



# CPED STAFF REPORT

Prepared for the Heritage Preservation Commission

HPC Agenda Item #1

February 3, 2015

BZH-28514

## HERITAGE PRESERVATION APPLICATION SUMMARY

*Property Location:* 316 Third Avenue North  
*Project Name:* T3  
*Prepared By:* [Hilary Dvorak](#), Principal Planner, (612) 673-2639  
*Applicant:* Robert Pfefferle  
*Project Contact:* Robert Pfefferle with Hines  
*Ward:* 3  
*Neighborhood:* North Loop  
*Request:* Construct a new seven-story, approximately 263,000 square foot office building  
*Required Applications:*

<b>Certificate of Appropriateness</b>	To construct a new seven-story, approximately 263,000 square foot office building in the Minneapolis Warehouse Historic District.
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## HISTORIC PROPERTY INFORMATION

<b>Current Name</b>	North Loop Green
<b>Historic Name</b>	The Cut
<b>Historic Address</b>	333 Washington Avenue North
<b>Original Construction Date</b>	Not applicable
<b>Original Architect</b>	Not applicable
<b>Original Builder</b>	Not applicable
<b>Original Engineer</b>	Not applicable
<b>Historic Use</b>	Industrial/Railroads
<b>Current Use</b>	Surface parking lot
<b>Proposed Use</b>	Office building

<b>Date Application Deemed Complete</b>	January 13, 2015	<b>Date Extension Letter Sent</b>	Not Applicable
<b>End of 60-Day Decision Period</b>	March 12, 2015	<b>End of 120-Day Decision Period</b>	Not Applicable

**CLASSIFICATION**

<b>Local Historic District</b>	Warehouse Historic District
<b>Period of Significance</b>	1865 – 1930
<b>Criteria of Significance</b>	<i>Criteria 1:</i> The property is associated with significant events or with periods that exemplify broad patterns of cultural, political, economic or social history. <i>Criteria 4:</i> The property embodies the distinctive characteristics of an architectural or engineering type or style, or method of construction. <i>Criteria 6:</i> The property exemplifies works of master builders, engineers, designers, artists, craftsmen or architects.
<b>Date of Local Designation</b>	1978
<b>Date of National Register Listing</b>	1989
<b>Applicable Design Guidelines</b>	<a href="#">Minneapolis Warehouse District Design Guidelines</a> (2010 ) <a href="#">Design Guidelines for On-Premise Signs and Awnings</a> (2003)

**SUMMARY**

**BACKGROUND.** The Minneapolis Warehouse Historic District contains the former rail yard for the Great Northern (now known as Burlington Northern Santa Fe-BNSF) & Minneapolis St. Louis Railroads (now known as Union Pacific). The area also contains the active BNSF rail corridor. This area is bounded by Washington Avenue to the north and Fifth Street North to the south and follows the former alignment of Fourth Avenue North prior to the railroads.

The original rail corridor and rail yards crossed Washington Avenue North, Third, Fourth, and Fifth Streets North at-grade. This made reliable access to the land to the west of the rail yards very difficult. The 1890 settlement of a lawsuit led to the creation of the landscape of this area. The grade was lowered for the Great Northern and Minneapolis & St. Louis rail yards. The rail yards were separated from the rail corridor by an additional grade change supported by a stone retaining wall. The lowering of the grade necessitated additional access to the rail yards and resulted in the formation of Traffic Street. The lowering of the rail corridor and rail yards grade resulted in the bridging of the site. This bridging reconnected this portion of the city and provided improved access to the land that resulted in additional commercial development for the warehousing and manufacturing industries.

Historically, the site contained railroad depots, sheds, offices and railroad tracks. The site no longer contains buildings, and only one rail line is active at this time, but the alteration to the landscape created by the railroads on the site is extant. Located within the site is the North Star Commuter Rail Station. The station currently connects to the METRO Blue Line at street level. Future plans for this site include an expanded rail station with connection for multiple modes of transit and transportation. The site’s original design intent of the lowered grade remains intact. The lowered grade relative to the adjacent properties is a significant feature on the site. This feature exemplifies the transformative power of the railroads on the landscape of the district and is emblematic of the interconnectedness of the railroads and the warehouse industry in the commercial growth of Minneapolis in the late nineteenth and early twentieth centuries.

This area contains a portion of the only rail yard in downtown Minneapolis that has yet to be developed. Development of other rail yards have resulted in building designs that leave the former rail yard indistinguishable from the other areas of the City and do not offer any clues to the importance of what was once there. It is important to convey the significance of this area in new development and provide for a visual context that sets this area off from the adjacent historic buildings.

**APPLICANT'S PROPOSAL.** The site is located within the boundaries of the North Loop Green Planned Unit Development (PUD). The boundaries of the PUD extend from Washington Avenue North to 5<sup>th</sup> Street North and from the railroad tracks to Dock Street, a private road. The first phase of the PUD, Dock Street Flats, was reviewed and approved by the City of Minneapolis in 2012. As part of the first phase, Dock Street was built that extends from Washington Avenue North, on the north end of the PUD site, into the site. Dock Street will be utilized to access this proposed phase of the PUD and subsequent phases as none of the remaining parcels within the PUD have street frontage.

The applicant is proposing to construct a new seven-story, approximately 263,000 square foot office building with one level of underground parking. The building will be located between Dock Street Flats and the 3<sup>rd</sup> Street North viaduct. The building will be constructed of heavy timber. The exterior materials proposed for the building include COR-TEN corrugated panel, COR-TEN metal plank and glass.

**RELATED APPLICATIONS.** In 2012, the Heritage Preservation Commission approved a Certificate of Appropriateness application to allow for the construction of a new six-story residential building near Washington Avenue North. The City Planning Commission also approved land use applications for this development in 2012.

**PUBLIC COMMENTS.** Comment letters are attached for reference. Any additional correspondence received prior to the public meeting will be forwarded on to the Heritage Preservation Commission for consideration.

## ANALYSIS

### CERTIFICATE OF APPROPRIATENESS

The Department of Community Planning and Economic Development has analyzed the application to allow the construction of a new seven-story, approximately 263,000 square foot office building in the Warehouse Historic District based on the following [findings](#):

1. *The alteration is compatible with and continues to support the criteria of significance and period of significance for which the landmark or historic district was designated.*

Railroads shaped the Warehouse District. The Rail Yards Area is significant for the manipulation of the grade, which was a result of the 1890 settlement of a lawsuit between the railroads and the City of Minneapolis. This feature exemplifies the transformative power of the railroads on the landscape of the district and is emblematic of the interconnectedness of the railroads and the warehouse industry in the commercial growth of Minneapolis in the late nineteenth and early twentieth century. This physical alteration allowed the Warehouse District and the City to be more connected and helped facilitate the further growth of warehousing, manufacturing, and the rail industry in the area. The significant feature of the site is the lowered grade relative to the adjacent properties and the bridging and other connections the lowered grade subsequently required.

The proposed development is compatible with and supports the criteria of significance and period of significance for the Warehouse Historic District. The proposed development will maintain the lowered grade of the site. The location of the building will draw people into the site which will help activate the area again. The building will reinforce the 80-foot wide Third Street North view corridor as the building will be constructed up to the southern edge of the view corridor. The existing Dock Street Flats building was constructed up to the northern edge of the view corridor.

2. *The alteration is compatible with and supports the interior and/or exterior designation in which the property was designated.*

The proposed development is compatible with and supports the exterior designation of the Warehouse Historic District. The proposed placement, massing and design of the building will be compatible with the *Minneapolis Warehouse Historic District Design Guidelines*.

3. *The alteration is compatible with and will ensure continued integrity of the landmark or historic district for which the district was designated.*

Both the City of Minneapolis Heritage Preservation Regulations and the National Register of Historic Places identify integrity as the authenticity of historic properties and recognize seven aspects that define a property's integrity: location, design, setting, materials, workmanship, feeling, and association.

The proposed development is compatible with and will ensure continued integrity of the Warehouse Historic District for which the district was designated based on the evidence below.

**Location:** The proposed development will not impair the district's integrity of location, as the applicant is not proposing to change the location of any contributing resources.

**Design:** The *Minneapolis Warehouse Historic District Design Guidelines* say that new buildings in the Rail Yards area should offset themselves through building design as long as the lowered grade relative to its surroundings and the connections bridging the site that were facilitated by the lowered grade are maintained. The proposed placement and design of the development maintain these features of the site.

**Setting:** The significance of the site is the lowered grade relative to its surroundings and the connections bridging the site that were facilitated by the lowered grade. The proposed development will maintain the lowered grade of the site. The location of the building will draw people into the site which will help activate the area again. The building will reinforce the 80-foot wide Third Street North view corridor as the building will be constructed up to the southern edge of it. All of this will have a positive impact on the setting of the area.

**Materials:** The proposed building will be constructed out of heavy timber which was the construction method for many of the warehouse and mill buildings historically. The exterior materials proposed for the building include COR-TEN corrugated panel, COR-TEN metal plank and glass. These materials are compatible with the *Minneapolis Warehouse Historic District Design Guidelines*.

**Workmanship:** The proposed development would not alter any historic buildings in the district and would not impair the integrity of workmanship.

**Feeling:** Any new construction in a historic district will have an impact on the integrity of feeling. New construction will inevitably stand out as modern and different from the historic surroundings. The *Minneapolis Warehouse Historic District Design Guidelines* say that new buildings in this area should offset themselves through design while maintaining the site features such as the lowered grade and the former connections that once crossed the site. The proposed placement of the development maintains these features of the site.

**Association:** The proposed development would not impair the district's integrity of association with the rail yards as the lowered grade is maintained.

4. *The alteration will not materially impair the significance and integrity of the landmark, historic district or nominated property under interim protection as evidenced by the consistency of alterations with the applicable design guidelines adopted by the commission.*

The *Minneapolis Warehouse Historic District Design Guidelines* were adopted in 2010. The design guidelines were created to protect the integrity of the historic district. The design guidelines allow for sensitive and compatible changes that will allow the district to maintain its growth as an urban neighborhood that supports a variety of commercial, industrial and residential uses.

Three distinctive character areas of the historic district, differentiated based on the development patterns and scale of the buildings, were identified in the design guidelines: nineteenth century warehouse, twentieth century warehouse, and rail yards. The subject property is located in the rail yards character area.

The Rail Yards area is different from the rest of the Warehouse Historic District. This is the site of the former rail yard for the Great Northern (now known as the Burlington Northern Santa Fe-BNSF) and the Minneapolis & St. Louis Railroads (now known as the Union Pacific). The area also contains a portion of the active BNSF rail corridor. Historically, the site contained railroad depots, sheds, offices and railroad tracks. It did not contain warehousing, manufacturing or other buildings or structures found in the other parts of the district.

This area contains a portion of the only rail yard in downtown Minneapolis that has yet to be developed. Development of other rail yards have resulted in building designs that leave the former rail yard indistinguishable from the other areas of the City and do not offer any clues to the importance of what was once there. It is important to convey the significance of this area in new development and provide for a visual context that sets this area off from the adjacent historic buildings.

The following design guidelines for the Rail Yards area are applicable to the applicant's proposal:

## **SITE DESIGN**

**Corridors:** The area contains three distinct corridors that represent the railroad activity in the area and the connections created by the lowered grade of the site. The following guidelines provide guidance on how to preserve and interpret these features in the rail yard character area.

*Requirement:*

- 4.1. A 30 foot wide corridor over the existing BNSF rail corridor shall remain open to visual access and not decked over or built over by buildings.

*Advisory:*

- 4.2. Alterations to the width of the active BNSF rail corridor to accommodate train infrastructure are appropriate if the 30 foot wide corridor is retained.
- 4.3. Bridging and skyways over the corridor or rail yard are appropriate (more guidance provided under Connections: 4.7 – 4.12).

**Staff Comment:**

The development will not be built over the 30-foot wide corridor over the existing BNSF rail corridor nor will any alterations be done to it.

The bridging of the rail yard and corridor for Third and Fourth Streets North was integral to reconnecting the area west of the rail yards area with the rest of downtown. The bridging of the streets allowed for the view corridors to remain uninterrupted along these street corridors, while freight depots were constructed underneath the bridges. The viaducts erected over the

site for Third and Fourth Streets North has been removed. The site is now bisected by the interstate 94 viaducts that connect to Third and Fourth Streets North on the east side of the rail yards. However, the view corridors for Third and Fourth Streets North remain.

*Requirement:*

- 4.4. The 80 foot wide Third Street North View Corridor shall remain unobstructed to the sky and be preserved through the site. New development in the rail yard is allowed below the historic bridge deck height, but shall not encroach on this feature above that level.
- 4.5. The Fourth Street North Corridor shall be preserved through the site. The corridor is obscured by the current I-94 Viaducts. The preservation of this corridor can be done interpretatively through design and it is not required to remain unobstructed like the Third Street North Corridor.

*Advisory:*

- 4.6. Reconstructing a bridge over the rail yards and rail corridor at Third Street North is strongly encouraged.

**Staff Comment:**

The building will reinforce the 80-foot wide Third Street North view corridor. The building will be constructed up to the southern edge of the view corridor. The existing Dock Street Flats building was constructed up to the northern edge of the view corridor. Together, the two buildings will reinforce the edge of the view corridor.

The building will not encroach into the Fourth Street North view corridor nor will there be any bridges constructed over the rail yards as part of this development.

**Connections:** In 1890, the grade of the rail corridor and the rail yards were lowered and the bridges were constructed to facilitate better connections through and to the Rail Yard Area. These alterations facilitated the further growth of the railroad, warehousing, manufacturing and other industries in the warehouse district. Allowing connections to and through the site in future development is in keeping with the spirit, intent and outcome of the design of this historic feature.

The Washington Avenue North steel truss bridge is the only existing bridge to remain in the district. The Third, Fourth and Fifth Streets North bridges have all been replaced or removed. The Washington Avenue North bridge steel superstructure is original; however it sits on concrete deck, supports and abutments that were replaced in the early twenty-first century.

*Requirement:*

- 4.7. The Washington Avenue North steel truss bridge superstructure shall remain on the site.
- 4.8. The Third Street North bridge abutment on the west side of the rail corridors is intact and shall be preserved in place.
- 4.9. Skyways or bridges are allowed over the BNSF rail corridor.
- 4.10. Decking over the BNSF corridor is not allowed.
- 4.11. The number of skyways or bridges that are allowed is not specified. The width of the skyways or bridges shall not exceed the historic width of the viaducts that bridged the area. When the skyways and bridges exceed the width of the viaducts or their number is too great they create a decked feel and the design, feeling, and association of the corridor is lost.

*Other Considerations:*

- 4.12. Skyways between new construction and historic buildings on the east side of the Rail Yards Area will be considered if connecting to a secondary facade and it will not conceal or damage the freight transfer features of these buildings.

**Staff Comment:**

The development will not impact the Washington Avenue North steel truss bridge superstructure or the Third Street North bridge abutment on the west side of the rail corridors. In addition, there are no skyways proposed over the BNSF corridor as part of this development.

**Grade Separation:** The lowered grade of the rail yards and even lower grade of the active rail corridor are integral character defining features of the district. Development in the rail yards area will likely occur at multiple levels with the connections and access at the current rail yards grade level, connections and access at the bridge deck height of Fifth Street North and rail access at the level of the active rail corridor. The multiple levels of activity are in keeping with the character of the rail yard area. The historic grade elevations of the rail yards area are encouraged to be incorporated into the design of new construction.

*Requirement:*

- 4.13. The lowered elevations in the Rail Yard Area shall not be lost in the development of the area.

*Advisory:*

- 4.14. Design interpretation, visual, or physical access is encouraged to convey the lower elevation of the area.

**Staff Comment:**

The proposed development will maintain the lowered grade of the site. In addition, the location of the building will draw people into the site which will help activate the area again.

**DESIGN FOR NEW BUILDINGS**

The rail yards contained brick freight depots, covered sheds and railroad office buildings. The buildings were long and narrow, built to accommodate trains and railcars. The buildings exhibited a variety of forms with gabled and flat roofs, and brick and steel exterior materials. The depot and rail yard facilities were removed prior to the listing of the district on the National Register of Historic Places in 1989.

Development of other rail yards downtown have integrated those yards with surrounding development by developing the buildings to the traditional building forms of the surrounding district. New development in the rail yards area should be true to itself and not be designed to resemble warehouse buildings or freight houses.

It is important that new construction offset itself through design in this area, but it is important that new construction preserve the features of the site outlined in the Site Design Guidelines.

**Setbacks:** In the Nineteenth and Twentieth Century Warehouse Areas the street wall created by the built-to-line building location and fenestration patterns of the buildings is an important character defining feature. Only the Washington Avenue North portion of the Rail Yards Area is along a street where a distinct street wall is evident and was historically represented by a former building wall.

The east boundary of the rail yards area abuts buildings in the Twentieth Century Warehouse character area of the district. The rear of these buildings contains integrated designs to

accommodate freight transfer from railroad cars. These features are important to defining the character of the district. Constructing new buildings immediately adjacent to the rear of these buildings would obscure these important character defining features.

*Requirement:*

- 4.15. The Washington Avenue North street wall shall be reinforced with new development along this portion of the site. A built-to-line setback of zero feet is required.
- 4.16. No specific setback shall be required for buildings or other features along Fifth Street North. There was never a street wall there to protect.
- 4.17. A separation shall be maintained between new buildings or structures and the historic buildings on the east side of Rail Yards Area. Skyways over this separation will be considered per the guidelines in Connections (4.7-4.12).

**Staff Comment:**

The proposed development will not be built near Washington Avenue North or Fifth Street North. The proposed building will be located 80 feet south of the historic Union Plaza and Traffic Zone buildings. These buildings are located on the north side of the 80-foot wide Third Street North view corridor.

**Building Height:** The height of new buildings can affect the relationships of the district. The following guideline exists to ensure that the relationships and context are preserved and not lost due to the height of new buildings.

*Requirement:*

- 4.18. The height of new buildings shall not exceed 20 stories.

*Other Considerations:*

- 4.19. Additional height will be considered if evidence is provided that shows the additional height is compatible with adjacent historic resources and the other contexts of the district.

**Staff Comment:**

The proposed building will be eight stories tall.

There are no other guidelines addressing building design or materials. Buildings shall stand apart from the design of the adjacent historic buildings. It is important to recognize that the buildings in the district have lasted over 100 years. New construction in this area should be designed to last for the same.

**Staff Comment:**

The proposed building is not designed to resemble warehouse buildings or freight houses, but is informed by the simplicity and clear functional expression of the surrounding warehouse context. The building design marries traditional, industrial proportions with modern materials and detailing. The proposed building is respectful of the neighborhood and responds to its context with a simple massing.

The exterior materials proposed for the building include COR-TEN corrugated panel, COR-TEN metal plank and glass. The building will have a strong vertical rhythm of piers. These piers frame the large window openings - generating interest and animation along the street faces, and when viewed from afar. There are two distinct patterns of window planned for the building.

The street level building base is subtly distinguished from the upper levels by its proportion of glass retail storefront and canopies. The upper floors feature rhythmic punched window openings, playing on historic proportions but using modern, highly efficient windows. The topmost floor features a slightly taller proportion of punched windows, referencing the existing 'bottom-middle-top' designs of many buildings in the district.

## **DESIGN GUIDELINES FOR ON-PREMISE SIGNS AND AWNINGS**

The *Design Guidelines for On-Premise Signs and Awnings* were adopted in 2003. The following design guidelines are applicable to the proposal:

### **In General:**

*Number of Signs:* Each principal building entrance that faces a public street, or each ground floor principal use, whichever is less, is allowed two signs. A corner lot with a principal entrance on each street is allowed two signs per street frontage. The two signs may be a combination of one wall sign, one projecting sign, one ground sign, one banner, and awning signage. However, a property may not have both a projecting sign and a ground sign. Only one of the signs should be illuminated, except that banners and awning signs should never be illuminated. Awning signs are limited to ground floor awnings and are subject to the specific guidelines for awnings and awning signs. Parking lot signs are subject to the specific guidelines for signs accessory to parking lots.

*Location of building signs:* Wherever possible, signs should be placed in traditional sign locations including the storefront sign band area. Signs should not obscure or damage architectural features including windows, doors, pilasters, columns and historic signs. Building signs should be located only on the primary façade of the building adjacent to the street and should be no higher than 14 feet, except as otherwise provided in the specific guidelines for wall signs.

### **Wall Signs:**

- i. Location. Wall signs should be located between the first and second floor and should not be higher than fourteen (14) feet, except where the historic sign band is higher. Wall signs should not conceal architectural features or obstruct openings.
- ii. Size. Wall signs should be no more than two (2) feet high and thirty-two (32) square feet in area and should not extend outward from the building more than eight (8) inches.
- iii. Materials. Wall signs may be constructed of wood, metal, painted fiberglass or painted plastic.
- iv. Installation. Wall signs should be attached to the building through the mortar joints. If illuminated, a wall sign should be placed adjacent to or over a permanent mounting plate for electrification. Electrical conduit and lighting fixtures should be attached to the top of the wall sign, and should not be attached to the building. Wall signs should not be painted directly on the surface of the building, except as part of the maintenance or restoration of an existing historic sign.

### **Staff Comment:**

The applicant is proposing to have four wall signs on the building. One of the wall signs would be located near the building entrance on the south side of the building. The other three signs would be located near the top of the building. The signs located near the top of the building would be located on the north, east and south sides of the building. Since the building does not have frontage on a public street none of the signs meet the general locational requirements.

The “T3” sign located near the base of the building would be made out of painted metal and the individual letters would be pin-mounted to the building. The sign would be greater than 32 square feet in area and greater than two feet high. It would not be located more than 14 feet above grade.

The three wall signs located near the top of the building are all greater than 32 square feet in area and greater than two feet high in size. All of these signs would be located approximately 90 feet above grade, 76 feet higher than allowed by the guidelines.

While the site is unique in that it does not have frontage on a public street, the signs would be visible to much of the Warehouse Historic District. CPED finds that this would be injurious to the use and enjoyment of other property in the vicinity and impede the normal and orderly development and improvement of surrounding property. Therefore, CPED is recommending that there be no signs located near the top of the building.

**Ground Signs:**

- i. Location. Ground signs should not obscure the significant architectural detail of adjacent buildings.
- ii. Size. Ground signs should be no more than thirty-two (32) square feet in area and should not be higher than eight (8) feet.
- iii. Materials. Ground signs should be constructed of materials similar to those found on the existing building or compatible with the existing building. Acceptable materials include brick, stone, stucco, metal or wood.

**Staff Comment:**

The applicant is proposing to construct a concrete wall in front of the building. The form of the wall would be in the shape of an “L”. The length of the wall would be five feet on one side and 15 feet on the other. The wall would be five feet tall. The wall would be made out of board form concrete. The sign would be located where the two ends of the wall meet. The sign would read “T” on one side and “3” on the other. The sign would be recessed into the concrete wall one inch and would be a different color. The overall amount of signage is less than 32 square feet and the sign is less than eight feet in height.

5. *The alteration will not materially impair the significance and integrity of the landmark, historic district or nominated property under interim protection as evidenced by the consistency of alterations with the recommendations contained in The Secretary of the Interior's Standards for the Treatment of Historic Properties.*

The proposed development will be consistent with the following Secretary of the Interior's Standards for Rehabilitation:

1. A property shall be used for its historic purpose or be placed in a new use that requires minimal change to the defining characteristics of the building and its site and environment.
2. The historic character of a property shall be retained and preserved. The removal of historic materials or alteration of features and spaces that characterize a property shall be avoided.
9. New additions, exterior alterations, or related new construction shall not destroy historic materials that characterize the property. The new work shall be differentiated from the old and shall be compatible with the massing, size, scale, and architectural features to protect the historic integrity of the property and its environment.

10. New additions and adjacent or related new construction shall be undertaken in such a manner that if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.

The defining feature of the site is the lowered grade relative to the adjacent properties and the bridging and other connections the lowered grade subsequently required. The proposed development is compatible with and supports the Secretary of the Interior's Standards for Rehabilitation. The proposed development will maintain the lowered grade of the site. The location of the building will draw people into the site which will help activate the area again. The building will reinforce the 80-foot wide Third Street North view corridor as the building will be constructed up to the southern edge of the view corridor.

6. *The certificate of appropriateness conforms to all applicable regulations of this preservation ordinance and is consistent with the applicable policies of the comprehensive plan and applicable preservation policies in small area plans adopted by the city council.*

The proposed development will conform to all applicable regulations of this preservation ordinance and would be consistent with the following policies of the comprehensive plan:

**Heritage Preservation Policy 8.1: Preserve, maintain, and designate districts, landmarks, and historic resources which serve as reminders of the city's architecture, history, and culture.**

8.1.1 Protect historic resources from modifications that are not sensitive to their historic significance.

8.1.2 Require new construction in historic districts to be compatible with the historic fabric.

8.1.3 Encourage new developments to retain historic resources, including landscapes, incorporating them into new development rather than removal.

**Heritage Preservation Policy 8.5: Recognize and preserve the important influence of landscape on the cultural identity of Minneapolis.**

8.5.1 Identify and protect important historic and cultural landscapes.

7. *Destruction of any property. Before approving a certificate of appropriateness that involves the destruction, in whole or in part, of any landmark, property in an historic district or nominated property under interim protection, the commission shall make findings that the destruction is necessary to correct an unsafe or dangerous condition on the property, or that there are no reasonable alternatives to the destruction. In determining whether reasonable alternatives exist, the commission shall consider, but not be limited to, the significance of the property, the integrity of the property and the economic value or usefulness of the existing structure, including its current use, costs of renovation and feasible alternative uses. The commission may delay a final decision for a reasonable period of time to allow parties interested in preserving the property a reasonable opportunity to act to protect it.*

The project does not involve the destruction of the property.

