximately 96 feet in length. No other signs are proposed on site. Therefore a total of 144 square feet of signage would be permitted. The sign would be internally lit. The proposed sign is in compliance with the sign regulations.
- Signs are subject to Chapters [531](#) and [543](#) of the Zoning Code. Any additional signs on the premises are required to meet the requirements of Chapter 543 of the Zoning Code and they require a separate permit.

Dumpster Screening – *Meets requirements*

- Trash would be stored in the back of the loading area that is located inside of the building.

Screening of Mechanical Equipment – *Meets requirements*

- Rooftop mechanical equipment is proposed and would be screened as required by Section 535.70 of the Zoning Code.

Lighting – *Meets requirements with Conditions of Approval*

- All proposed fixtures on site would be wall mounted down lights. A photometric plan was submitted as part of the application and will be required with the final submittal. Adjustments to the lighting plan will need to be made in order to eliminate spillover onto adjacent properties. Lighting of building façades or roofs shall be located, aimed and shielded so that light is directed only onto the façade or roof. All lighting will need to be downcast and shielded to avoid undue glare. All lighting shall comply with Chapters 535 and 541 and Planning Staff shall review the details of the fixtures in the final review prior to permit issuance.

Impervious Surface Area – *Not applicable*

Specific Development Standards – *Not applicable*

As noted in the above listed response to finding #6 in the conditional use permit application to allow 8 additional supportive housing dwelling units, the existing and proposed development complies with the specific development standards for supportive housing.

DP Overlay District Standards –

- The DP Overlay District was established to preserve significant and useful buildings and to protect the unique character of the downtown area and the mixed-use downtown neighborhoods by restricting the establishment or expansion of surface parking lots and establishing certain minimum and maximum off-street parking standards in the downtown area.
- Accessory parking lots are allowed as a conditional use provided the parking lot is located on the same zoning lot as the principal use served and provided the number of spaces does not exceed 20 spaces.
- According to the standards outlined in Section 551.765, all other uses not specifically identified are required to provide not less than one parking space for each 4,000 square feet of gross floor in excess of 4,000 square feet. The applicant is proposing to include a total of 11 off-street parking spaces on the property which is less than the 23 spaces that are required. A variance has been applied for and the findings are addressed above.
- The proposal complies with all other applicable DP Overlay District provisions provided the conditional use permit to allow the proposed 11 surface parking spaces is approved.

3. Conformance with the applicable policies of *The Minneapolis Plan for Sustainable Growth*.

See the above listed response to finding #5 in the conditional use permit application to allow 11 surface parking spaces in the DP Overlay District. The policies and implementation steps apply to the site plan review application as well.

4. Conformance with applicable development plans or objectives adopted by the City Council.

See the above listed response to finding #5 in the conditional use permit application to allow 11 surface parking spaces in the DP Overlay District. The policies outlined in the *North Loop Small Area Plan* apply to the site plan review application as well.

5. Alternative compliance.

The Planning Commission or zoning administrator may approve alternatives to any site plan review requirement upon finding that project meets one of three criteria required for [alternative compliance](#). Alternative compliance is requested for the following requirements:

- **Building Placement.** The proposed building has frontage on three public streets – 7th Street North, Oak Lake Avenue and 7th Avenue North. The building is configured somewhat awkwardly with the rear of the building located within approximately 1 foot to 6 feet from the property line along 7th Avenue North, approximately 16 feet to 36 feet from the property line along Oak Lake Avenue to accommodate an approximately 1,800 square foot potential outdoor seating area for a future retail/café tenant and only a small corner of the building is located within 8 feet of 7th Street North. Alternative compliance is necessary for building placement along two of the three street frontages. CPED Staff would recommend that the Planning Commission grant alternative compliance along 7th Street North, and adjacent to Oak Lake Avenue provided the potential outdoor seating area is reduced in size. The building could be redesigned to comply with the building placement provision along 7th Street North specifically by locating the loading and office area directly adjacent to the public street, but it would require that the site be entirely reconfigured. The loading and office areas could be designed to have transparency through the site. As previously noted, the size of the outdoor space adjacent to the public street is too large, and without an identified tenant and the possibility that the retail/café space would never be filled, the area would be ultimately be inactive and no seating

ever installed. Staff would recommend that the Planning Commission reduce the area proposed at 1,800 square feet to half of that amount at 900 square feet.

- **Area between the building and lot lines.** The area between the building and lot lines along all three street frontages includes various amenities – landscaping and potential seating should there ever be a café/retail tenant along Oak Lake Avenue and 7th Avenue North, and parking along 7th Street North. Alternative compliance would be necessary along 7th Street North. Staff would recommend that the Planning Commission grant alternative compliance in this specific circumstance provided the landscaping and screening adjacent to 7th Street North is enhanced and that a decorative fence be installed within the area.
- **Location of on-site parking.** The on-site accessory surface parking lot is primarily located to the interior of the site but due to the configuration of the building, the accessible parking space is located between the building and the lot line along 7th Street North. Alternative compliance would be necessary. Staff would recommend that the Planning Commission grant alternative compliance in this specific circumstance provided the landscaping and screening adjacent to 7th Street North is enhanced and that a decorative metal fence be installed within the area.
- **30% window requirement.** The south elevation of the proposed structure is not meeting the window requirement as 30% are required and 0% is being provided. This is due to the fact that the applicant is proposing clear glass that looks into display windows, which does not meet the requirements of the site plan review ordinance. Ground-floor windows facing the street must allow views into and out of the building. Shelving or other fixtures cannot block such views. Display windows do not allow views into and out of a building. Alternative compliance is necessary. Staff would recommend that the Planning Commission require compliance as it would be practical to include windows that meet the 30% window requirement on this elevation especially given that much of the north elevation and the west elevation are exempt from the window requirements. For example, the floor plan could be modified to include a corridor along the entire length of the elevation where intermittent activity (customers accessing units, custodial and office workers, etc.) would occur. Further, this is a 218 foot long building wall adjacent to a public street and public sidewalk with no windows that allow views into the building.
- **10% window requirement.** Regarding the floors above the first floor of the building (floors 2 through 4), there are some floors on each elevation that exceed the 10% requirement and others that do not, specifically on the north and west elevations of the building. The windows are even in their distribution and vertical in their proportion. Alternative compliance would be necessary for the 10% window requirement. Staff would recommend that the Planning Commission require compliance with the provision on all elevations, floors 2-4, as applicable in order to provide breaks in the massing that is provided on the exterior building walls.
- **Walkway connection.** A well-lit walkway at least 4 feet in width does not connect the buildings and the adjacent public sidewalks to the on-site parking. The only parking space that is connected to the building and the sidewalk via a walkway is the single accessible parking space provided adjacent to 7th Street North. Alternative compliance would be necessary. Staff would recommend that the Planning Commission require compliance with this provision as it would be practical to expect the provision of a walkway to allow customers to have a safe and direct connection from all on-site parking spaces to the principal building entrance.
- **Landscaped yard and screening.** A 7-foot wide landscaped yard is required between the parking lot and the public street. This would apply for the accessible parking space located adjacent to North 7th Street and for the two parallel spaces on the west side of the building off of 7th Avenue North. The screening requirement is 3 feet in height and 60% opaque and not less than one tree shall be provided for each 25 linear feet of parking lot frontage. Two canopy trees would be required adjacent to 7th Street North and one adjacent to 7th Avenue North. There is a landscaped yard adjacent to 7th Street North that varies in width from 1 foot to 23

feet and an 11 foot landscaped yard adjacent to 7th Avenue North. Proposed screening along both frontages would not meet the requirements. Alternative compliance would be necessary. CPED Staff would recommend granting partial alternative compliance along 7th Street North for the width of the landscaped yard but that the Planning Commission require full compliance with the screening provisions, linear canopy tree provision, as well as install a decorative metal fence as an alternative compliance measure.

- **20% landscaping requirement.** The Zoning Code requires that at least 20 percent of the site not occupied by buildings be landscaped. The total site area is 37,088 square feet. The proposed building footprint is 19,325 square feet. A total of 3,553 square feet of landscaping would be necessary to meet the 20 percent requirement. The applicant is providing 3,250 square feet of landscaping, or 18% of the area not covered by buildings. Alternative compliance would be necessary. Staff would recommend that the Planning Commission require compliance with this provision as it would be practical to landscape the area to the north of the parking lot. This area is currently being used to access the adjacent property which will no longer be permitted. This approximate 8 foot area is not being landscaped.
- **Landscape quantities.** The Zoning Code requires that at least 7 canopy trees and 36 shrubs be provided. The applicant is proposing to install 4 canopy trees and 3 ornamental trees in addition to 115 shrubs. Alternative compliance would be necessary for the number of trees. Staff would recommend that the Planning Commission require compliance with the provision that requires 7 canopy trees as there are locations that are devoid of trees on the property. Further, Staff would recommend that the Planning Commission require that the applicant provide a combination of deciduous and evergreen shrubs on the property in order to provide a mix of plantings.
- **Proximity to on-site deciduous tree.** The surface parking lot consists of 11 spaces. Of the 11 parking spaces, 9 of these spaces within the surface parking lot are more than 50 feet from an on-site deciduous tree. Alternative compliance is necessary. Staff would recommend that the Planning Commission require compliance with this provision as it would be practical to plant trees in the applicable landscape areas to meet this requirement.
- **Areas not covered by buildings, etc.** There are areas not occupied by buildings, etc., that are not covered by landscaping. Specifically in the area to the north of the parking lot. This area is currently being used to access the adjacent property which will no longer be permitted. This approximate 8 foot area is not being landscaped. Alternative compliance would be necessary. Staff would recommend that the Planning Commission require that this area be landscaped.
- **CPTED principles.** The proposal appears to comply with standards regarding crime prevention through environmental design including but not limited to surveillance, lighting, space delineation, natural access control, etc., with the exception being the entire south elevation of the building. This is due to the fact that the applicant is proposing clear glass that looks into display windows, which does not meet the requirements of the site plan review ordinance. Ground-floor windows facing the street must allow views into and out of the building. Shelving or other fixtures cannot block such views. Display windows do not allow views into and out of a building. Further, this is a 218 foot long building wall adjacent to a public street and public sidewalk with no windows that allow views into and out of the building. Alternative compliance would be necessary for this provision. Staff would recommend as previously noted that the Planning Commission require compliance with the 30% window requirement along the south elevation of the building.

RECOMMENDATIONS

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt staff findings for the applications by Beck Law Office, on behalf of Lock Up Minneapolis 701, LLC, for the properties located at addresses 701 7th Street North & 618 7th Avenue North:

A. Conditional Use Permit to allow an 11-space surface parking lot in the DP Overlay District.

Recommended motion: Approve the conditional use permit for an 11-space surface parking lot, subject to the following conditions:

1. The conditional use permit shall be recorded with Hennepin County as required by Minn. Stat. 462.3595, subd. 4 before building permits may be issued or before the use or activity requiring a conditional use permit may commence. Unless extended by the zoning administrator, the conditional use permit shall expire if it is not recorded within two years of approval.

B. Variance of the off-street parking requirement.

Recommended motion: Approve the variance for a reduction in the off-street parking requirement from 23 spaces to 11 spaces.

C. Variance of the off-street loading requirement.

Recommended motion: Approve the variance for a reduction in the off-street loading requirement from 3 large spaces to 3 small spaces.

D. Site Plan Review for a new self-service storage building.

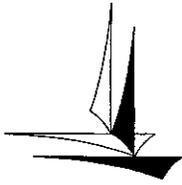
Recommended motion: Approve the site plan review application for a new 4-story, approximately 95,640 square foot self-service storage facility, subject to the following conditions:

1. CPED Staff review and approval of the final site, elevation, lighting and landscaping plans before building permits may be issued.
2. All site improvements shall be completed by April 13, 2017, unless extended by the Zoning Administrator, or the permit may be revoked for non-compliance.
3. The office space and retail/café space shall each have a separate principal entrance facing the public street.
4. In the interim period until a tenant is located for the retail/café space, the area must meet window requirements and allow clear views into and out of the space. Stacking and storage of materials in front of the windows shall not be permitted.
5. The outdoor space adjacent to Oak Lake Avenue shall be reduced in size to approximately 900 square feet and shall be landscaped with built in planters that are maintained year-round regardless of whether there is an occupant of the retail/café space.
6. The south elevation of the structure adjacent to 7th Avenue North shall meet the 30% window requirement per Section 530.120 of the Zoning Code. Display windows do not count towards the 30% window requirement.
7. The building shall be modified to comply with the 10% window requirement on all elevations, floors 2-4, as applicable per Section 530.120 of the Zoning Code.
8. A minimum of a two inch reveal between the panels proposed on the exterior building walls shall be provided so that the building complies with the blank wall provision per Section 530.120 of the Zoning Code.

9. All landscaping and screening requirements shall be incorporated into the final plans adjacent to 7th Street North and 7th Avenue North as applicable per Section 530.170 of the Zoning Code.
10. The final landscape plan shall be modified to comply with the 20% landscape requirement by removing asphalt and incorporating landscaping adjacent to the north property line in the area of the previously vacated alley.
11. The final landscape plan shall be modified to incorporate the required quantities, to comply with the proximity of parking spaces to on-site deciduous trees and to include a combination of deciduous and evergreen shrubs on the property in order to provide a mix of plantings as required by Section 530.160 and 530.170 of the Zoning Code.
12. A well-lit walkway at least 4 feet shall connect the building and the adjacent public sidewalks to all on-site parking as required by Section 530.130 of the Zoning Code.
13. All roof-top mechanical units shall be screened in compliance with Section 535.70 of the Zoning Code.
14. The lighting plan shall be modified to eliminate spillover onto adjacent properties. All lighting will need to be downcast and shielded to avoid undue glare. All lighting shall comply with Chapters 535 and 541.
15. Should a retail tenant/café tenant be secured in the building, the development shall comply with the bicycle parking provisions as required in Section 541.180 of the Zoning Code.

ATTACHMENTS

1. PDR report
2. Written description and findings submitted by applicant
3. Correspondence
4. Zoning map
5. Plans – survey, site, landscape, civil, elevations
6. Photos



Minneapolis Development Review
250 South 4th Street
Room 300
Minneapolis, MN 55415

Preliminary Development Review Report

Development Coordinator Assigned: **PATRICIA MURZYN**
(612) 673-5827
patricia.murzyn@minneapolismn.gov

Status *
RESUBMISSION REQUIRED

Tracking Number:	PDR 1001281
Applicant:	LOCKUP MINNEAPOLIS 701 LLC 800 FRONTAGE ROAD NORTHFIELD, IL 60093
Site Address:	701 7TH ST N 618 7TH AVE N
Date Submitted:	28-JAN-2015
Date Reviewed:	04-FEB-2015

Purpose

The purpose of the Preliminary Development Review (PDR) is to provide Customers with comments about their proposed development. City personnel, who specialize in various disciplines, review site plans to identify issues and provide feedback to the Customers to assist them in developing their final site plans.

The City of Minneapolis encourages the use of green building techniques. For additional information please check out our green building web page at: http://www.ci.minneapolis.mn.us/mdr/GreenBuildingOptions_home.asp.

DISCLAIMER: *The information in this review is based solely on the preliminary site plan submitted. The comments contained in this report are preliminary ONLY and are subject to modification.*

Project Scope

New 5 story 99,985 sq foot storage facility with 11 surface parking stalls.

Review Findings (by Discipline)

Addressing

- Per City of Minneapolis Street Naming and Address Standard V1.22, the City of Minneapolis holds authority for assignment of all addresses, verification, change, and/or additions. Each assigned address number uses the street that provides the best/direct access for life safety equipment and best/direct access to the occupants.
- The address for the new storage building proposed at 701 7th Street North and 618 7th Avenue North will be 701 7th Street North. This address meets the City of Minneapolis Street Naming and Address Standard requirements.

*Approved: You may continue to the next phase of developing your project.

*Resubmission Required: You cannot move forward or obtain permits until your plans have been resubmitted and approved.

□ Business Licensing

- There is no Minneapolis Business Licensing review required for the proposed project.

□ Zoning - Planning

- The following land use applications are necessary to develop the project as proposed:
 1. Conditional Use Permit to allow an increase in height from 4 stories or 56 feet to 5 stories or 56 feet.
 2. Conditional Use Permit to allow an 11-space accessory surface parking lot in the DP Overlay District;
 3. A variance of the off-street parking requirement from 24 spaces to 11 spaces;
 4. A variance of the off-street loading requirement of 3 large spaces;
 5. A variance to allow two wall signs at a height taller than 28 feet on the northeast and west elevations of the building;
 6. A variance to allow signage on a non-primary building wall (west) where no signage is permitted;
 7. A variance to allow a wall sign on the west elevation of the building that exceeds the allowable area;
 8. Site plan review.
- The comments are for preliminary purposes only. The land use applications filed for the project are currently incomplete.

□ Parks - Forestry

- Contact Craig Pinkalla (612-499-9233 cpinkalla@minneapolisparcs.org.) regarding any questions related to planting, removal or the process for protection of trees during construction in the city right of way.
- Effective January 1, 2014, the City of Minneapolis and the Minneapolis Park and Recreation Board adopted an update to the existing Parkland Dedication Ordinance. The adopted City of Minneapolis Parkland Dedication ordinance is located in Section 598.340 of the City's Land Subdivision ordinance:
 - <http://library.municode.com/index.aspx?clientid=11490>.
- As adopted, the fee in lieu of dedication for new residential units is \$1,500 per unit (affordable units excluded per ordinance) and for commercial and industrial development it is \$200 per development employee (as defined in ordinance). Any dedication fee (if required) must be paid at the time of building permit issuance.
- There is also an administration fee that is 5% of the calculated park dedication fee.
- As proposed, for your PROJECT, the calculated dedication fee is as follows:
- Park Dedication Fee Calculation =

• Non-Residential Commercial Space	= \$7,600
• 5% of \$7,600 (Administration Fee)	= \$ 380
• Total Park Dedication Administrative Fee:	\$7,980
- This is a preliminary calculation based on your current proposal; a final calculation will be made at the time of building permit submittal.
- For further information, please contact Patty Murzyn 612-673-5827.

□ Fire Safety

- The Applicant shall provide for the following:
 - Required fire suppression system throughout building.
 - Fire department connection must be located on the address side of building and within 150 feet of a fire hydrant.
 - Required fire alarm system throughout building.
 - Maintain fire department apparatus access at all times.

□ Historical Preservation Committee

- There is no HPC flag on this property. HPC review is not required at this time. However, HPC review is required for any wrecking permits pertaining to the removal of any existing structures.

❑ Construction Code Services

- Assuming an S-1 occupancy with Type 2B construction, the west wall is required to be one hour rated. The maximum area of exterior openings on the west wall is 25%.
- The assumed occupancy and construction type allows 3 stories plus an increase of one story for the sprinkler system for a total of 4 stories. The proposed height of 5 stories would require Type 2A construction. Type 2A involves 1 hour structural frame, bearing walls, floors and roofs.
- A Service Availability Charge (SAC) determination will need to be submitted to the Metropolitan Council for the proposed project. Please refer to this link for more information or Contact Karon Cappaert at 651-602-1118 or karon.cappaert@metc.state.mn.us.
http://www.ci.minneapolis.mn.us/mdr/docs/sac_availability_charge.pdf.

❑ Right of Way

- An encroachment permit shall be required for all streetscape elements in the Public right-of-way such as: plants & shrubs, planters, tree grates and other landscaping elements, sidewalk furniture (including bike racks and bollards), and sidewalk elements other than standard concrete walkways such as pavers, stairs, raised landings, retaining walls, access ramps, and railings (NOTE: railings may not extend into the sidewalk pedestrian area). Please contact Bob Boblett at (612) 673-2428 for further information.
- Note to the Applicant: Any elements of an earth retention system and related operations (such as construction crane boom swings) that fall within the Public right-of-way will require an encroachment permit application. If there are to be any earth retention systems which will extend outside the property line of the development then a plan must be submitted showing details of the system. All such elements shall be removed from the Public right-of-way following construction with the exception of tie-backs which may remain but must be uncoupled and de-tensioned. Please contact Bob Boblett at (612) 673-2428 for further information.
- The site plan indicates proposed access for erosion control purposes to the vacated alley right-of-way and private property along the north property line. Unless the Applicant can provide documentation that the adjacent property Owner has allowed access; vehicular access across a third parties property cannot be approved and will not be allowed at this location.
- Note to the Applicant: The survey provided as part of the site plan indicates a vacated alley along the north property line; including a Utility Easement (Doc #9790474). The terms of the easement maintain access on the existing alley surface for the City of Minneapolis related to operations and maintenance of City storm and sanitary infrastructure. Consequently, the impacted property Owners cannot inhibit access on the existing alley surface and access must be maintained as part of any development.

❑ Sidewalk

- The plan as submitted meets the requirements of the Public Works Sidewalk Inspections Division.
- Note to the Applicant: Any currently defective sidewalks or other concrete infrastructure within the public right of way, or any concrete infrastructure damaged during construction, must be removed and replaced.

❑ Water

- All existing and proposed underground Public Utilities (water, sanitary sewer, and storm drain) shall be shown on the site plan with corresponding pipe sizes and types. For Public watermain infrastructure records call (612) 673-2865. Any existing connections not in use shall be noted on the plans for removal, and shall be removed per the requirements of the Utility Connections Department, call (612) 673-2451 for more information.
- The water service connections to Oak Lake Ave. shall be designed to avoid bends into the proposed building; rather it is recommended to run water service lines straight from the main into the proposed building to the meter location. The City does not issue 2" service taps; service connections will need to be a 6x4" then reduced to a 2" service line at the gate. Please contact Rock Rogers at (612) 673-2286, to review domestic and fire service design, connections, and sizes.

□ Street Design

- Note to the Applicant: There are numerous multi-modal transit oriented projects are planned for 7th St. N. in the near future, including Arterial Bus Rapid Transit (ABRT), associated bus stations, protected bike lanes and potential other Metro Transit related projects. Consequently, future business access to 7th St. N. will be impacted and restricted, and as such will not be allowed. It is recommended that alternative footprints and parking lot layouts be considered that provide for site access off of 7th Ave. N. via Oak Lake Ave. For further information and details, please contact Paul Miller at (612) 673-3603.
- For detailed information related to City of Minneapolis standard specifications, details, and standard plates refer to the following: <http://www.minneapolismn.gov/publicworks/plates/index.htm>.
- The existing curb cut along 7th St. S. shall be completely removed and replaced with appropriate curb and sidewalk.
- All driveway aprons shall be designed and constructed to City standards. All driveway aprons shall be shown graphically correct on all related plan sheets. Please refer to the following: http://www.minneapolismn.gov/publicworks/plates/public-works_road. . Add the appropriate details from the ROAD-2000 Series: Driveways (ROAD-2000, ROAD-2001, ROAD-2002, ROAD-2003) to the plans.
- All curb & gutter in the Public right-of-way shall be designed and constructed to City standards, curb & gutter to be City standard B624 Curb and Gutter. Please refer to the following: http://www.minneapolismn.gov/publicworks/plates/public-works_road. . Add the appropriate details from the ROAD-1000 Series - Curbs and Gutters (ROAD-1003, and ROAD-1010) to the plans.