Before approving a Certificate of Appropriateness, and based upon the evidence presented in each application submitted, the Commission shall make findings that alterations are proposed in a manner that demonstrates that the Applicant has made adequate consideration of the following documents and regulations:

8. *The description and statement of significance in the original nomination upon which designation of the landmark or historic district was based.*

The applicant has demonstrated adequate consideration for the description and statement of significance in the original nomination upon which the Warehouse Historic District was based. The proposed development will be compatible with the *Minneapolis Warehouse Historic District Design Guidelines*.

9. *Where applicable, adequate consideration of Title 20 of the Minneapolis Code of Ordinances, Zoning Code, Chapter 530, Site Plan Review.*

This development will require land use approvals, including site plan review. The land use applications have not been submitted at this time.

10. *The typology of treatments delineated in the Secretary of the Interior's Standards for the Treatment of Historic Properties and the associated guidelines for preserving, rehabilitating, reconstructing, and restoring historic buildings.*

The proposed development will comply with the Secretary of the Interior's Standards for the Treatment of Historic Properties for new construction.

Before approving a Certificate of Appropriateness that involves alterations to a property within an historic district, the Commission shall make findings based upon, but not limited to, the following:

11. *The alteration is compatible with and will ensure continued significance and integrity of all contributing properties in the historic district based on the period of significance for which the district was designated.*

The proposed development is compatible with and will ensure continued significance and integrity of all contributing properties in the historic district based on the period of significance for the Warehouse Historic District. The proposed development will be compatible with the *Minneapolis Warehouse Historic District Design Guidelines*.

12. *Granting the certificate of appropriateness will be in keeping with the spirit and intent of the ordinance and will not negatively alter the essential character of the historic district.*

The spirit and intent of the City of Minneapolis' Heritage Preservation Regulations is to preserve historically significant buildings, structures, sites, objects, districts, and cultural landscapes of the community while permitting appropriate changes to be made to these properties. Granting the certificate of appropriateness will be in keeping with the spirit and intent of the ordinance and will not negatively alter the essential character of the historic district.

13. *The certificate of appropriateness will not be injurious to the significance and integrity of other resources in the historic district and will not impede the normal and orderly preservation of surrounding resources as allowed by regulations in the preservation ordinance.*

With the exception of the proposed signage at the top of the building, granting the certificate of appropriateness will not be injurious to the significance and integrity of other resources in the historic districts. As described in finding 4, CPED finds that the three proposed wall signs at the top of the building would be injurious to the use and enjoyment of other property in the vicinity and impede the normal and orderly development and improvement of surrounding property. Therefore, CPED is recommending that there be no signs located near the top of the building. With that exception, the proposed development will be compatible with the *Minneapolis Warehouse Historic District Design Guidelines*.

## RECOMMENDATIONS

### **Recommendation of the Department of Community Planning and Economic Development for the Certificate of Appropriateness:**

The Department of Community Planning and Economic Development recommends that the Heritage Preservation Commission adopt the above findings and **approve** the Certificate of Appropriateness to allow for the construction of a new seven-story, approximately 263,000 square foot office building located at 316 Third Avenue North, subject to the following conditions:

1. By ordinance, approvals are valid for a period of two years from the date of the decision unless required permits are obtained and the action approved is substantially begun and proceeds in a continuous basis toward completion. Upon written request and for good cause, the planning director may grant up to a one year extension if the request is made in writing no later than February 3, 2017.
2. By ordinance, all approvals granted in this Certificate of Appropriateness shall remain in effect as long as all of the conditions and guarantees of such approvals are observed. Failure to comply with such conditions and guarantees shall constitute a violation of this Certificate of Appropriateness and may result in termination of the approval.
3. There shall be no signs located near the top of the building. All signs should be designed in accordance with the *Design Guidelines for On-Premise Signs and Awnings*.

## ATTACHMENTS

1. Project description, project history and required findings submitted by the applicant
2. Zoning map
3. Architectural drawings, renderings, landscape plan and civil plans
4. Photos of the site
5. Notifications to Council Member Frey and the North Loop Neighborhood Association
6. Comment letters

**T3**  
**PROJECT DESCRIPTION, PROJECT AREA HISTORY AND**  
**REQUIRED FINDINGS**  
**12/18/14**

**PROJECT DESCRIPTION:**

Hines North Loop Green LLC (Hines) proposes to construct the T3 Office Project (T3), a seven-story heavy timber office building containing approximately 210,000 rentable square feet. The project will be Phase 2 of the North Loop Green Planned Unit Development (PUD) located at 333 Washington Avenue North in the Rail Yards Area of the Minneapolis Warehouse Historic District. The T3 Office Project will redevelop a site currently used as a surface parking lot into a multi-tenant office development that provides new Class A office space and street level commercial activity in proximity to numerous alternative modes of transportation including light rail, commuter rail, bus, regional trails and bikeways, and the pedestrian skyway system. The T3 Office Project is anticipated to be constructed in 2015 - 2016. Hines also controls and intends to redevelop the remainder of the Rail Yards Area that lies southwest of the project site, but there are no current plans or schedule for future project phases.

The two-lane private road, Dock Street, that was constructed as part of the PUD Phase 1 Dock Street Apartments project will provide the main access to and from the site. Dock Street was constructed by and is maintained by Hines, but is open to the public. The portion of Dock Street running from Washington Avenue North connecting to the project access drive located within the 3<sup>rd</sup> Street North View Corridor was built during the construction of the Phase 1 project. Future extension of Dock Street will occur as redevelopment of the Rail Yards Area proceeds.

The connection to/from the Cedar Lake Trail to the North Loop Neighborhood that was constructed with the Phase 1 Dock Street Apartments project will remain intact. The connection from the trail allows users to travel through the site on sidewalks or shared bicycle access along Dock Street.

**PROJECT AREA HISTORY:**

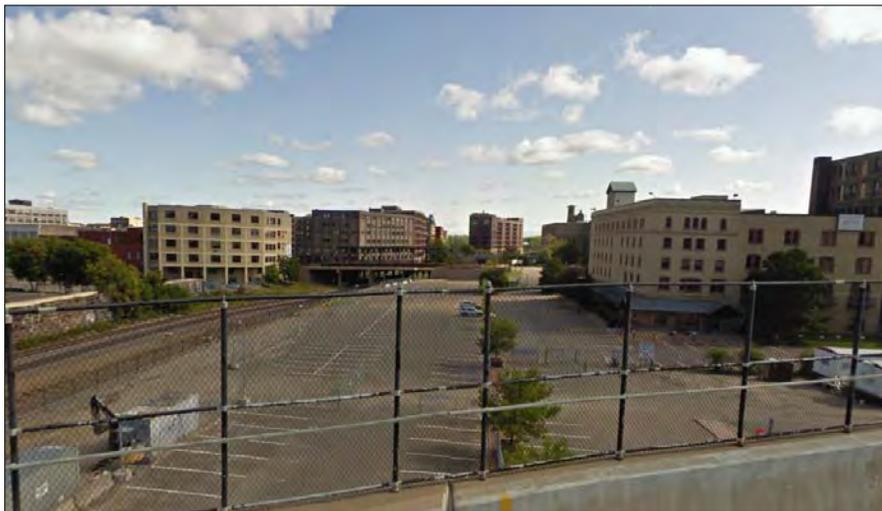
The proposed project is located in the Minneapolis Warehouse Historic District, which was designated a local historic district in 1978 and revised in 2010. In 1989, the Minneapolis Warehouse Historic District was listed in the National Register of Historic Places (NRHP). The district served as the city's warehouse and wholesaling district during the period of significance, 1865-1930, and became a major distribution and jobbing center for the upper Midwest. The Minneapolis Warehouse Historic District Design Guidelines (2010) separate the district into three distinctive character areas; the proposed project is located within the Rail Yards Area of the historic district.

The North Loop Green PUD was studied in an Environmental Assessment Worksheet (EAW) that was prepared for the anticipated phased redevelopment in the Rail Yards Area. As part of the EAW, additional research was provided. Landscape Research LLC provided a Phase I historic resources survey and concluded that surrounding properties, Knoblach Bros Warehouse (and annex) and the grade separation, the Cut, of the Minneapolis & St. Louis Railroad and Great Northern Railway are contributing features. Two Pines Resource Group LLC provided a Phase IA literature search for archaeological potential and concluded that “the land use history of the project area also indicates that the railroad grade separation project that took place in 1890-91 significantly modified the landscape of the entirety of the study area and removed any pre-1891 archaeological deposits within the Area of Potential Effect (APE). Furthermore the types of structures present within the project area after 1891 are unlikely to have associated archaeological deposits with information potential.” Based on these findings, Two Pines Resource Group recommended against subsurface archaeological testing.

A brief history of the project area is excerpted from the EAW below:

**‘Evolution of the Rail Yards Area and Warehouse Historic District Setting’, by Landscape Research LLC**

The North Loop Green PUD occupies the heart of the Minneapolis Warehouse Historic District’s Rail Yard Area. This grade-separated area - also known as the Cut -was the center of transport facilities that served acres of industrial buildings housing a diverse collection of agricultural implement, warehouse and manufacturing firms. This narrative examines the history of the area and the design principles and guidelines described in the Minneapolis Warehouse Historic District Design Guidelines (CPED 2010).



View to north/northwest, 2011.

## **Early Character**

Thirty years of industrial development preceded the grade separation project of 1890-91 along 4th Avenue N. By the early 1860s, the grid-plan of the original Town of Minneapolis grew to the west on additions platted by speculators. The character of the area was cast in 1867 when the St. Paul and Pacific Railroad cut a path along early Dakota Street (4th Avenue N.). The St. Paul and Pacific immediately constructed a passenger depot and freight house. The railroad company's next owner, the St. Paul Minneapolis and Manitoba Railway, constructed a new freight depot and expanded the old. In 1877 they built a new passenger depot just to the south of the project area. A line of brick and wood-clad storefronts along Washington Avenue N. and 3rd Street N. framed the area. The increasing bulk of surrounding buildings reflected the city's growing importance as a manufacturing and warehousing center. Most notably, the limestone-clad, heavy-timber framed six-story Moline, Milburn & Stoddard Company's agricultural implement building at the northwest corner of 3rd Avenue N. and 3rd Street N. was completed in 1886. (This is the present-day "Traffic Zone" Building.) Designed by Joseph Haley, it was exemplary of a generation of heavy timber-framed, thick walled stone buildings that reflected the city's industrial boom of the 1880s and 1890s.

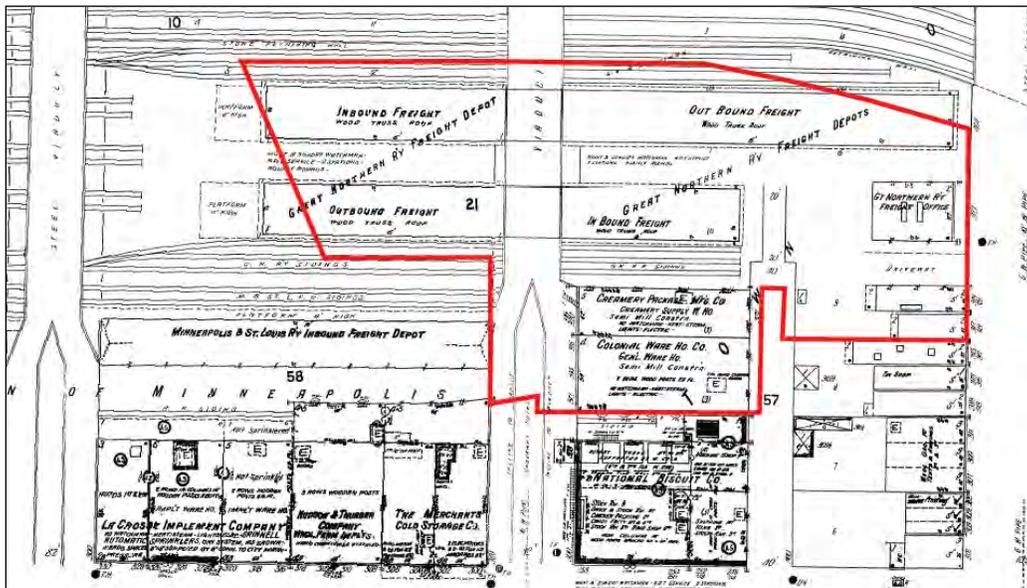
## **Grade Separation: 1891**

Separation of rail, streetcar, and wheeled traffic was a growing issue across the city, with daily complaints of delays and safety struggles recorded in the local press. In 1891, following an increasingly industrial development that supplanted a scattering of surviving first-generation houses and shops, the mainline and spurs of the Great Northern Railroad (which had just acquired the St. Paul Minneapolis and Manitoba) and the Minneapolis and St. Louis Railway were placed in a deep grade separation. Bridges were built over Washington Avenue N., and 3rd, 4th, 5th, and 6th Streets N. Present-day Traffic Street was converted from an alley to serve the lowered rail yards from 3rd Avenue N. The embankment of the railway was shored by a deep stone wall at Washington Avenue and along much of its length.

The grade separation improved access to the area west of the rail yard and opened up new areas to commercial development. All of the structures within the present project area were either removed or altered. The Great Northern replaced its earlier depots with expansive brick structures serving inbound and outbound freight. The gable-roofed, low-profile buildings were placed under the 3rd Street viaduct. A companion freight office building was placed to the north at 327 Washington Avenue N. No complete historic photos of the Great Northern buildings have been located, but the nearby Minneapolis and St. Louis structures suggest gable-roofed, utilitarian brick structures with simple arched openings. A system of wood sheds, platforms, loading docks, and ramps served the structures. More visible from street level, however, were the 3rd Street viaduct and the system of other iron bridges placed across the cut.



Minneapolis and St. Louis Freight Depot under construction, 1890.



1930 Sanborn Fire Insurance Map: Dock Street Project Area and former Great Northern Freight Houses outlined in red. The Knoblauch Bros. Warehouse and annex (now Union Plaza) are shown at the south edge.

Several large warehouses next swept away the collection of early stores, hotels, restaurants, and even a single dwelling that still edged the west side of the project area. The design and orientation of the new buildings reflected the advantages of the grade separation: a five-story, cream-brick warehouse at 318-20 3rd Street N. was linked by a spur to the main line. Designed by Warren H. Hayes, the Knoblauch Brothers real estate firm erected the building, apparently as an investment. Although their primary business was shoe manufacture, they developed property throughout downtown Minneapolis. It was first leased to the Minnesota Moline Plow Company and then to the Creamery Package Manufacturing Company. An annex addressed as 314 3rd Street N. (ca. 1898) housed the creamery firm and then the Colonial Warehouse. In 1909 the warehouse firm advertised it as the “finest storage location in the city.”

While the buildings along the Washington Avenue side of the block remained on the same footprint as their earlier counterparts, permit index cards indicate that those that were not replaced entirely were raised on the same lot. The addition of a basement story was necessary to accommodate the grade change from the front of the lot on Washington Avenue to the now-lowered rear lot.

### **Twentieth-Century Decline**

During the 1920s and 1930s the economy of the warehouse district was battered. National changes in shipping rates and the loss of the railroads' shipping monopoly to trucking, the economic downturn of the Great Depression, and the accompanying decline of the farm machinery industry were factors in the decline. In 1939 the Great Northern's inbound freight depot was demolished, reflecting changing patterns of transportation. Much of the twentieth century was marked by stagnation and declining use for area properties. Other buildings within the project area were removed: 319 Washington Avenue sometime before 1951 and 321 Washington Avenue in 1959.



**The Warehouse District, looking east, in 1930.  
The grade separation and rail corridor at Washington Avenue N. are shown at arrow.**

### **Statement of Required Findings for Certificate of Appropriateness per Chapter 599 of the Minneapolis Code of Ordinances:**

- 1. The alteration is compatible with and continues to support the criteria of significance and period of significance for which the landmark or historic district was designated.**

The Minneapolis Warehouse Historic District is historically significant as an early example of commercial growth as the city's warehouse and wholesaling district. The period of significance, 1865-1930, covers the period when the district was a major distribution and jobbing center for the northwest.

The proposed project is new construction on a surface parking lot. The Great Northern Freight Depot sat on this site 1891-1930 and a brick freight office building was located at the street-wall on Washington Avenue. The grade-separation of the rails (1891), referred to as the Cut, will remain in tact as well as the view corridor of Third Street North. The overall proposal is compatible with the district and guidelines. See findings #4 and #5 for further description.

**2. The alteration is compatible with and supports the interior and/or exterior designation in which the property was designated.**

The proposed alteration is compatible with and supports the designated historic district. The height, materials, and massing of the proposed new construction are not out of character with the surrounding fabric of the historic district. See findings #4 and #5 for further description.

**3. The alteration is compatible with and will ensure continued integrity of the landmark or historic district for which the district was designated.**

The City of Minneapolis and the National Register of Historic Places identify the seven aspects of a property's integrity. Based upon the evidence provided below, the proposed project will impact, but not negatively impair the integrity of the district.

*Location:* The proposed project does not alter the district's location and will not impair the district's integrity of location.

*Design:* The height, materials, and fenestration of the proposed project are not out of character with the neighboring fabric in the historic district and will not damage the district's integrity of design.

*Setting:* The proposed project will infill an area of the existing surface parking lot and preserve the grade separation of the Cut as well as the 80-foot wide view corridor of Third Avenue North. The proposal will not negatively impair the district's integrity of setting.

*Materials:* The project will not remove historic buildings materials. The materials of the proposed project will not damage the district's integrity of materials.

*Workmanship:* The proposed project will not remove evidence of workmanship evident in historic buildings; thus the project will not impair the district's integrity of workmanship.

*Feeling:* The proposed project maintains a building height and massing similar to the surrounding building fabric and will be constructed on a non-historic surface parking lot. The project will not negatively impair the district's integrity of feeling.

*Association:* The project is new construction on a surface parking lot. The area was historically a rail yard during the period of significance, but the historic association was

damaged when the rail buildings were razed and the surface parking lot was created. The proposed project will not greatly impair the area's integrity of association because it was previously altered.

**4. The alteration will not materially impair the significance and integrity of the landmark, historic district or nominated property under interim protection as evidenced by the consistency of alterations with the applicable design guidelines adopted by the commission.**

The applicable design guidelines for this project are the Warehouse Historic District Design Guidelines (March 2010). The purpose of these guidelines is to protect the integrity of the district. The proposed project falls within the Rail Yards Area of the district, which "is different from the rest of the Warehouse District" and "historically, the site contained railroad depots, sheds, offices and railroad tracks." The area "did not contain warehousing, manufacturing or other buildings or structures found in other parts of the district." The Rail Yards Area is significant for the manipulation of grade and "the significant feature of the site is the lowered grade relative to the adjacent properties...It is important to convey the significance of this area in new development and provide for a visual context that sets this area off from the adjacent historic buildings."

As discussed more fully below, the project is consistent with the applicable design guidelines for the Warehouse Historic District; thus, the project will not materially impair the significance and integrity of the district.

**Site Design:**

**Corridors**

*"The area contains three distinct corridors that represent the railroad activity in the area and the connections created by the lowered grade of the site."*

The proposed design follows the Guidelines and preserves these (corridor and connection) features and strives to interpret them through site and building design. These features act as key drivers and shapers of the proposed design. (Please see the design response description for Requirements 4.1 through 4.5 and Advisory 4.6 below)

**Requirement 4.1**

*"A 30 foot wide corridor over the existing BNSF rail corridor shall remain open to visual access and not decked over or built over by buildings"* - The proposed design maintains the 30 foot corridor and additional width as required for the Cedar Lake Trail which is contained in an easement along the east side of the BNSF rail corridor. The massing of the proposed design is meant to strengthen the rail corridor visual presence through a strong façade line that gives additional shape and definition along the East side of the corridor.

#### Advisory 4.2

*"Alterations to the width of the active BNSF rail corridor to accommodate train infrastructure are appropriate if the 30 foot wide corridor is retained"* - The proposed design does not alter the 30-foot wide corridor. The proposed design and building placement demarcate the historic rail corridor and give shape and definition to this continuous rail corridor.

#### Advisory 4.3

*"Bridging and skyways over the corridor or rail yard are appropriate"* - The proposed design does not have a bridge or skyway, but does not preclude these features in the future.

#### Requirement 4.4

*"The 80 foot wide Third Street North View Corridor shall remain unobstructed to the sky and be preserved through the site. New development in the rail yard is allowed below the historic bridge deck height, but shall not encroach on this feature above that level"* - The proposed design maintains the 80-foot Third Street North Corridor as an open to the sky, unobstructed view corridor. The massing of the proposed design is meant to strengthen the view corridor by shaping the edge of the corridor along the 80 foot setback line.

#### Requirement 4.5

*"The Fourth Street North Corridor shall be preserved through the site. The corridor is obscured by the current I-94 Viaducts. The preservation of this corridor can be done interpretatively through design and it is not required to remain unobstructed like the Third Street North Corridor."* - The proposed design maintains the Fourth Street North Corridor.

#### Advisory 4.6

*"Reconstructing a bridge over the rail yards and rail corridor at Third Street North is strongly encouraged"* - The proposed design does not propose a bridge over the BNSF corridor extension at the Third Street North Corridor, but it does not preclude adding a bridge in the future.

### **Connections**

*"In 1890 the grade of the rail corridor and the rail yards were lowered and the bridges were constructed to facilitate better connections through and to the Rail Yard Area. These alterations facilitated the further growth of the railroad, warehousing, manufacturing and other industries in the warehouse district. Allowing connections to and through the site in future development is in keeping with the spirit, intent and outcome of the design of this historic feature."*

*"The Washington Avenue North steel truss bridge is the only existing bridge to remain in the district. The Third, Fourth and Fifth Street North bridges have all been replaced or removed. The Washington Avenue North bridge steel superstructure is original; however it sits on concrete deck, supports and abutments that were replaced in the early twenty-first century."*

The proposed site design connects to Dock Street, the new access roadway that was built for the Phase 1 Dock Street Apartments project, which provides a gateway down into the Cut off Washington Avenue North near the east end of the North Washington Steel truss bridge. Dock Street emphasizes the 12-foot drop from the Washington Avenue grade down into the Cut.

The proposed site design features connect with the exterior interpretive pedestrian / vehicular landscape that was constructed with the Phase 1 Dock Street Apartments project which celebrates the Third Street North visual and spatial corridor through the Cut. This feature is down in the Cut, emphasizing the lowered elevation. This landscape feature serves a significant interpretive role while providing access to the Cedar Lake Trail for pedestrians and bicycles and access for vehicles and pedestrians into both the Phase 1 Dock Street Apartments Project and the Phase 2 T3 Office Project.

The proposed design meets all the Connections Guidelines and uses the Guidelines as drivers and shapers of the design.

#### Requirement 4.7

*"The Washington Avenue North steel bridge superstructure shall remain on the site"* – The proposed project is outside the area of the existing Washington Avenue steel bridge. However, the design of the Phase 1 Dock Street Apartment project symbolically and functionally maintained the bridge connecting the 19th century warehouse area to the 21st century warehouse area.

#### Requirement 4.8

*"The Third Street North bridge abutment on the west side of the rail corridors is intact and shall be preserved in place"* - The Third Street bridge abutment is outside the project area and will not be affected.

#### Requirement 4.9

*"Skyways or bridges are allowed over the BNSF rail corridor"* - The proposed design does not currently have skyways or bridges but it does not preclude them from being added in the future.

#### Requirement 4.10

*"Decking over the BNSF corridor is not allowed."* - The proposed design does not have decking over the BNSF corridor.

#### Requirement 4.11

*"The number of skyways or bridges that are allowed is not specified. The width of the skyways or bridges shall not exceed the historic width of the viaducts that bridged the area. When the skyways and bridges exceed the width of the viaducts or their number is too great they create a decked feel and the design, feeling and association of the corridor is lost."* - The proposed design does not have any skyways or bridges.

#### Other Considerations 4.12

*"Skyways between new construction and historic buildings on the east side of the rail Yards Area will be considered if connecting to a secondary facade and it will not conceal or damage the freight transfer features of these buildings"* - The proposed design does not have skyways between new construction and historic buildings.

#### **Grade Separation**

*"The lowered grade of the rail yards and even lower grade of the active rail corridor are integral character defining features of the district... The historic grade elevations of the Rail Yards Area are encouraged to be incorporated into the design of new construction."*

#### Requirement 4.13

*"The lowered elevations in the Rail Yards Area shall not be lost in the development of the area."* -. The historic grade elevations of the Rail Yards Area are incorporated into the design in the following ways:

- The proposed site design connects to Dock Street, the new access roadway that was built for the Phase 1 Dock Street Apartments project, which provides a gateway down into the Cut off Washington Avenue North near the east end of the North Washington Steel truss bridge. Dock Street emphasizes the 12-foot drop from the Washington Avenue grade down into the Cut.
- The proposed site design features connect with the exterior interpretive pedestrian/vehicular landscape that was constructed with the Phase 1 Dock Street Apartments project which celebrates the Third Street North visual and spatial corridor through the Cut. This feature is down in the Cut, emphasizing the lowered elevation. This landscape feature serves a significant interpretive role while providing access to the Cedar Lake Trail for pedestrians and bicycles and access for vehicles and pedestrians into both the Phase 1 Dock Street Apartments Project and the Phase 2 T3 Office Project.

#### Advisory 4.14

*"Design interpretation, visual or physical access is encouraged to convey the lower elevation of the area"* - (See proposed design description at 4.13 above) The landscape design that was constructed within the 80' Third Street North right-of-way as part of the Phase 1 Dock Street Apartments Project features visual rail elements that are embedded in the pavement that recall the existence of the temporary linear, narrow, north-south freight houses and rail spurs that were demolished and eradicated without clear photographic records. The proposed design does not affect this visual element.

#### **Design for New Buildings:**

*"New development in the Rail Yards Area should be true to itself and not designed to resemble warehouse buildings or freight houses...It is important that new construction*

*offset itself through design in this area, but it is important that new construction preserve the features of the site outlined in the Site Design Guidelines."*

The proposed building is shaped by the *Warehouse Historic District Design Guidelines* (March 2010) and specifically the section, "Rail Yards Area Site Design Guidelines of Corridors, Connections and Grade Separation." The proposed building is not designed to resemble warehouse buildings or freight houses, but is informed by the "Rail Yards Area Guidelines" and by the simplicity and clear functional expression of the surrounding warehouse context. This specific "Rail Yards Area" was historically filled with long, depot sheds and train platforms (razed 1939-1950s) before the surface parking lots of the 1970s.

The proposed design marries traditional, industrial proportions with modern materials and detailing. The proposed T3 project is respectful of its neighborhood and responds to its context with a simple massing. The form, color and exterior articulation create a quiet building – responding to and complementing the historical buildings rather than drawing attention to it.

By studying the existing building stock in the Warehouse District, the design team identified common design themes for the massing and window systems. Four different approaches to window composition emerged from the neighborhood and ultimately two solutions were deemed most appropriate for the building's use, location and views in and out of the building.

This neighborhood analysis of the historical building typologies also informed the strong vertical rhythm of piers clad in pre-weathered Corten steel. These piers frame the large window openings - generating interest and animation along the street faces, and when viewed from afar.

The street level building base is subtly distinguished from the upper levels by its proportion of glass retail storefront and canopies. The upper floors feature rhythmic punched window openings, playing on historic proportions but using modern, highly efficient windows. The topmost floor features a slightly taller proportion of punched windows, referencing the existing 'bottom-middle-top' designs of many buildings in the district.

Inside the building, exposed mass timber columns and floor slabs recall the heavy timber construction of the building's predecessors. While the historic brick-and-timber structures of the district used old growth solid timbers for columns, beams, and even floor joists, T3's modern technological approach uses engineered wood components (chiefly glu-lam and cross-laminated timber). These modern materials bring the warmth and beauty of wood to the interior, while being sustainable, renewable, and structurally predictable.

Building service systems such as sprinkler lines, HVAC , and electrical components will be efficiently designed and organized, then left exposed within the finished space, alluding to the industrial character of historic buildings. Again, modern technology will

allow these systems to be quiet, environmentally efficient, and enhance tenant comfort and enjoyment of the space.

## **Setbacks**

*"Only the Washington Avenue North portion of the Rail Yards Area is along a street where a distinct street wall is evident and was historically represented by a former building wall."*

### Requirement 4.15

*"The Washington Avenue North street wall shall be reinforced with new development along this portion of the site. A built-to-line setback of zero feet is required."* - The Phase 1 Dock Street Apartments project was built at the County's "no build easement" (due to the bridge abutments and piles) along Washington Avenue North which reinforced the street wall. The T3 project is not adjacent to Washington Avenue.

### Requirement 4.16

*"No specific setback shall be required for buildings or other features along Fifth Street North. There was never a street wall there to protect."* - The proposed project falls outside the Fifth Street North area of the Rail Yards Area.

### Requirement 4.17

*"A separation shall be maintained between new buildings or structures and the historic buildings on the east side of the Rail Yards Area. Skyways over this separation will be considered per the guidelines in Connections (4.7-4.12)."* - The proposed design maintains a separation between the new building and the historic Union Plaza Building (Creamery Package Manufacturing Company and Annex; 1895, 1898).

## **Building Height**

### Requirement 4.18

*"The height of the new buildings shall not exceed 20 stories"* - The proposed design does not exceed 20 stories. The proposed design is 7 stories.

### Other Considerations 4.19

*"Additional height will be considered if evidence is provided that shows the additional height is compatible with adjacent historic resources and the other contexts of the district."* - The proposed design does not exceed the height guidelines.

- 5. The alteration will not materially impair the significance and integrity of the landmark, historic district or nominated property under interim protection as evidenced by the consistency of alterations with the recommendations contained in The Secretary of the Interior's Standards for the Treatment of Historic Properties.**

There are ten general standards for “rehabilitation.” This project proposes new construction in the district and the following guidelines apply:

*Rehabilitation standard #1* of *The Standards* states “a property will be used as it was historically or be given a new use that requires minimal change to its distinctive materials, features, spaces, and spatial relationship.” –The historic building and use at this location have been demolished and a surface parking lot currently occupies the project area. The proposed use is a commercial office building that recalls the historic spatial relationships.

*Rehabilitation standard #2* states “historic character of a property will be retained and preserved. The removal of distinctive materials or alteration of features, spaces, and spatial relationships that characterize a property will be avoided.” - The proposed building will not demolish historic buildings or negatively affect historic materials, features or spatial relationships. The proposed design maintains the visual open corridor of the Grade Separation-Cut as well as Third Street North.

*Rehabilitation standard #3* states “each property will be recognized as a physical record of its time, place, and use. Changes that create a false sense of historical development, such as adding conjectural features or elements from other historic properties, will not be undertaken.” - Historically the lot was occupied by a train freight depot and service buildings, which were demolished before the historic district was designated. The project will replace the non-contributing surface parking lot with a commercial office building. The new building does not mimic or create a false sense of the district’s historical development.

*Rehabilitation standard #4* states “changes to a property that have acquired historic significance in their own right will be retained and preserved.” - The proposed building will occupy a non-contributing surface parking lot that lacks historic significance.

*Rehabilitation standard #5* states “that distinctive materials, features, finishes, and construction techniques or examples of craftsmanship that characterize a property will be preserved.” - The project will not result in the removal or damage to historic materials and historic visual corridors will be retained.

*Rehabilitation standards #6* states “that deteriorated features will be repaired rather than replaced...replacement of missing features will be substantiated by documentary and physical evidence.” – The historic fabric at this location no longer remains. The project will not replace or mimic historic features.

*Rehabilitation standard #7* states “that chemical or physical treatments, if possible, will be undertaken using the gentlest means possible.” - The project proposes new construction and will not damage historic features.

*Rehabilitation standard #8* states “archeological resources will be protected and preserved in place. If such resources must be disturbed, mitigation measures will be undertaken.” - Two Pines Resource Group completed a Phase IA literature search for archaeological potential and concluded that “the land use history of the project area also indicates that the railroad grade separation project that took place in 1890-91 significantly modified the landscape of the entirety of the study area and removed any pre-1891 archaeological deposits within the Area of Potential Effect (APE). Furthermore the types of structures present within the project area after 1891 are unlikely to have associated archaeological deposits with information potential.” Based on these findings, Two Pines Resource Group recommended against subsurface archaeological testing.

*Rehabilitation standard #9* states “new additions, exterior alterations, or related new construction will not destroy historic materials, features, and spatial relationships that characterize the property. The new work shall be differentiated from the old and will be compatible with the historic materials, features, size, scale and proportion, and massing to protect the integrity of the property and its environment.”

*Spatial relationships:* Historically the lot was occupied by a train depot and service buildings, which were demolished before the historic district was designated. The project will replace the non-contributing surface parking lot with a commercial office building. Historic spatial relationships are recreated. The proposed project preserves the open view shed of Third Street as well as the railroad Cut.

*Differentiated:* The new design is differentiated from the old by not mimicking historic materials and openings, but is compatible to the surrounding fabric’s materials, features, size, scale, proportion and massing. The proposed corrugated and flat panel Corten steel siding are industrial types, but not repetitive of historic types. Openings are even and regular, but not filled with a historic, wood, double-hung sash. The size, scale, proportion and massing of the flat-roof, seven-story building is similar to that of the surrounding historic fabric. The building is “broken” at the middle to offset the length of the building wall.

*Rehabilitation standards #10* states “new additions and adjacent or related new construction will be undertaken in such a manner that, if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.” -This building does not physically touch historic buildings or fabric and can be removed in the future without damaging historic resources.

- 6. The certificate of appropriateness conforms to all applicable regulations of this preservation ordinance and is consistent with the applicable policies of the comprehensive plan and applicable preservation policies in small area plans adopted by the city council.**

Plan policy 8.1 of *The Minneapolis Plan for Sustainable Growth* states that the City will “preserve, maintain, and designate districts, landmarks, and historic resources which

serve as reminders of the city’s architecture, history and culture.” - The proposed project does not necessarily “preserve” the district, but it is compatible infill.

Action 8.1.2 of the comprehensive plan requires “new construction in historic districts to be compatible with the historic fabric.” - The applicant has followed the guidelines for the historic district and specific Rail Yards Area to provide a compatible design in the historic district.

Action 8.1.3 of the comprehensive plan encourages “new developments to retain historic resources, including landscapes, incorporating them into new development rather than removal.” - The project retains the grade separation of the Cut as well as the 80-foot width of the Third Street viewshed.

Plan policy 8.3 of the comprehensive plans states that the City will “explore and protect potential archeological resources in the city.” - As part of the EAW, Two Pines Resource Group conducted a Phase IA literature search for archaeological potential and concluded that historic fabric before the 1891 Cut was severely damaged and further archaeological investigation is not recommended.

- 7. Destruction of any property. Before approving a certificate of appropriateness that involves the destruction, in whole or in part of any landmark property in an historic district or nominated property under interim protections, the commission shall make findings that the destruction is necessary to correct an unsafe or dangerous condition on the property.**

Demolition of historic property is not proposed at this site.

- 8. Adequate consideration of the description and statement of significance in the original nomination upon which designation of the landmark or historic district was based.**

The applicant has addressed the historic significance of the project area in the Environmental Assessment Worksheet (EAW) as well as working with the historic consulting firm, Landscape Research LLC, throughout the design process. Architect, Michael Green Architecture, and historic consultant, Landscape Research, have worked closely to shape the development and ensure consistency with the district guidelines.

- 9. Where applicable, adequate consideration of Title 20 of the Minneapolis Code of Ordinances, Zoning Code, Chapter 530, Site Plan Review.**

The proposal also requires Site Plan Review and other land use permits. The applicant has met with Planning Commission staff, and will submit plans for Planning Commission review after the Heritage Preservation Commission review.

**10. The typology of treatments delineated in *The Secretary of the Interior's Standards for the Treatment of Historic Properties* and the associated guidelines for preserving, rehabilitating, reconstructing, and restoring historic buildings.**

The applicant is proposing new construction. These standards are reviewed in finding #5 and the proposal is in compliance.

**11. The alteration is compatible with and will ensure continued significance and integrity of all contributing properties in the historic district based on the period of significance for which the district was designated.**

The proposed project is a sensitive design and respects the surrounding buildings and period of significance for the district.

**12. Granting the certificate of appropriateness will be in keeping with the spirit and intent of the ordinance and will not negatively alter the essential character of the historic district.**

The spirit and intent of the Heritage Preservation Regulations is to preserve historically significant buildings, structures, site, objects, districts, and cultural landscapes while permitting appropriate alterations.

The proposed project is appropriate in the district and does not have a negative impact on the surrounding historic district.

**13. The certificate of appropriateness will not be injurious to the significance and integrity of other resources in the historic district and will not impede the normal and orderly preservation of surrounding resources as allowed by regulations in the preservation ordinance.**

The proposed project will not impede the preservation of surrounding historic resources nor is it injurious to the district. Historic bridge abutments, view sheds and rail way Cut will remain and are respected in the design proposal.





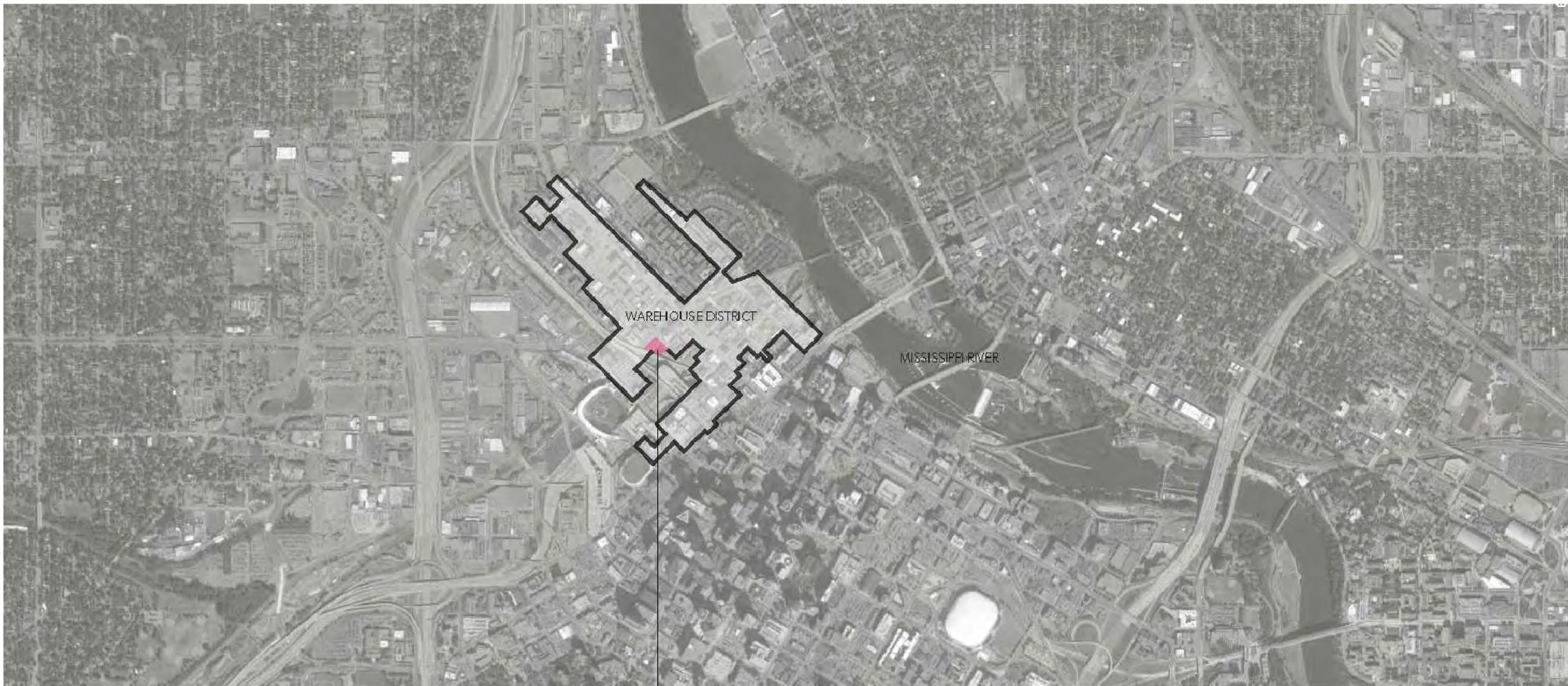
# MINNEAPOLIS T3

HISTORIC PRESERVATION COMMISSION

15 DECEMBER 2014

HINES NORTHSTAR CROSSINGS LIMITED PARTNERSHIP  
90 SOUTH 7TH STREET, SUIT 150  
MINNEAPOLIS, MN 55402

MGA | MICHAEL GREEN ARCHITECTURE  
with DLR GROUP



Project site

LOCATION PLAN



**DRAWING LIST**

ARCHITECTURAL

A001	COVER DRAWING LIST/PROJECT INFO/LOCATION	NTS NTS
A004	SITE PHOTOS - STREET VIEWS	
A005	SITE PHOTOS - AERIAL VIEWS	
A006	BUILDING TYPOLOGIES- WINDOW TYPES	
A201	ELEVATIONS - NORTH	1/16" = 1'-0"
A202	ELEVATIONS - EAST	1/16" = 1'-0"
A203	ELEVATIONS - WEST	1/16" = 1'-0"
A204	ELEVATIONS - SOUTH	1/16" = 1'-0"
A041	HPC - PROJECT DESCRIPTION TEXT	NTS
A042	HPC - CONTEXT PERSPECTIVES	NTS
A043	HPC - CONTEXT PERSPECTIVES	NTS
A044	HPC - CONTEXT PERSPECTIVES	NTS
A050	AREA PLAN - NORTH LOOP GREEN	1/60" = 1'-0"
A051	RENDERED SITE PLAN	1/16" = 1'-0"
A100	PLAN - PARKING LEVEL	1/16" = 1'-0"
A101	PLAN - LEVEL 1	1/16" = 1'-0"
A102	PLAN - LEVELS 2	1/16" = 1'-0"
A103	PLAN - LEVELS 3-7	1/16" = 1'-0"
A108	PLAN - ROOF	1/16" = 1'-0"
A010	RENDERINGS	NTS
A011	RENDERINGS	NTS
A012	RENDERINGS	NTS
A013	RENDERINGS	NTS
A014	RENDERINGS	NTS
A801	EXTERIOR DETAILS	1/2" = 1'-0"
A802	EXTERIOR DETAILS	1/2" = 1'-0"

LANDSCAPE

L100	LANDSCAPE PLAN	1/16" = 1'-0"
<u>CIVIL</u>		
C1-1	EXISTING CONDITION PLAN	NTS
C1-2	SITE DEMOLITION PLAN	NTS
C2-1	SITE PLAN	NTS
C3-1	GRADING AND DRAINAGE PLAN	NTS
C3-2	STORM WATER POLLUTION PREVENTION	NTS
C4-1	UTILITY PLAN	NTS
C4-2	SCHEMATIC UTILITY PLAN	NTS
C8-1	DETAIL SHEET	NTS
C8-2	DETAIL SHEET	NTS

**PROJECT TEAM**

CLIENT

Hines  
Wells Fargo Center  
90 South 7th Street, Suite 150  
Minneapolis, MN 55402  
Contact: David Spillman

DESIGN ARCHITECT  
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ARCHITECT OF RECORD

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Seattle, WA 98101-2699  
Contact: Leif Johnson

MECHANICAL + ELECTRICAL

Dunham Engineering  
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Minneapolis, MN 55402  
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ELEVATOR

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LANDSCAPE

Damon Farber Associates  
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Minneapolis, MN 55401  
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GEOTECHNICAL

Loucks Associates  
7200 Hennipack Lane, Suite 300  
Maple Grove, MN 55369  
Contact: Henry D. Nelson

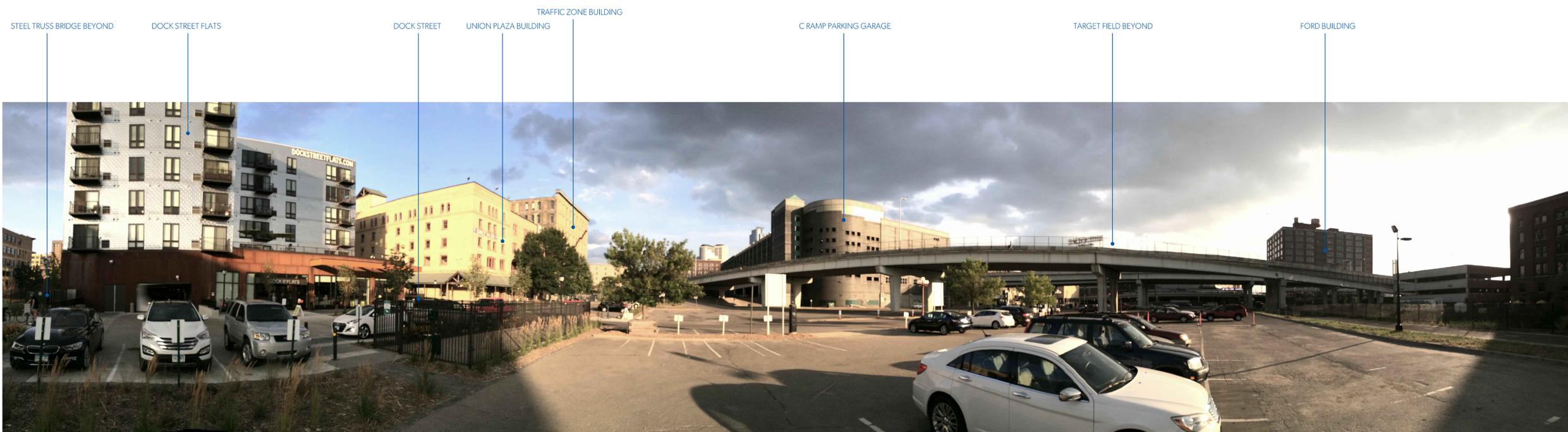
**MICHAEL GREEN ARCHITECTURE**  
63 E CORDOVA STREET  
VANCOUVER BC  
CANADA V6A 1K3

**DLR Group**  
520 NICOLLET MALL, SUITE 200  
MINNEAPOLIS MN  
USA 55402

DATE	REVISION	DESCRIPTION
2014 12 15		ISSUED FOR HPC CoA
2014 12 05		ISSUED FOR PRICING ADDEN.
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2014 11 03		ISSUED FOR PRICING
2014 10 27		ISSUED FOR HPC/PLANNING
2014 10 15		ISSUED FOR SCHEMATIC DESIGN

**MINNEAPOLIS T3**  
MINNEAPOLIS, MN  
2014 015

**A001**  
PROJECT INFORMATION



1 **EXISTING SITE LOOKING FROM BIKE PATH**

A004



2 **EXISTING SITE LOOKING FROM BIKE PATH**

A004



3 **EXISTING SITE LOOKING FROM PARKING LOT**

A004

**MICHAEL GREEN ARCHITECTURE**  
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CANADA V6A 1K3

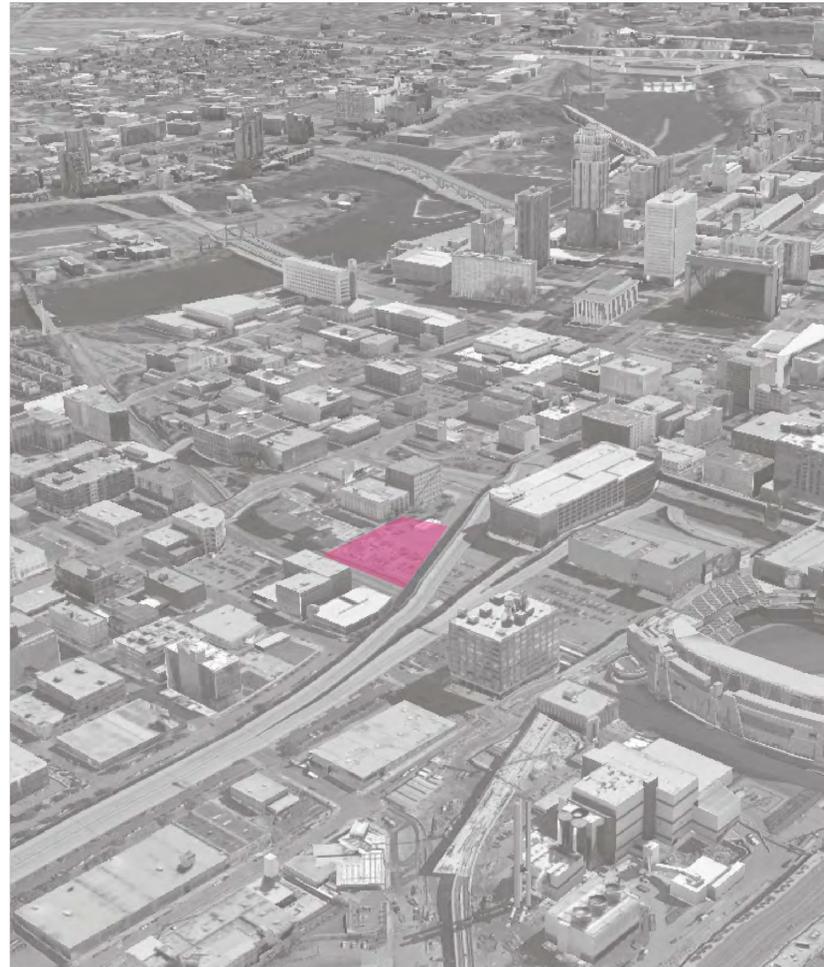
**DLR Group**  
520 NICOLLET MALL, SUITE 200  
MINNEAPOLIS MN  
USA 55402

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2014 10 15		ISSUED FOR SCHEMATIC DESIGN