□ Traffic and Parking

- Note to the Applicant: There are numerous multi-modal transit oriented projects are planned for 7th St. N. in the near future, including Arterial Bus Rapid Transit (ABRT), associated bus stations, protected bike lanes and potential other Metro Transit related projects. Consequently, future business access to 7th St. N. will be impacted and restricted, and as such will not be allowed. It is recommended that alternative footprints and parking lot layouts be considered that provide for site access off of 7th Ave. N. via Oak Lake Ave. For further information and details, please contact Paul Miller at (612) 673-3603.
- The Applicant shall note the location of any existing Metro Transit "bus stops" on the site plan.
- Current ordinance states that all maneuvers associated with loading, parking or sanitation pick up for a private development shall occur on private property. Please provide a narrative explaining the trash removal operations and show turning maneuvers for all truck type vehicles that will be using the loading dock/parking entrance areas.
- Per City Ordinance the Applicant shall provide for (and identify) a solid waste collection point (SWCP) on the site plans. The location of the SWCP is subject to the review and approval of the Public Works Department.
- Please contact Bill Prince at (612) 673-3901 regarding existing and proposed street lighting. All street lighting (existing and proposed) shall be shown clearly on the site plan.
- Note to the Applicant: Please add the following notes to the site plan:
 - Street lighting installed as part of the Project shall be inspected by the City. Contractors shall arrange for inspections with the Traffic Department, please contact Dave Prehall at (612) 673-5759 for further information. Any lighting installations not meeting City specifications will be required to be reinstalled at Owner expense.
 - An obstruction permit is required anytime construction work is performed in the Public right-of-way. Please contact Scott Kramer at (612) 673-2383 regarding details of sidewalk and lane closures. Log on to <http://minneapolis.mn.roway.net/>. for a permit.
 - Contact Allan Klugman at (612) 673-2743 prior to construction for the temporary removal/temporary relocation of any City of Minneapolis signal system that may be in the way of construction.
 - All costs for relocation and/or repair of City Traffic facilities shall be borne by the Contractor and/or Property Owner.
- Contact Doug Maday at (612) 673-5755 prior to construction for the removal of any City of Minneapolis right of way signs that may be in the way of construction.

□ Sewer Design

- Stormwater Management: Please identify the square footage of the existing impervious area in the Development Summary on Sheet C2.0.
- Please note, the stormwater treatment manhole may not be eligible for a Stormwater Utility Fee Quality Credit. If interested in obtaining this credit please contact Stormwater Utility Administration for further assistance or questions, 612-673-2965.
- Utility Connections: Please identify the invert of the existing storm sewer manhole proposed to be connected to within Oak Lake Ave.
- Non Stormwater Discharges: Detail all mechanical and non-stormwater discharges. Non-stormwater discharges are not permitted unless approved by the City of Minneapolis. Non-stormwater discharges not declared and approved will not be permitted. If there currently are none and nothing is proposed declare this status on the plans.
- For comments or questions on Public Works Surface Water & Sewers Division related requirements please contact Jeremy Strehlo, (Professional Engineer) at (612) 673-3973, or jeremy.strehlo@minneapolismn.gov.

□ Environmental Health

- In October 2014 a Temporary Monitoring Well Permit was issued for an environmental soil and groundwater investigation for Lock Up Storage. If the analysis indicated impacted soils or groundwater is or may be present on the site it is recommended that the project developer enter the Voluntary Investigation and Cleanup program at the Minnesota Pollution Control Agency and obtain a letter of no association for this project. Local permit and approval requirements for addressing impacted soils are identified below.
- If impacted soil is encountered during site activities work will need to stop and notification provided to the MN State Duty officer at (615) 649-5451.
- If dewatering is required during site construction see below for city permit requirements. The scope of work calls for underground parking. The highest groundwater level expected for this site should be determined and used in establishing the lowest level for underground parking. The underground parking and other subgrade structures should be designed to prevent infiltration of groundwater without the need for a permanent dewatering system being installed. If a continuously operating permanent dewatering system is needed it must be approved as part of the sanitary sewer and storm drain site plan approval prior to construction beginning.
- No construction, demolition or commercial power maintenance equipment shall be operated within the city between the hours of 6:00 p.m. and 7:00 a.m. on weekdays or during any hours on Saturdays, Sundays and state and federal holidays, except under permit. Contact Environmental Services at 612-673-3867 for permit information.
- Permits and approval are required from Environmental Services for the following activities: Temporary storage of impacted soils on site prior to disposal or reuse; Reuse of impacted soils on site; Dewatering and discharge of accumulated storm water or ground water, underground or aboveground tank installation or removal, well construction or sealing. Contact Tom Frame at 612-673-5807 for permit applications and approvals.
- A review of the project, permits issued and an inspection from Environmental Service for identification of equipment and site operations that require annual registration with the City of Minneapolis will occur for this project.

END OF REPORT

**PROPOSED DEVELOPMENT OF 701 N. 7th STREET
MINNEAPOLIS, MINNESOTA**

SUBMITTED TO:

**PLANNING COMMISSION
MINNEAPOLIS, MINNESOTA**

SUBMITTED BY:

**BRB DEVELOPMENT, LLC
THE LOCK UP SELF STORAGE CENTERS
800 FRONTAGE ROAD
NORTHFIELD IL 60093
847.446.7872
JANUARY 8, 2015
Revised March 11, 2015
Revised March 27, 2015**

CITY OF MINNEAPOLIS, MINNESOTA

PROPOSED DEVELOPMENT OF THE LOCK UP SELF STORAGE CENTER AT 701 N. 7th STREET, MINNEAPOLIS, MINNESOTA

COMPANY HISTORY AND INTRODUCTION

The Lock Up Self Storage Centers were started in 1976 by two friends and entrepreneurs, Robert Soudan, Sr. and Charles Sample, Sr. From their initial project on Willow Road in Northbrook, Illinois, the company has steadily grown to be a leader in the self storage industry. Renowned nationally for building award winning projects, The Lock Up specializes in building interior-access, climate-controlled facilities in high-end communities nationwide.

The Lock Up Self Storage Centers (and its development arm, BRB Development) currently owns and operates 30 storage facilities located in 8 states. Since 2007, we also have owned and operated a self storage platform in Europe.

In 2005 we formed a long term investment partnership with Evergreen Real Estate Partners, a controlled affiliate of the Washington State Investment Board (WSIB). WSIB represents and manages the pension and retirement contributions for all state employees in the Washington State system.

Product Attributes:

We prefer to locate in dense, affluent communities where our target real estate has:

- Good visibility
- Good traffic flow and access
- Our current property portfolio is located near or adjacent to:
 - Major Regional Malls
 - Grocery Anchored Community Centers
 - Professional Office Parks
 - Medical Office Buildings
 - Entertaining and Dining
 - Residential Neighborhoods
 - We do not locate in industrial parks, as it is negatively viewed by our customers

Customer Profile:

Across our portfolio nationally, roughly 75% of our customers are residential users, 20% are commercial users and the remaining 5% are college students and military personnel.

Customer Profile (continued):

The majority of our customers are women. They are the decision makers and they base their storage decisions on several key criteria:

- Safety and Security
 - Large glass enclosed offices with a professional staff
 - Enclosed loading and unloading areas
 - High Tech Security
 - Motion sensor lighting

- An attractive and comfortable facility that has:
 - Architecturally attractive building design
 - A climate controlled environment for the safety of their goods (both heated and air conditioned)
 - Fully carpeted hallways and storage units, to cut down on noise and dirt
 - Hallway intercoms, to communicate at all times with the property manager

Typically, over 90% of our customers come from within a 5 mile radius of our project location. In effect, our customers are our neighbors. As a result, we pride ourselves on working with the communities that we operate within. We strive to be an asset within the community.

THE PROJECT

We propose a 4-story, fully-enclosed, climate-controlled, investment grade self storage facility with a full basement. The architectural style is illustrated in the attached drawings and renderings, as well as our existing facilities in other high-end communities around the U.S. Examples of current facilities we have built in Illinois, Minnesota and Florida can be found on our website at www.thelockup.com. Specific examples of Twin City facilities are:

- Minneapolis – 1355 Industrial Boulevard
- Bloomington – 221 American Boulevard
- Plymouth – 2960 Empire Lane
- Eden Prairie – 12995 Valley View Road
- Golden Valley – 6250 Olson Memorial Highway (under construction)

The building is sited to respect North 7th Street to the northeast, Oak Lake Avenue to the east and 7th Avenue North to the south. The front door of the building will be located at the northeast corner, on North 7th Street. Lock Up customers in vehicles will access the property via North 7th Avenue and proceed around the west and north sides of the building to the front door on North 7th Street. Vehicle access to the property will be restricted to passenger vehicles and trucks not exceeding 30 feet in total length. Notice of this restriction will be posted at the vehicle entrance to the property on 7th Avenue North, and in the storage contracts signed by customers.

A glass enclosed office will wrap around the front door at the northeast corner of the building with additional space for seating and product display. To the east of the office area, at the corner of Oak Lake Avenue and 7th Avenue North, is 1,500 square feet of space that, in the future (when

the neighborhood has developed and can support it), will be available to be retrofitted to accommodate a café, coffee shop or other small business that could make use of the plaza area for outdoor seating. Adjacent to the west side of the office area will be an enclosed drive-in loading and unloading area with three interior loading docks for Lock Up customers. The loading docks will allow up to 30-foot trucks, the maximum length allowed on the property, to be fully enclosed, behind glass doors, while loading and unloading.

Trash dumpsters will be located inside the loading area, in the location shown on the first floor plan. Trash trucks will pull in to the property via the 7th Avenue North vehicle access and pull around the building to the loading area. The driver will enter the building on foot, wheel the dumpster to the truck, empty it and wheel it back to the designated location in the loading area. The trash truck will then exit onto 7th Avenue North. The driveway and loading area will accommodate all trash truck turning movements.

An access-code activated elevator is located immediately off of the loading area. Energy efficient lighting is controlled by strategically placed motion sensors. Customer movement within the facility is tracked and recorded via a high tech digital security system. The building is also climate controlled using energy efficient HVAC units. The Lock Up is one of the few self storage operators in the U.S. that has fully carpeted hallways and storage units.

The Project will be professionally staffed and open to customers:

Monday – Friday	8:00 AM to 6:00 PM
Saturday	8:00 AM to 6:00 PM
Sunday	9:00 AM to 5:00 PM

We are closed for major holidays.

If a customer needs to access their storage unit before or after our normal operating hours, they can only access the facility via an individual access code which also monitors their movements. The only hours that they can independently access their unit would be:

Mornings:	6:00 AM to 8:00 AM
Evenings:	6:00 PM to 8:00 PM

701 North 7th Street is currently occupied by a vacant single story industrial building. This building will be removed and an entirely new building developed.

The proposed building will be a 4-story, 98,180 square foot facility constructed of painted precast wall panels with brick formliner and smooth textures. The facility will include display windows on all sides and signage as shown.

Building Height	Proposed	53'-0"
	Allowed	56'-0"
Building Area/F.A.R.	Proposed	95,640 sf/ 2.57
	Allowed	100,137 sf/ 2.7
Parking	Proposed	11 spaces

COMPREHENSIVE PLAN AND ZONING CONSISTENCY

Comprehensive Plan:

The property is designated by the Comprehensive Plan for Transitional Industrial Uses and is in the “Lower Mix” Land Use District in the North Loop Small Area Plan (the “Plan”). The Plan states, with respect to the Lower Mix District:

The Lower Mix District currently is an eclectic mix of public works properties, the Minneapolis Farmers Market, large warehouses, private industrial and commercial businesses, and social service housing providers. This Plan proposes to continue this diversity of uses.

The Plan also places the property in Development Intensity District B. The Plan calls for the following in Development Intensity District B:

Appropriate building types include housing, commercial, and industrial structures of two (2) to ten (10) stories. This type is generally consistent with the bulk and height of historic warehouse structures in the neighborhood.

Planning Staff has stated that the Project is not consistent with the Plan, but have not identified any specific provision in the Plan to support this position. In fact, the Plan calls for large warehouses of 2 to 10 stories in the Lower Mix Land Use District, and makes no exception for properties adjacent to North 7th Street or Oak Lake Avenue. The Project could not be more consistent with the Plan.

However, even if the Project could, arguably, be said to be inconsistent with some language in the Plan, this is not a legally-sufficient reason for denying approval of a permitted use. When local officials enact ordinances designating a specific use as permitted in a particular district, they determine by implication that the permitted use is consistent with existing land uses and may not deny approval of the use based on general statements of purpose in a Comprehensive Plan. PTL, LLC v. Chisago County Board of Commissioners, 656 NW.2d 567 (Minn. Ct. of App. 2003).

The Project is a 4-story, 95,640 square foot commercial warehouse building, which is exactly what the North Loop Small Area Plan calls for in this location. The project also provides the opportunity for a mixed use component with the future retail/coffee shop tenant space.

Zoning:

The property is located in the I-2, Medium Industrial Zoning District. Pursuant to Section 550.30 and Table 550-1 of the Zoning Ordinance, “self service storage” is an “industrial use” and a “permitted use” in the I-2 Zoning District.

Site Plan Review:

As a nonresidential building of 20,000 or more square feet, the building requires site plan review. Pursuant to Section 530.70 of the Zoning Ordinance, the Planning Commission shall approve the site plan if:

- (1) The site plan conforms to all applicable standards of this Chapter.
- (2) The site plan conforms to all applicable regulations of this Zoning Ordinance and is consistent with the applicable policies of the Comprehensive Plan and applicable small area plans adopted by the City Council.

The building has been located and designed to comply with the standards set forth in Sections 530.110 and 530.120, which regulate building placement and building design, as follows:

- The first floor of the building is placed 1' to 6' from the lot line on the adjacent public street to the south. The three cut outs that are setback 6 feet provide additional articulation along this wall of the building and provide space for enhanced landscaping, as suggested by the Planning Commission.
- Also as suggested by the Planning Commission at the December 4, 2014, and January 29, 2015, Committee of the Whole meetings, setbacks along the adjacent streets to the northeast and east have been increased in order to provide the public amenity of a landscaped plaza which can, in the future, accommodate outdoor seating.
- The principal entry of the building faces the front lot line.
- The building walls include architectural detail, recesses and projections, windows and entries to divide the building into twenty-five (25) foot or shorter sections.
- The building includes no plain face concrete block as an exterior material and all exterior materials comply with the requirements.
- The primary entrance is clearly defined and emphasized through the use of architectural features, including storefront glazing and canopies.
- The walls that face public streets (and parking lot for upper floors) include a total of over forty one percent (41%) window area on the first floor (30% per elevation required), and sixteen percent (16%) window area on the floors above (10% per elevation on each floor required). The first floor windows allow views into and out of the building at eye level. We are proposing a deviation from the requirement that the windows be evenly spaced on the north façade in order to improve the architectural appearance of the building by grouping windows on the corners and creating a landmark architectural feature on the 7th Street façade of the building.

The building has more windows than required by the site plan review standards. However, Planning Staff has stated that the Project does not meet the glazing requirement because clear glass that looks into display windows does not meet the requirements of the ordinance. This position:

- Is inconsistent with the position Staff and the Planning Commission took with respect to Lock Up's existing facility on Industrial Boulevard, which was approved under the same ordinance provisions governing this Project. At one point in this process Staff said they thought the Industrial Boulevard project turned out pretty well. The idea that the site plan review requirements should be interpreted differently for this Project because it is 8 years later and the project is on a different street is not sustainable. The project is in the same zoning district, is subject to the same site plan review standards and must be treated the same as the Industrial Boulevard project.
- Is contrary to the language of the site plan review ordinance itself, which expressly exempts permitted uses, including self storage, from the requirement for active uses. The clear intent of this ordinance is to recognize that permitted industrial uses, including self storage, do not have active uses and therefore do not need to have clear windows looking into active uses. Zoning ordinances must be interpreted according to their plain and ordinary meaning and **in favor of the property owner** (emphasis added). Frank's Nursery Sales, Inc. v. City of Roseville, 295 NW.2d 604 (Minn. 1980). The plain meaning of this ordinance is the meaning given it when the Lock Up on Industrial Boulevard was developed.

The proposed self-storage use is primarily a warehouse and, as such, inherently does not have a great deal of active uses on the ground floor, except at the office and interior loading areas. However, Section 550.120(b)(3) specifically exempts Industrial Uses in Table 550-1 from the requirement for ground floor active functions. The Project is identified in Table 550-1 as a permitted industrial use and, as such, is not subject to the requirement for active functions on the ground floor. All active areas on the ground floor, including the office, the internal loading and unloading areas, and the future retail/café space, have significant amounts of storefront glazing.

Alternative Compliance:

Section 530.80 of the Site Plan Review Ordinance allows for the approval of site plan improvements, including architectural enhancements, in lieu of strict compliance with the requirements when strict compliance is impractical and the proposed alternative meets the intent of the Zoning Ordinance and the Comprehensive Plan.

Setbacks along North 7th Street and Oak Lake Avenue:

With respect to the building setbacks along North 7th Street and Oak Lake Avenue, strict compliance with the requirement that the building be within eight feet of the property line is impractical in light of the Planning Commission's desire that the building provide a less bulky appearance and a more pedestrian friendly environment at this intersection, which is planned to be a major pedestrian corridor in the future. In response to the suggestions of the Planning Commission, the northeast corner of the building has been redesigned as a "clock tower" which

reaches out to North 7th Street. The building pulls back to the west to provide extensive landscaped pedestrian areas, including an area on the east side for a future sidewalk café.

Setbacks to the north and south of the clock tower vary to accommodate the future sidewalk café area. Alternative compliance from the eight foot maximum setback is appropriate because:

- The building has been pulled back from the street at the request of the Planning Commission, and at significant cost to the developer. Reducing the footprint for the building has created the need to include a basement, and even then the overall square footage has gone down from the original proposal.
- The building does respect the eight foot setback guideline by extending the clock tower to approximately one foot from the property line along North 7th Street.
- The increased setbacks to the north and south provide additional space for pedestrian movement, potential future transit facilities, significantly increased landscaping, the potential future sidewalk café and, in general, a significantly improved pedestrian environment.

Windows:

For the reasons discussed above, we believe that the Project fully meets and, in fact, exceeds the site plan review standards for windows. However, even if the Planning Commission accepts the Staff position that this Project should be treated differently than the Industrial Boulevard project and that windows without active uses behind them should not count as windows, alternative compliance is appropriate because:

- It is impractical to have active uses behind the windows of a warehouse or self storage facility. This is why the standards exempt permitted uses such as self storage from the requirement for active uses.
- In lieu of windows looking into non-existing active uses, the building includes, at the suggestion of the Planning Commission, a clock tower as a landmark at the North 7th Street and Oak Lake Avenue intersection.
- The building also includes glass along almost the entire first floor frontage along North 7th Street and Oak Lake Avenue, including glass along the entire length of the office area and into the area for the future retail/café use.
- The building includes a retail/café area at the North 7th Street/Oak Lake Avenue intersection in order to focus the activity that the building can generate on this important intersection.

CONDITIONAL USE PERMIT AND VARIANCE FOR OFF STREET PARKING

The zoning ordinance requires 24 parking spaces for the project as proposed. However, both the zoning ordinance (through the Downtown Parking Overlay District) and the applicable Comprehensive Planning documents encourage minimizing off street parking, and the DP District requires a conditional use permit for any off street parking.

BRB has developed self-storage facilities throughout the country, including five in the Twin Cities area. Their experience is that peak parking demand for a facility this size will be 6-12

spaces. Eleven parking spaces are proposed to ensure that enough spaces are provided so that customers will be able to access a parking space and safely enter the building. The parking spaces are located off of the public streets, along the north property line, as far out of view of the public street as possible.

The facility, of course, needs some off street parking. Pursuant to the applicable City policies and ordinances, off street parking provided should be the minimum necessary to safely meet the needs of the customers using the building, and the parking should be located to minimize its visual effect. This is what is proposed with the eleven space parking area to the north of the building.

The standards for a variance to reduce the number of parking spaces are met, as follows:

- (1) Providing eleven parking spaces to meet the expected peak demand is a reasonable proposal to provide the parking required to serve the use and minimize off-street parking in the DP District. The applicant's experience with its facilities across the country, including its facility on Industrial Boulevard in Minneapolis, confirms that eleven spaces will be sufficient to meet the demand. The property is also well served by transit.
- (2) The use is a unique use which needs far less parking than the zoning ordinance contemplates. This enables the developer to reduce the parking provided to meet the goals of the Comprehensive Plan and the DP District.
- (3) Reducing off street parking to the minimum necessary to meet the needs of the use is in keeping with the spirit and intent of the zoning ordinance and the Comprehensive Plan.
- (4) Reducing the number of parking spaces will not increase congestion or have any impact on public safety. Enough spaces are provided so customers will be able to park in the lot.

The standards for approval of a Conditional Use permit for the eleven spaces are also met, as follows:

- (1) A parking area, where customers can park their vehicles while they move goods to and from their storage units, or wait for access to the internal loading area, is necessary to prevent these activities from occurring on public streets and will protect the public health, safety, comfort and general welfare.
- (2) Providing sufficient parking for the facility will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.
- (3) Adequate utilities, access roads, drainage, necessary facilities or other measures have been provided for the parking area to function appropriately.

- (4) Measures to minimize traffic congestion in the public streets are not needed, due to the low volume of traffic the use generates.
- (5) The self-storage facility is consistent with the applicable policies of the comprehensive plan, and is a permitted use in the zoning district.
- (6) The self-storage facility, and its eleven space parking area, do, in all other respects, conform to the applicable regulations of the I-2 Industrial zone in which it is located.
- (7) The parking area will be located on the same zoning lot as the principal use, as required by Section 551.760 of the Zoning Ordinance.
- (8) The number of parking spaces does not exceed twenty (20) spaces, as required by Section 551.760.