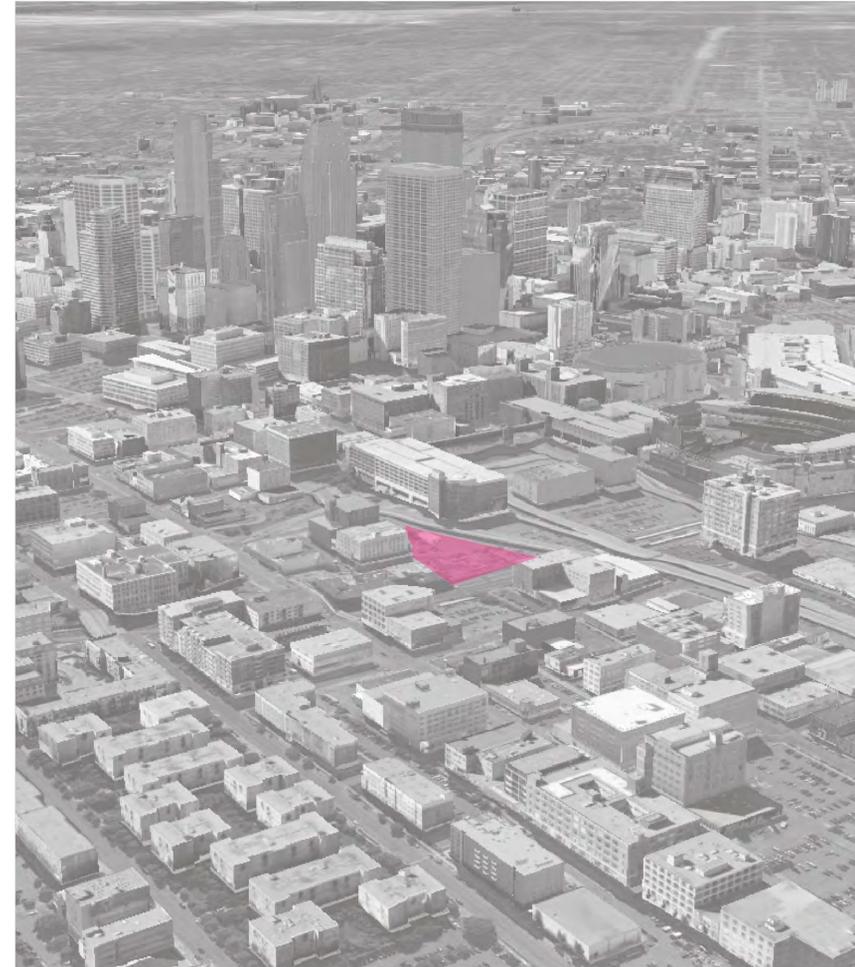
**MINNEAPOLIS T3**  
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1 **LOOKING NORTH**  
A003



2 **LOOKING WEST**  
A002



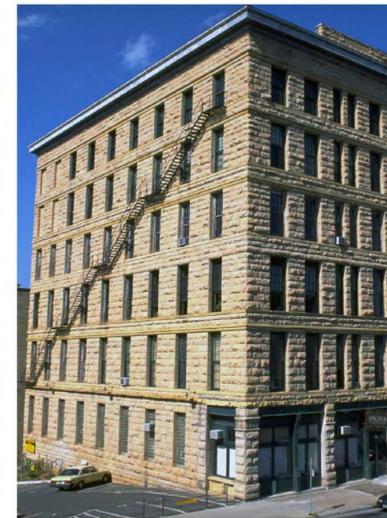
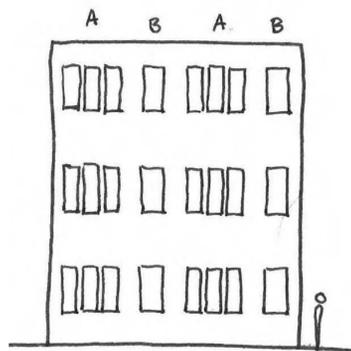
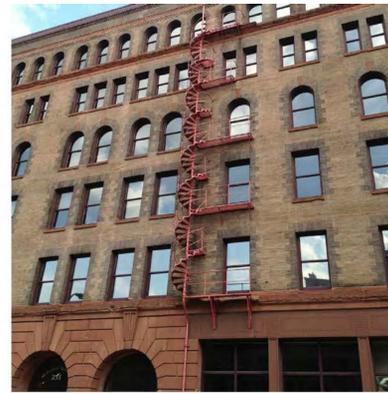
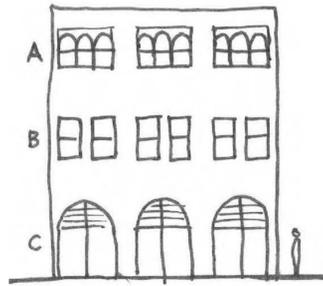
3 **LOOKING SOUTH**  
A003

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2014 10 15		ISSUED FOR SCHEMATIC DESIGN
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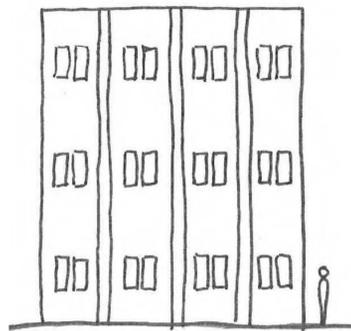


The proposed design marries traditional, industrial proportions with modern materials and detailing. The proposed T3 project is respectful of its neighborhood and responds to its context with a simple massing. The form and exterior articulation create a quiet building- responding to and complementing the historical buildings rather than drawing attention to it.

By studying the existing building stock in the warehouse district, the design team identified common design themes for the massing and window systems. Four different approaches to window composition emerged from the neighborhood and ultimately two solutions were deemed most appropriate for the building's use, location and views in and out of the building.

This neighborhood analysis of the historical building typologies also informed the strong vertical rhythm of piers clad in pre-weathered corrugated corten steel. These piers frame the large window openings - generating interest and animation along the street faces, and when viewed from afar.

The street level building base is subtly distinguished from the upper levels by its proportion of glass retail storefront. The upper floors feature rhythmic punched window openings, playing on historic proportions but using modern, highly efficient windows.



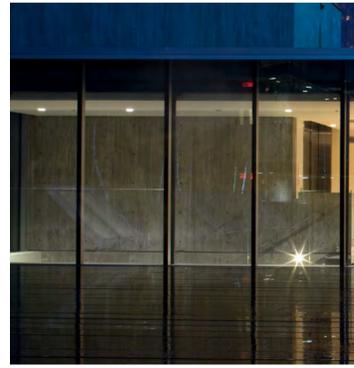
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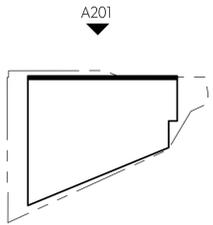
1 FLAT PANEL CORTEN



2 CORRUGATED CORTEN



3 GLAZING



1 NORTH ELEVATION  
3/32"=1'-0"

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63 E CORDOVA STREET  
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MINNEAPOLIS, MN  
2014 015

# A201

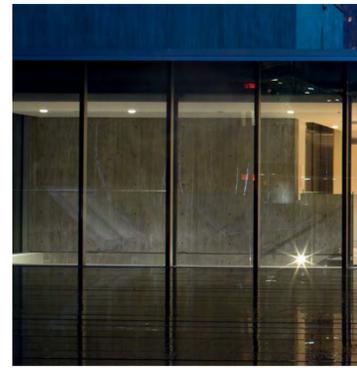
NORTH ELEVATION



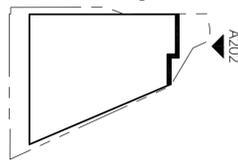
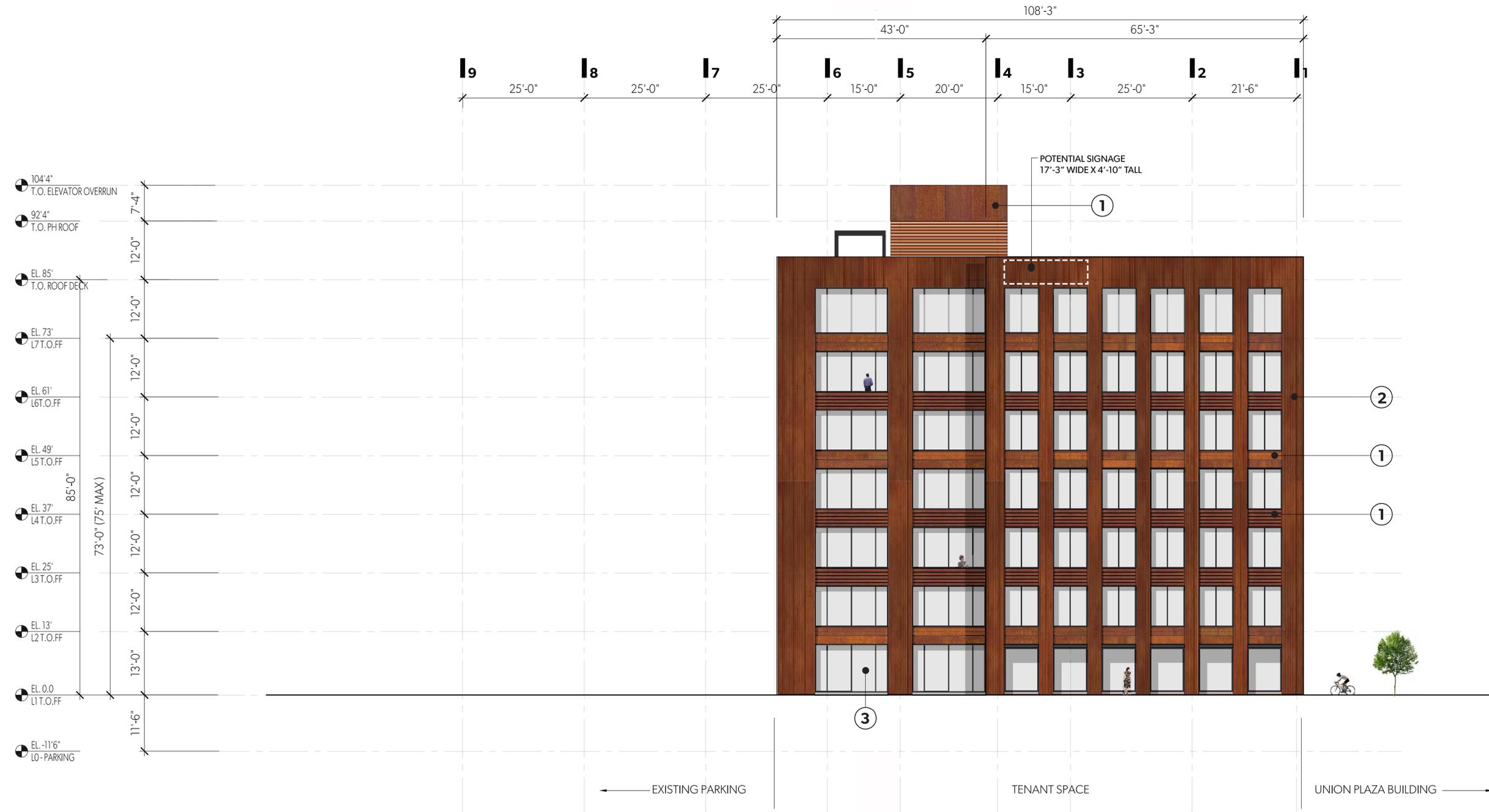
1 FLAT PANEL CORTEN



2 CORRUGATED CORTEN



3 GLAZING



**MICHAEL GREEN ARCHITECTURE**  
63 E CORDOVA STREET  
VANCOUVER BC  
CANADA V6A 1K3

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**MINNEAPOLIS T3**  
MINNEAPOLIS, MN  
2014 015

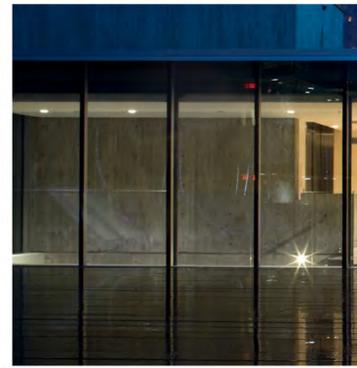




1 FLAT PANEL CORTEN



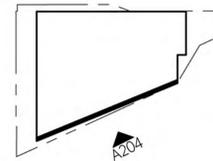
2 CORRUGATED CORTEN



3 GLAZING



4 ARCHITECTURAL CONCRETE



1 SOUTH ELEVATION  
3/32"=1'-0"

**MICHAEL GREEN ARCHITECTURE**  
63 E CORDOVA STREET  
VANCOUVER BC  
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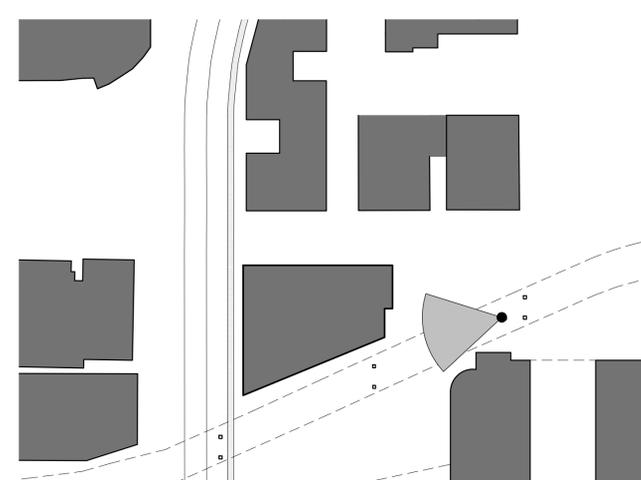
**MINNEAPOLIS T3**  
MINNEAPOLIS, MN  
2014 015

**A204**  
SOUTH ELEVATION



POTENTIAL SIGNAGE

1 **CONTEXT VIEW FROM I-94W**  
A041



**MICHAEL GREEN ARCHITECTURE**  
63 E CORDOVA STREET  
VANCOUVER BC  
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VANCOUVER BC  
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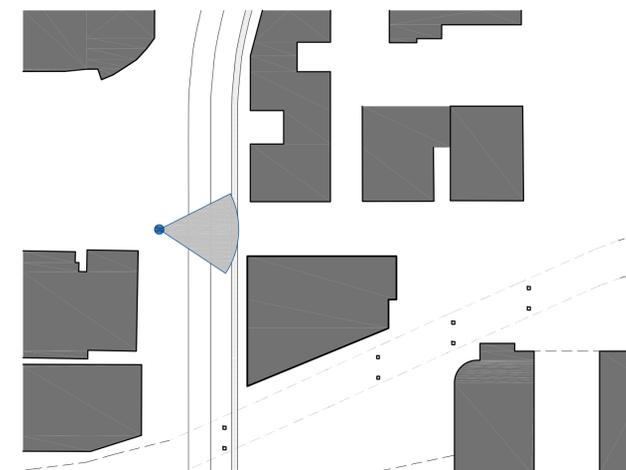
**A042**  
CONTEXT RENDERINGS

1 **CONTEXT VIEW FROM DOCK STREET**  
A042





1 **CONTEXT VIEW FROM ACROSS RAILROAD 1**  
A043



**MICHAEL GREEN ARCHITECTURE**  
63 E CORDOVA STREET  
VANCOUVER BC  
CANADA V6A 1K3

**DLR Group**  
520 NICOLLET MALL, SUITE 200  
MINNEAPOLIS MN  
USA 55402

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MINNEAPOLIS, MN  
2014 015

**A043**  
CONTEXT RENDERINGS



1 **CONTEXT VIEW FROM ACROSS RAILROAD 2**  
A044

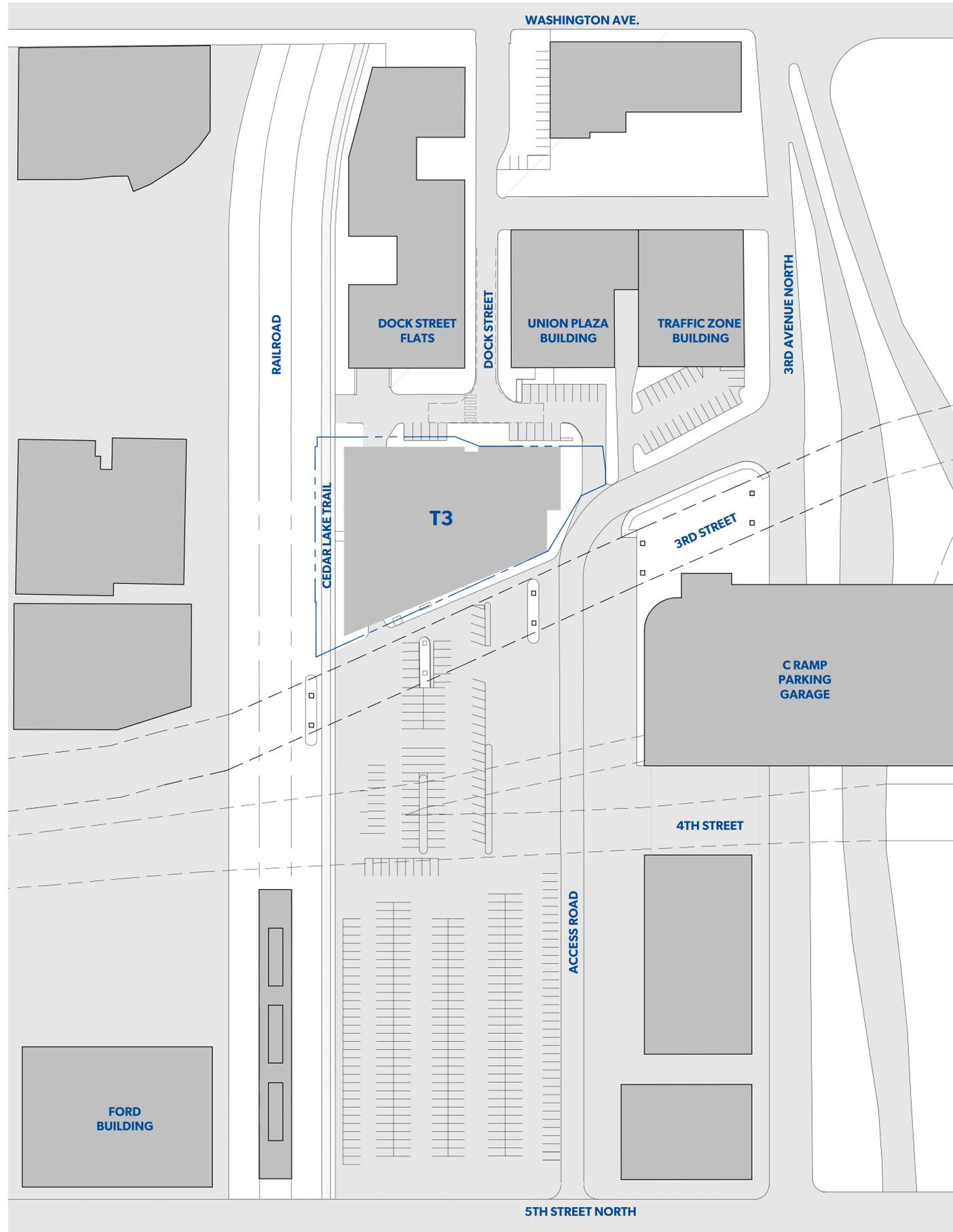


**MICHAEL GREEN ARCHITECTURE**  
63 E CORDOVA STREET  
VANCOUVER BC  
CANADA V6A 1K3

**DLR Group**  
520 NICOLLET MALL, SUITE 200  
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MINNEAPOLIS, MN  
2014 015



1  
A050

**AREA PLAN**  
1/60"=1'0"



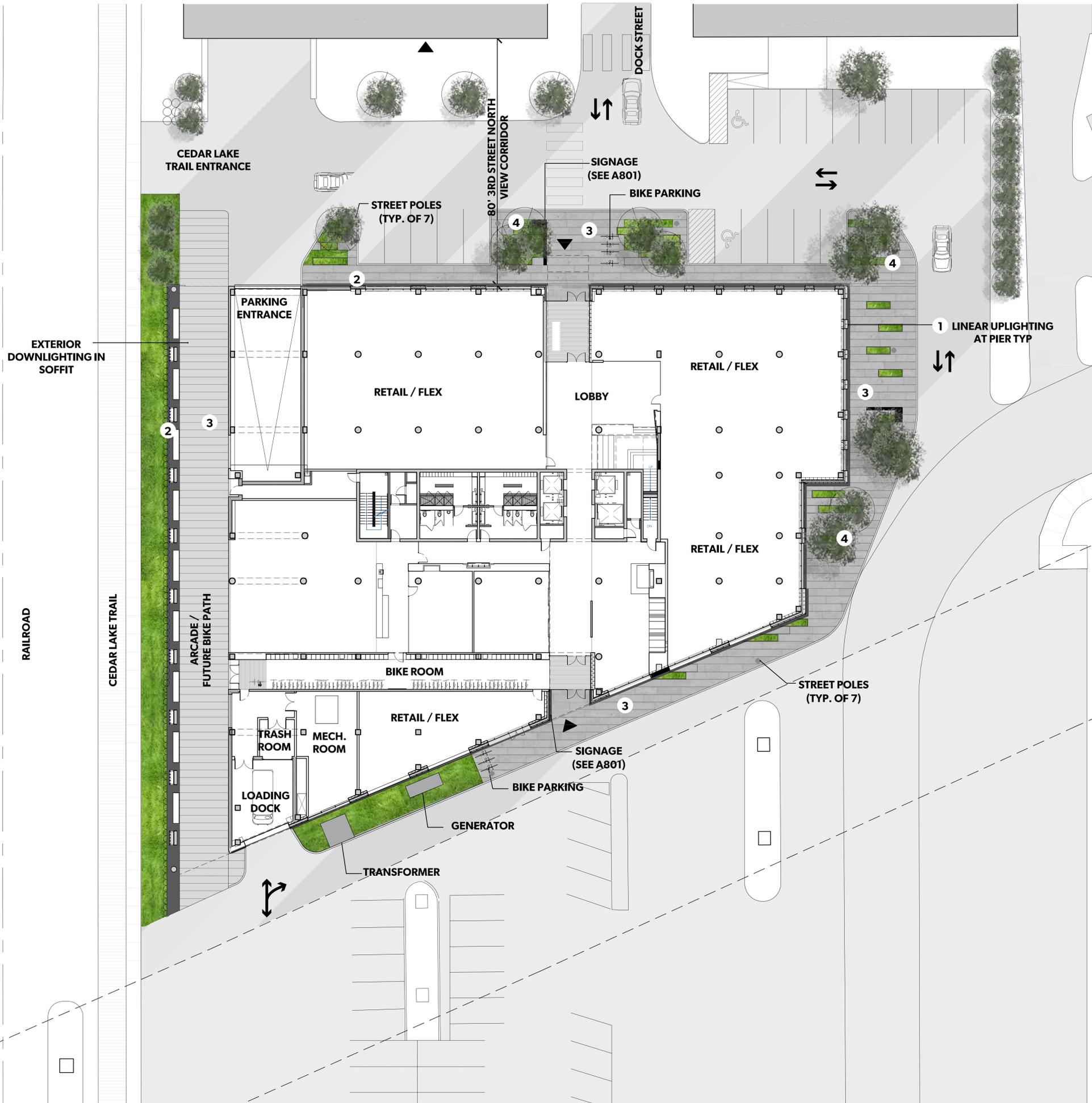
**MICHAEL GREEN ARCHITECTURE**  
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**MINNEAPOLIS T3**  
MINNEAPOLIS, MN  
2014 015

**A050**  
AREA PLAN



1. UPLIGHTING AT BASE - TYPICAL



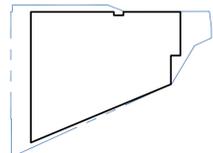
2. GRAVEL STRIP - TYPICAL



3. CAST IN PLACE CONCRETE



4. PLANTING BED WITH SEATING AREA



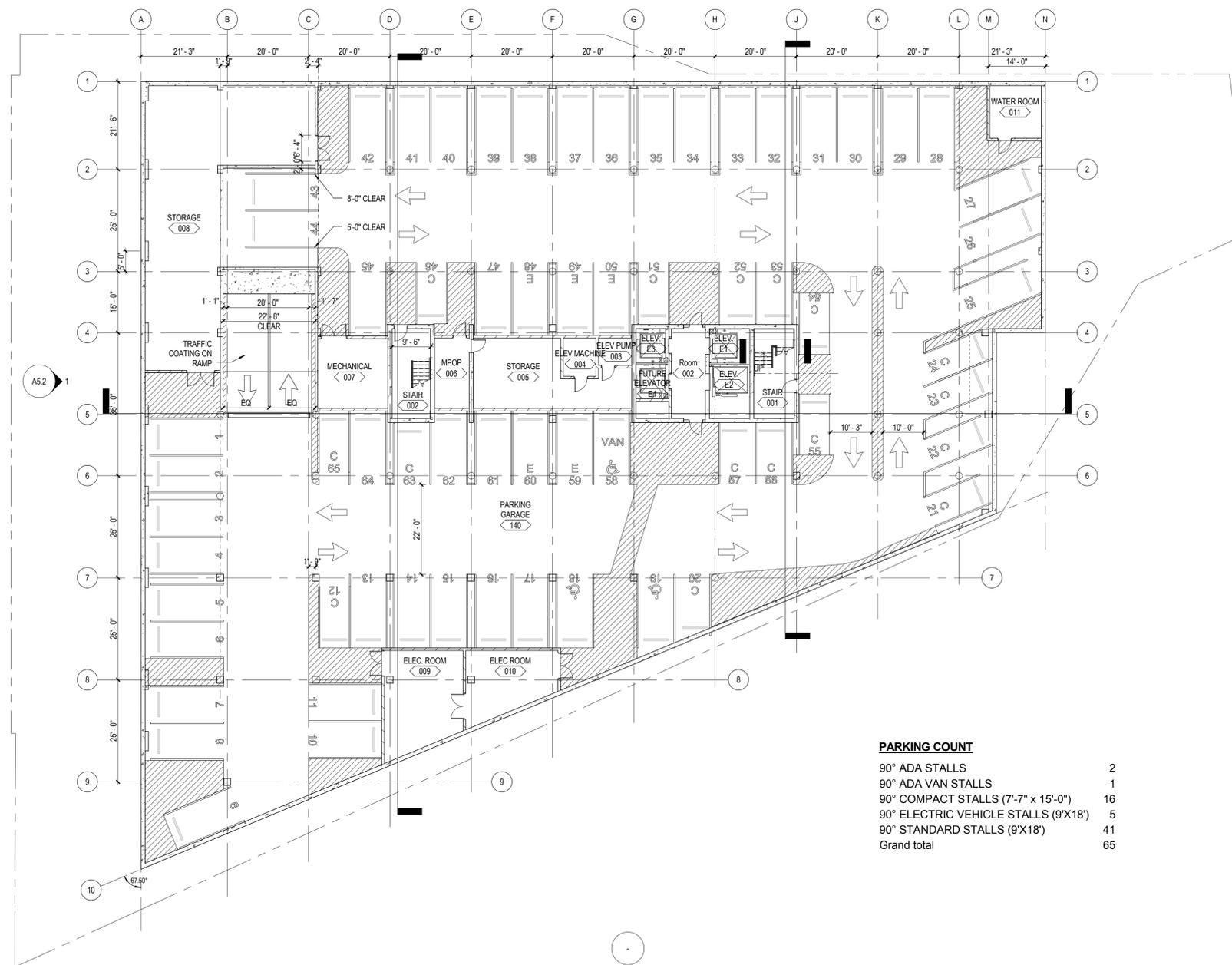
**MICHAEL GREEN ARCHITECTURE**  
63 E CORDOVA STREET  
VANCOUVER BC  
CANADA V6A 1K3