VARIANCE FOR LOADING DOCK DEPTH

The Zoning Ordinance requires three 50-foot deep loading docks for the Project. Lock Up's experience, based on dozens of self storage facilities they operate across the country, including the Industrial Boulevard facility, is that only two 30-foot deep docks will be required for the Project.

Number of Docks:

In an effort to reduce the variances requested for the Project, Lock Up is showing, and will include in the Project, three loading docks. If the Planning Commission believes it is preferable to have no more loading docks than necessary, Lock Up would not object to a condition of approval that there be only two loading docks.

Depth of Docks:

With respect to the depth of the docks, Lock Up is requesting a variance to allow the 30-foot depth that the Project needs, rather than the 50-foot depth the Ordinance requires. This variance is requested to minimize the paved areas on the site.

The standards for a variance to reduce the depth of the loading docks are met, as follows:

- (1) There are practical difficulties unique to the property because the Public Works Department has prohibited vehicle access to 7th Street North. If large trucks could access off 7th Street North, less additional pavement would be required on site to accommodate turning and backing movements into the loading docks. With the requirement to access off 7th Avenue North and come around the building to the front door on 7th Street North, additional pavement would be required to accommodate turning movements for 50 foot trucks. The better solution is to restrict truck lengths to the 30 feet needed for the Project and reduce the dock length and the exterior paved area.

- (2) Reducing the length of the loading docks to the 30 feet needed allows the Project to be developed with less pavement in the driveway, loading and maneuvering areas. This is in keeping with the spirit and intent of the Comprehensive Plan and the DP District to minimize paved areas and is a reasonable approach to meeting the Project's needs and the City's planning goals.
- (3) Restricting the length of trucks allowed and reducing the length of the loading docks and the size of paved maneuvering areas will not alter the essential character of the area or be injurious to the use or enjoyment of other property in the vicinity. The proposed reduction is consistent with the planning for the area and will not be detrimental to the health, safety or welfare of the general public or of those utilizing the property or nearby properties. In fact, smaller trucks are generally safer, particularly in an urbanized area, and the restriction prohibiting large trucks will have a beneficial impact on the public and the area.

Peter Beck

From: Peter Beck
Sent: Wednesday, March 11, 2015 9:07 AM
To: 'blong.yang@minneapolismn.gov'
Cc: Peter Beck
Subject: Lock Up Self Storage - 701 North 7th Street
Attachments: 701 N 7th St. - Self Storage 01-21-15.pdf

Councilmember Yang,

This note is to update you on the proposed Lock Up Self Storage at 701 7th Street North.

In response to comments from City staff and the Planning Commission made at a Planning Commission Committee of the Whole meeting on February 4, 2015, and to subsequent comments from the Department of Public Works, we have made several revisions to the plans, including:

- 1) We have eliminated all sign variances. All signage will comply with City Code.
- 2) We have added a third loading dock, as required by city code, to avoid a variance to have only two (which is all the project needs). All three docks will be interior.
- 3) We have reduced the project from five stories at grade to four stories and a basement, so that no CUP related to height will be required (even though the project never exceeded the 56 foot height limit, a CUP was required for 5 stories).
- 4) We have increased the window area. All facades will have the required percentage of glass.
- 5) We have redesigned the 7th Street side of the project to include a stronger architectural element and improved street presence and appearance.
- 6) In response to a decision by the Department of Public Works to not allow access to North 7th Street, vehicle access will now be off North 7th Avenue. The front door, office, loading docks and parking for the facility will remain in the Northeast corner of the building.

We believe these revisions address the comments of the Planning Commission and that the project complies with all requirements of the zoning ordinance.

As we mentioned when we met with you, the North Loop Neighborhood Association has indicated their support for the project. Their January 21, 2015, letter advising us of their support for the 5 story project we showed you will be included with the revised plans which I will forward to you later today. We have offered to present the revisions outlined above to the NLNA.

Finally, we are required to document that we have provided you with the following information:

- 1) Description of the project: The project is now a 95,640 square foot, four story self-storage facility with three interior loading docks, eleven surface parking spaces, an office area facing North 7th Street and 1500 square feet of future café/retail space at the corner of North 7th Street and Oak Lake Avenue.
- 2) Land use applications we are aware of: Site plan review; a variance to reduce the required parking from 24 spaces to eleven; and a CUP to have any surface parking at all.
- 3) Address of the property: 701 7th Street North and 618 7th Avenue North.
- 4) Applicant information: Lock Up 701 LLC; 800 Frontage Road; Northfield, IL 60093; Local representative: Peter Beck; 800 Nicollet Mall, #2600; Minneapolis, MN 55402; 612-991-1350; peter@peterbecklaw.com

We will submit revised plans today and will press ahead in our efforts to get the project before the Planning Commission and, if necessary, the City Council. I will forward the revised plans and related materials later today. Please let me know if you would like to meet to review the revised plans.

Thank you,

Peter Beck

2600 US Bancorp Center
800 Nicollet Mall
Minneapolis, MN 55402
(612) 991-1350

peter@peterbecklaw.com

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Peter Beck

From: Peter Beck
Sent: Wednesday, March 11, 2015 8:32 AM
To: 'DJ Heinle'
Cc: Peter Beck
Subject: FW: NLNA P&Z Action on Lock Up
Attachments: 701 N 7th St. - Self Storage 01-21-15.pdf

DJ,

Thanks again to you, the Z&P Committee and the Board for your support for this project. I want to update you on our current status. In response to comments from City staff and the Planning Commission, we have made some revisions to the plans, including:

- 1) We have eliminated all sign variances. All signage will strictly comply with City Code.
- 2) We have added a third loading dock, as required by city code, to avoid a variance to have only the two required for the project. All three docks will be interior.
- 3) We have reduced the project from 5 stories at grade to four stories and a basement, so that no CUP related to height will be required (even though the project never exceeded the 56 foot height limit, a CUP was required for 5 stories).
- 4) We have increased the window area. All facades will have the required percentage of glass.
- 5) We have redesigned the 7th Street side of the project to include a stronger architectural element and improved street presence and appearance.
- 6) In response to a decision by the Department of Public Works to not allow access to North 7th Street, vehicle access will now be off North 7th Avenue. The front door, office, loading docks and parking for the facility will remain in the locations shown in the plans the NLNA reviewed.

We would be happy to meet again with the Committee to present these revised plans if you would like. Please let me know.

In addition, we have been advised by city staff that our notices to the NLNA have been deficient, in part because we did not include both of the current street addresses for the property (in spite of the fact that the city has also advised us that the address for the new building will, in fact, be 701 7th Street North). Therefore, for the record:

- 1) Description of the project: The project is now a 95,640 square foot, four story self-storage facility with three interior loading docks, eleven surface parking spaces, an office area facing North 7th Street and 1500 square feet of future café/retail space at the corner of North 7th Street and Oak Lake Avenue.
- 2) Land use applications we are aware of: Site plan review; a variance to reduce the required parking from 24 spaces to eleven spaces; and a CUP to have any surface parking at all.
- 3) Address of the property: 701 7th Street North and 618 7th Avenue North.
- 4) Applicant information: Lock Up 701 LLC; 800 Frontage Road; Northfield, IL 60093; Local representative: Peter Beck; 800 Nicollet Mall, #2600; Minneapolis, MN 55402; 612-991-1350; peter@peterbecklaw.com

Thanks again for your support. I will forward the revised plans later today.

Peter

Peter Beck

2600 US Bancorp Center
800 Nicollet Mall
Minneapolis, MN 55402
(612) 991-1350

peter@peterbecklaw.com

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-----Original Message-----

From: DJ Heinle [mailto:dj.heinle@tkda.com]
Sent: Wednesday, January 28, 2015 10:51 PM
To: Peter Beck
Cc: Larry Smith; Decker, David R (david_decker@uhg.com); 'David Frank' (david0frank@gmail.com)
Subject: RE: NLNA P&Z Action on Lock Up

Peter,

I'm pleased to report that your development was approved tonight. Please see the attached motion and feel free to share with anyone.

Thanks,

DJ Heinle, AIA | Education/Government Market Manager
444 Cedar Street, Suite 1500, Saint Paul, MN 55101 | P: 651-292-4481 | C: 612-387-6531 | www.tkda.com

-----Original Message-----

From: Peter Beck [mailto:peter@peterbecklaw.com]
Sent: Wednesday, January 21, 2015 10:21 PM
To: DJ Heinle
Cc: Larry Smith
Subject: RE: NLNA P&Z Action on Lock Up

DJ,

I see that the Board meeting is Wednesday. We meet with the PC Committee of the Whole on Thursday. We do not need a letter by Thursday, but it would be helpful if we could fill them in on the Board action. I'll email you Thursday morning for an update.

Thanks again for your help,

Peter Beck

2600 US Bancorp Center
800 Nicollet Mall

Minneapolis, MN 55402
(612) 991-1350

peter@peterbecklaw.com

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-----Original Message-----

From: DJ Heinle [mailto:dj.heinle@tkda.com]
Sent: Wednesday, January 21, 2015 9:50 PM
To: Peter Beck
Cc: Larry Smith
Subject: Re: NLNA P&Z Action on Lock Up

Peter,

The P+Z committee unanimously approved the project as presented. We'll bring it with all our motions for Board approval next week and I hope to be able to issue your letter at that time.

Thanks,

DJ Heinle, AIA
Education/Government Market Manager

TKDA
p 651-292-4481
c 612-387-6531
www.tkda.com<http://www.tkda.com>
Sent from my handheld phone

On Jan 21, 2015, at 9:44 PM, Peter Beck <peter@peterbecklaw.com<mailto:peter@peterbecklaw.com>> wrote:

DJ,

Thanks again for the opportunity to present the plan revisions for the proposed Lock Up facility to the P&Z Committee. When you have a minute, Larry and I would appreciate knowing what action the Committee took on the proposed project.

Thanks,

Peter Beck

<image001.jpg>

2600 US Bancorp Center
800 Nicollet Mall
Minneapolis, MN 55402
(612) 991-1350



**NORTH
LOOP**
NEIGHBORHOOD

January 21, 2015
Larry Smith
Peter Beck
2600 US Bancorp Center
800 Nicollet Mall
Minneapolis, MN 55402
Sent via email only

Motion of Support – 701 N 7th St. Self Storage

Dear Peter and Larry,

Your proposal on January 21st for the Self Storage facility at 701 N 7th St. consisted of a 5 story structure with possible coffee shop tenant space, a 56' tall building. The conceptual plans included a landscape area at the corner of N 7th Ave and Oak Lake Ave, surface parking, precast concrete panels and metal panels. A sign is planned on the building to face the freeway.

The North Loop Neighborhood Association supports the planned development as presented.

Sincerely,
North Loop Neighborhood Association

DJ Heinle

Digitally signed by DJ Heinle
DN: cn=DJ Heinle, o=TKDA,
ou=Architecture,
email=dj.heinle@tkda.com, c=US
Date: 2015.01.28 22:49:46 -06'00'

DJ Heinle, AIA
Co-Chair P+Z Committee

January 9, 2015

Minneapolis Planning Commission
Public Service Center
250 S 4th St, Room 300
Minneapolis, MN 55415

Re: 701 N. 7th Street North
618 - 7th Avenue North

To Whom It May Concern:

Weisman Investment Company, owner of the properties located at 701 7th Street North and 618 7th Avenue North, has agreed to sell the properties to Lock Up Minneapolis 701, LLC and in furtherance of such sale authorizes the application of Lock Up Minneapolis 701, LLC for the necessary approvals to develop a Lock Up Self-Storage Facility on the properties.

WEISMAN INVESTMENT COMPANY

A handwritten signature in black ink that reads "Ronald P. Weisman". The signature is written in a cursive style with a long horizontal flourish at the end.

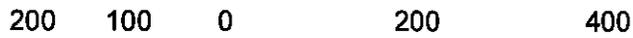
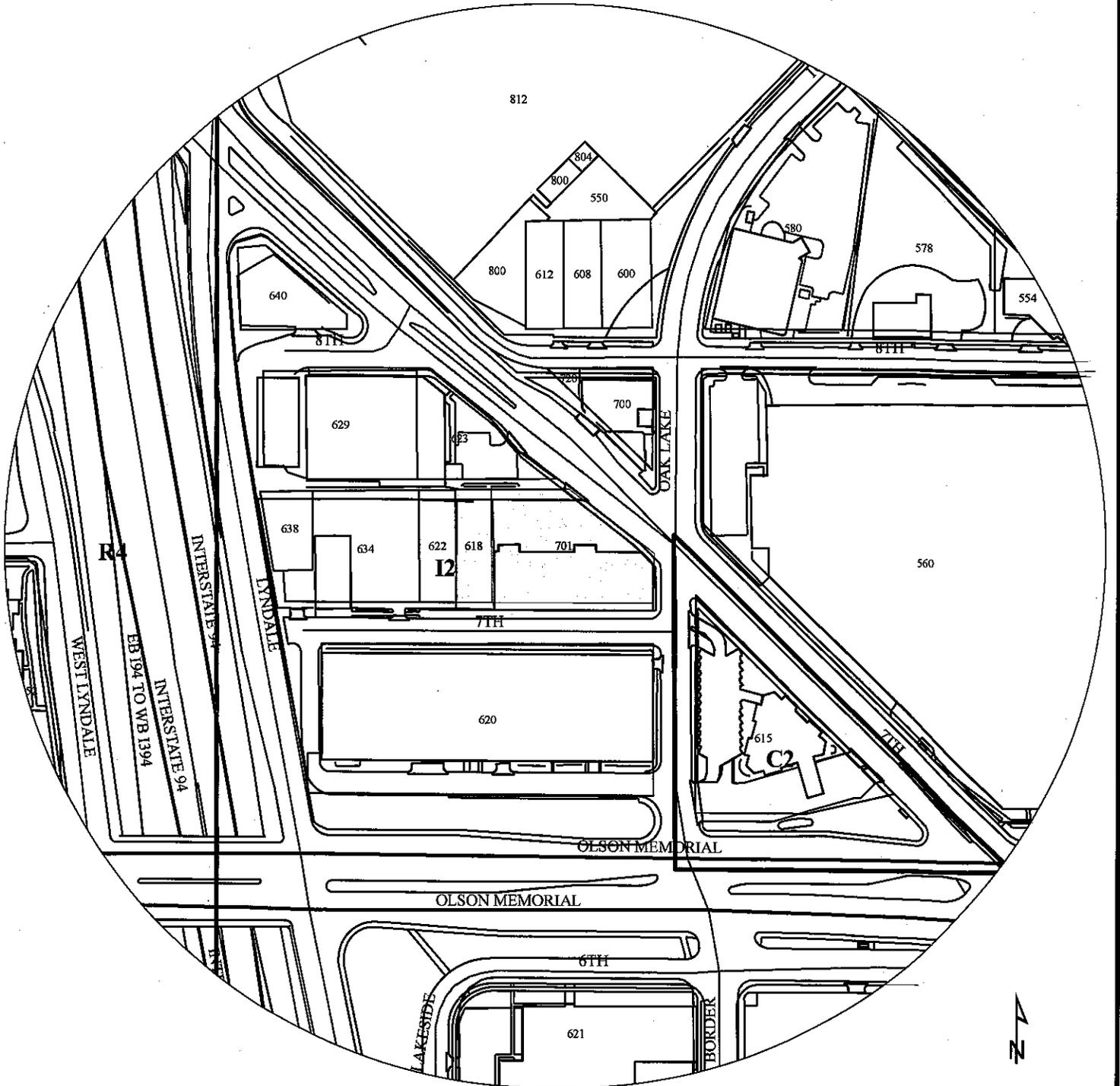
By: Ronald P. Weisman

Lock Up Minneapolis 701, LLC

NAME OF APPLICANT

5th

WARD

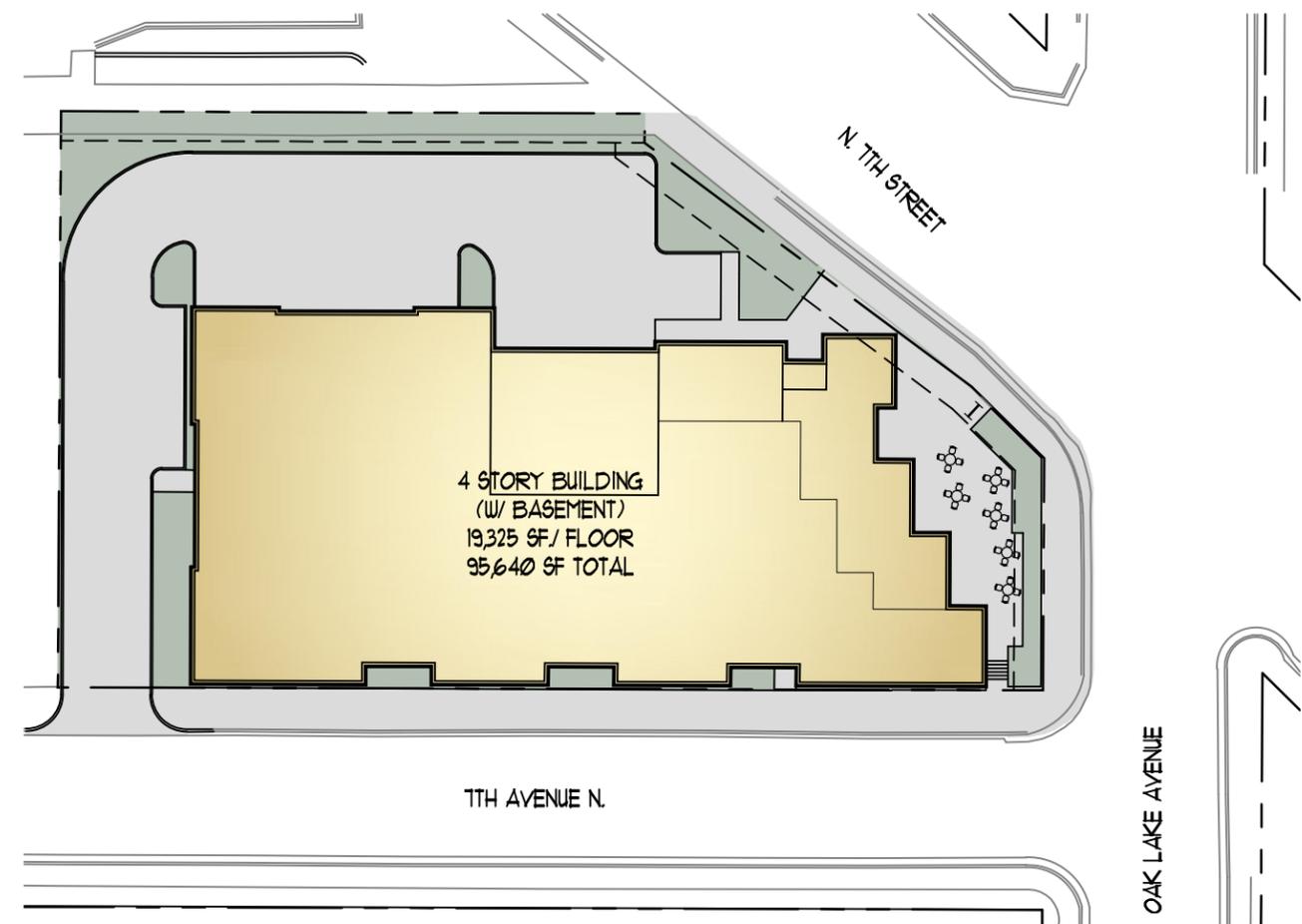


PROPERTY ADDRESS

701 7th Street North & 618 7th Avenue North

FILE NUMBER

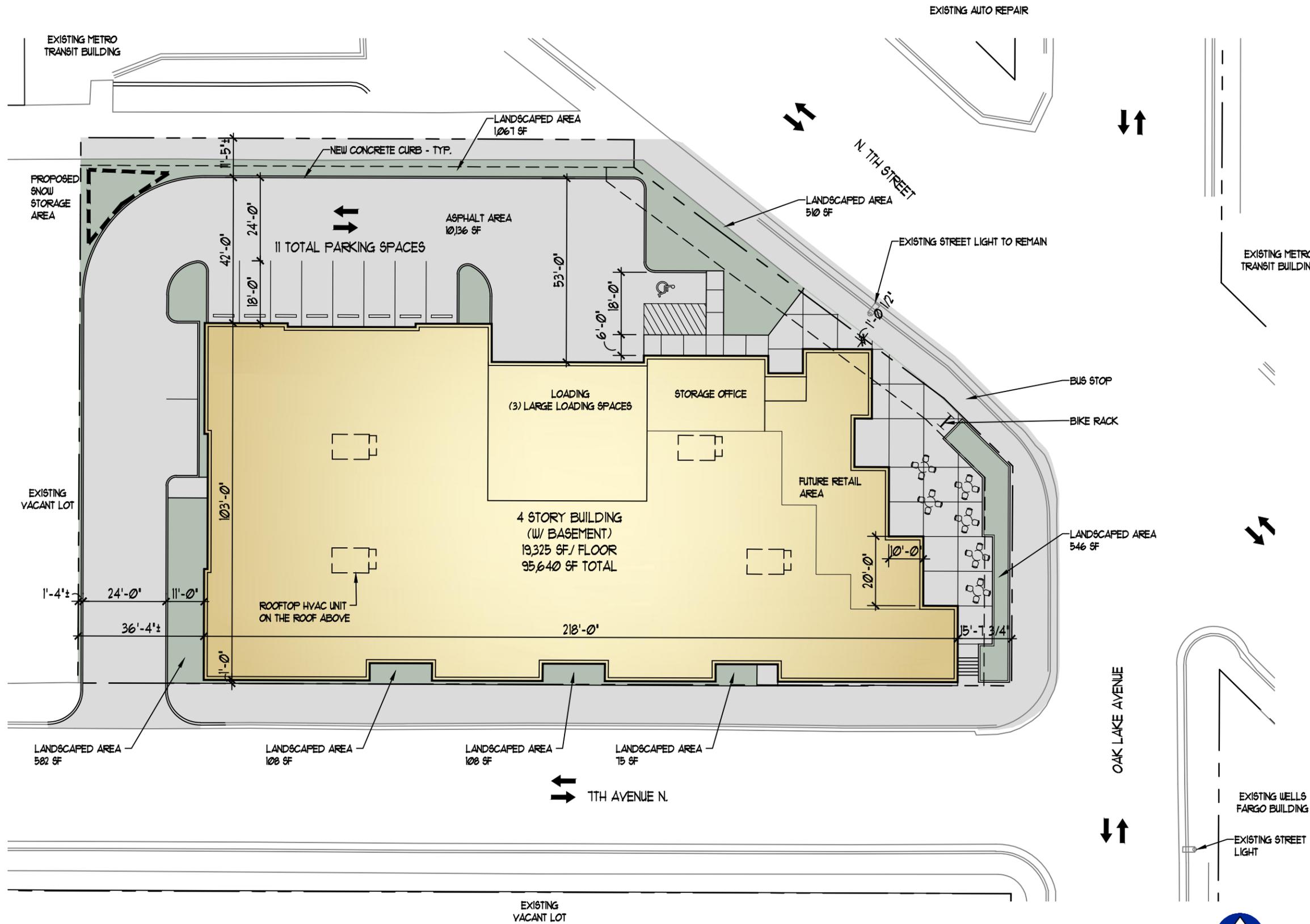
BZZ-7012



THE LOCKUP SELF STORAGE

701 North 7th Street, Minneapolis, Minnesota

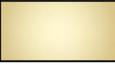




DEVELOPMENT DATA:

SITE AREA:	37,088 SF.
ZONING CLASSIFICATION:	1-2
ALLOWED F.A.R.	2.1 MAX
ALLOWED HEIGHT	56'-0"
EXISTING TOTAL IMPERVIOUS AREA	
PROPOSED DEVELOPMENT:	
BUILDING AREA:	95,640 SF.
BASEMENT:	18,165 SF.
FIRST FLOOR:	19,325 SF.
SECOND FLOOR:	19,325 SF.
THIRD FLOOR:	19,325 SF.
FOURTH FLOOR:	18,900 SF.
F.A.R.	2.51
MAX HEIGHT	53'-0"
PROVIDED PARKING	11 SPACES
TOTAL ASPHALT AREA:	10,136 SF.
TOTAL SIDEWALK AREA:	2,592 SF.
TOTAL IMPERVIOUS AREA:	32,053 SF.
TOTAL GREENSPACE AREA:	5,035 SF.

SITE SURFACE LEGEND

	BUILDING FOOTPRINT
	PARKING LOT ASPHALT
	SIDEWALK
	LANDSCAPING AND GRASS

THE LOCKUP SELF STORAGE
701 North 7th Street, Minneapolis, Minnesota

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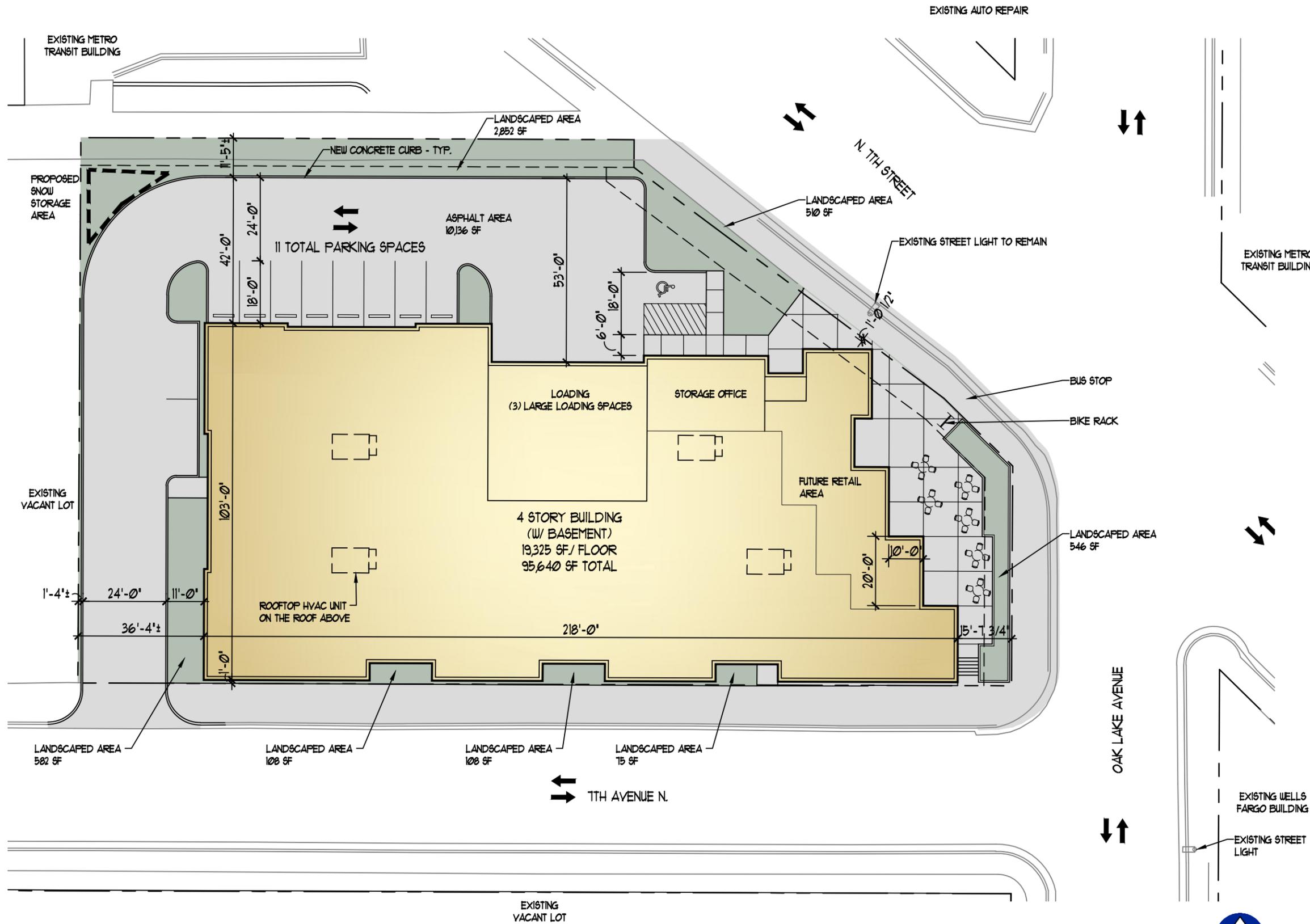

SITE PLAN
1" = 30'-0"
03.09.15



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Riverwoods, IL



DEVELOPMENT DATA:

SITE AREA:	37,088 SF.
ZONING CLASSIFICATION:	1-2
ALLOWED F.A.R.	2.1 MAX
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TOTAL GREENSPACE AREA:	5,035 SF.

SITE SURFACE LEGEND

	BUILDING FOOTPRINT
	PARKING LOT ASPHALT
	SIDEWALK
	LANDSCAPING AND GRASS

THE LOCKUP SELF STORAGE
701 North 7th Street, Minneapolis, Minnesota

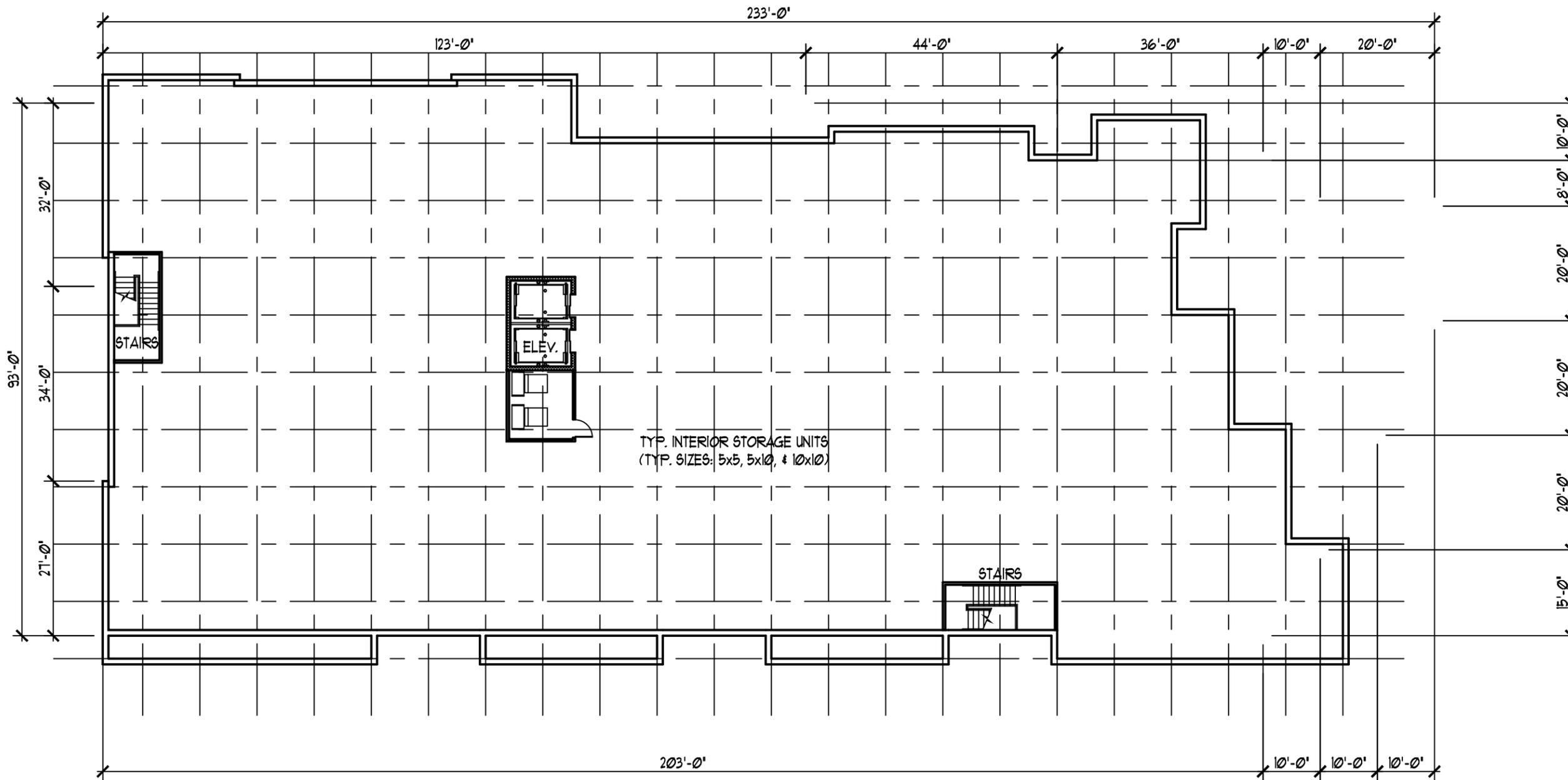
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SITE PLAN
1" = 30'-0"
03.09.15

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THE LOCKUP SELF STORAGE

701 North 7th Street, Minneapolis, Minnesota

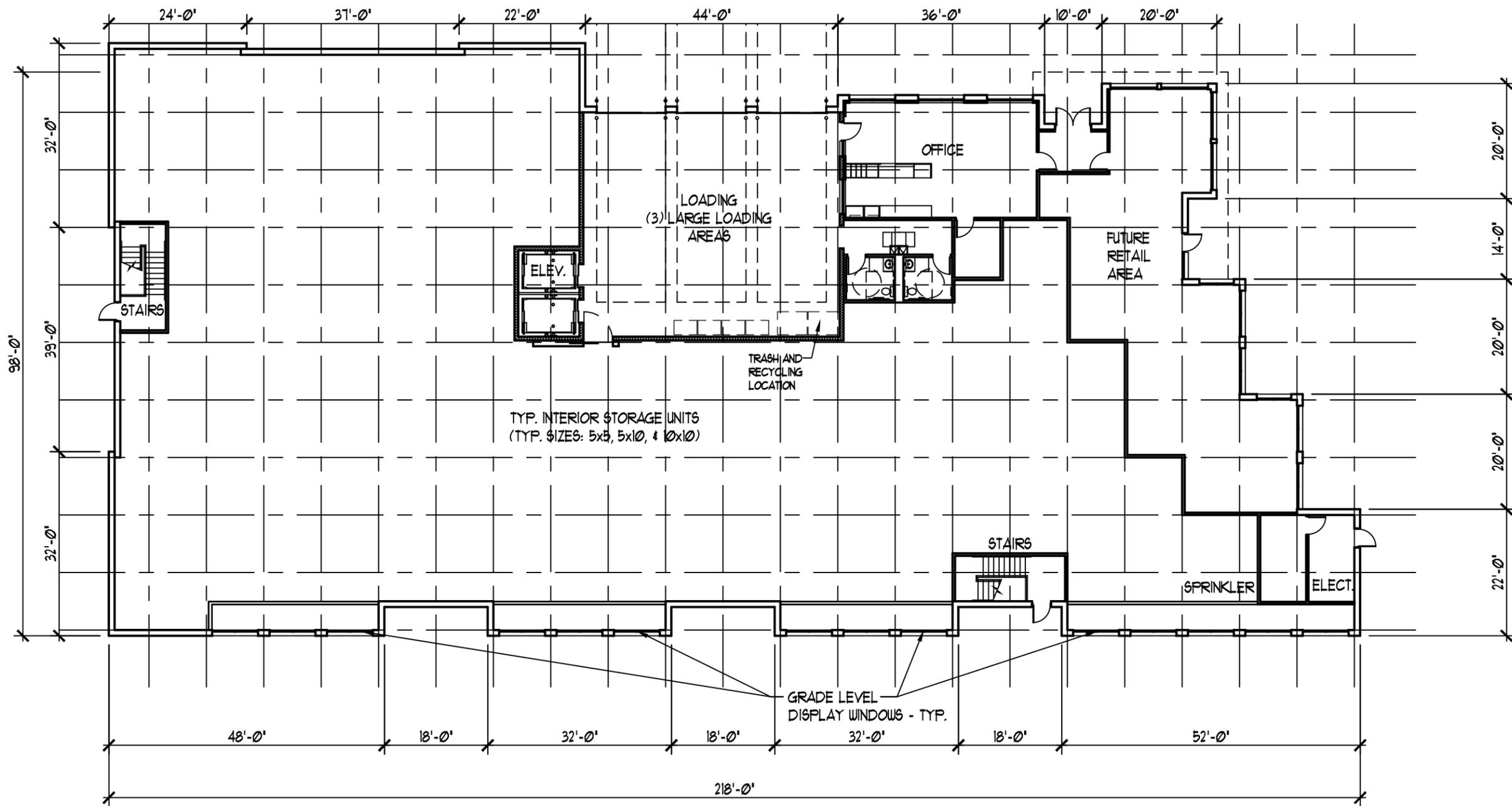


BASEMENT FLOOR PLAN

1" = 20'-0"

03.09.15





THE LOCKUP SELF STORAGE

701 North 7th Street, Minneapolis, Minnesota



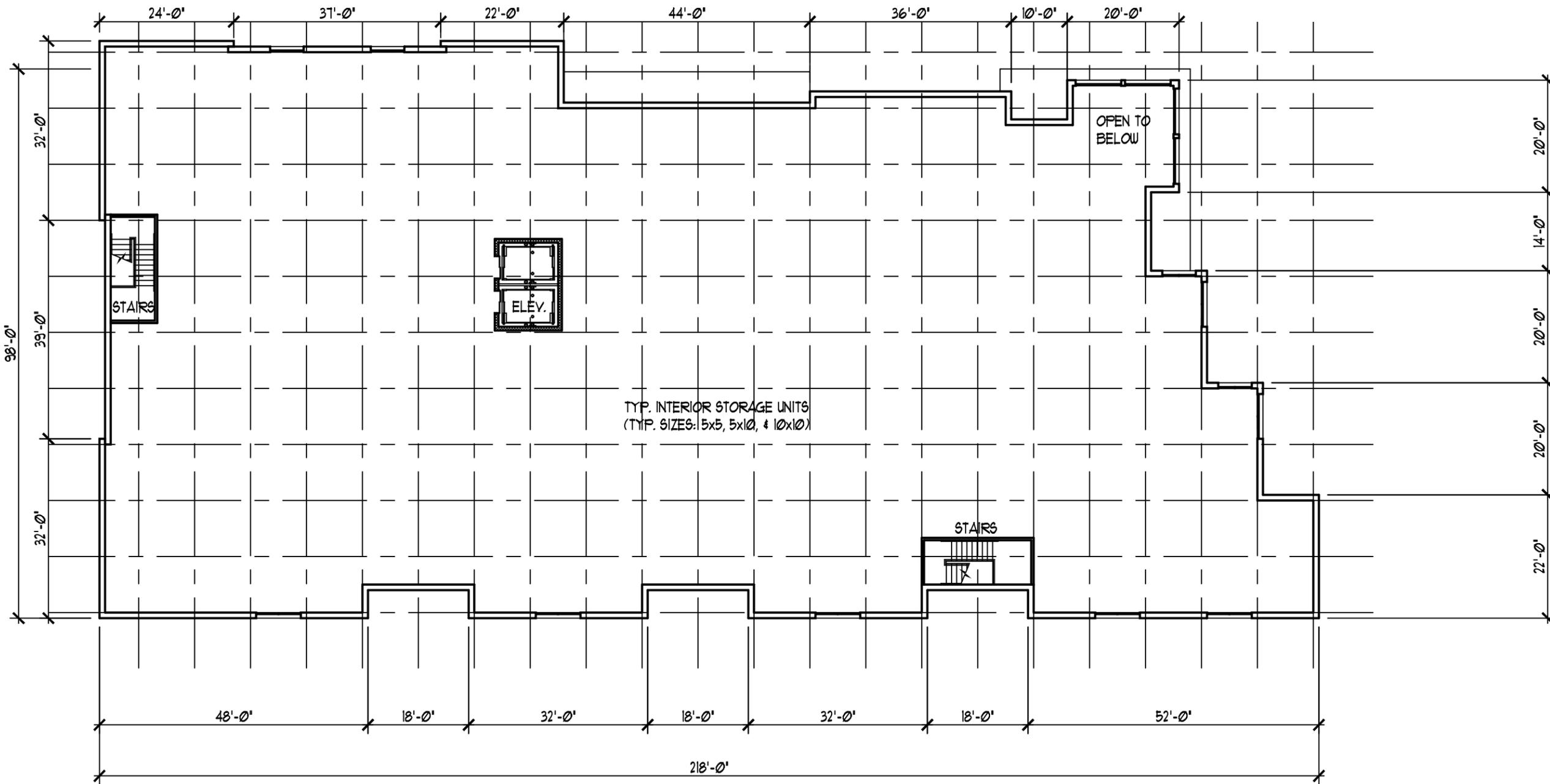
FIRST FLOOR PLAN

1" = 20'-0"

03.09.15



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THE LOCKUP SELF STORAGE

701 North 7th Street, Minneapolis, Minnesota



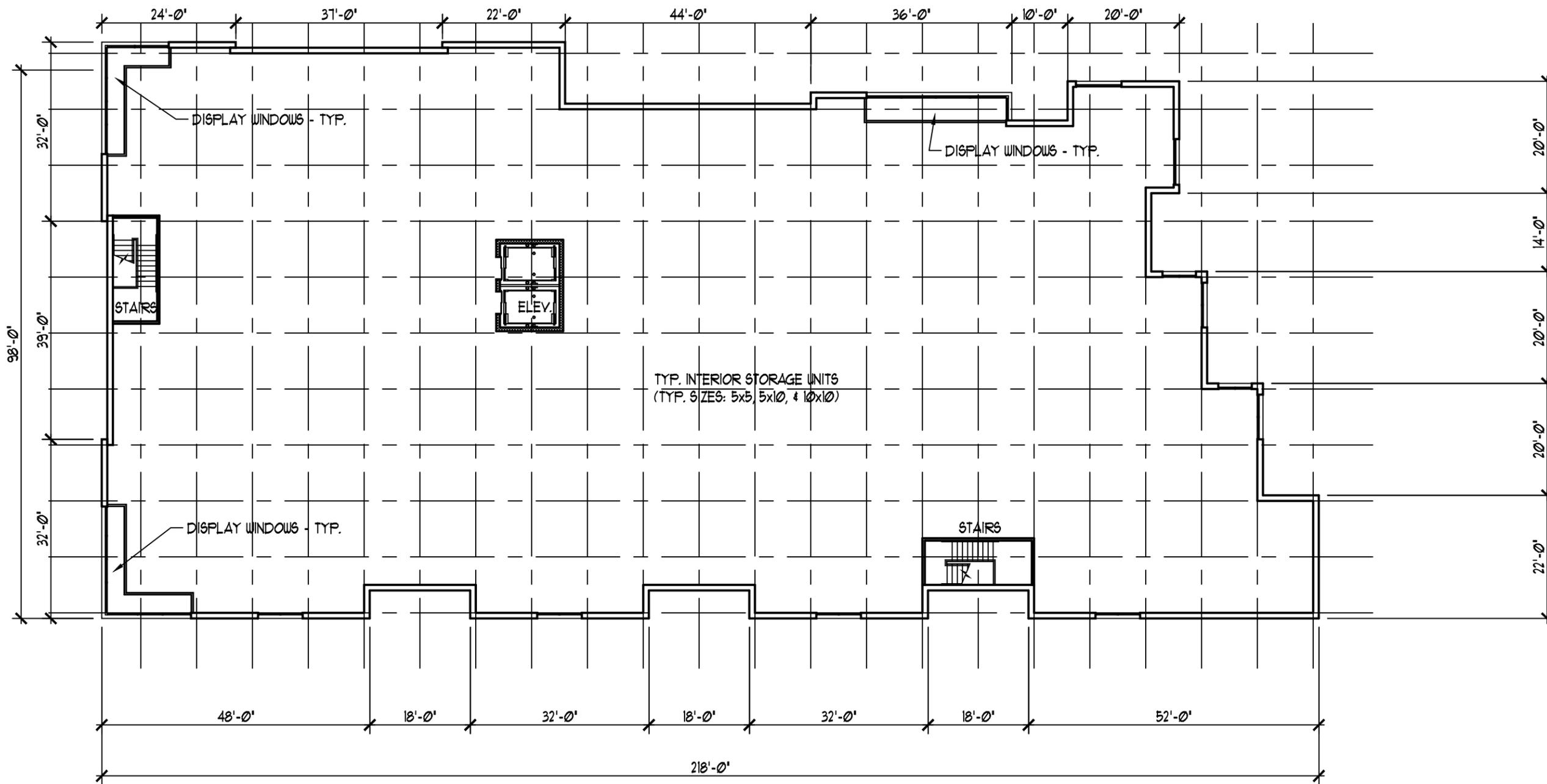
SECOND FLOOR PLAN

1" = 20'-0"