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**PARKING COUNT**

90° ADA STALLS	2
90° ADA VAN STALLS	1
90° COMPACT STALLS (7'-7" x 15'-0")	16
90° ELECTRIC VEHICLE STALLS (9'X18')	5
90° STANDARD STALLS (9'X18')	41
Grand total	65

1 **FLOOR PLAN - LOWER LEVEL**  
A100 1/16" = 1'-0"

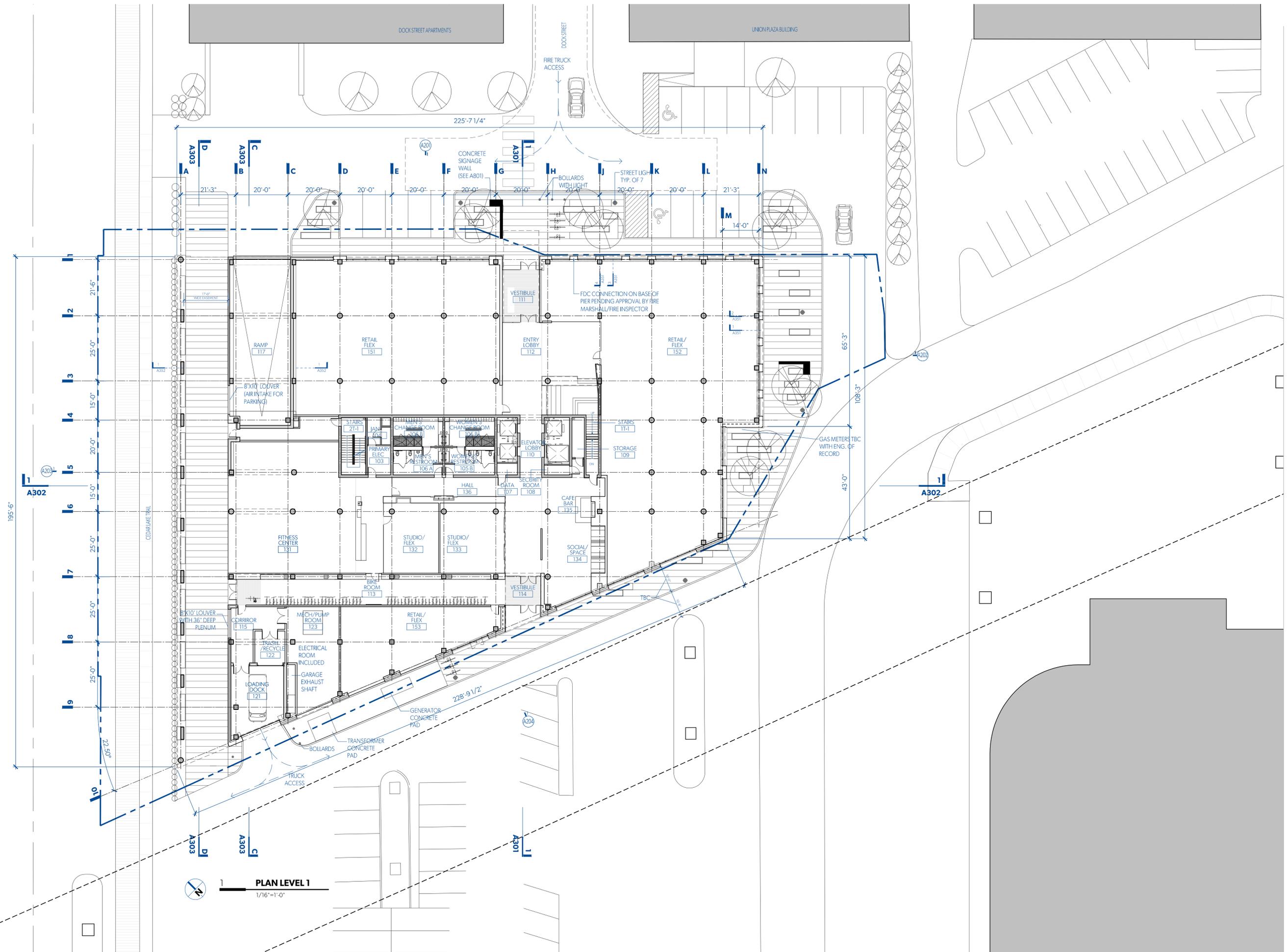
**MICHAEL GREEN ARCHITECTURE**  
63 E CORDOVA STREET  
VANCOUVER BC  
CANADA V6A 1K3

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520 NICOLLET MALL, SUITE 200  
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DATE	REVISION	DESCRIPTION
2014.12.15		ISSUED FOR HPC CoA
2014.12.05		SD ADDENDUM
2014.11.03		ISSUED FOR PRICING

**Hines T3**  
MINNEAPOLIS, MN 55401  
40-14127-00

**A100**  
FLOOR PLAN - LOWER LEVEL



195'-6"

**PLAN LEVEL 1**  
1/16"=1'-0"

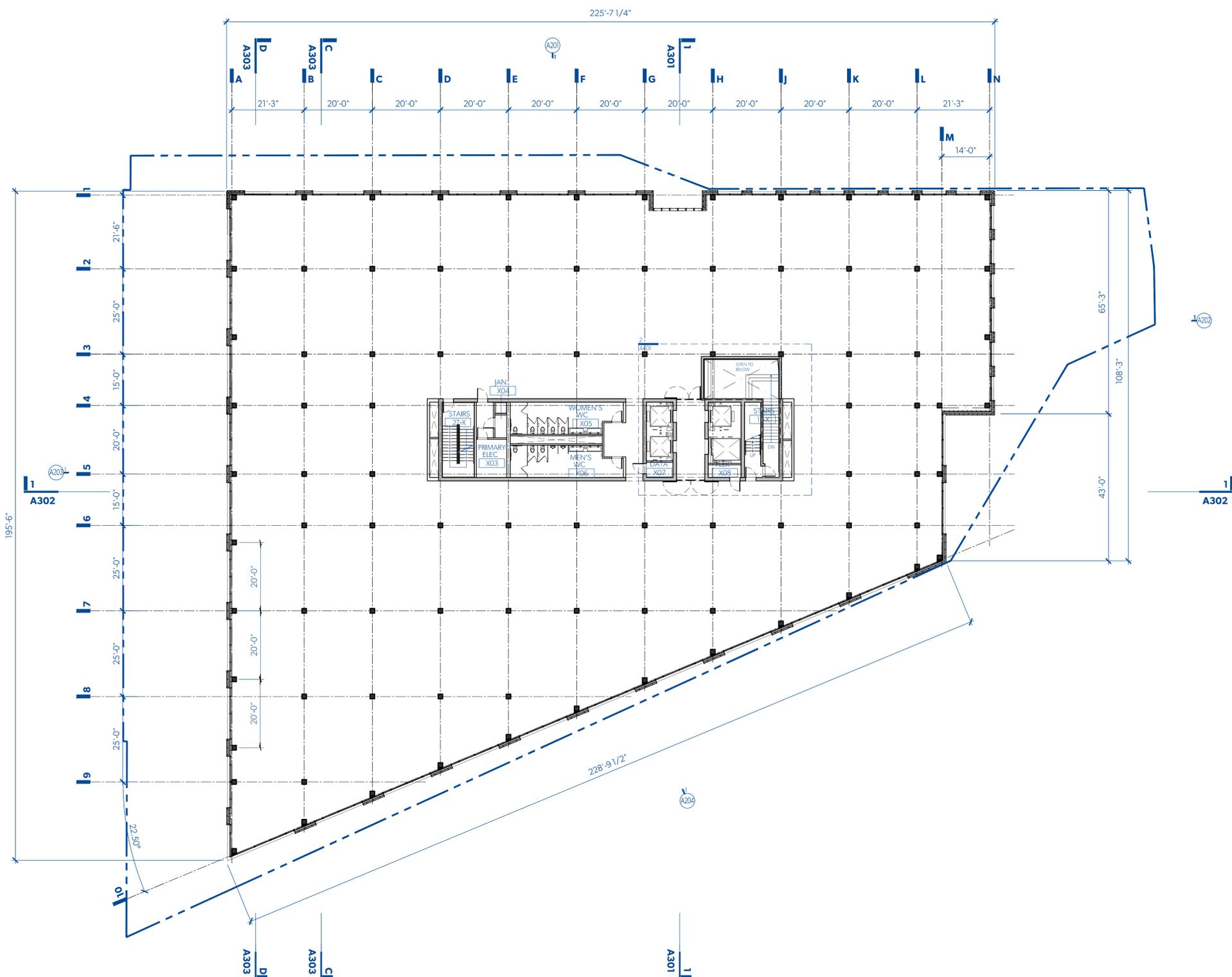
**MICHAEL GREEN ARCHITECTURE**  
63 E CORDOVA STREET  
VANCOUVER BC  
CANADA V6A 1K3

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MINNEAPOLIS, MN  
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**A101**  
PLAN - LEVEL 1



**PLAN LEVEL 2**  
1/16"=1'-0"

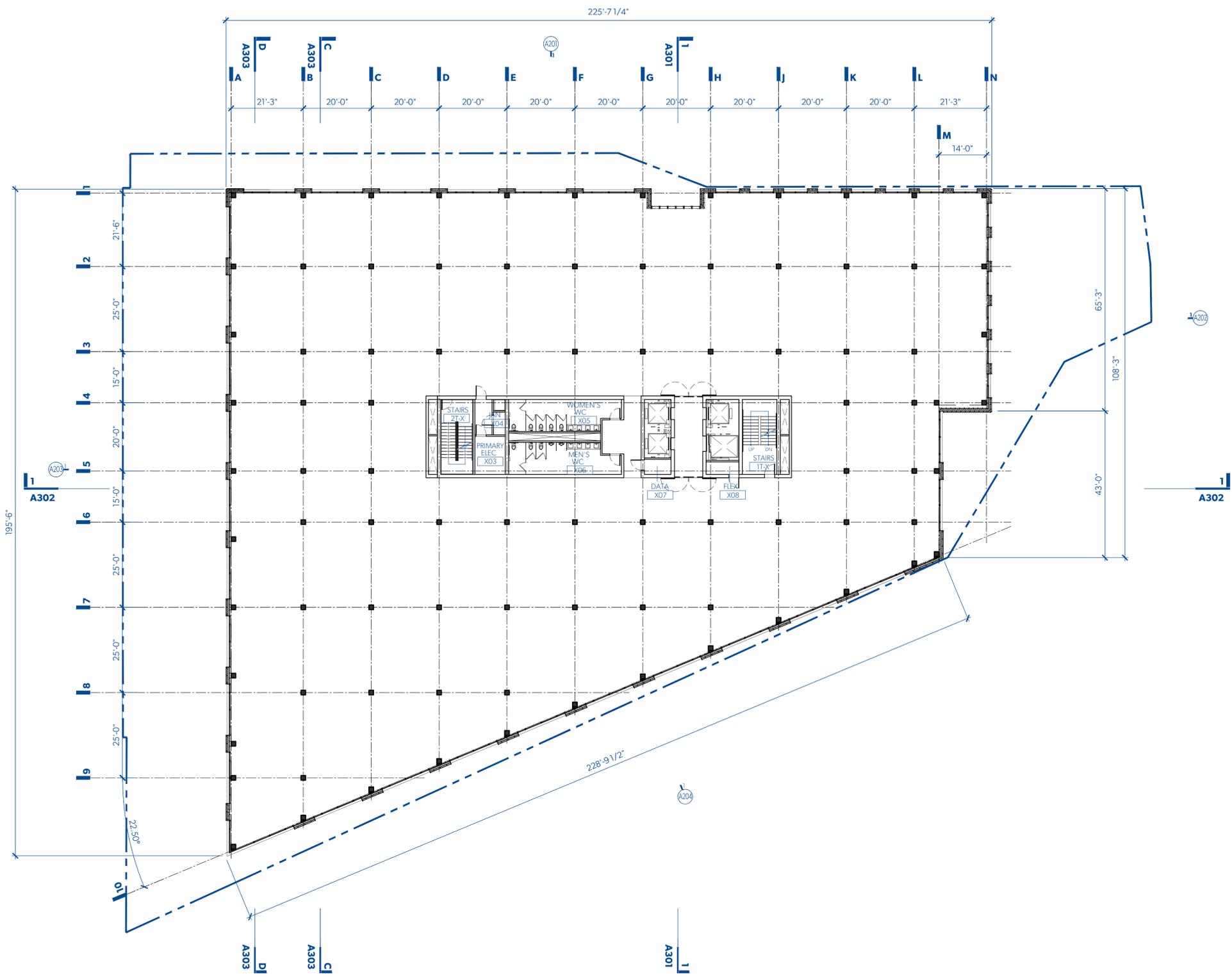
**MICHAEL GREEN ARCHITECTURE**  
63 E CORDOVA STREET  
VANCOUVER BC  
CANADA V6A 1K3

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**A102**  
PLAN - LEVEL 2



**PLAN - LEVELS 3-7**  
1/16"=1'-0"

**MICHAEL GREEN ARCHITECTURE**  
63 E CORDOVA STREET  
VANCOUVER BC  
CANADA V6A 1K3

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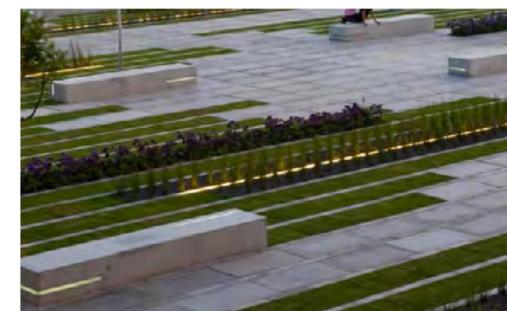
**MINNEAPOLIS T3**  
MINNEAPOLIS, MN  
2014 015



1. UPLIGHTING AT BASE - TYPICAL



2. GRAVEL BALLAST



3. PLANTING BED WITH SEATING AREA



4. TRELIS WITH SEATING AREA

**MICHAEL GREEN ARCHITECTURE**  
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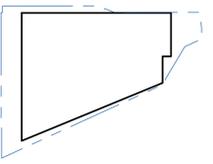
**MINNEAPOLIS T3**  
MINNEAPOLIS, MN  
2014 015

**A108**  
PENTHOUSE + ROOF



**MGA**  
© MGA 2014

**DLR Group**



**MICHAEL GREEN ARCHITECTURE**  
63 E CORDOVA STREET  
VANCOUVER BC  
CANADA V6A 1K3

**DLR Group**  
520 NICOLLET MALL, SUITE 200  
MINNEAPOLIS MN  
USA 55402

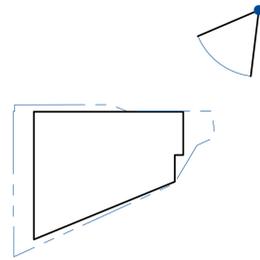
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2014 10 15		ISSUED FOR SCHEMATIC DESIGN

**MINNEAPOLIS T3**  
MINNEAPOLIS, MN  
2014 015

**A010**  
RENDERINGS



POTENTIAL SIGNAGE



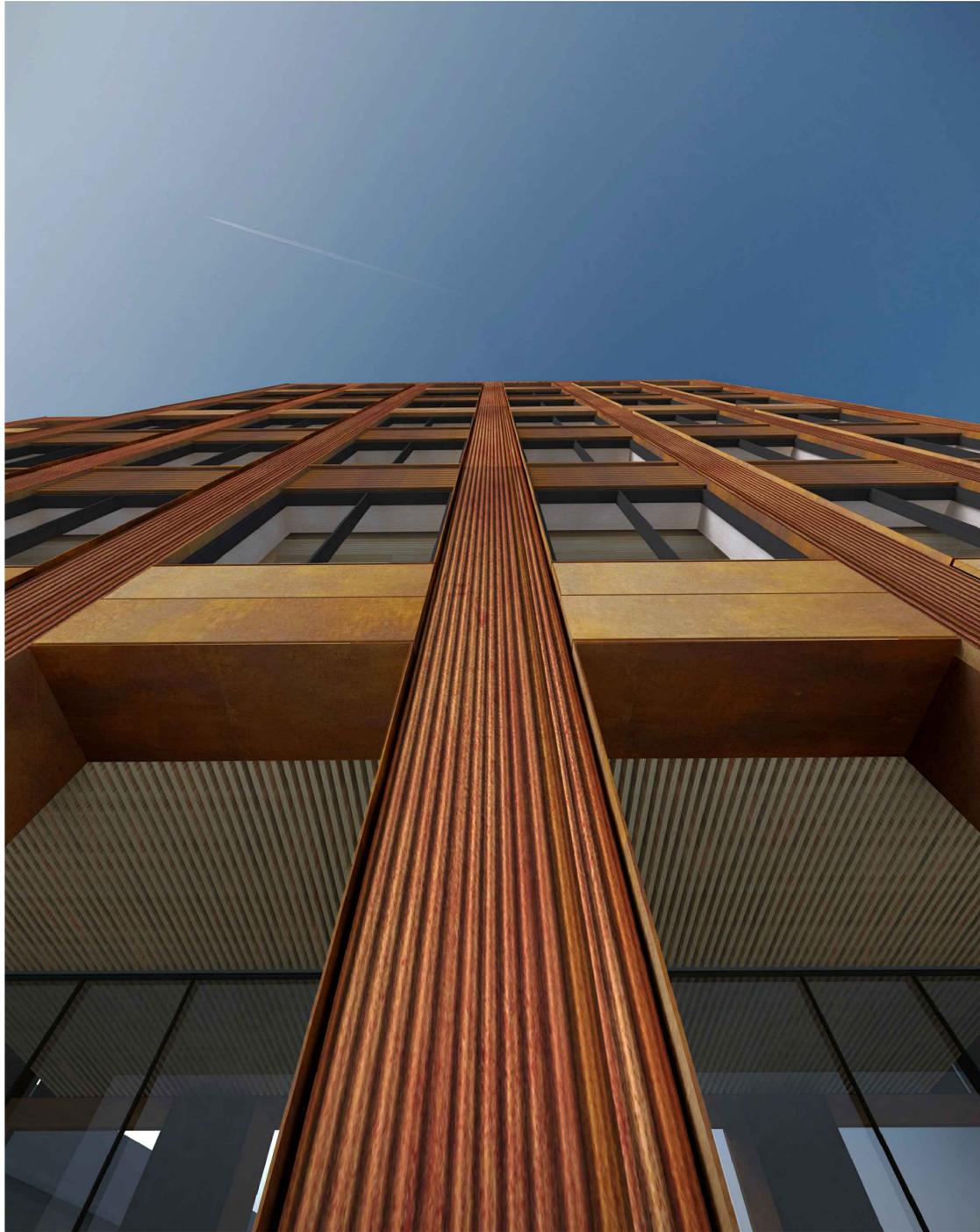
**MICHAEL GREEN ARCHITECTURE**  
63 E CORDOVA STREET  
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CANADA V6A 1K3

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USA 55402

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**MINNEAPOLIS T3**  
MINNEAPOLIS, MN  
2014 015

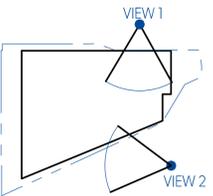
**A011**  
RENDERINGS



1 **EXTERIOR FACADE LOOKING UP**  
A012



2 **SIGNAGE AT SOUTH ENTRY**  
A012



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**MINNEAPOLIS T3**  
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2014 015



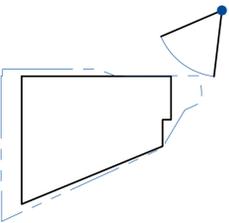
1 NORTH EAST CORNER - DAYTIME

A013



2 NORTH EAST CORNER AT NIGHT

A013



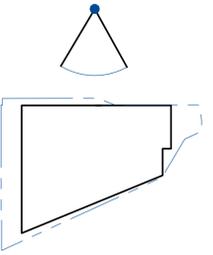
**MICHAEL GREEN ARCHITECTURE**  
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**A013**  
RENDERINGS



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MINNEAPOLIS, MN  
2014 015

**A014**  
RENDERINGS

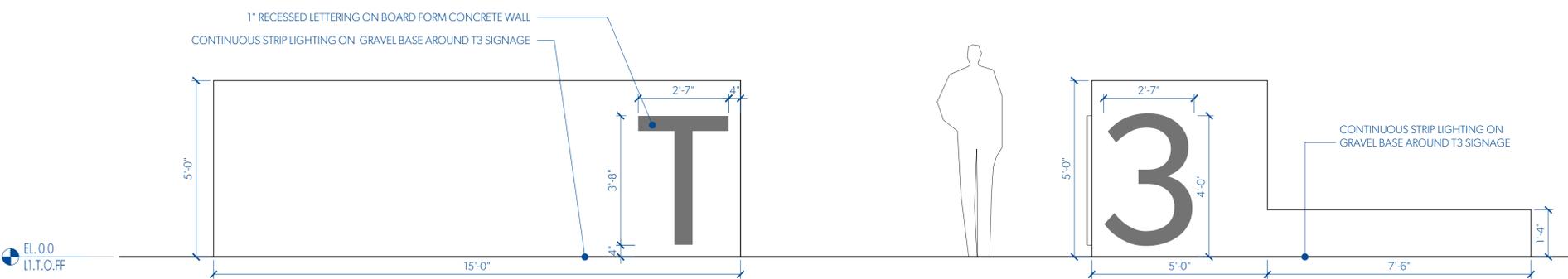
**MICHAEL GREEN ARCHITECTURE**  
63 E CORDOVA STREET  
VANCOUVER BC  
CANADA V6A 1K3

**DLR Group**  
520 NICOLLET MALL, SUITE 200  
MINNEAPOLIS MN  
USA 55402

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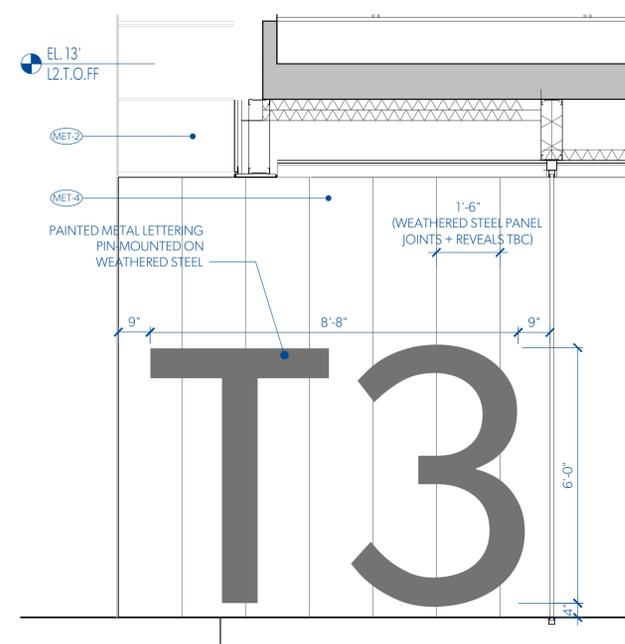
**MINNEAPOLIS T3**  
MINNEAPOLIS, MN  
2014 015