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THE LOCKUP SELF STORAGE

701 North 7th Street, Minneapolis, Minnesota



THIRD FLOOR PLAN

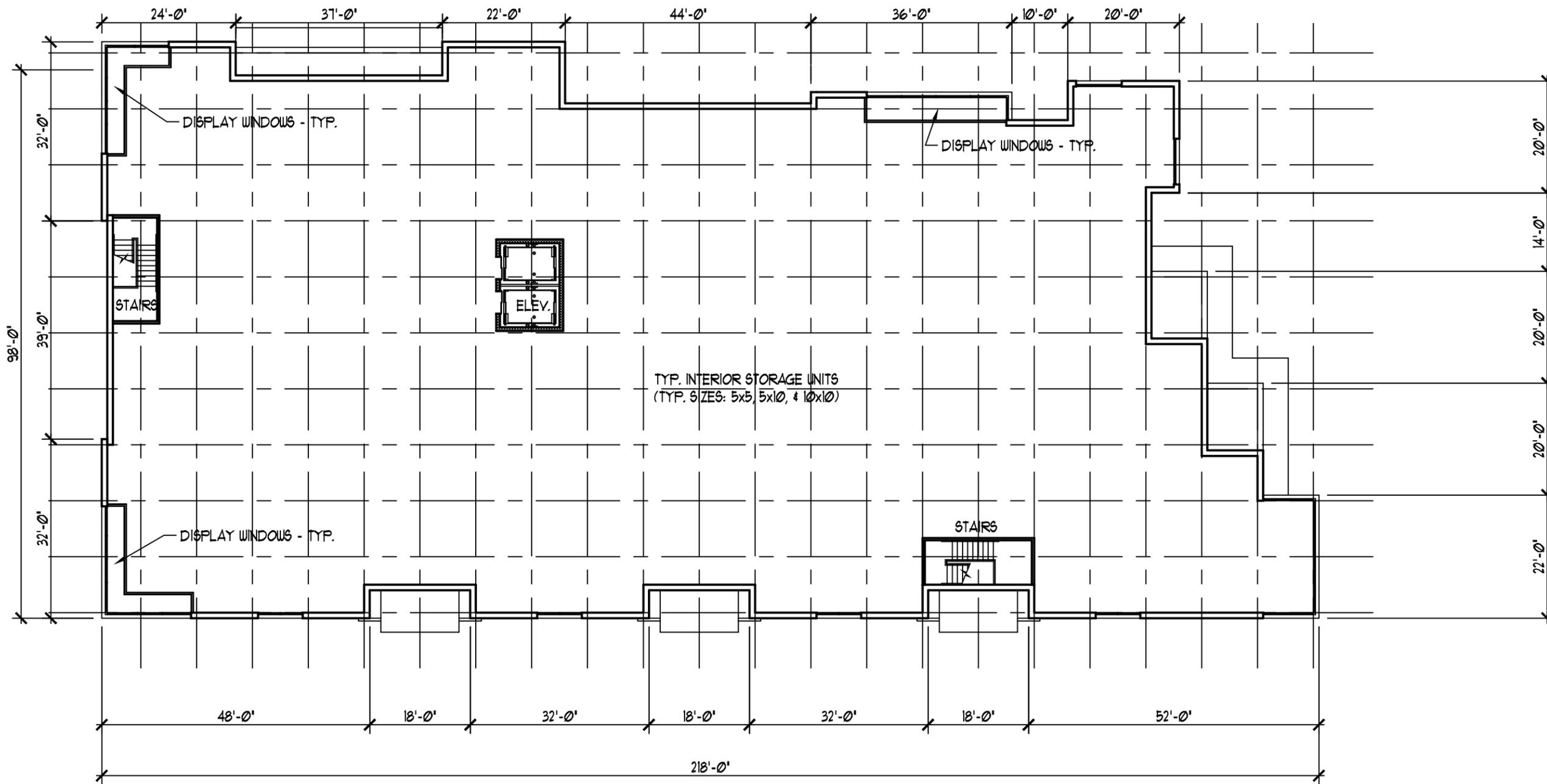
1" = 20'-0"

03.09.15



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THE LOCKUP SELF STORAGE

701 North 7th Street, Minneapolis, Minnesota



FOURTH FLOOR PLAN

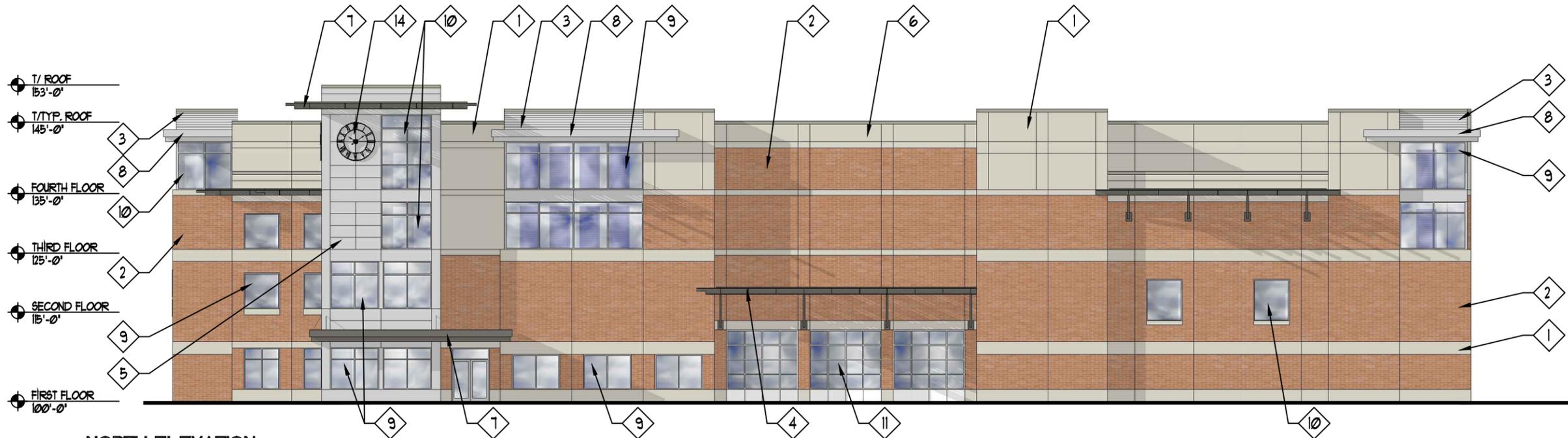
1" = 20'-0"

03.09.15



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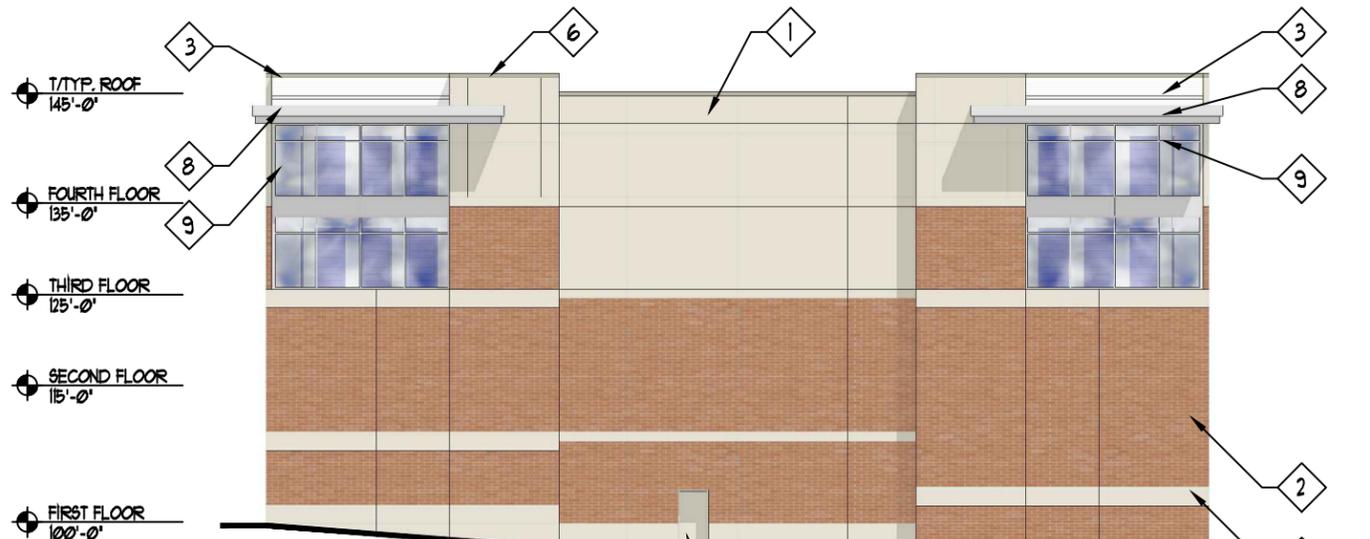
NORTH ELEVATION

WINDOW AREA REQUIREMENTS:

FLOOR	REQ'D	PROVIDED
FIRST (30%)	158 SF	253 SF
SECOND (10%)	218 SF	220 SF
THIRD (10%)	218 SF	346 SF
FOURTH (10%)	218 SF	384 SF

FLOOR	ELEVATION																												
	NORTH					EAST					SOUTH					WEST					TOTAL			TOTAL (REQUIRED WALLS ONLY)*					
	wall area**	required glazing	provided glazing	wall area	required glazing	provided glazing	wall area	required glazing	provided glazing	wall area	required glazing	provided glazing	wall area	required glazing	provided glazing	wall area	reqd. glaz.	prov. glaz.	wall area	required glazing	provided glazing	wall area	required glazing	provided glazing					
1	528	30%	158	48%	253	824	30%	247	48%	398	1744	30%	523	35%	612	824	0%	0	0%	0	3920	929	1263.0	3096	30%	928.8	40.8%	1263.0	
2	2180	10%	218	10.1%	220	1030	10%	103	23%	232	2180	10%	218	11%	240	1030	0%	0	0%	0	6420	539	692.0	5390	10%	539	12.8%	692.0	
3	2180	10%	218	15.9%	346	1030	10%	103	16%	164	2180	10%	218	14%	315	1030	0%	0	30%	309	6420	539	1134.0	5390	10%	539	15.3%	825.0	
4	2180	10%	218	17.6%	384	1030	10%	103	26%	272	2180	10%	218	18%	390	1030	0%	0	30%	309	6420	539	1355.0	5390	10%	539	19.4%	1046.0	
total	7068		812		1203	3914		556		1066	8284		1177		1557	3914		0		618	23180	2546	4444	19266	13.2%	2546	19.9%	3826	

*Totals in this column relate to total applicable wall area facing public streets or parking lot only (public street only for first floor) since this is what is required by the code.
 **Per code, wall area on first floor need not include walls directly facing a parking lot.



WEST ELEVATION

WINDOW AREA REQUIREMENTS:

FLOOR	REQ'D	PROVIDED
FIRST (0%)	0 SF	0 SF
SECOND (0%)	0 SF	0 SF
THIRD (0%)	0 SF	309 SF
FOURTH (0%)	0 SF	309 SF

MATERIALS LEGEND:

- ① PRECAST CONCRETE PANEL - SMOOTH FINISH
COLOR: SHERWIN WILLIAMS, SW 1036 ACCESSIBLE BEIGE
- ② PRECAST CONCRETE PANEL - BRICK FORMLINER
COLOR: SHERWIN WILLIAMS, SW 2803 ROCKWOOD TERRA COTTA
- ③ RIBBED PREFINISHED METAL PANEL, COLOR: PAC-CLAD, BONE WHITE
- ④ PAINTED STEEL SUNSHADE COLOR: MATCH PAC CLAD MEDIUM BRONZE
- ⑤ SMOOTH PREFINISH METAL PANEL, COLOR: PAC-CLAD, SILVER
- ⑥ PREFINISHED MTL. COPING, COLOR: PAC-CLAD, SANDSTONE
- ⑦ PREFINISHED METAL CANOPY
COLOR: PAC CLAD, MEDIUM BRONZE
- ⑧ PREFINISHED METAL OVERHANG, COLOR: PAC-CLAD, SILVER
- ⑨ CLEAR ANODIZED ALUM. FRAMING W/ CLEAR INSULATED GLAZING
- ⑩ CLEAR ANODIZED ALUM. FRAMING W/ INSULATED SPANDREL GLAZING
COLOR: GRAY
- ⑪ INSULATED MTL. OVERHEAD SECTIONAL DOOR W/ GLASS LITES
COLOR: CLEAR ANODIZED ALUMINUM
- ⑫ PAINTED HOLLOW METAL DOOR & FRAME
COLOR: SHERWIN WILLIAMS, SW 1036 ACCESSIBLE BEIGE
- ⑬ INTERNALLY ILLUMINATED LED SIGN - SEE SIGNAGE DETAILS
- ⑭ INTERNALLY ILLUMINATED CLOCK FACE



THE LOCKUP SELF STORAGE

701 North 7th Street, Minneapolis, Minnesota

BUILDING ELEVATIONS

1" = 20'-0"

03.09.15



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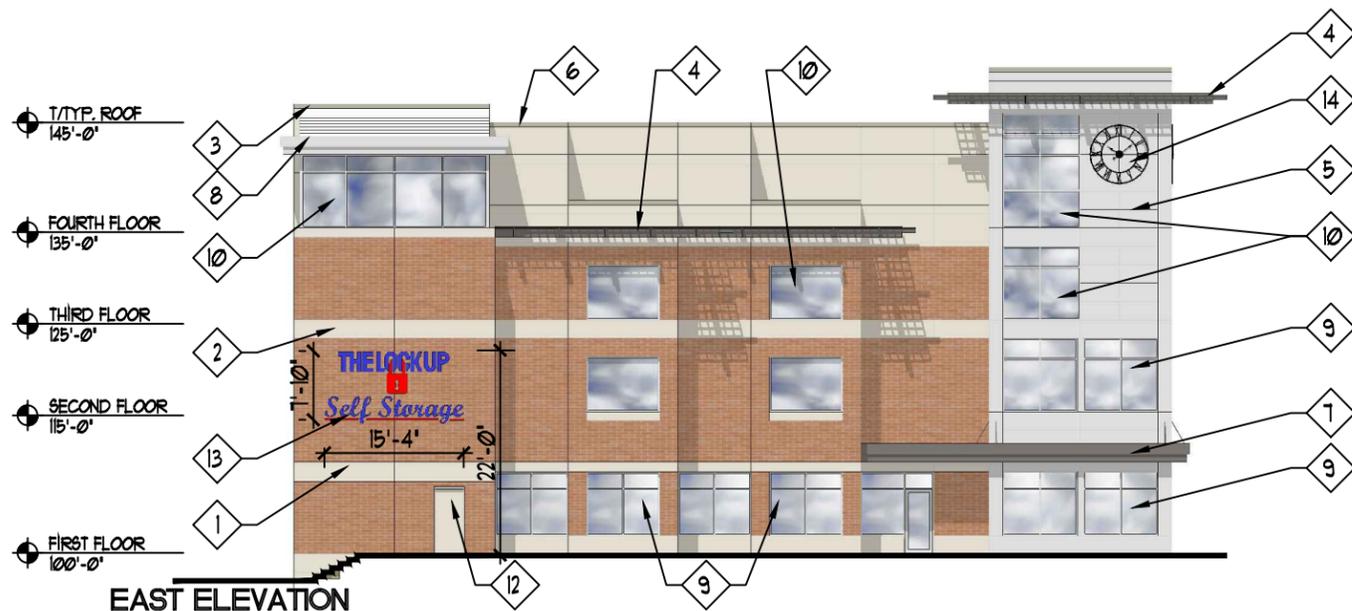
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SOUTH ELEVATION

WINDOW AREA REQUIREMENTS:

FLOOR	REQ'D	PROVIDED
FIRST (30%)	523 SF	612 SF
SECOND (10%)	218 SF	240 SF
THIRD (10%)	218 SF	315 SF
FOURTH (10%)	218 SF	390 SF



EAST ELEVATION

EAST WINDOW AREA REQUIREMENTS:

FLOOR	REQ'D	PROVIDED
FIRST (30%)	241 SF	398 SF
SECOND (10%)	103 SF	232 SF
THIRD (10%)	103 SF	164 SF
FOURTH (10%)	103 SF	272 SF

MATERIALS LEGEND:

- ① PRECAST CONCRETE PANEL - SMOOTH FINISH
COLOR: SHERWIN WILLIAMS, SW 1036 ACCESSIBLE BEIGE
- ② PRECAST CONCRETE PANEL - BRICK FORMLINER
COLOR: SHERWIN WILLIAMS, SW 2803 ROCKWOOD TERRA COTTA
- ③ RIBBED PREFINISHED METAL PANEL, COLOR: PAC-CLAD, BONE WHITE
- ④ PAINTED STEEL SUNSHADE COLOR: MATCH PAC CLAD MEDIUM BRONZE
- ⑤ SMOOTH PREFINISH METAL PANEL, COLOR: PAC-CLAD, SILVER
- ⑥ PREFINISHED MTL. COPING, COLOR: PAC-CLAD, SANDSTONE
- ⑦ PREFINISHED METAL CANOPY
COLOR: PAC CLAD, MEDIUM BRONZE
- ⑧ PREFINISHED METAL OVERHANG, COLOR: PAC-CLAD, SILVER
- ⑨ CLEAR ANODIZED ALUM. FRAMING W/ CLEAR INSULATED GLAZING
- ⑩ CLEAR ANODIZED ALUM. FRAMING W/ INSULATED SPANDREL GLAZING
COLOR: GRAY
- ⑪ INSULATED MTL. OVERHEAD SECTIONAL DOOR W/ GLASS LITES
COLOR: CLEAR ANODIZED ALUMINIUM
- ⑫ PAINTED HOLLOW METAL DOOR & FRAME
COLOR: SHERWIN WILLIAMS, SW 1036 ACCESSIBLE BEIGE
- ⑬ INTERNALLY ILLUMINATED LED SIGN - SEE SIGNAGE DETAILS
- ⑭ INTERNALLY ILLUMINATED CLOCK FACE



THE LOCKUP SELF STORAGE

701 North 7th Street, Minneapolis, Minnesota

BUILDING ELEVATIONS

1" = 20'-0"

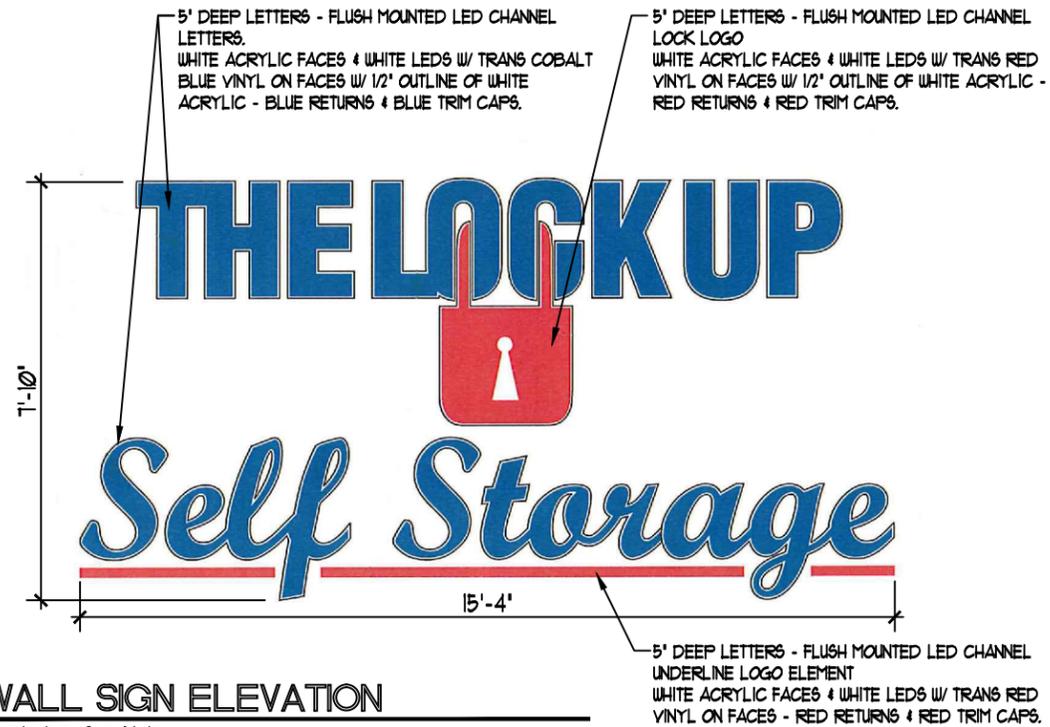
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WALL SIGN ELEVATION
NOT TO SCALE