**A801**  
EXTERIOR DETAILS

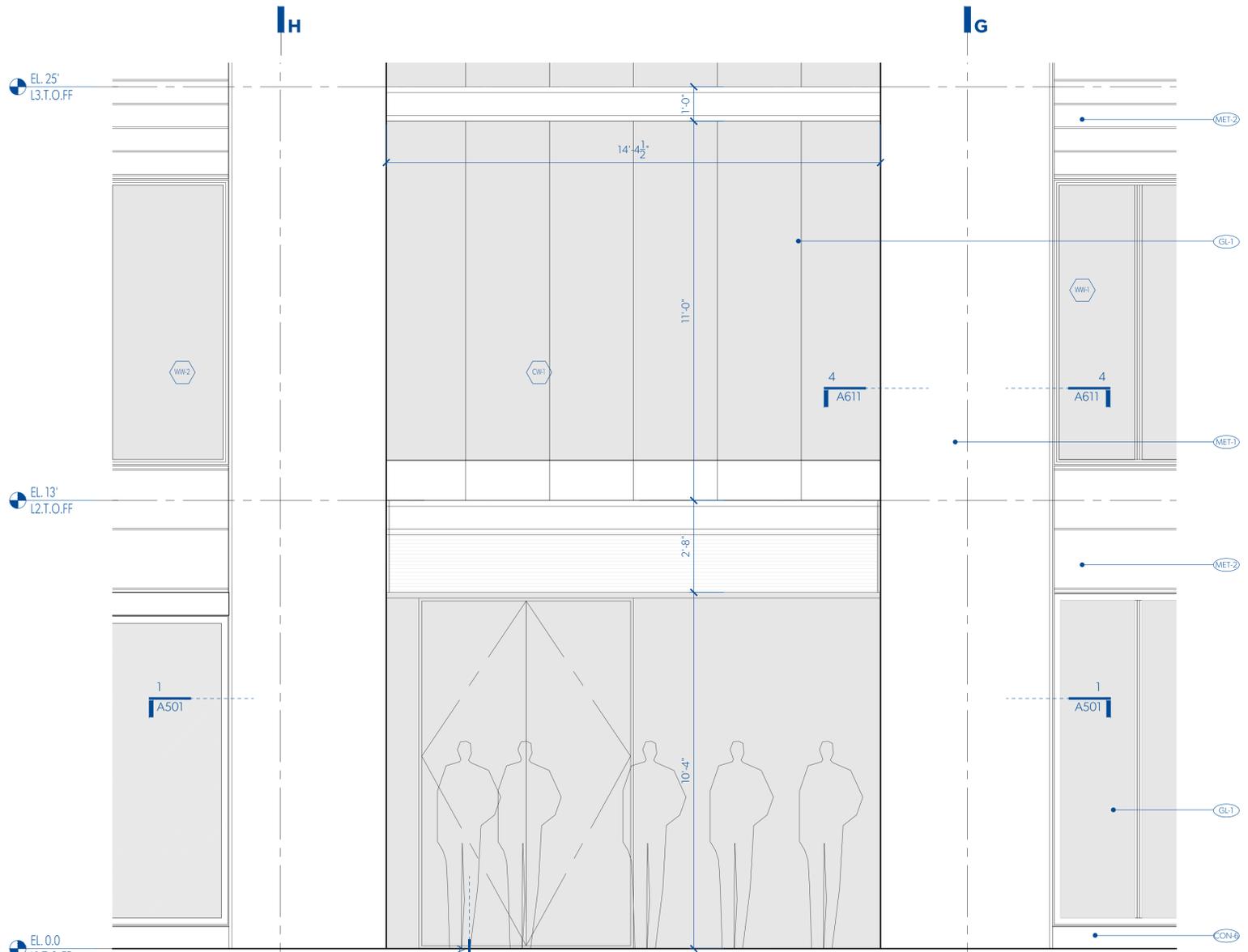


**2 NORTH ENTRANCE SIGNAGE - EAST ELEVATION**  
1/2"=1'-0"

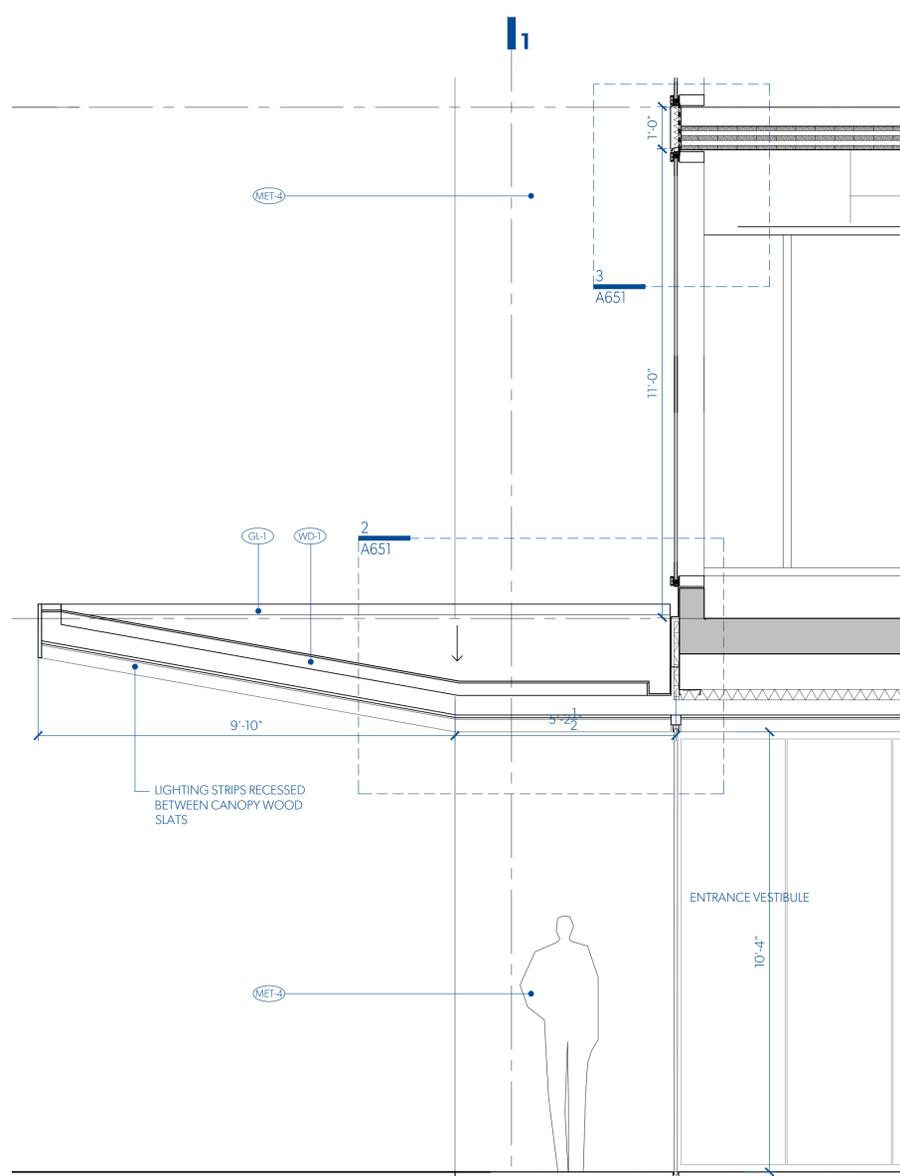
**3 NORTH ENTRANCE SIGNAGE - NORTH ELEVATION**  
1/2"=1'-0"



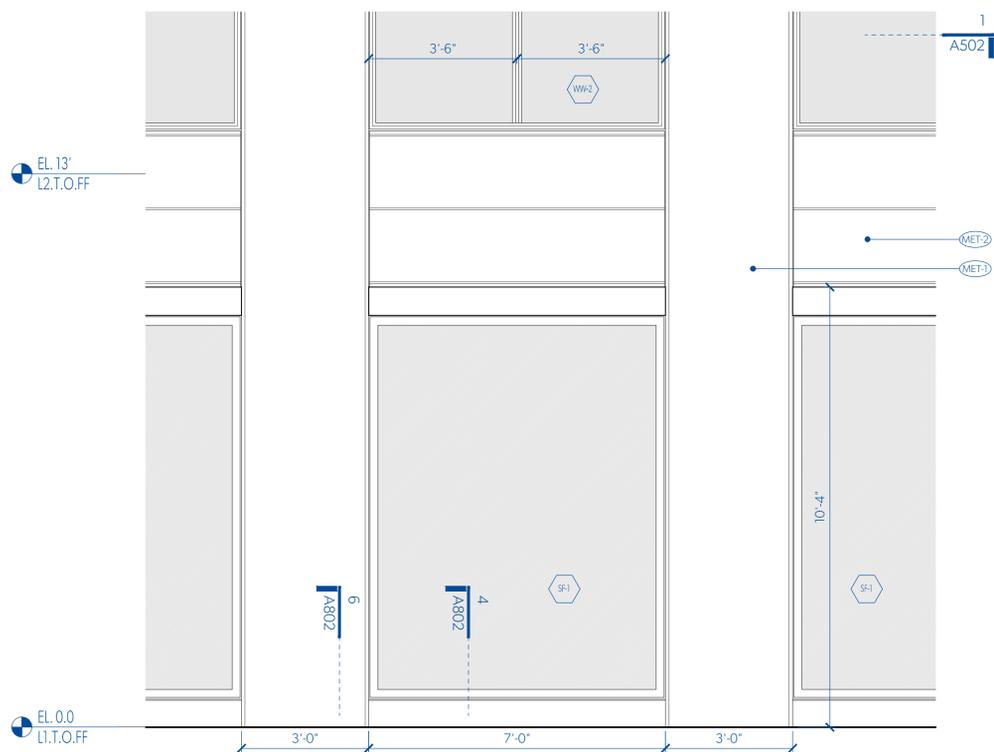
**4 SOUTH ENTRANCE SIGNAGE - PARTIAL SECTION**  
A801 1/2"=1'-0"



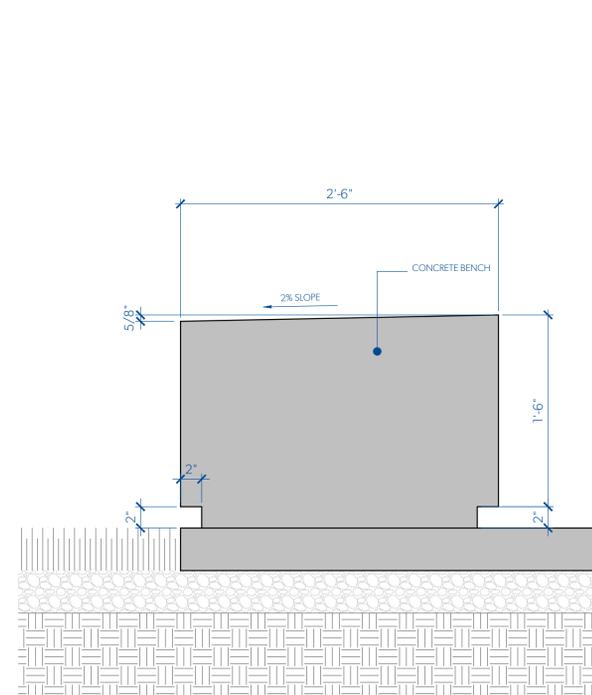
**1 NORTH ENTRANCE - PARTIAL ELEVATION**  
1/2"=1'-0"



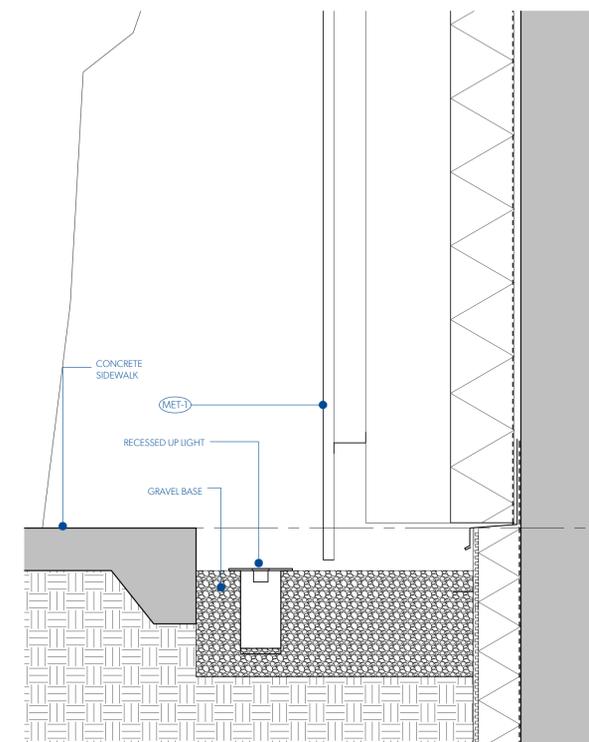
**5 NORTH ENTRANCE - PARTIAL SECTION**  
1/2"=1'-0"



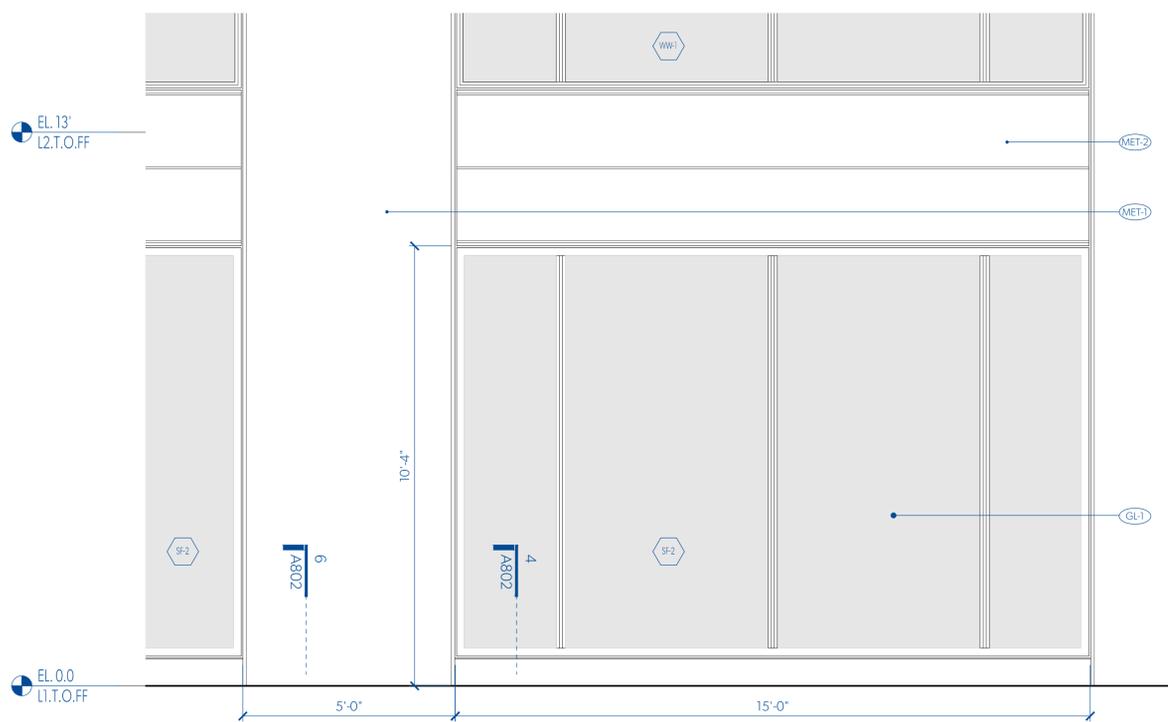
**1 ENLARGED PARTIAL ELEVATION - VERTICAL WINDOWS**  
1/2"=1'-0"



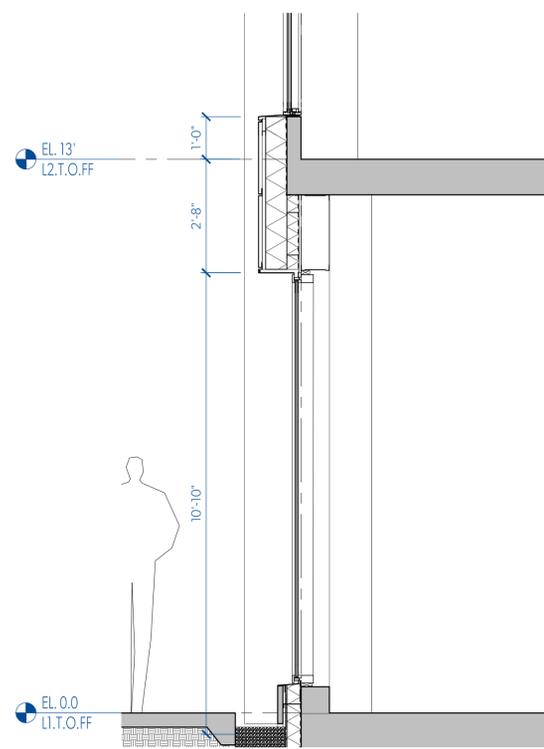
**5 CONCRETE BENCH DETAIL**  
1-1/2"=1'-0"



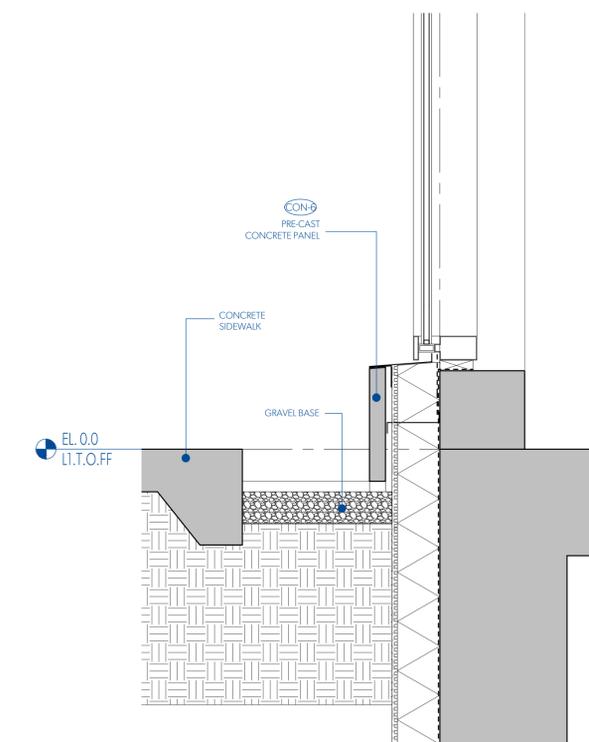
**6 GRAVEL AT PIER BASE - DETAIL**  
1-1/2"=1'-0"



**2 ENLARGED PARTIAL ELEVATION - HORIZONTAL WINDOWS**  
1/2"=1'-0"



**3 HORIZONTAL WINDOWS - SECTION**  
1/2"=1'-0"



**4 GRAVEL AT BASE - DETAIL**  
1-1/2"=1'-0"

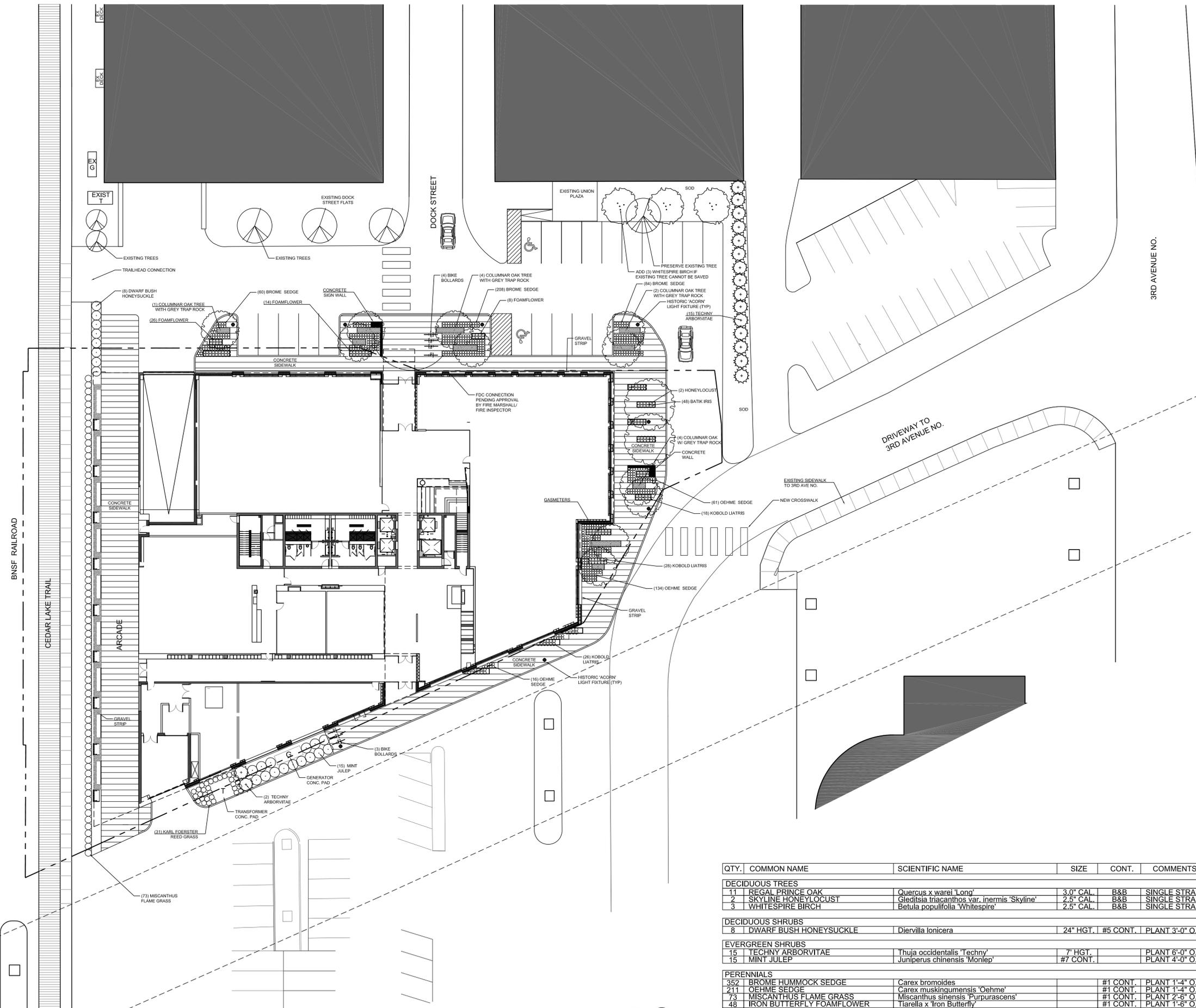
**MICHAEL GREEN ARCHITECTURE**  
63 E CORDOVA STREET  
VANCOUVER BC  
CANADA V6A 1K3

**DLR Group**  
520 NICOLLET MALL, SUITE 200  
MINNEAPOLIS MN  
USA 55402

DATE	REVISION	DESCRIPTION
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2014 10 27		ISSUED FOR HPC/PLANNING
2014 10 15		ISSUED FOR SCHEMATIC DESIGN

**MINNEAPOLIS 3**  
MINNEAPOLIS, MN  
2014 015

**A802**  
EXTERIOR DETAILS



**1** LANDSCAPE PLAN  
1" = 20'-0"



QTY.	COMMON NAME	SCIENTIFIC NAME	SIZE	CONT.	COMMENTS
<b>DECIDUOUS TREES</b>					
11	REGAL PRINCE OAK	<i>Quercus x warei</i> 'Long'	3.0" CAL.	B&B	SINGLE STRAIGHT LEADER
2	SKYLINE HONEYLOCUST	<i>Gleditsia triacanthos</i> var. <i>inermis</i> 'Skyline'	2.5" CAL.	B&B	SINGLE STRAIGHT LEADER
3	WHITESPIRE BIRCH	<i>Betula populifolia</i> 'Whitespire'	2.5" CAL.	B&B	SINGLE STRAIGHT LEADER
<b>DECIDUOUS SHRUBS</b>					
8	DWARF BUSH HONEYSUCKLE	<i>Diervilla lonicera</i>	24" HGT.	#5 CONT.	PLANT 3'-0" O.C.
<b>EVERGREEN SHRUBS</b>					
15	TECHNY ARBORVITAE	<i>Thuja occidentalis</i> 'Techny'	7' HGT.		PLANT 6'-0" O.C.
15	MINT JULEP	<i>Juniperus chinensis</i> 'Monlep'	#7 CONT.		PLANT 4'-0" O.C.
<b>PERENNIALS</b>					
352	BROME HUMMOCK SEDGE	<i>Carex bromoides</i>		#1 CONT.	PLANT 1'-4" O.C.
211	OEHME SEDGE	<i>Carex muskingumensis</i> 'Oehme'		#1 CONT.	PLANT 1'-4" O.C.
73	MISCANTHUS FLAME GRASS	<i>Miscanthus sinensis</i> 'Purpurascens'		#1 CONT.	PLANT 2'-6" O.C.
48	IRON BUTTERFLY FOAMFLOWER	<i>Tiarella x 'Iron Butterfly'</i>		#1 CONT.	PLANT 1'-6" O.C.
48	BATIK IRIS	<i>Iris germanica</i> 'Batik'		#1 CONT.	PLANT 1'-6" O.C.
72	KOBOLD LIATRIS	<i>Liatris spicata</i> 'Kobold'		#1 CONT.	PLANT 1'-6" O.C.
31	KARL FOERSTER REED GRASS	<i>Calamagrostis x acutiflora</i> 'Karl Foerster'		#1 CONT.	PLANT 2'-0" O.C.