THE LOCKUP SELF STORAGE

701 North 7th Street, Minneapolis, Minnesota

BUILDING SIGNAGE



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THE LOCKUP SELF STORAGE

701 North 7th Street, Minneapolis, Minnesota

NORTHEAST VIEW

03.09.15



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THE LOCKUP SELF STORAGE

701 North 7th Street, Minneapolis, Minnesota

SOUTHEAST VIEW

03.09.15



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THE LOCKUP SELF STORAGE

701 North 7th Street, Minneapolis, Minnesota

SOUTHWEST VIEW

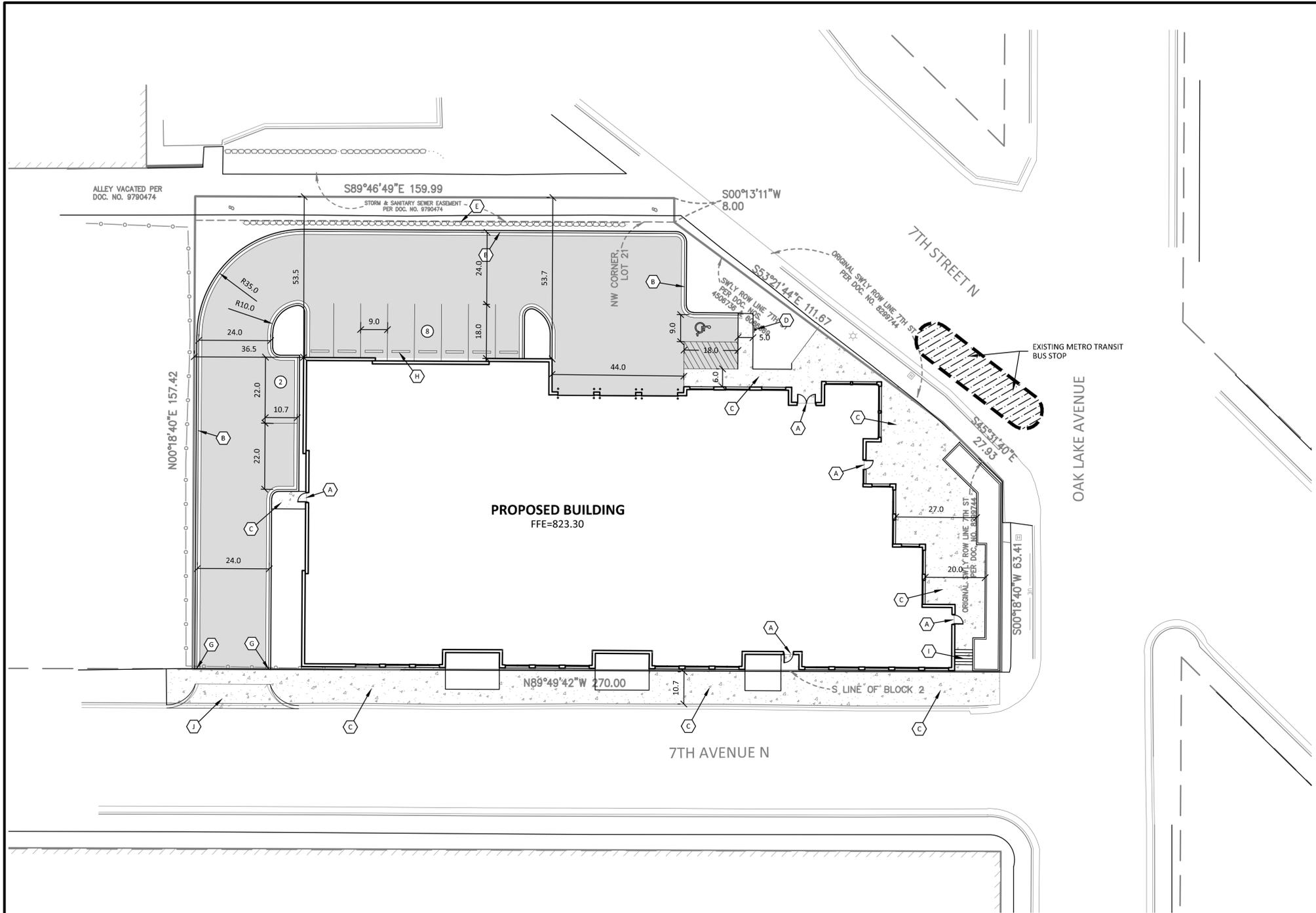
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Riverwoods, IL



LEGEND

	PROPOSED	EXISTING	
PROPERTY LIMIT	---	---	STANDARD DUTY ASPHALT PAVING
CURB & GUTTER	---	---	CONCRETE PAVING
EASEMENT	---	---	
BUILDING	---	---	
RETAINING WALL	○-○-○-○	---	
SIGN	●		
PIPE BOLLARD	⊙		
NUMBER OF PARKING STALLS PER ROW	⊗		
KEY NOTE	⬡		

DEVELOPMENT SUMMARY

AREAS			
GROSS SITE AREA	37,088 SF	0.85 AC	
PROPOSED IMPERVIOUS AREA	33,688 SF	0.77 AC	
PROPOSED PERVIOUS AREA	3,400 SF	0.08 AC	
IMPERVIOUS PERCENTAGE		90.6%	
PERVIOUS PERCENTAGE		9.4%	

- DEVELOPMENT NOTES**
- ALL DIMENSIONS ARE ROUNDED TO THE NEAREST TENTH FOOT.
 - ALL DIMENSIONS SHOWN ARE TO THE FACE OF CURB TO FACE OF CURB UNLESS OTHERWISE NOTED.
 - ALL AREAS ARE ROUNDED TO THE NEAREST SQUARE FOOT.
 - ALL PARKING STALLS TO BE 9' IN WIDTH AND 18' IN LENGTH UNLESS OTHERWISE INDICATED.
 - CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF EXIT PORCHES, RAMPS, PRECISE BUILDING DIMENSIONS AND EXACT BUILDING UTILITY ENTRANCE LOCATIONS.
 - SEE ARCHITECTURAL PLANS FOR SIGNAGE DETAILS.
 - SEE ARCHITECTURAL PLANS FOR LIGHT EXACT LOCATIONS OF LIGHT POLES AND PHOTOMETRICS.
 - REFER TO ALTA SURVEY FOR LOT BOUNDARIES, LOT NUMBERS, LOT AREAS, AND LOT DIMENSIONS.
 - ALL GRADIENTS ON SIDEWALKS ALONG THE ADA ROUTE SHALL HAVE A MAXIMUM LONGITUDINAL SLOPE OF 5% (1:20), EXCEPT AT CURB RAMPS (1:12), AND A MAXIMUM CROSS SLOPE OF 2.08% (1:48). THE MAXIMUM SLOPE IN ANY DIRECTION ON AN ADA PARKING STALL OR ACCESS ISLE SHALL BE 2.08% (1:48). THE CONTRACTOR SHALL REVIEW AND VERIFY THE GRADIENT IN THE FIELD ALONG THE ADA ROUTES PRIOR TO PLACING CONCRETE OR BITUMINOUS. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY IF THERE IS A DISCREPANCY BETWEEN THE GRADIENT IN THE FIELD VERSUS THE DESIGN GRADIENT.
 - "NO PARKING" SIGNS SHALL BE PLACED AS REQUIRED BY CITY.

- KEY NOTES**
- BUILDING STOOPS & DOORS - REFER TO ARCHITECTURAL PLANS.
 - B-612 CONCRETE CURB AND GUTTER.
 - CONCRETE SIDEWALK.
 - HANDICAP PARKING SIGN.
 - MODULAR BLOCK RETAINING WALL.
 - ACCESSIBLE RAMP.
 - CURB TAPER.
 - CONCRETE STOP BLOCK.
 - CONCRETE STAIRS - REFER TO ARCHITECTURAL PLANS.
 - CONCRETE DRIVEWAY APRON.

Sambatek
www.sambatek.com
12800 Whitewater Drive, Suite 300
Mimnetonka, MN 55343
763.476.6010 telephone
763.476.8532 facsimile
Engineering | Surveying | Planning | Environmental

Client
PARTNERS IN DESIGN ARCHITECTS
2610 LAKE COOK ROAD, SUITE 280
RIVERWOODS, IL 60015

Project
THE LOCK-UP

Location
701 N 7TH STREET
MINNEAPOLIS, MN

Certification

PRELIMINARY

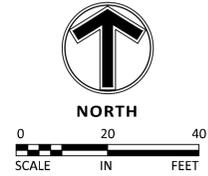
Summary
Designed: JFJ Drawn: JFJ
Approved: MCA Book / Page: 1102/60
Phase: PDR SUBMIT Initial Issue: 03/10/2015

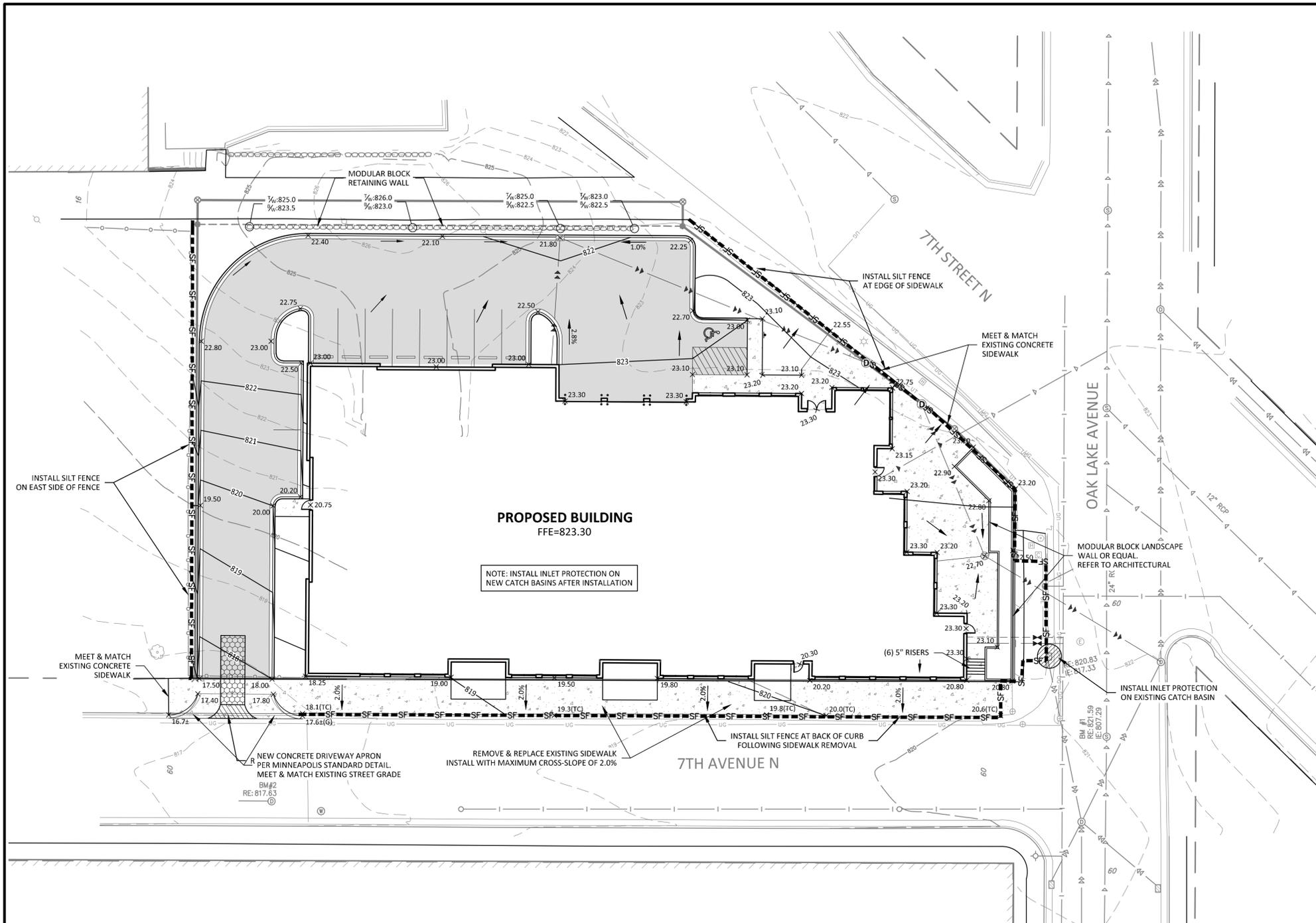
Revision History
No. Date By Submittal / Revision

Sheet Title
SITE PLAN

Sheet No. Revision
C2.0

Project No. BRB20163





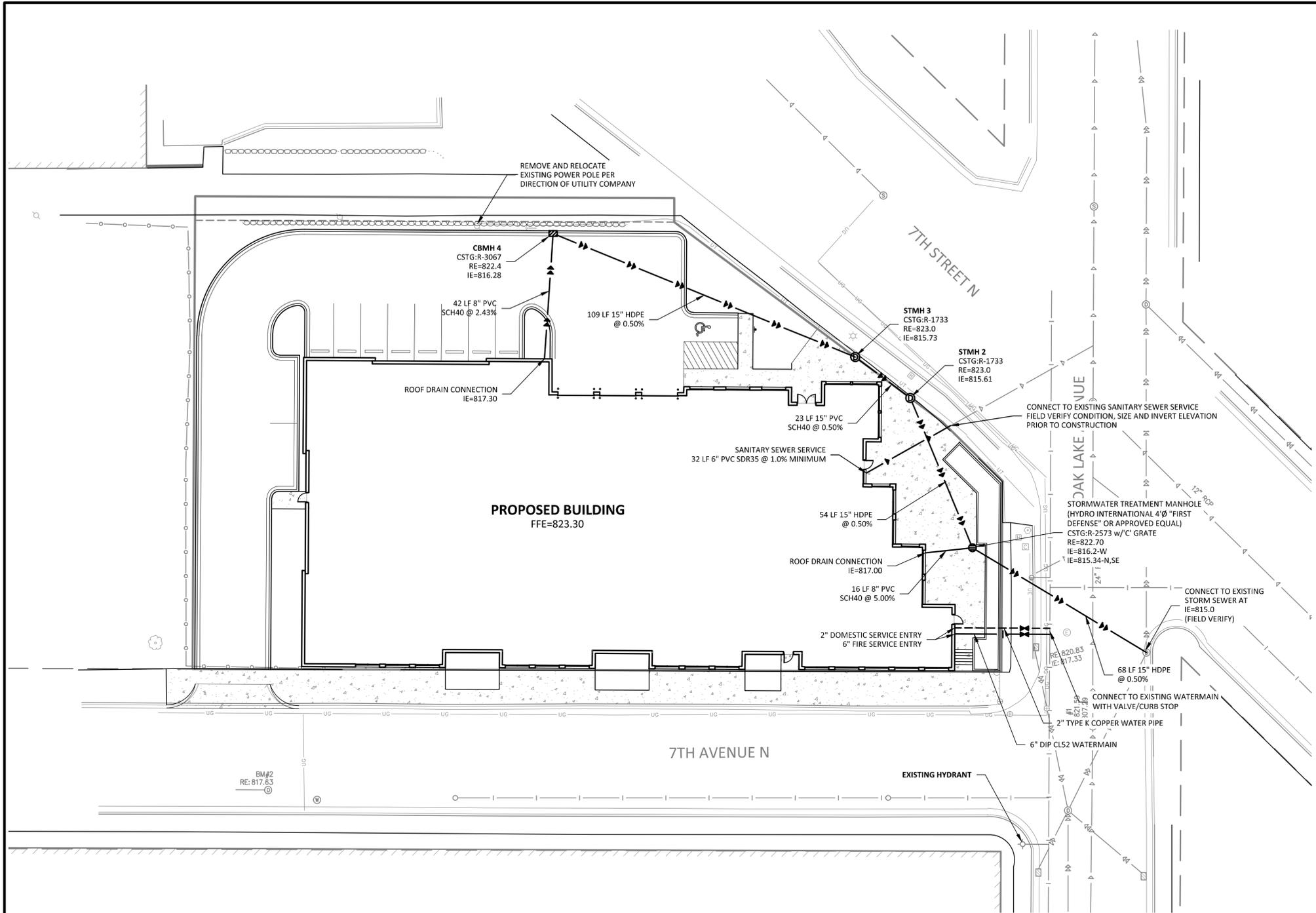
LEGEND

	PROPOSED	EXISTING
PROPERTY LIMIT	---	---
CURB & GUTTER	---	---
STORM SEWER	⊙	⊙
DRAINTILE	---	---
BUILDING	---	---
RETAINING WALL	⊖	⊖
WETLAND LIMITS	---	---
TREELINE	---	---
SPOT ELEVATION	962.5 X 902	962.5 X 902
CONTOUR	---	---
RIP RAP	⊖	⊖
OVERFLOW ELEV.	EOE 902.5	ST 5 1042.56
SOIL BORINGS	---	---

- GRADING NOTES**
- PROPOSED CONTOURS ARE TO FINISHED SURFACE ELEVATION. SPOT ELEVATIONS ALONG PROPOSED CURB DENOTE GUTTER GRADE.
 - CONTRACTOR SHALL REVIEW PAVEMENT GRADIENT AND CONSTRUCT "GUTTER OUT" WHERE WATER DRAINS AWAY FROM CURB. ALL OTHER AREAS SHALL BE CONSTRUCTED AS "GUTTER IN" CURB.
 - ALL GRADIENT ON SIDEWALKS ALONG THE ADA ROUTE SHALL HAVE A MAXIMUM LONGITUDINAL SLOPE OF 5% (1:20), EXCEPT AT CURB RAMPS (1:12), AND A MAXIMUM CROSS SLOPE OF 2.08% (1:48). THE MAXIMUM SLOPE IN ANY DIRECTION ON AN ADA PARKING STALL OR ACCESS AISLE SHALL BE IN 2.08% (1:48). THE CONTRACTOR SHALL REVIEW AND VERIFY THE GRADIENT IN THE FIELD ALONG THE ADA ROUTES PRIOR TO PLACING CONCRETE OR BITUMINOUS. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY IF THERE IS A DISCREPANCY BETWEEN THE GRADIENT IN THE FIELD VERSUS THE DESIGN GRADIENT.
 - THE CONTRACTOR IS CAUTIONED THAT "THE SUBSURFACE UTILITY INFORMATION SHOWN ON THESE PLANS IS A UTILITY QUALITY LEVEL D. THIS QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF C/ASCE 38-02 TITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA". THE CONTRACTOR AND/OR SUBCONTRACTORS SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, BY CONTACTING THE NOTIFICATION CENTER (GOPHER STATE ONE FOR MINNESOTA AT 1-800-252-1166). THE CONTRACTOR AND/OR SUBCONTRACTOR AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY HIS OR HER FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UTILITIES (UNDERGROUND AND OVERHEAD). IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.
 - THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASES OF THIS PROJECT. THE CONTRACTOR WILL BE HELD SOLELY RESPONSIBLE FOR ANY DAMAGES TO THE ADJACENT PROPERTIES OCCURRING DURING THE CONSTRUCTION PHASES OF THIS PROJECT.
 - SAFETY NOTICE TO CONTRACTORS: IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS ON THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS. THE DUTY OF THE ENGINEER OR THE DEVELOPER TO CONDUCT CONSTRUCTION REVIEW OF THE CONTRACTOR'S PERFORMANCE IS NOT INTENDED TO INCLUDE REVIEW OF THE ADEQUACY OF THE CONTRACTOR'S SAFETY MEASURES IN, ON OR NEAR THE CONSTRUCTION SITE.
 - THE CONTRACTOR SHALL COMPLETE THE SITE GRADING CONSTRUCTION IN ACCORDANCE WITH THE REQUIREMENTS OF THE OWNER'S SOILS ENGINEER. ALL SOIL TESTING SHALL BE COMPLETED BY THE OWNER'S SOILS ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL REQUIRED SOIL TESTS AND INSPECTIONS WITH THE SOILS ENGINEER.
 - PRIOR TO PLACEMENT OF THE AGGREGATE BASE, A TEST ROLL WILL BE REQUIRED ON THE STREET AND PARKING AREA SUBGRADE. THE CONTRACTOR SHALL PROVIDE A LOADED TANDEM AXLE TRUCK WITH A GROSS WEIGHT OF 25 TONS. THE TEST ROLLING SHALL BE AT THE DIRECTION OF THE SOILS ENGINEER AND SHALL BE COMPLETED IN AREAS AS DIRECTED BY THE SOILS ENGINEER. THE SOILS ENGINEER SHALL DETERMINE WHICH SECTIONS OF THE STREET OR PARKING AREA ARE UNSTABLE. CORRECTION OF THE SUBGRADE SOILS SHALL BE COMPLETED IN ACCORDANCE WITH THE REQUIREMENTS OF THE SOILS ENGINEER.
 - REPLACE ALL SUBGRADE SOIL DISTURBED DURING THE CONSTRUCTION THAT HAVE BECOME UNSUITABLE AND WILL NOT PASS A TEST ROLL. REMOVE UNSUITABLE SOIL FROM THE SITE AND IMPORT SUITABLE SOIL AT NO ADDITIONAL COST TO THE OWNER.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING AND MAINTAINING VEHICULAR AND PEDESTRIAN TRAFFIC CONTROL DEVICES SUCH AS BARRICADES, WARNING SIGNS, DIRECTIONAL SIGNS, FLAGMEN AND LIGHTS TO CONTROL THE MOVEMENT OF TRAFFIC WHERE NECESSARY. TRAFFIC CONTROL DEVICES SHALL CONFORM TO APPROPRIATE MINNESOTA DEPARTMENT OF TRANSPORTATION STANDARDS.
 - RETAINING WALL(S) SHALL BE CONSTRUCTED OF MODULAR BLOCK MATERIAL. THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER AND LOCAL AUTHORITY CERTIFIED ENGINEERING DRAWINGS, DESIGN CALCULATIONS AND SOIL BORINGS. THE CERTIFIED ENGINEER FOR THE RETAINING WALL(S) SHALL PROVIDE CONSTRUCTION OBSERVATIONS OF THE RETAINING WALL IMPROVEMENT, AND A LETTER CERTIFYING THE INSTALLATION OF THE WALL(S) WAS CONSTRUCTED IN CONFORMANCE WITH THE PLANS AND SPECIFICATIONS.

THE SUBSURFACE UTILITY INFORMATION SHOWN ON THESE PLANS IS A UTILITY QUALITY LEVEL D. THIS QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF ASCE/C 38-02, TITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA". THE CONTRACTOR AND/OR SUBCONTRACTORS SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK, BY CONTACTING THE NOTIFICATION CENTER (GOPHER STATE ONE FOR MINNESOTA). THE CONTRACTOR AND/OR SUBCONTRACTOR AGREES TO BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES, WHICH MIGHT BE OCCASIONED BY HIS OR HER FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UTILITIES (UNDERGROUND AND OVERHEAD).

IF THE CONTRACTOR ENCOUNTERS ANY DRAIN TILE WITHIN THE SITE, HE OR SHE SHALL NOTIFY THE ENGINEER WITH THE LOCATION, SIZE, INVERT AND IF THE TILE LINE IS ACTIVE. NO ACTIVE DRAIN TILE SHALL BE BACKFILLED WITHOUT APPROVAL FROM THE PROJECT ENGINEER.



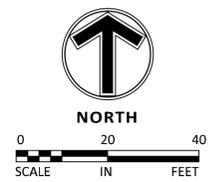
LEGEND

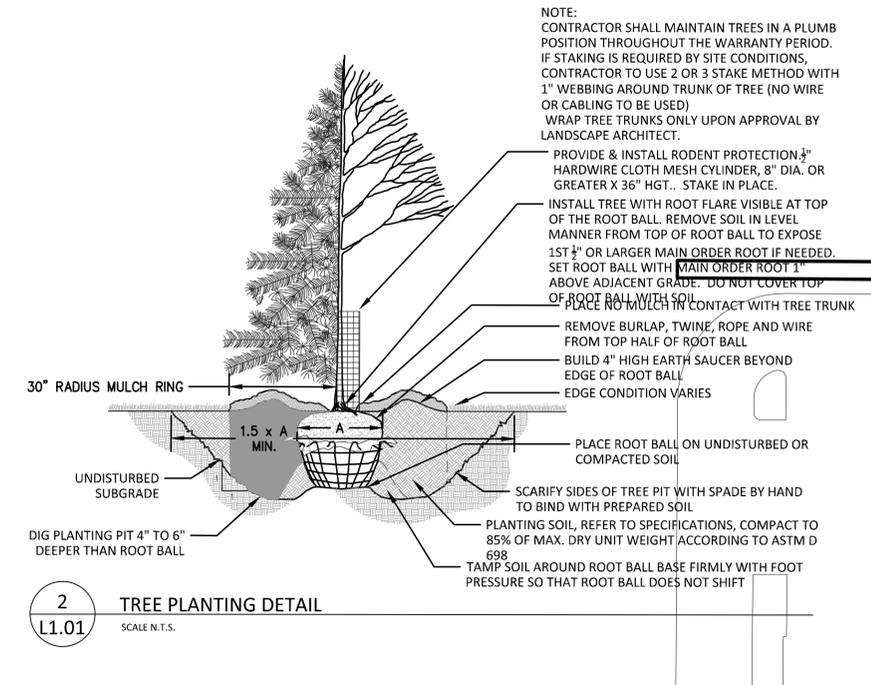
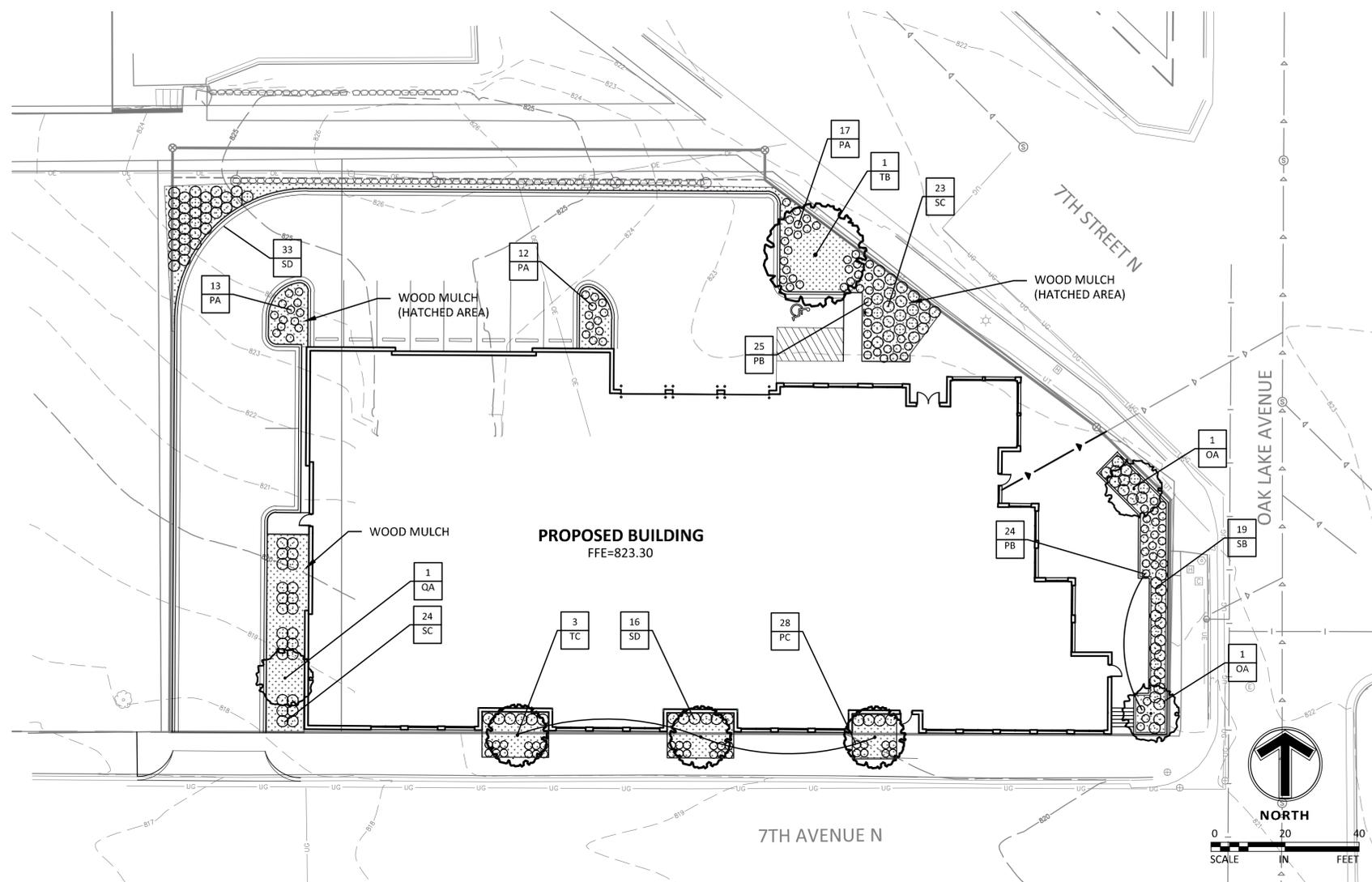
	PROPOSED	EXISTING
CURB & GUTTER		
STORM SEWER		
SANITARY SEWER		
FORCEMAIN (SAN.)		
WATERMAIN		
EASEMENT		
DRAINTILE		
GAS LINE		
ELECTRIC		
TELEPHONE		

UTILITY CONSTRUCTION NOTES

- A. THE UTILITY IMPROVEMENTS FOR THIS PROJECT SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE "STANDARD UTILITIES SPECIFICATIONS" AS PUBLISHED BY THE CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM), EXCEPT AS MODIFIED HEREIN. CONTRACTOR SHALL OBTAIN A COPY OF THESE SPECIFICATIONS.
 1. ALL UTILITIES SHALL BE CONSTRUCTED IN ACCORDANCE TO CITY REQUIREMENTS.
 2. CONTRACTOR SHALL NOT OPEN, TURN OFF, INTERFERE WITH, OR ATTACH ANY PIPE OR HOSE TO OR TAP WATERMAIN BELONGING TO THE CITY UNLESS DULY AUTHORIZED TO DO SO BY THE CITY. ANY ADVERSE CONSEQUENCES OF ANY SCHEDULED OR UNSCHEDULED DISRUPTIONS OF SERVICE TO THE PUBLIC ARE THE LIABILITY OF THE CONTRACTOR
 3. A MINIMUM VERTICAL SEPARATION OF 18 INCHES IS REQUIRED AT ALL WATERMAIN AND SEWER MAIN (BUILDING, STORM AND SANITARY) CROSSINGS.
- B. ALL MATERIALS SHALL BE AS SPECIFIED IN CEAM SPECIFICATIONS EXCEPT AS MODIFIED HEREIN.
 1. ALL MATERIALS SHALL COMPLY WITH THE REQUIREMENTS OF THE CITY.
 2. ALL SANITARY SEWER TO BE PVC SDR-35, UNLESS NOTED OTHERWISE.
 3. ALL WATERMAIN TO BE DUCTILE IRON - CLASS 52, WITH 7.5 FEET MINIMUM COVER.
 4. ALL STORM SEWER PIPE TO BE DUAL-WALL HDPE PIPE OR REINFORCED CONCRETE PIPE WITH R-4 JOINTS AND RUBBER GASKETS.
- C. CONTRACTOR SHALL REFER TO ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF VESTIBULE, EXIT PORCHES, RAMP, TRUCK DOCKS, PRECISE BUILDING DIMENSIONS AND EXACT BUILDING UTILITY ENTRANCE LOCATIONS.
- D. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANY AT LEAST 48 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS. THE LOCATIONS OF SMALL UTILITIES SHALL BE OBTAINED BY THE CONTRACTOR, BY CALLING GOPHER STATE ONE CALL AT 454-0002.
- E. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASES OF THIS PROJECT. THE CONTRACTOR WILL BE HELD SOLELY RESPONSIBLE FOR ANY DAMAGES TO THE ADJACENT PROPERTIES OCCURRING DURING THE CONSTRUCTION PHASES OF THIS PROJECT.
- F. SAFETY NOTICE TO CONTRACTORS: IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS ON THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS. THE DUTY OF THE ENGINEER OR THE DEVELOPER TO CONDUCT CONSTRUCTION REVIEW OF THE CONTRACTOR'S PERFORMANCE IS NOT INTENDED TO INCLUDE REVIEW OF THE ADEQUACY OF THE CONTRACTOR'S SAFETY MEASURES IN, ON OR NEAR THE CONSTRUCTION SITE.
- G. ALL AREAS OUTSIDE THE PROPERTY BOUNDARIES THAT ARE DISTURBED BY UTILITY CONSTRUCTION SHALL BE RESTORED IN KIND. SODDED AREAS SHALL BE RESTORED WITH 6 INCHES OF TOPSOIL PLACED BENEATH THE SOD.
- H. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING AND MAINTAINING TRAFFIC CONTROL DEVICES SUCH AS BARRICADES, WARNING SIGNS, DIRECTIONAL SIGNS, FLAGMEN AND LIGHTS TO CONTROL THE MOVEMENT OF TRAFFIC WHERE NECESSARY. TRAFFIC CONTROL DEVICES SHALL CONFORM TO APPROPRIATE MINNESOTA DEPARTMENT OF TRANSPORTATION STANDARDS.
- I. ALL SOILS TESTING SHALL BE COMPLETED BY AN INDEPENDENT SOILS ENGINEER. EXCAVATION FOR THE PURPOSE OF REMOVING UNSTABLE OR UNSUITABLE SOILS SHALL BE COMPLETED AS REQUIRED BY THE SOILS ENGINEER. THE UTILITY BACKFILL CONSTRUCTION SHALL COMPLY WITH THE REQUIREMENTS OF THE SOILS ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL REQUIRED SOILS TESTS AND SOIL INSPECTIONS WITH THE SOILS ENGINEER.
- J. PRIOR TO PLACEMENT OF AGGREGATE BASE, A TEST ROLL WILL BE REQUIRED ON THE STREET AND PARKING AREA SUBGRADE. THE CONTRACTOR SHALL PROVIDE A LOADED TANDEM AXLE TRUCK WITH A GROSS WEIGHT OF 25 TONS. THE TEST ROLLING SHALL BE AT THE DIRECTION OF THE SOILS ENGINEER AND SHALL BE COMPLETED IN AREAS AS DIRECTED BY THE SOILS ENGINEER. THE SOILS ENGINEER SHALL DETERMINE WHICH SECTIONS OF THE STREET OR PARKING AREA ARE UNSTABLE. CORRECTION OF THE SUBGRADE SOILS SHALL BE COMPLETED IN ACCORDANCE WITH THE REQUIREMENTS OF THE SOILS ENGINEER.
- K. THE CONTRACTOR SHALL SUBMIT 2 COPIES OF SHOP DRAWINGS FOR MANHOLE AND CATCH BASIN STRUCTURES TO ENGINEER PRIOR TO FABRICATION. CONTRACTOR SHALL ALLOW 5 WORKING DAYS FOR SHOP DRAWING REVIEW.
- L. THE CONTRACTOR AND THEIR SUPPLIER SHALL DETERMINE THE MINIMUM DIAMETER REQUIRED FOR EACH STORM SEWER STRUCTURE.

PRELIMINARY





2
L1.01 TREE PLANTING DETAIL
SCALE N.T.S.

1
L1.01 LANDSCAPE PLAN
1"=20'

NOTES

GENERAL NOTES:

- THE CONTRACTOR SHALL INSPECT THE SITE AND BECOME FAMILIAR WITH THE EXISTING CONDITIONS RELATING TO THE NATURE AND SCOPE OF THE WORK.
- THE CONTRACTOR SHALL VERIFY PLAN LAYOUT AND BRING TO THE ATTENTION OF THE LANDSCAPE ARCHITECT DISCREPANCIES WHICH MAY COMPROMISE THE DESIGN OR INTENT OF THE LAYOUT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLYING WITH ALL APPLICABLE CODES, REGULATIONS, AND PERMITS GOVERNING THE WORK.
- THE CONTRACTOR SHALL PROTECT EXISTING ROADS, CURBS/GUTTERS, TRAILS, TREES, LAWNS AND SITE ELEMENTS DURING CONSTRUCTION. DAMAGE TO SAME SHALL BE REPAIRED AND/OR REPLACED AT NO ADDITIONAL COST TO THE OWNER.
- LOCATE AND VERIFY ALL UTILITIES, INCLUDING IRRIGATION LINES, WITH THE OWNER FOR PROPRIETARY UTILITIES AND Gopher State One Call 48 HOURS BEFORE DIGGING. CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION AND REPAIR OF ANY DAMAGES TO SAME. NOTIFY THE LANDSCAPE ARCHITECT OF ANY CONFLICTS TO FACILITATE PLANT RELOCATION.
- THE CONTRACTOR SHALL COORDINATE THE PHASES OF CONSTRUCTION AND PLANTING INSTALLATION WITH OTHER CONTRACTORS WORKING ON SITE.
- THE CONTRACTOR SHALL REVIEW THE SITE FOR DEFICIENCIES IN SITE CONDITIONS WHICH MIGHT NEGATIVELY AFFECT PLANT ESTABLISHMENT, SURVIVAL OR WARRANTY. UNDESIRABLE SITE CONDITIONS SHALL BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT PRIOR TO BEGINNING OF WORK.
- THE PLAN TAKES PRECEDENCE OVER THE LANDSCAPE LEGEND IF DISCREPANCIES EXIST. QUANTITIES SHOWN IN THE PLANTING SCHEDULE ARE FOR THE CONTRACTOR'S CONVENIENCE. CONTRACTOR TO VERIFY QUANTITIES SHOWN ON THE PLAN.
- THE SPECIFICATIONS TAKE PRECEDENCE OVER THE PLANTING NOTES AND GENERAL NOTES.
- EXISTING TREES AND SHRUBS TO REMAIN SHALL BE PROTECTED TO THE DRIP LINE FROM ALL CONSTRUCTION TRAFFIC, STORAGE OF MATERIALS ETC. WITH 4' HT. ORANGE PLASTIC SAFETY FENCING ADEQUATELY SUPPORTED BY STEEL FENCE POSTS 6" O.C. MAXIMUM SPACING.
- LONG-TERM STORAGE OF MATERIALS OR SUPPLIES ON-SITE WILL NOT BE ALLOWED.
- CONTRACTOR SHALL REQUEST IN WRITING, A FINAL ACCEPTANCE INSPECTION.

PLANTING NOTES:

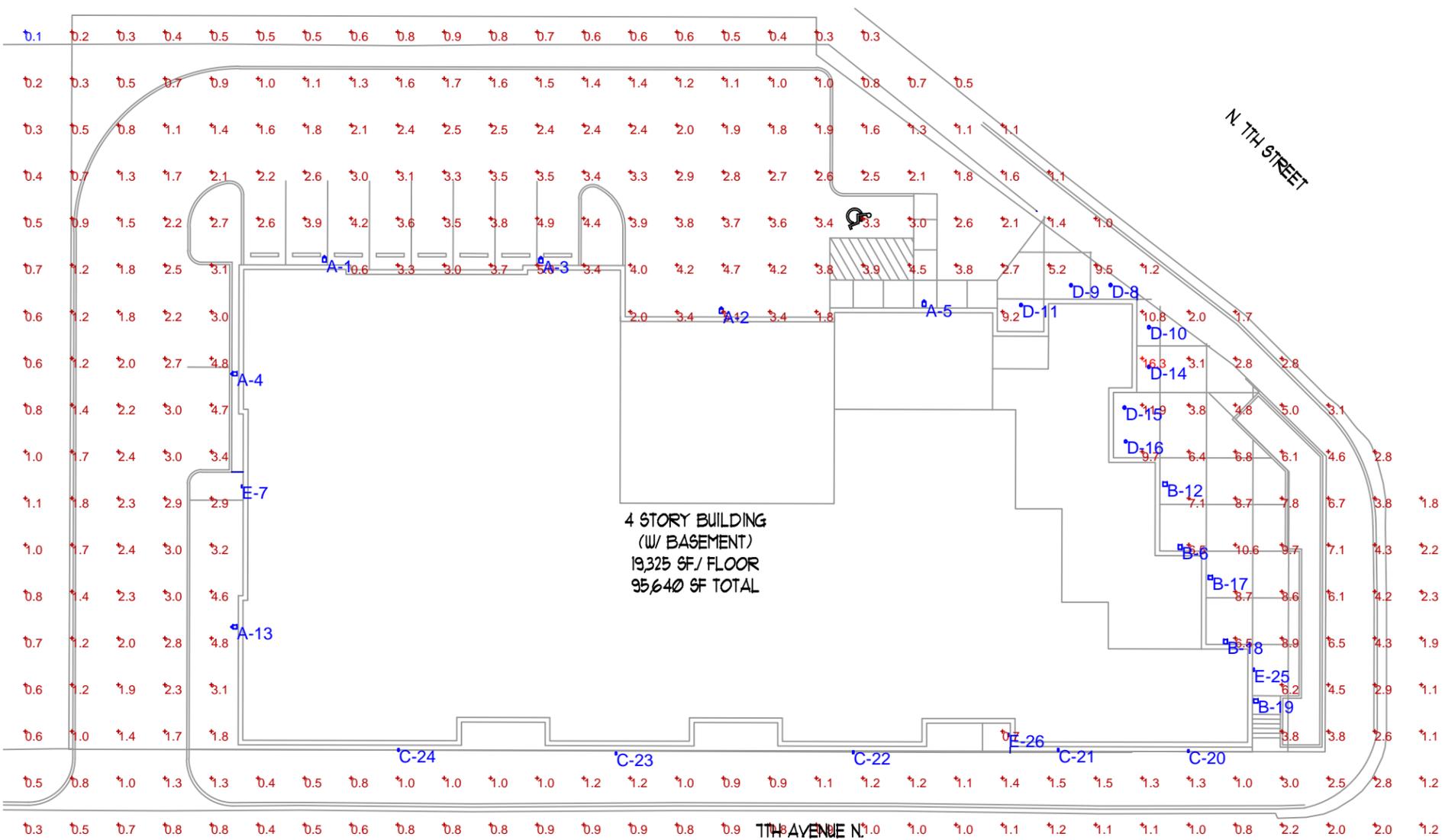
- NO PLANTS SHALL BE INSTALLED UNTIL FINAL GRADING AND CONSTRUCTION HAS BEEN COMPLETED IN THE IMMEDIATE AREA.
- A GRANULAR PRE-EMERGENT HERBICIDE SHALL BE APPLIED TO ALL PLANT BEDS AT THE MANUFACTURERS RECOMMENDED RATE PRIOR TO PLANT INSTALLATION.
- ALL PLANTING STOCK SHALL CONFORM TO THE "AMERICAN STANDARD FOR NURSERY STOCK," ANSI-Z66, LATEST EDITION, OF THE AMERICAN ASSOCIATION OF NURSERYMEN, INC. AND SHALL CONSTITUTE MINIMUM QUALITY REQUIREMENTS FOR PLANT MATERIALS.
- OVERSTORY TREES SHALL BEGIN BRANCHING NO LOWER THAN 6' ABOVE PAVED SURFACES.
- ALL PLANTS MUST BE HEALTHY, VIGOROUS MATERIAL, FREE OF PESTS AND DISEASE AND BE

- CONTAINER GROWN OR BALLED AND BURLAPPED AS INDICATED IN THE LANDSCAPE LEGEND.
- PLANT MATERIALS TO BE INSTALLED PER PLANTING DETAILS.
- ALL TREES MUST BE STRAIGHT TRUNKED AND FULL HEADED AND MEET ALL REQUIREMENTS SPECIFIED.
- THE LANDSCAPE ARCHITECT RESERVES THE RIGHT TO REJECT ANY PLANTS WHICH ARE DEEMED UNSATISFACTORY BEFORE, DURING, OR AFTER INSTALLATION.
- NO SUBSTITUTIONS OF PLANT MATERIAL SHALL BE ACCEPTED UNLESS APPROVED IN WRITING BY THE LANDSCAPE ARCHITECT.
- ALL PLANT MATERIAL QUANTITIES, SHAPES OF BEDS AND LOCATIONS SHOWN ARE APPROXIMATE. CONTRACTOR SHALL BE RESPONSIBLE FOR COMPLETE COVERAGE OF ALL PLANTING BEDS AT SPACING SHOWN AND ADJUSTED TO CONFORM TO THE EXACT CONDITIONS OF THE SITE. THE LANDSCAPE ARCHITECT SHALL APPROVE THE STAKING LOCATION OF ALL PLANT MATERIALS PRIOR TO INSTALLATION.
- ALL PLANTING AREAS MUST BE COMPLETELY MULCHED AS SPECIFIED.
- MULCH: SHREDDED HARDWOOD MULCH, CLEAN AND FREE OF NOXIOUS WEEDS OR OTHER DELETERIOUS MATERIAL, IN ALL MASS PLANTING BEDS AND FOR TREES, UNLESS INDICATED AS ROCK MULCH ON DRAWINGS. SUBMIT SAMPLE TO LANDSCAPE ARCHITECT PRIOR TO DELIVERY ON-SITE FOR APPROVAL. DELIVER MULCH ON DAY OF INSTALLATION. USE 4" FOR SHRUB BEDS, AND 3" FOR PERENNIAL/GROUND COVER BEDS, UNLESS OTHERWISE DIRECTED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL MULCHES AND PLANTING SOIL QUANTITIES TO COMPLETE THE WORK SHOWN ON THE PLAN.
- USE ANTI-DESICCANT (WILT-PRUF OR APPROVED EQUAL) ON DECIDUOUS PLANTS MOVED IN LEAF AND FOR EVERGREENS MOVED ANYTIME. APPLY AS PER MANUFACTURER'S INSTRUCTION. ALL EVERGREENS SHALL BE SPRAYED IN THE LATE FALL FOR WINTER PROTECTION DURING WARRANTY PERIOD.
- PLANTING SOIL FOR TREES, SHRUBS AND GROUND COVERS: FERTILE FRIABLE LOAM CONTAINING A LIBERAL AMOUNT OF HUMUS AND CAPABLE OF SUSTAINING VIGOROUS PLANT GROWTH. IT SHALL COMPLY WITH MNDOT SPECIFICATION 3877 TYPE B SELECT TOPSOIL. MIXTURE SHALL BE FREE FROM HARDPACK SUBSOIL, STONES, CHEMICALS, NOXIOUS WEEDS, ETC. SOIL MIXTURE SHALL HAVE A PH BETWEEN 6.1 AND 7.5 AND 10-0-10 FERTILIZER AT THE RATE OF 3 POUNDS PER CUBIC YARD. IN PLANTING BEDS INCORPORATE THIS MIXTURE THROUGHOUT THE ENTIRE BED BY ROTO-TILING IT INTO THE TOP 12" OF SOIL AT A 1:1 RATIO WITH EXISTING SOIL.
- ANY PLANT STOCK NOT PLANTED ON DAY OF DELIVERY SHALL BE HELED IN AND WATERED UNTIL INSTALLATION. PLANTS NOT MAINTAINED IN THIS MANNER WILL BE REJECTED.
- ALL PLANTS SHALL BE GUARANTEED FOR TWO COMPLETE GROWING SEASONS (APRIL 1 - NOVEMBER 1), UNLESS OTHERWISE SPECIFIED. THE GUARANTEE SHALL COVER THE FULL COST OF REPLACEMENT INCLUDING LABOR AND PLANTS.
- CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT AT LEAST 3 DAYS PRIOR TO PLANNED DELIVERY. THE CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT AT LEAST 24 HOURS IN ADVANCE OF BEGINNING PLANT INSTALLATION.
- SEASONS/TIME OF PLANTING AND SEEDING: NOTE: THE CONTRACTOR MAY ELECT TO PLANT IN OFF-SEASONS ENTIRELY AT HIS/HER RISK. DECIDUOUS
- POTTED PLANTS: 4/1 - 6/1; 9/21 - 11/1