**MICHAEL GREEN ARCHITECTURE**

63 E CORDOVA STREET  
VANCOUVER BC  
CANADA V6A 1K3

**DLR Group**

520 NICOLLET MALL, SUITE 200  
MINNEAPOLIS MN  
USA 55402

**DAMON FARBER ASSOCIATES**

401 SECOND AVENUE NORTH, SUITE 410  
MINNEAPOLIS MN  
USA 55401

DATE	DESCRIPTION
2015 01 12	REISSUED FOR HPC CoA
2014 12 24	ISSUED FOR DESIGN DEVELOPMENT
2014 12 15	ISSUED FOR HPC CoA
2014 12 05	ISSUED FOR PRICING ADDENDUM
2014 11 17	ISSUED FOR PDR REVIEW
2014 11 03	ISSUED FOR PRICING
2014 10 27	ISSUED FOR HPC/PLANNING
2014 10 15	ISSUED FOR SCHEMATIC DESIGN

**MINNEAPOLIS T3**

MINNEAPOLIS, MN  
2014 015

**L100**  
LANDSCAPE PLAN



CALL BEFORE YOU DIG!

Gopher State One Call

TWIN CITY AREA: 651-454-0002  
TOLL FREE: 1-800-252-1166

WARNING:

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CALLING FOR LOCATIONS OF ALL EXISTING UTILITIES. THEY SHALL COOPERATE WITH ALL UTILITY COMPANIES IN MAINTAINING THEIR SERVICE AND / OR RELOCATION OF LINES.

THE CONTRACTOR SHALL CONTACT GOPHER STATE ONE CALL AT 651-454-0002 AT LEAST 48 HOURS IN ADVANCE FOR THE LOCATIONS OF ALL UNDERGROUND WIRES, CABLES, CONDUITS, PIPES, MANHOLES, VALVES OR OTHER BURIED STRUCTURES BEFORE DIGGING. THE CONTRACTOR SHALL REPAIR OR REPLACE THE ABOVE WHEN DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.

SURVEY LEGEND

AS ASH	☐ CATCH BASIN	—> STORM SEWER
AP APPLE	⊙ STORM MANHOLE	—> SANITARY SEWER
BA BASSWOOD	○ SANITARY MANHOLE	— — WATERMAIN
BI BIRCH	⊙ WATER MANHOLE	—S— SANITARY SEWER SERVICE
BO BOXELDER	◇ HYDRANT	—W— WATER SERVICE
BU BUCKEYE	⊗ GATE VALVE	—C— CULVERT
BS BUSH	⊗ POWER POLE	—CTV— UNDERGROUND CABLE TV
CA CATULPA	⊗ LIGHT POLE	—ELE— UNDERGROUND ELECTRIC
CE CEDAR	⊗ YARD LIGHT	—FO— UNDERGROUND FIBER OPTIC
CO COTTONWOOD	← GUY WIRE	—GAS— UNDERGROUND GAS
DE DEAD TREE	⊗ SIGN	—TEL— UNDERGROUND TELEPHONE
EL ELM	⊗ SPOT ELEVATION	—UTL— UNDERGROUND UTILITY
FR MISC FRUIT	⊗ A/C UNIT	—OH— OVERHEAD UTILITY
FI FIR	⊗ CABLE TV PEDESTAL	—TRFC— UNDERGROUND TRAFFIC
HA HACKBERRY	⊗ ELECTRIC TRANSFORMER	—DT— DRAIN TILE
HI HICKORY	⊗ TELEPHONE PEDESTAL	—FM— FORCE MAIN
IR IRONWOOD	⊗ UTILITY PEDESTAL	—X— CHAIN LINK FENCE
LO LOCUST	⊗ ELECTRIC MANHOLE	—O— POST FENCE
MA MAPLE	⊗ GAS VALVE	—O— WOOD FENCE
OA OAK	⊗ TELEPHONE MANHOLE	—T— TRAFFIC SIGNAL
PA PALM	⊗ UTILITY MANHOLE	—C— CONCRETE CURB
PI PINE	⊗ ELECTRIC METER	—CON— CONCRETE
PO POPLAR	⊗ GAS METER	—872— CONTOUR
SP SPRUCE	⊗ HAND HOLE	—RR— RAILROAD TRACKS
TR TREE (GEN)	⊗ MONITORING WELL	☀ CONIFEROUS TREE
WA WALNUT	⊗ POST INDICATOR VALVE	☀ DECIDUOUS TREE
WI WILLOW	⊗ SOIL BORING	

THE EXISTING CONDITIONS PLAN IS BASED ON THE PARTIAL BOUNDARY AND TOPOGRAPHIC SURVEY PROVIDED BY LOUCKS ASSOCIATES DATED 04-24-2014.

Project Name:

T3  
MINNEAPOLIS

Minneapolis, Minnesota

Owner/Developer:

HINES

90 South Seventh Street  
Suite 150  
Minneapolis, MN 55402

Professional Services:

**LOUCKS ASSOCIATES**

Planning • Civil Engineering • Land Surveying  
Landscape Architecture • Environmental

7200 Hennepin Lane - Suite 300  
Maple Grove, MN 55369  
Telephone: (763) 424-5505  
www.LoucksAssociates.com

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14019E C1-1.DWG/Layout1

Submital:

11/06/14	SCHEMATIC UTILITY
11/12/14	PDR SUBMITTAL
12/05/14	PRICING ADDENDUM
12/15/14	ISSUED FOR HPC CoA
12/19/14	CHECK SET - ISSUED FOR DD
12/24/14	ISSUED FOR DESIGN DEVELOPMENT
01/07/15	RESPONSE TO PDR COMMENTS
01/12/15	REISSUED FOR HPC CoA

Professional Signature:

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Nicholas M. Mannel - PE

45861 License No. Date

Quality Control:

Project Lead:	NMM	Drawn By:	TRG
Checked By:	CHL	Review Date:	01/12/15

Sheet Index:

C1-1	EXISTING CONDITIONS PLAN
C1-2	DEMOLITION PLAN
C2-1	SITE PLAN
C3-1	GRADING & DRAINAGE PLAN
C3-2	STORM WATER POLLUTION PREVENTION PLAN - SWPPP
C4-1	UTILITY PLAN NORTH
C4-2	UTILITY PLAN SOUTH
C8-1	DETAIL SHEET
C8-2	DETAIL SHEET

Sheet Title:

EXISTING CONDITIONS PLAN

Project No.:

14019.OF

Sheet No.:

C1-1

Plotted: 01/09/2015 3:22 PM W:\2014\14019\CADD\DATA\CVILL.dwg Sheet Files



CALL BEFORE YOU DIG!

Gopher State One Call

TWIN CITY AREA: 651-454-0002  
TOLL FREE: 1-800-252-1166

WARNING:

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GENERAL NOTES

- 1. MINNESOTA STATE STATUTE REQUIRES NOTIFICATION PER "GOPHER STATE ONE CALL" PRIOR TO COMMENCING ANY GRADING, EXCAVATION OR UNDERGROUND WORK.
2. THE CONTRACTOR SHALL FIELD VERIFY LOCATIONS AND ELEVATIONS OF EXISTING UTILITIES AND TOPOGRAPHIC FEATURES PRIOR TO COMMENCEMENT OF CONSTRUCTION ACTIVITY.
3. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASE OF THIS PROJECT.
4. THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING AND MAINTAINING TRAFFIC CONTROL DEVICES SUCH AS BARRICADES, WARNING SIGNS, DIRECTIONAL SIGNS, FLAGMEN AND LIGHTS TO CONTROL THE MOVEMENT OF TRAFFIC WHERE NECESSARY.
5. IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS ON THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING THE PERFORMANCE OF THE WORK.
6. THE DUTY OF THE ENGINEER OR THE DEVELOPER TO CONDUCT CONSTRUCTION REVIEW OF THE CONTRACTOR'S PERFORMANCE IS NOT INTENDED TO INCLUDE REVIEW OF THE ADEQUACY OF THE CONTRACTOR'S SAFETY MEASURES IN, OR NEAR THE CONSTRUCTION SITE.
7. BEFORE BEGINNING CONSTRUCTION THE CONTRACTOR SHALL INSTALL EROSION AND SEDIMENTATION CONTROL MEASURES IN ACCORDANCE WITH NPDES PERMIT REQUIREMENTS, BEST MANAGEMENT PRACTICES, STATE AND LOCAL REQUIREMENTS AND THE DETAILS SHOWN ON THE DETAIL SHEET(S) OF THE PROJECT PLANS.
8. ALL CONSTRUCTION PERMITS, APPLICATIONS AND FEES ARE THE RESPONSIBILITY OF THE CONTRACTOR.
9. ALL ENTRANCES AND CONNECTIONS TO CITY STREETS SHALL BE CONSTRUCTED PER THE REQUIREMENTS OF THE STATE AND LOCAL JURISDICTIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL PERMITS AND NOTIFICATIONS AS REQUIRED.
10. ALL STREET REPAIRS AND PATCHING SHALL BE PERFORMED PER THE REQUIREMENTS OF THE CITY. ALL TRAFFIC CONTROL SHALL BE PROVIDED BY THE CONTRACTOR AND SHALL BE ESTABLISHED PER THE REQUIREMENTS OF THE MINNESOTA MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND THE CITY. THIS SHALL INCLUDE ALL SIGNAGE, BARRICADES, FLASHERS AND FLAGGERS AS NEEDED. ALL PUBLIC STREETS SHALL BE OPEN TO TRAFFIC AT ALL TIMES.
11. ADJUST ALL EXISTING STRUCTURES, BOTH PUBLIC AND PRIVATE TO THE PROPOSED GRADES WHERE DISTURBED AND COMPLY WITH ALL REQUIREMENTS OF THE UTILITY OWNERS. STRUCTURES BEING RESET TO PAVED AREAS MUST MEET OWNERS REQUIREMENTS FOR TRAFFIC LOADING.
12. EXISTING TOPOGRAPHY PROVIDED BY LOUCKS ASSOCIATES, PARTIAL BOUNDARY AND TOPOGRAPHIC SURVEY DATED 04/24/14.

SITE DEMOLITION NOTES

- 1. CONTRACTOR SHALL REMOVE AND/OR RELOCATE EXISTING PRIVATE UTILITIES AS NECESSARY. CONTRACTOR TO COORDINATE ACTIVITIES WITH UTILITY COMPANIES.
2. CONTRACTOR SHALL PROTECT SURFACE AND SUBSURFACE FEATURES NOT NOTED FOR REMOVAL.
3. CONTRACTOR TO CLEAR AND GRUB EXISTING VEGETATION WITHIN CONSTRUCTION LIMITS, STRIP TOP SOIL, AND STOCKPILE ON-SITE. REFER TO GRADING PLAN AND SWPPP FOR SEDIMENT AND EROSION CONTROL REQUIREMENTS.
4. CLEAR AND GRUB AND REMOVE ALL TREES, VEGETATION AND SITE DEBRIS PRIOR TO GRADING. ALL REMOVED MATERIAL SHALL BE HAULED FROM THE SITE DAILY. ALL CLEARING AND GRUBBING AND REMOVALS SHALL BE PERFORMED PER THE CONTRACT SPECIFICATIONS. EROSION CONTROL MEASURES SHALL BE IMMEDIATELY ESTABLISHED UPON REMOVAL. SEE THE STORMWATER POLLUTION PREVENTION PLAN (SWPPP).
5. CONTRACTOR SHALL REMOVE ALL SITE SURFACE FEATURES WITHIN REMOVAL LIMITS UNLESS OTHERWISE NOTED.

SITE DEMOLITION LEGEND

- REMOVE CONCRETE PAVEMENT
REMOVE BITUMINOUS PAVEMENT
SAWCUT PAVEMENT
REMOVE TREE

NOTE:

ALL ITEMS IN BOLD ARE TO BE REMOVED.



BENCHMARKS

TOP NUT HYDRANT ALONG NORTHWEST SIDE OF 3RD AVE. N., ELEVATION = 841.21 (NGVD1929)
MINNEAPOLIS SURVEY MONUMENT: CENTER OF 2ND AVE. N. AND 4TH ST. N. ELEVATION = 848.98 (NGVD1929)

Project Name:

T3 MINNEAPOLIS

Minneapolis, Minnesota

Owner/Developer:

HINES
90 South Seventh Street
Suite 150
Minneapolis, MN 55402

Professional Services:



Planning • Civil Engineering • Land Surveying
Landscape Architecture • Environmental
7200 Hennick Lane - Suite 300
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14019E C1-2.DWG/Layout1

Submittal:

Table with 2 columns: Date and Description. Includes entries for Schematic Utility, PDR Submittal, Pricing Addendum, etc.

Professional Signature:

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Nicholas M. Manned - PE

License No. Date

Quality Control:

NMM TRG

Project Lead: CHL Drawn By: 01/12/15

Checked By: Review Date:

Sheet Index:

Table listing sheet indices and titles: C1-1 EXISTING CONDITIONS PLAN, C1-2 DEMOLITION PLAN, C2-1 SITE PLAN, C3-1 GRADING & DRAINAGE PLAN, C3-2 STORM WATER POLLUTION PREVENTION PLAN - SWPPP, C4-1 UTILITY PLAN NORTH, C4-2 UTILITY PLAN SOUTH, C8-1 DETAIL SHEET, C8-2 DETAIL SHEET

Sheet Title:

SITE DEMOLITION PLAN

Project No.:

14019.OF

Sheet No.:

C1-2

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CALL BEFORE YOU DIG!

Gopher State One Call

TWIN CITY AREA: 651-454-0002  
TOLL FREE: 1-800-252-1166

WARNING:

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CALLING FOR LOCATIONS OF ALL EXISTING UTILITIES. THEY SHALL COOPERATE WITH ALL UTILITY COMPANIES IN MAINTAINING THEIR SERVICE AND / OR RELOCATION OF LINES.

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SITE NOTES

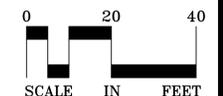
- ALL PAVING, CONCRETE CURB, GUTTER AND SIDEWALK SHALL BE FURNISHED AND INSTALLED IN ACCORDANCE WITH THE DETAILS SHOWN PER SHEET C8-1 AND THE REQUIREMENTS OF THE CITY. SEE LANDSCAPE PLANS FOR ANY ADDITIONAL HARDSCAPE APPLICATIONS
- THE CITY DEPARTMENT OF ENGINEERING, BUILDING INSPECTIONS DEPARTMENT AND THE CONSTRUCTION ENGINEER SHALL BE NOTIFIED AT LEAST 48 HOURS PRIOR TO WORK WITHIN THE STREET RIGHT OF WAY (SIDEWALK, STREET OR DRIVEWAYS)
- MINNESOTA STATE STATUTE REQUIRES NOTIFICATION PER "GOPHER STATE ONE CALL" PRIOR TO COMMENCING ANY GRADING, EXCAVATION OR UNDERGROUND WORK.
- THE CONTRACTOR SHALL PRESERVE AND MAINTAIN ANY EXISTING STREET LIGHTS AND TRAFFIC SIGNS PER THE REQUIREMENTS OF THE CITY
- CLEAR AND GRUB AND REMOVE ALL TREES, VEGETATION AND SITE DEBRIS PRIOR TO GRADING. ALL REMOVED MATERIAL SHALL BE HAULED FROM THE SITE DAILY. ALL CLEARING AND GRUBBING AND REMOVALS SHALL BE PERFORMED PER THE CONTRACT SPECIFICATIONS. EROSION CONTROL MEASURES SHALL BE IMMEDIATELY ESTABLISHED UPON REMOVAL. (SEE SHEET C3-2)
- THE CONTRACTOR SHALL BE REQUIRED TO OBTAIN ALL PERMITS FROM THE CITY AS REQUIRED FOR ALL WORK WITH THE STREET AND PUBLIC RIGHT OF WAY
- STALL STRIPING TO BE 4" WHITE PAINT. REFER TO DETAILS FOR HANDICAPPED STALL STRIPING. ALL STRIPING TO BE EPOXY.
- ALL CURB TERMINI TO HAVE A 3 FOOT TAPER UNLESS OTHERWISE NOTED.
- ALL CURB DIMENSIONS SHOWN ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
- BITUMINOUS IMPREGNATED FIBER BOARD TO BE PLACED AT FULL DEPTH OF CONCRETE ADJACENT TO EXISTING STRUCTURES AND BEHIND CURB ADJACENT TO DRIVEWAYS AND SIDEWALKS.
- IN THE EVENT OF A SIGNIFICANT SNOWFALL, SNOW SHALL BE REMOVED BY TRUCK OR SNOW MELT MACHINE.

SITE DATA

DISTURBED AREA:	1.59 AC
TOTAL SITE AREA:	1.04 AC
EXISTING IMPERVIOUS AREA:	1.04 AC (100%)
PROPOSED IMPERVIOUS AREA:	1.02 AC (98%)

PAVEMENT TYPES

BITUMINOUS PAVEMENT	
CONCRETE SIDEWALK	
CONCRETE PAVEMENT	



BENCHMARKS

TOP NUT HYDRANT ALONG NORTHWEST SIDE OF 3RD AVE. N., ELEVATION = 841.21 (NGVD1929)

MINNEAPOLIS SURVEY MONUMENT: CENTER OF 2ND AVE. N. AND 4TH ST. N. ELEVATION = 848.98 (NGVD1929)

Project Name:

T3  
MINNEAPOLIS

Minneapolis, Minnesota

Owner/Developer:

HINES

90 South Seventh Street  
Suite 150  
Minneapolis, MN 55402

Professional Services:



Planning • Civil Engineering • Land Surveying  
Landscape Architecture • Environmental

7200 Hemlock Lane - Suite 300  
Maple Grove, MN 55369  
Telephone: (763) 424-5505  
www.LoucksAssociates.com

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14019E C2-1.DWG/Layout1

Submital:

11/06/14	SCHEMATIC UTILITY
11/12/14	PDR SUBMITTAL
12/05/14	PRICING ADDENDUM
12/15/14	ISSUED FOR HPC CoA
12/19/14	CHECK SET - ISSUED FOR DD
12/24/14	ISSUED FOR DESIGN DEVELOPMENT
01/07/15	RESPONSE TO PDR COMMENTS
01/12/15	REISSUED FOR HPC CoA

Professional Signature:

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Nicholas M. Mannel - PE

45861 License No. Date

Quality Control:

NMM TRG

Project Lead: CHL Drawn By: 01/12/15

Checked By: Review Date:

Sheet Index:

C1-1	EXISTING CONDITIONS PLAN
C1-2	DEMOLITION PLAN
C2-1	SITE PLAN
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C3-2	STORM WATER POLLUTION PREVENTION PLAN - SWPPP
C4-1	UTILITY PLAN NORTH
C4-2	UTILITY PLAN SOUTH
C8-1	DETAIL SHEET
C8-2	DETAIL SHEET

Sheet Title:

SITE PLAN

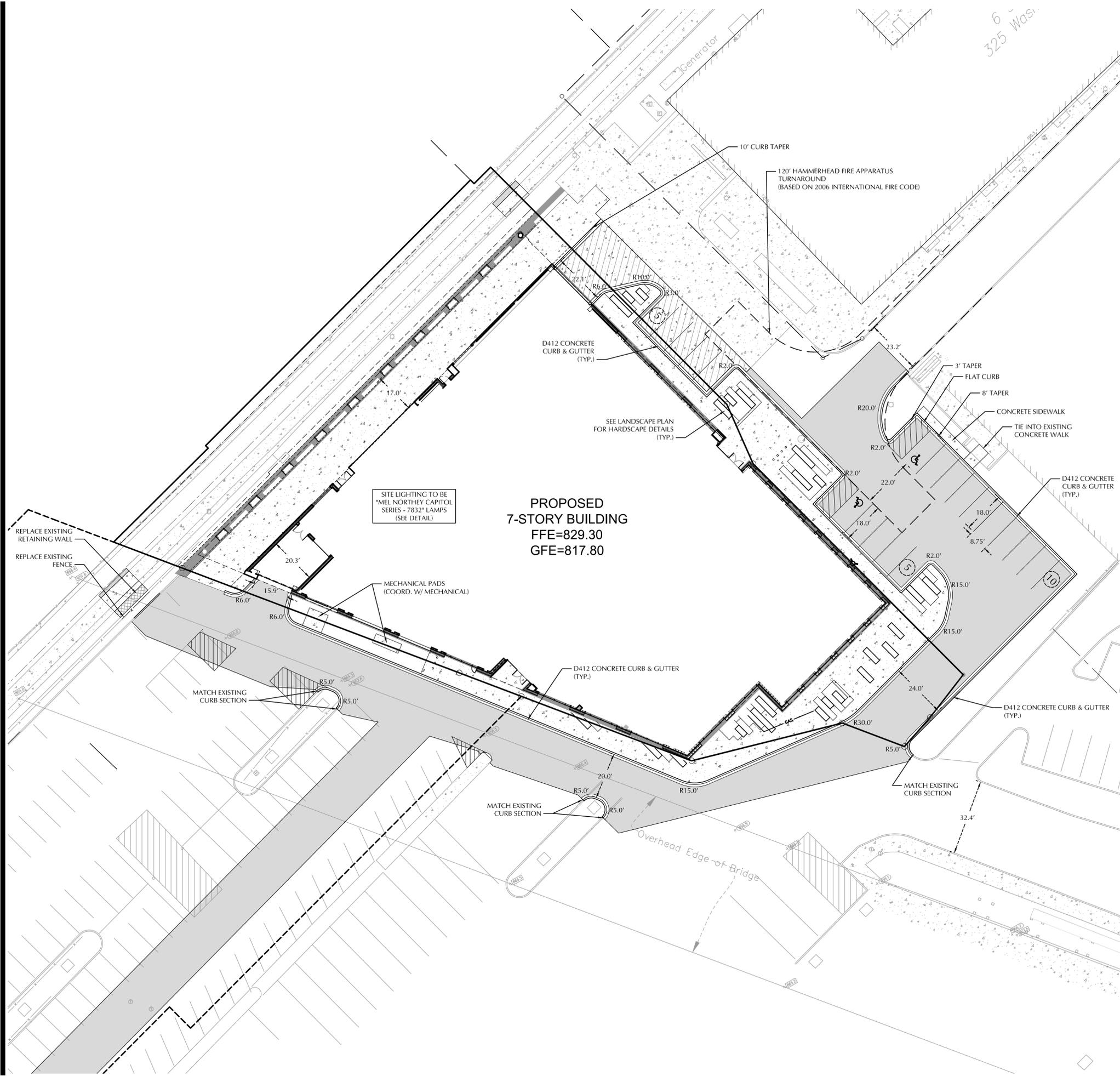
Project No.:

14019.OE

Sheet No.:

C2-1

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SITE LIGHTING TO BE "MEL NORTHEY CAPITOL SERIES - 7832" LAMPS (SEE DETAIL)

PROPOSED 7-STORY BUILDING  
FFE=829.30  
GFE=817.80

6 -  
325 Wasi.

Overhead Edge of Bridge



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14019E C3-1.DWG/Layout1

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C4-2	UTILITY PLAN SOUTH
C8-1	DETAIL SHEET
C8-2	DETAIL SHEET

Sheet Title:

GRADING AND DRAINAGE PLAN

Project No.:

14019.OE

Sheet No.:

C3-1

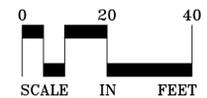
**GRADING, DRAINAGE & EROSION CONTROL NOTES**

- ALL DISTURBED UNPAVED AREAS ARE TO RECEIVE MINIMUM OF 4 INCHES OF TOP SOIL AND SOD OR SEED. THESE AREAS SHALL BE WATERED BY THE CONTRACTOR UNTIL THE SOD OR SEED IS GROWING IN A HEALTHY MANNER.
- THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO AVOID PROPERTY DAMAGE TO ADJACENT PROPERTIES DURING THE CONSTRUCTION PHASE OF THIS PROJECT. THE CONTRACTOR WILL BE HELD RESPONSIBLE FOR ANY DAMAGES TO ADJACENT PROPERTIES OCCURRING DURING THE CONSTRUCTION PHASE OF THIS PROJECT.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING AND MAINTAINING TRAFFIC CONTROL DEVICES SUCH AS BARRICADES, WARNING SIGNS, DIRECTIONAL SIGNS, FLAGMEN AND LIGHTS TO CONTROL THE MOVEMENT OF TRAFFIC WHERE NECESSARY. PLACEMENT OF THESE DEVICES SHALL BE APPROVED BY THE ENGINEER PRIOR TO PLACEMENT. TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE APPROPRIATE MINNESOTA DEPARTMENT OF TRANSPORTATION STANDARDS.
- IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS ON THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING THE PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS.
- THE DUTY OF THE ENGINEER OF THE DEVELOPER TO CONDUCT CONSTRUCTION REVIEW OF THE CONTRACTORS PERFORMANCE IS NOT INTENDED TO INCLUDE REVIEW OF THE ADEQUACY OF THE CONTRACTORS SAFETY MEASURES IN, OR NEAR THE CONSTRUCTION SITE.
- BEFORE BEGINNING CONSTRUCTION THE CONTRACTOR SHALL INSTALL A TEMPORARY ROCK ENTRANCE PAD AT ALL POINTS OF VEHICLE EXIT FROM THE PROJECT SITE. SAID ROCK ENTRANCE PAD SHALL BE MAINTAINED BY THE CONTRACTOR FOR THE DURATION OF THE PROJECT. SEE DETAILS SHOWN ON SHEET C3-2 FOR THE PROJECT PLANS
- EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE ESTABLISHED AROUND THE ENTIRE SITE PERIMETER AND IN ACCORDANCE WITH NPDES PERMIT REQUIREMENTS, BEST MANAGEMENT PRACTICES, CITY REQUIREMENTS AND THE DETAILS SHOWN ON SHEET C8-1 OF THE PROJECT PLANS.
- ALL ENTRANCES AND CONNECTIONS TO CITY STREET SHALL BE PERFORMED PER THE REQUIREMENTS OF THE CITY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL PERMITS AND NOTIFICATIONS AS REQUIRED BY THE CITY.
- ADJACENT STREETS AND ALLEYS MUST BE SWEEPED TO KEEP THEM FREE OF SEDIMENT. CONTRACTOR MUST MONITOR CONDITIONS AND SWEEP AS NEEDED OR WITHIN 24 HOURS OF NOTICE BY THE CITY.
- STREETS MUST BE CLEANED AND SWEEPED WHENEVER TRACKING OF SEDIMENTS OCCURS AND BEFORE SITES ARE LEFT IDLE FOR WEEKENDS AND HOLIDAYS. A REGULAR SWEEPING SCHEDULE MUST BE ESTABLISHED.
- ADJUST ALL EXISTING STRUCTURES, BOTH PUBLIC AND PRIVATE TO THE PROPOSED GRADES WHERE DISTURBED AND COMPLY WITH ALL REQUIREMENTS OF THE UTILITY OWNERS. STRUCTURES BEING RESET TO PAVED AREAS MUST MEET OWNERS REQUIREMENTS FOR TRAFFIC
- DUST MUST BE ADEQUATELY CONTROLLED.
- SEE UTILITY PLAN FOR STORM SEWER INFORMATION.
- EXISTING TOPOGRAPHY PROVIDED BY LOUCKS ASSOCIATES PARTIAL TOPOGRAPHIC SURVEY DATED 04/24/14.
- SPOT ELEVATIONS REPRESENT FINISHED SURFACE GRADES AT GUTTER LINE, FACE OF BUILDING, OR EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
- SEE SITE PLAN FOR CURB AND BITUMINOUS TAPER LOCATIONS.