- DECIDUOUS /B&B: 4/1 - 6/1; 9/21 - 11/1
- EVERGREEN POTTED PLANTS: 4/1 - 6/1; 9/21 - 11/1
- EVERGREEN B&B: 4/1 - 5/1; 9/21 - 11/1
- TURF/LAWN SEEDING: 4/1 - 6/1; 7/20 - 9/20
- NATIVE MIX SEEDING: 4/1 - 7/20; 9/20-10/20
- MAINTENANCE SHALL BEGIN IMMEDIATELY AFTER EACH PORTION OF THE WORK IS IN PLACE. PLANT MATERIAL SHALL BE PROTECTED AND MAINTAINED UNTIL THE INSTALLATION OF THE PLANTS IS COMPLETE. INSPECTION HAS BEEN MADE, AND PLANTINGS ARE ACCEPTED EXCLUSIVE OF THE GUARANTEE. MAINTENANCE SHALL INCLUDE WATERING, CULTIVATING, MULCHING, REMOVAL OF DEAD MATERIALS, RE-SETTING PLANTS TO PROPER GRADE AND KEEPING PLANTS IN A PLUMB POSITION. AFTER ACCEPTANCE, THE OWNER SHALL ASSUME MAINTENANCE RESPONSIBILITIES. HOWEVER, THE CONTRACTOR SHALL CONTINUE TO BE RESPONSIBLE FOR KEEPING THE TREES PLUMB THROUGHOUT THE GUARANTEE PERIOD.
- ANY PLANT MATERIAL WHICH DIES, TURNS BROWN, OR DEFOLIATES (PRIOR TO TOTAL ACCEPTANCE OF THE WORK) SHALL BE PROMPTLY REMOVED FROM THE SITE AND REPLACED WITH MATERIAL OF THE SAME SPECIES, QUANTITY, AND SIZE AND MEETING ALL LANDSCAPE LEGEND SPECIFICATIONS.
- WATERING: MAINTAIN A WATERING SCHEDULE WHICH WILL THOROUGHLY WATER ALL PLANTS ONCE A WEEK IN EXTREMELY HOT, DRY WEATHER. WATER MORE OFTEN AS REQUIRED BY INDICATIONS OF HEAT STRESS SUCH AS WILTING LEAVES. CHECK MOISTURE UNDER MULCH PRIOR TO WATERING TO DETERMINE NEED. CONTRACTOR SHALL MAKE THE NECESSARY ARRANGEMENTS FOR WATER.

IRRIGATION NOTES:

- IRRIGATION SYSTEM TO BE DESIGN/BUILD. CONTRACTOR TO SUBMIT SHOP DRAWINGS FOR APPROVAL OF SYSTEM LAYOUT PRIOR TO INSTALLATION.
- ALL PLANT BEDS TO RECEIVE DRIP LINE IRRIGATION, WITH A MINIMUM DESIGN OF .5" IRRIGATION PER WEEK.
- CONTRACTOR TO INSTALL A TOTAL OF 4 QUICK COUPLERS AT THE CORNERS OF THE PROPERTY. A 2.5" TYPE K SOURCE PIPE IS PROVIDED BY MECHANICAL.



LUMINAIRE LOCATIONS						
No.	Label	Location		MH	Orientation	Tilt
		X	Y			
1	A	1554.3	381.1	16.0	0.0	0.0
2	A	1639.5	370.1	16.0	0.0	0.0
3	A	1600.8	380.9	16.0	0.0	0.0
4	A	1535.0	356.7	16.0	270.0	0.0
5	A	1683.1	371.6	16.0	0.0	0.0
6	B	1738.2	319.5	16.0	0.0	0.0
7	E	1536.5	332.5	8.0	270.0	0.0
8	D	1723.2	375.7	10.0	0.0	0.0
9	D	1714.7	375.7	10.0	0.0	0.0
10	D	1731.5	366.7	10.0	0.0	0.0
11	D	1704.0	371.4	10.0	0.0	0.0
12	B	1734.9	333.0	16.0	90.0	0.0
13	A	1535.0	302.4	16.0	270.0	0.0
14	D	1731.5	358.2	10.0	0.0	0.0
15	D	1726.2	349.4	10.0	0.0	0.0
16	D	1726.5	342.2	10.0	0.0	0.0
17	B	1744.7	313.0	16.0	90.0	0.0
18	B	1747.9	299.3	16.0	0.0	0.0
19	B	1754.4	286.5	16.0	90.0	0.0
20	C	1739.9	275.8	16.0	180.0	0.0
21	C	1711.9	276.0	16.0	180.0	0.0
22	C	1667.9	275.5	16.0	180.0	0.0
23	C	1616.9	275.3	16.0	180.0	0.0
24	C	1570.2	276.0	16.0	180.0	0.0
25	E	1754.0	293.2	8.0	90.0	0.0
26	E	1701.3	279.2	8.0	270.0	0.0

STATISTICS						
Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
Calc Zone	+	2.0 fc	16.3 fc	0.1 fc	163.0:1	20.0:1

LUMINAIRE SCHEDULE									
Symbol	Label	Qty	Catalog Number	Description	Lamp	File	Lumens	LLF	Watts
	A	6	DSX1 LED 30C 1000 40K T4M MVOLT	DSX1 LED WITH (1) 30 LED LIGHT ENGINES, TYPE T4M OPTIC, 4000K, @ 1050mA	LED	DSX1_LED_30 C_1000_40K_T4M_MVOLT.i	Absolute	0.95	105
	B	5	DSX1 LED 30C 1000 40K T3M MVOLT	DSX1 LED WITH (1) 30 LED LIGHT ENGINES, TYPE T3M OPTIC, 4000K, @ 1050mA	LED	DSX1_LED_30 C_1000_40K_T3M_MVOLT.i	Absolute	0.95	105
	C	5	DSXW1 LED 10C 530 40K T2M MVOLT	DSXW1 LED WITH 1 LIGHT ENGINE, 10 LED's, 530mA DRIVER, 4000K LED, TYPE 2 MEDIUM OPTIC	LED	DSXW1_LED_10C_530_40K_T2M_MVOLT.i	Absolute	0.95	20
	D	7	EVO 30/14 6AR 120	6" LED DOWNLIGHT, 3000K, 1400LUMENS, STANDARD DISTRIBUTION WITH CLEAR SEMI SPECULAR REFLECTOR	LED	EVO_30_14_6 AR_120.i	Absolute	0.95	24.6
	E	3	AFN	ARCHITECTURAL EMERGENCY LIGHTING. XENON LAMPS, WIDE THROW OPTICS, CLEAR PRISMATIC LENS	TWO 6-WATT FROSTED T3-1/4 WEDGE BASE XENON, HORIZONTAL POSITION.	AFN.i	104	0.86	12

THE LOCKUP SELF STORAGE

701 North 7th Street, Minneapolis, Minnesota



SITE LIGHTING PLAN

1" = 30'-0"

03.09.15



Partners in Design
ARCHITECTS

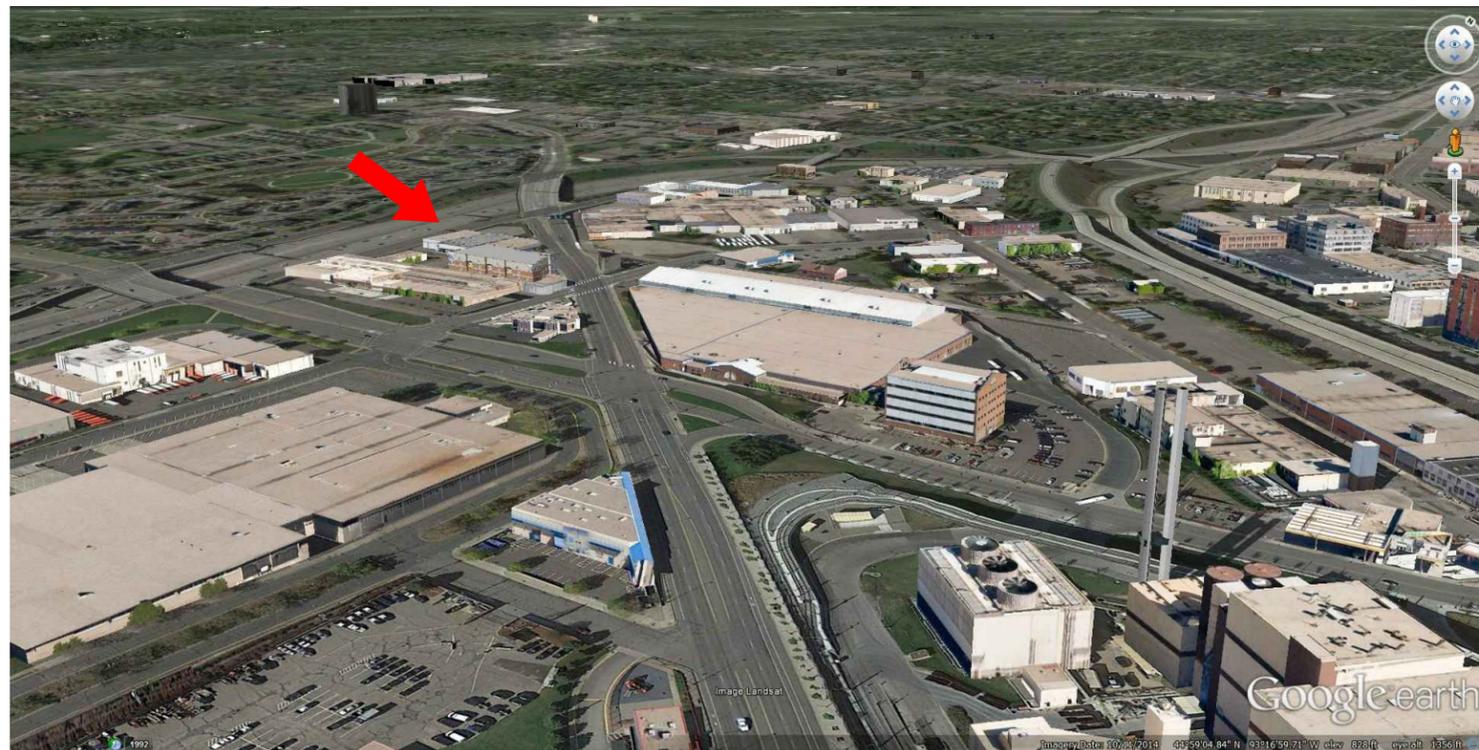
262.652.2800
Kenosha, WI

847.940.0300
Riverwoods, IL





OBLIQUE VIEW LOOKING SOUTH



OBLIQUE VIEW LOOKING NORTHWEST

NOTE:
 IMAGES SHOWN ARE FROM A GOOGLE
 EARTH PREVIEW. EXISTING BUILDINGS
 SHOWN ARE PER THE CURRENT GOOGLE
 EARTH DATABASE. ALSO NOTE THAT THE
 EXISTING BUILDING ON THE PROJECT SITE
 IS SHOWN. GOOGLE EARTH PREVIEW DOES
 NOT ALLOW THE EXISTING BUILDING MASSES
 TO BE REMOVED.



THE LOCKUP SELF STORAGE

701 North 7th Street, Minneapolis, Minnesota

DEVELOPMENT CONTEXT STUDIES

03.09.15



262.652.2800
 Kenosha, WI

847.940.0300
 Riverwoods, IL



LOOKING SOUTHEAST ON N. 7TH STREET TOWARD SITE



LOOKING NORTHWEST ON N. 7TH STREET TOWARD SITE

NOTE:
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03.09.15



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 Riverwoods, IL



LOOKING SOUTH ON OAK LANE AVENUE TOWARD SITE



LOOKING NORTH ON OAK LANE AVENUE TOWARD SITE

NOTE:
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 Riverwoods, IL



AERIAL CONTEXT

701 North 7th Street, Minneapolis, Minnesota

SITE CONTEXT AERIAL

03.09.15



Partners in Design
ARCHITECTS

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Kenosha, WI

847.940.0300
Riverwoods, IL



1A. LOOKING TOWARD SITE FROM NORTHWEST - NORTH 7TH ST.



1B. LOOKING TOWARD SITE FROM NORTHWEST - NORTH 7TH ST.



1C. METRO TRANSIT BUILDING - LOOKING SOUTHEAST



1D. METRO TRANSIT BUILDING - LOOKING SOUTHEAST

DEVELOPMENT CONTEXT PHOTOS

701 North 7th Street, Minneapolis, Minnesota

APPROACHING/PASSING SITE LOOKING SOUTHEAST





2A. METRO TRANSIT BUILDING LOOKING NORTHWEST - NORTH 7TH ST.



2B. LOOKING TOWARD SITE FROM SOUTHEAST - NORTH 7TH ST.



2C. METRO TRANSIT BUILDING LOOKING NORTHWEST - NORTH 7TH ST.



2D. LOOKING TOWARD SITE FROM EAST - NORTH 7TH ST.

DEVELOPMENT CONTEXT PHOTOS

701 North 7th Street, Minneapolis, Minnesota

APPROACHING SITE LOOKING NORTHWEST





3A. LOOKING NORTHEAST FROM SITE



3B. LOOKING EAST FROM SITE



3C. LOOKING NORTH FROM EAST SIDE OF SITE



3D. LOOKING TOWARD SITE FROM NORTH - OAK LAKE AVE.

DEVELOPMENT CONTEXT PHOTOS

701 North 7th Street, Minneapolis, Minnesota

IMMEDIATE NORTH EAST OF SITE





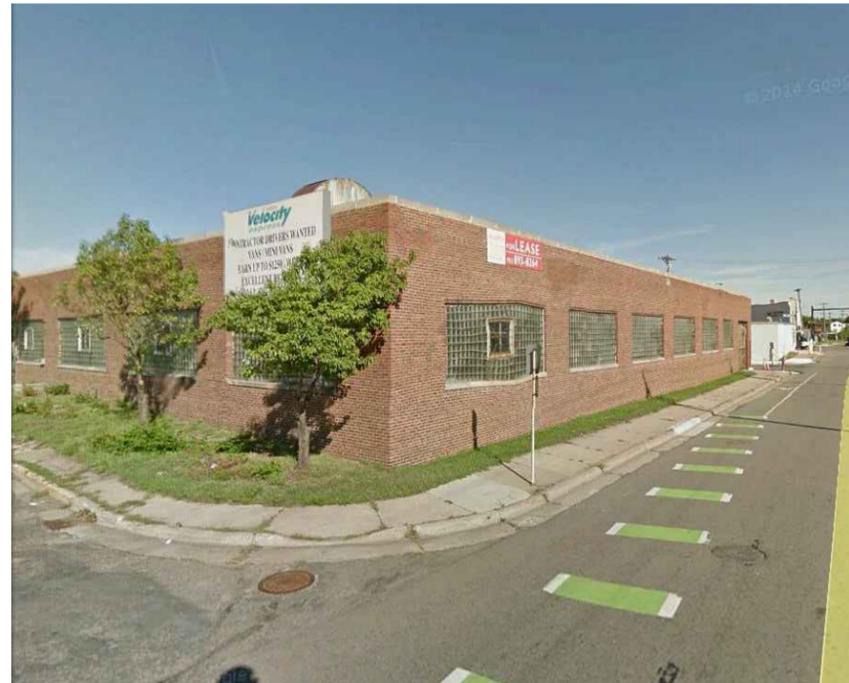
4A. LOOKING NORTH, WEST OF SITE - LYNDALE AVE. NORTH



4B. LOOKING TOWARD SITE FROM WEST - 7TH AVENUE NORTH



4C. LOOKING TOWARD SITE FROM WEST - 7TH AVENUE NORTH



4D. LOOKING TOWARD SITE FROM SOUTH - OAK LAKE AVE.

DEVELOPMENT CONTEXT PHOTOS

701 North 7th Street, Minneapolis, Minnesota

IMMEDIATE WEST & SOUTH OF SITE





5A. LOOKING NORTHWEST - NORTH 7TH ST.



5B. LOOKING SOUTH - NORTH 7TH ST.



5C. LOOKING SOUTHEAST - NORTH 7TH ST.



5D. LOOKING NORTHEAST - OAK LAKE AVE.

DEVELOPMENT CONTEXT PHOTOS

701 North 7th Street, Minneapolis, Minnesota

WELLS FARGO BANK - SOUTHEAST OF SITE





THE LOCKUP SELF STORAGE - BLOOMINGTON



THE LOCKUP SELF STORAGE - EDEN PRAIRIE



THE LOCKUP SELF STORAGE - INDUSTRIAL BOULEVARD



THE LOCKUP SELF STORAGE - PLYMOUTH

EXISTING LOCKUP FACILITIES

701 North 7th Street, Minneapolis, Minnesota

COMPLETED MINNESOTA PROJECTS

03.09.15



262.652.2800
Kenosha, WI

847.940.0300
Riverwoods, IL

Quality self storage



in quality communities

www.TheLockUp.com

THE LOCKUP

Self Storage



company overview

Family Owned – Family Managed

BRB Development, LLC is a family owned and operated company that has been in the self storage business since 1976. Continuing to be one of the most active and well-respected private companies in the industry, The Lock Up was started by Robert Soudan Sr. and Charles Sample Sr. who built his first facility in Glenview, Illinois. With the help of his son and son-in-law, Bob Soudan Jr. and Rick Hielscher, Mr. Soudan has significantly increased both the number and quality of facilities.

Currently, The Lock Up is in seven states. We own and manage over 2 million square feet of self storage. Additionally, we have recently partnered with a major pension fund that controls tens of billions of dollars.

Industry Leaders

We take pride in being leaders in our industry. The Lock Up has been cited as industry leaders and innovators in *The Wall Street Journal*, *Urban Land Institute*, and *Business 2.0*.

Lock Up's President, Bob Soudan Sr., is a past National President of the Self Storage Association (SSA). Bob Soudan, Jr. has served as a National Director of the SSA as well as Regional President. Additionally, all of the Lock Up principals have spoken as experts at SSA national conventions and seminars.

our product

Target Market and Location

The upscale design of our facilities cater to established, high end communities that demand the highest quality of building design, aesthetics, amenities, and services. The communities in which we build tend to be established, dense, high-income areas with high barriers to entry. Our upscale product is designed to attract customers who are looking for the best product, at competitive prices.

Our product has an extremely low impact on the communities in which we build. We generate less traffic per square foot than almost any other use. For example: We draw only six percent of the traffic of a similarly sized retail development. Additionally, we do not generate noise or pollution. Our burden on community services and infrastructure is as low as a single family home.

Because of our low impact and tasteful design, we have been able to build in many different zoning classifications. These include Commercial, Retail, Business, Industrial, and even Office Research. Because our use is so new and different, we work very closely with both the appropriate staff and zoning authority to familiarize them with our unique product and its effects on the community.

Continued on the far right panel





our product

Continued from the far left panel

Building Design

Architecturally, Lock Up facilities differ from traditional facilities. They are designed to have a retail or hotel-like appearance. Amenities include:

- Concrete, architectural block, pre-cast or EIFS exterior.
- Full climate control.
- Most units interior accessed.
- Large glass offices.
- 24-hour video surveillance system.
- Fully enclosed climate controlled drive-in unloading areas.
- Lighting triggered by motion detectors.
- Carpeted hallways and storage units.
- Large passenger elevators.

Our facilities are designed to be very bright with extra lighting throughout. Signage is always tastefully done to blend with the community. The typical Lock Up differs from a traditional storage facility in that almost all of the units are accessible only from the inside of the facility through a large, heated unloading area. From here customers unload their goods onto carts before proceeding to their unit.

The unloading areas are generally designed to accommodate two to three vehicles at one time through large overhead doors. Cars, pickups, vans, and rental trucks will fit easily inside the unloading areas. Doors are then closed for complete weather protection.

Security – Strong Yet Customer Friendly

We take pride in making our facilities safe for our customers, their goods, and the community. The Lock Up maintains a tenant/landlord relationship with our customers, much like that of an apartment or office building. Hazardous and explosive materials are not permitted under any circumstances. We even maintain the right to enter a customer's unit for inspection or maintenance purposes. Each door is equipped with a unique tamper-proof locking system.





services and community benefits

Our customers are people whose homes are on the market, people between homes, and people who need an extra closet or who'd like to reclaim their garage for its intended use. We service students, sales reps, merchants of all types, and businesses storing records. Storing at The Lock Up is like storing in a spare bedroom — a secure, well lit, comfortable atmosphere.



Contact Information

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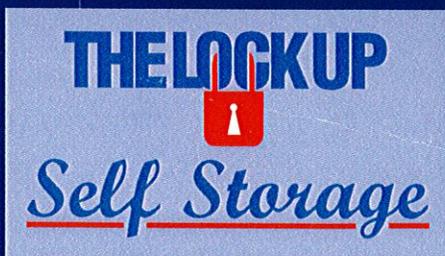
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Quality self storage
in quality communities

secure



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For more information on how a Lock Up Storage Center can provide needed benefits in your community, please contact our headquarters:

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