**BENCHMARKS**

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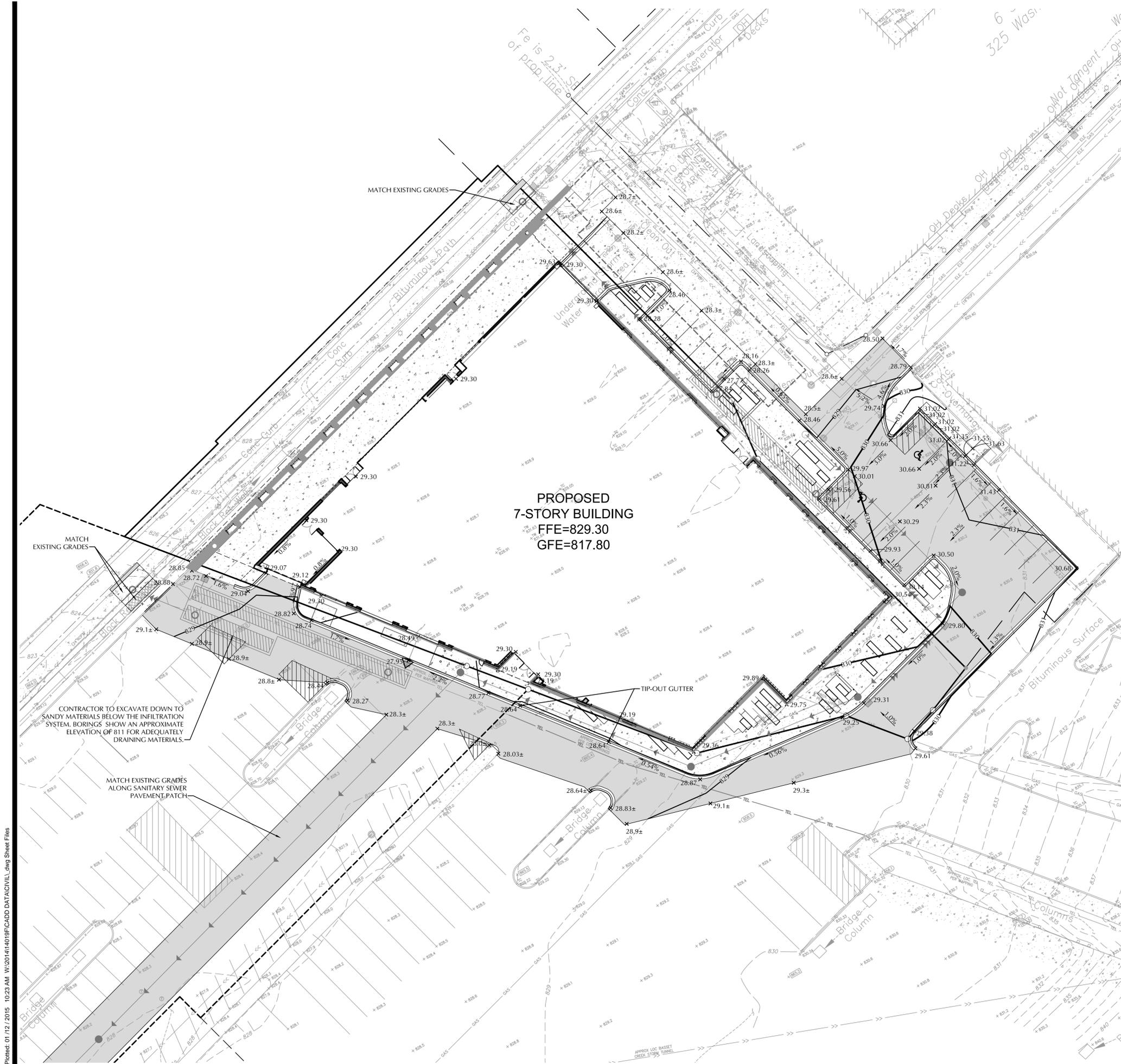


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PROPOSED  
7-STORY BUILDING  
FFE=829.30  
GFE=817.80



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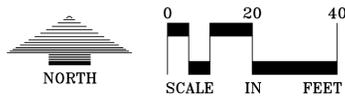


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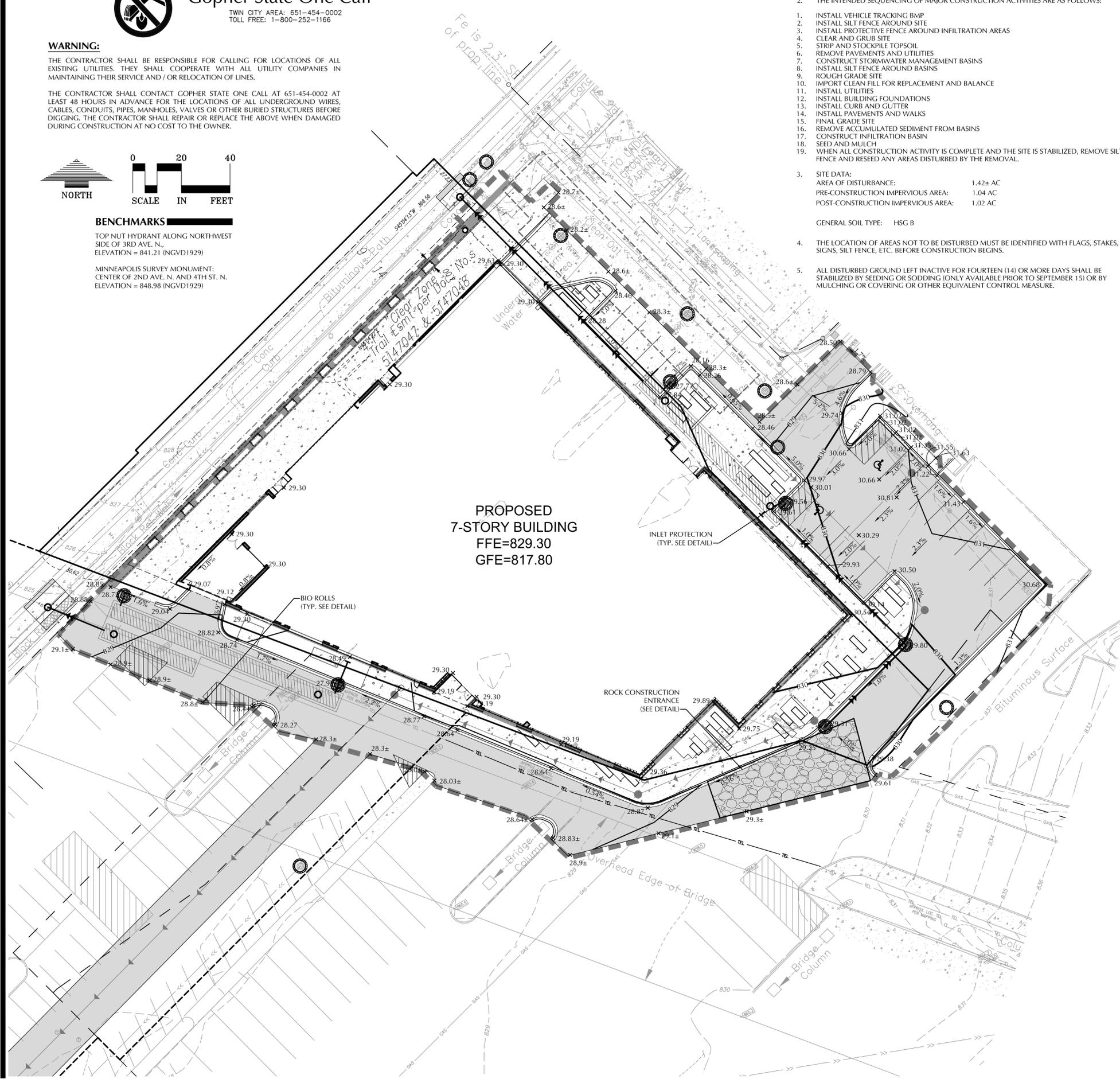
**SWPPP NOTES**

- THE NATURE OF THIS PROJECT WILL CONSIST OF CONSTRUCTING AN OFFICE BUILDING WITH ASSOCIATED SURFACE PAVEMENTS, UTILITIES AND STORMWATER MANAGEMENT.
- THE INTENDED SEQUENCING OF MAJOR CONSTRUCTION ACTIVITIES ARE AS FOLLOWS:
  - INSTALL VEHICLE TRACKING BMP
  - INSTALL SILT FENCE AROUND SITE
  - INSTALL PROTECTIVE FENCE AROUND INFILTRATION AREAS
  - CLEAR AND GRUB SITE
  - STRIP AND STOCKPILE TOPSOIL
  - REMOVE PAVEMENTS AND UTILITIES
  - CONSTRUCT STORMWATER MANAGEMENT BASINS
  - INSTALL SILT FENCE AROUND BASINS
  - ROUGH GRADE SITE
  - IMPORT CLEAN FILL FOR REPLACEMENT AND BALANCE
  - INSTALL UTILITIES
  - INSTALL BUILDING FOUNDATIONS
  - INSTALL CURB AND GUTTER
  - INSTALL PAVEMENTS AND WALKS
  - FINAL GRADE SITE
  - REMOVE ACCUMULATED SEDIMENT FROM BASINS
  - CONSTRUCT INFILTRATION BASIN
  - SEED AND MULCH
  - WHEN ALL CONSTRUCTION ACTIVITY IS COMPLETE AND THE SITE IS STABILIZED, REMOVE SILT FENCE AND RESEED ANY AREAS DISTURBED BY THE REMOVAL.
- SITE DATA:
 

AREA OF DISTURBANCE:	1.42± AC
PRE-CONSTRUCTION IMPERVIOUS AREA:	1.04 AC
POST-CONSTRUCTION IMPERVIOUS AREA:	1.02 AC

GENERAL SOIL TYPE: HSG B
- THE LOCATION OF AREAS NOT TO BE DISTURBED MUST BE IDENTIFIED WITH FLAGS, STAKES, SIGNS, SILT FENCE, ETC. BEFORE CONSTRUCTION BEGINS.
- ALL DISTURBED GROUND LEFT INACTIVE FOR FOURTEEN (14) OR MORE DAYS SHALL BE STABILIZED BY SEEDING OR SODDING (ONLY AVAILABLE PRIOR TO SEPTEMBER 15) OR BY MULCHING OR COVERING OR OTHER EQUIVALENT CONTROL MEASURE.
- ON SLOPES 3:1 OR GREATER MAINTAIN SHEET FLOW AND MINIMIZE RILLS AND/OR GULLIES. SLOPE LENGTHS CAN NOT BE GREATER THAN 75 FEET.  
DENOTES SLOPES GREATER THAN 3:1. ALL 3:1 SLOPES TO BE STABILIZED WITH EROSION CONTROL BLANKET
- ALL STORM DRAINS AND INLETS MUST BE PROTECTED UNTIL ALL SOURCES OF POTENTIAL DISCHARGE ARE STABILIZED.
- TEMPORARY SOIL STOCKPILES MUST HAVE EFFECTIVE SEDIMENT CONTROL AND CAN NOT BE PLACED IN SURFACE WATERS OR STORM WATER CONVEYANCE SYSTEMS. TEMPORARY STOCKPILES WITHOUT SIGNIFICANT AMOUNT OF SILT, CLAY, OR ORGANIC COMPOUNDS ARE EXEMPT EX: CLEAN AGGREGATE STOCK PILES, DEMOLITION CONCRETE STOCKPILES, SAND STOCKPILES.
- SEDIMENT LADEN WATER MUST BE DISCHARGED TO A SEDIMENTATION BASIN WHENEVER POSSIBLE. IF NOT POSSIBLE, IT MUST BE TREATED WITH THE APPROPRIATE BMP'S.
- SOLID WASTE MUST BE DISPOSED OF PROPERLY AND MUST COMPLY WITH MPCA DISPOSAL REQUIREMENTS.
- EXTERNAL WASHING OF CONSTRUCTION VEHICLES MUST BE LIMITED TO A DEFINED AREA OF THE SITE. RUNOFF MUST BE PROPERLY CONTAINED.
- NO ENGINE DEGREASING IS ALLOWED ON SITE.
- THE OWNER WHO SIGNS THE NPDES PERMIT APPLICATION IS A PERMITTEE AND IS RESPONSIBLE FOR COMPLIANCE WITH ALL TERMS AND CONDITIONS OF THE PERMIT. THE OPERATOR (CONTRACTOR) WHO SIGNS THE NPDES PERMIT APPLICATION IS A PERMITTEE FOR PARTS II.B., PART II.C., PART II.B-F, PART V, PART IV AND APPLICABLE CONSTRUCTION ACTIVITY REQUIREMENTS FOUND IN APPENDIX A, PART C. OF THE NPDES PERMIT AND IS JOINTLY RESPONSIBLE WITH THE OWNER FOR COMPLIANCE WITH THOSE PORTIONS OF THE PERMIT.
- TERMINATION OF COVERAGE-PERMITTEE(S) WISHING TO TERMINATE COVERAGE MUST SUBMIT A NOTICE OF TERMINATION (NOT) TO THE MPCA. ALL PERMITTEE(S) MUST SUBMIT A NOT WITHIN 30 DAYS AFTER ONE OR MORE OF THE FOLLOWING CONDITIONS HAVE BEEN MET:
  - FINAL STABILIZATION, PER NPDES PERMIT PART IV.G. HAS BEEN ACHIEVED ON ALL PORTIONS OF THE SITE FOR WHICH THE PERMITTEE IS RESPONSIBLE.
  - TRANSFER OF OWNERSHIP AS DESCRIBED IN THE PERMIT.
- INSPECTIONS
  - INITIAL INSPECTION FOLLOWING SILT FENCE INSTALLATION BY CITY REPRESENTATIVE IS REQUIRED.
  - EXPOSED SOIL AREAS: ONCE EVERY 7 DAYS AND WITHIN 24 HOURS FOLLOWING A 0.5" COVER 24 HOUR RAIN EVENT.
  - STABILIZED AREAS: ONCE EVERY 30 DAYS
  - FROZEN GROUND: AS SOON AS RUNOFF OCCURS OR PRIOR TO RESUMING CONSTRUCTION.
  - INSPECTION AND MAINTENANCE RECORDS MUST BE RETAINED FOR 3 YEARS AFTER FILING OF THE NOTICE OF TERMINATION AND MUST INCLUDE: DATE AND TIME OF ACTION, NAME OF PERSON(S) CONDUCTING WORK, FINDING OF INSPECTIONS AND RECOMMENDATIONS FOR CORRECTIVE ACTION, DATE AND AMOUNT OF RAINFALL EVENTS GREATER THAN 0.5 INCHES IN A 24 HOUR PERIOD.
- MINIMUM MAINTENANCE
  - SILT FENCE TO BE REPAIRED, REPLACED, SUPPLEMENTED WHEN NONFUNCTIONAL, OR 1/3 FULL; WITHIN 24 HOURS
  - SEDIMENT BASINS DRAINED AND SEDIMENT REMOVED WHEN REACHES 1/2 STORAGE VOLUME. REMOVAL MUST BE COMPLETE WITHIN 72 HOURS OF DISCOVERY.
  - SEDIMENT REMOVED FROM SURFACE WATERS WITHIN (7)SEVEN DAYS
  - CONSTRUCTION SITE EXITS INSPECTED, TRACKED SEDIMENT REMOVED WITH 24 HOURS.
  - PROVIDE COPIES OF EROSION INSPECTION RESULTS TO CITY ENGINEER FOR ALL EVENTS GREATER THAN 1/2" IN 24 HOURS
- THE SWPPP, INCLUDING ALL CHANGES TO IT, AND INSPECTIONS AND MAINTENANCE RECORDS MUST BE KEPT AT THE SITE DURING CONSTRUCTION ACTIVITY BY THE PERMITTEE(S) WHO HAVE OPERATIONAL CONTROL OF THE SITE.
- OWNER MUST KEEP RECORDS OF ALL PERMITS REQUIRED FOR THE PROJECT, THE SWPPP, ALL INSPECTIONS AND MAINTENANCE, PERMANENT OPERATION AND MAINTENANCE AGREEMENTS, AND REQUIRED CALCULATIONS FOR TEMPORARY AND PERMANENT STORM WATER MANAGEMENT SYSTEMS. THESE RECORDS MUST BE RETAINED FOR THREE YEARS AFTER FILING NPDES NOTICE OF TERMINATION.
- SWPPP MUST BE AMENDED WHEN:
  - THERE IS A CHANGE IN DESIGN, OPERATION, MAINTENANCE, WEATHER OR SEASONAL CONDITIONS THAT HAS A SIGNIFICANT EFFECT ON DISCHARGE
  - INSPECTIONS INDICATE THAT THE SWPPP IS NOT EFFECTIVE AND DISCHARGE IS EXCEEDING WATER QUALITY STANDARDS.
  - THE BMP'S IN THE SWPPP ARE NOT CONTROLLING POLLUTANTS IN DISCHARGES OR IS NOT CONSISTENT WITH THE TERMS AND CONDITIONS OF THE PERMIT.
- CONCRETE WASHOUT AREA
  - CONTRACTOR TO PROVIDE PREFABRICATED CONCRETE WASH-OUT CONTAINER WITH RAIN PROTECTION PER PLAN.
  - CONCRETE WASH-OUT TO BE IDENTIFIED WITH SIGNAGE STATING "CONCRETE WASHOUT AREA DO NOT OVERFILL"
  - CONCRETE WASHOUT WATER NEEDS TO BE PUMPED WITHIN 24 HOURS OF STANDING WATER IN WASHOUT AREA.
- IN THE EVENT OF ENCOUNTERING A WELL OR SPRING DURING CONSTRUCTION CONTRACTOR TO CEASE CONSTRUCTION ACTIVITY AND NOTIFY ENGINEER.
- PIPE OULETTES MUST BE PROVIDED WITH TEMPORARY OR PERMANENT ENERGY DISSIPATION WITHIN 24 HOURS AFTER CONNECTION TO A SURFACE WATER.
- FINAL STABILIZATION  
FINAL STABILIZATION REQUIRES THAT ALL SOIL DISTURBING ACTIVITIES HAVE BEEN COMPLETED AND THAT DISTURBED AREAS ARE STABILIZED BY A UNIFORM PERENNIAL VEGETATIVE COVER WITH 70% OF THE EXPECTED FINAL DENSITY, AND THAT ALL PERMANENT PAVEMENTS HAVE BEEN INSTALLED. ALL TEMPORARY BMP'S SHALL BE REMOVED, DITCHES STABILIZED, AND SEDIMENT SHALL BE REMOVED FROM PERMANENT CONVEYANCES AND SEDIMENTATION BASINS IN ORDER TO RETURN THE POND TO DESIGN CAPACITY.
- RESPONSIBILITIES
  - THE OWNER MUST IDENTIFY A PERSON WHO WILL OVERSEE THE SWPPP IMPLEMENTATION AND THE PERSON RESPONSIBLE FOR INSPECTION AND MAINTENANCE:  
 CONTACT: \_\_\_\_\_  
 COMPANY: \_\_\_\_\_  
 PHONE: \_\_\_\_\_
  - THE OWNER MUST IDENTIFY THE A PERSON WHO WILL BE RESPONSIBLE FOR LONG TERM OPERATIONS AND MAINTENANCE OF THE PERMANENT STORMWATER MANAGEMENT SYSTEM:  
 CONTACT: \_\_\_\_\_  
 COMPANY: \_\_\_\_\_  
 PHONE: \_\_\_\_\_
- THE WATERSHED DISTRICT OR THE CITY MAY HAVE REQUIREMENTS FOR INSPECTIONS OR AS-BUILT DRAWINGS VERIFYING PROPER CONSTRUCTION OF THE BMP'S.

Plotfile: 01/12/2015 10:41 AM W:\2014\14019\CADD\DATA\CVILL.dwg Sheet Files



**PROPOSED  
 7-STORY BUILDING  
 FFE=829.30  
 GFE=817.80**

Project Name: **T3 MINNEAPOLIS**

Minneapolis, Minnesota

Owner/Developer: **HINES**  
 90 South Seventh Street  
 Suite 150  
 Minneapolis, MN 55402

Professional Services: **LOUCKS ASSOCIATES**  
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14019E C3-2.DWG/Layout1

Submital:  
 11/06/14 SCHEMATIC UTILITY  
 11/12/14 PDR SUBMITTAL  
 12/05/14 PRICING ADDENDUM  
 12/15/14 ISSUED FOR HPC CoA  
 12/19/14 CHECK SET - ISSUED FOR DD  
 12/24/14 ISSUED FOR DESIGN DEVELOPMENT  
 01/07/15 RESPONSE TO PDR COMMENTS  
 01/12/15 REISSUED FOR HPC CoA

Professional Signature:  
 I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.  
 Nicholas M. Manned - PE  
 45861 License No. Date

Quality Control:  
 NMM Drawn By: TRG  
 Project Lead: CHL Drawn By: 01/12/15  
 Checked By: Review Date:

Sheet Index:  
 C1-1 EXISTING CONDITIONS PLAN  
 C1-2 DEMOLITION PLAN  
 C2-1 SITE PLAN  
 C3-1 GRADING & DRAINAGE PLAN  
 C3-2 STORM WATER POLLUTION PREVENTION PLAN - SWPPP  
 C4-1 UTILITY PLAN NORTH  
 C4-2 UTILITY PLAN SOUTH  
 C8-1 DETAIL SHEET  
 C8-2 DETAIL SHEET

Sheet Title: **STORM WATER POLLUTION PREVENTION PLAN**

Project No.: **14019.0E**

Sheet No.: **C3-2**



CALL BEFORE YOU DIG!

Gopher State One Call

TWIN CITY AREA: 651-454-0002  
TOLL FREE: 1-800-252-1166

Project Name:

T3  
MINNEAPOLIS

Minneapolis, Minnesota

Owner/Developer:

HINES  
90 South Seventh Street  
Suite 150  
Minneapolis, MN 55402

Professional Services:

**LOUCKS ASSOCIATES**

Planning • Civil Engineering • Land Surveying  
Landscape Architecture • Environmental  
7200 Hennepin Lane - Suite 300  
Maple Grove, MN 55369  
Telephone: (763) 424-5505  
www.LoucksAssociates.com

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14019E C4-1.DWG/C4-1

Submit:

11/06/14	SCHEMATIC UTILITY
11/12/14	PDR SUBMITTAL
12/05/14	ISSUED FOR ADDENDUM
12/15/14	ISSUED FOR HPC CoA
12/19/14	CHECK SET - ISSUED FOR DD
12/24/14	ISSUED FOR DESIGN DEVELOPMENT
01/07/15	RESPONSE TO PDR COMMENTS
01/12/15	RESPONSE FOR HPC CoA

Professional Signature:

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Nicholas M. Mannel - PE  
45861 License No. Date

Quality Control:

NMM Drawn By: CHL

Project Lead: CHL Checked By: CHL Review Date: 01/12/15

Sheet Index:

C1-1	EXISTING CONDITIONS PLAN
C1-2	DEMOLITION PLAN
C2-1	SITE PLAN
C3-1	GRADING & DRAINAGE PLAN
C3-2	STORM WATER POLLUTION PREVENTION PLAN - SWPPP
C4-1	UTILITY PLAN NORTH
C4-2	UTILITY PLAN SOUTH
C8-1	DETAIL SHEET
C8-2	DETAIL SHEET

Sheet Title:

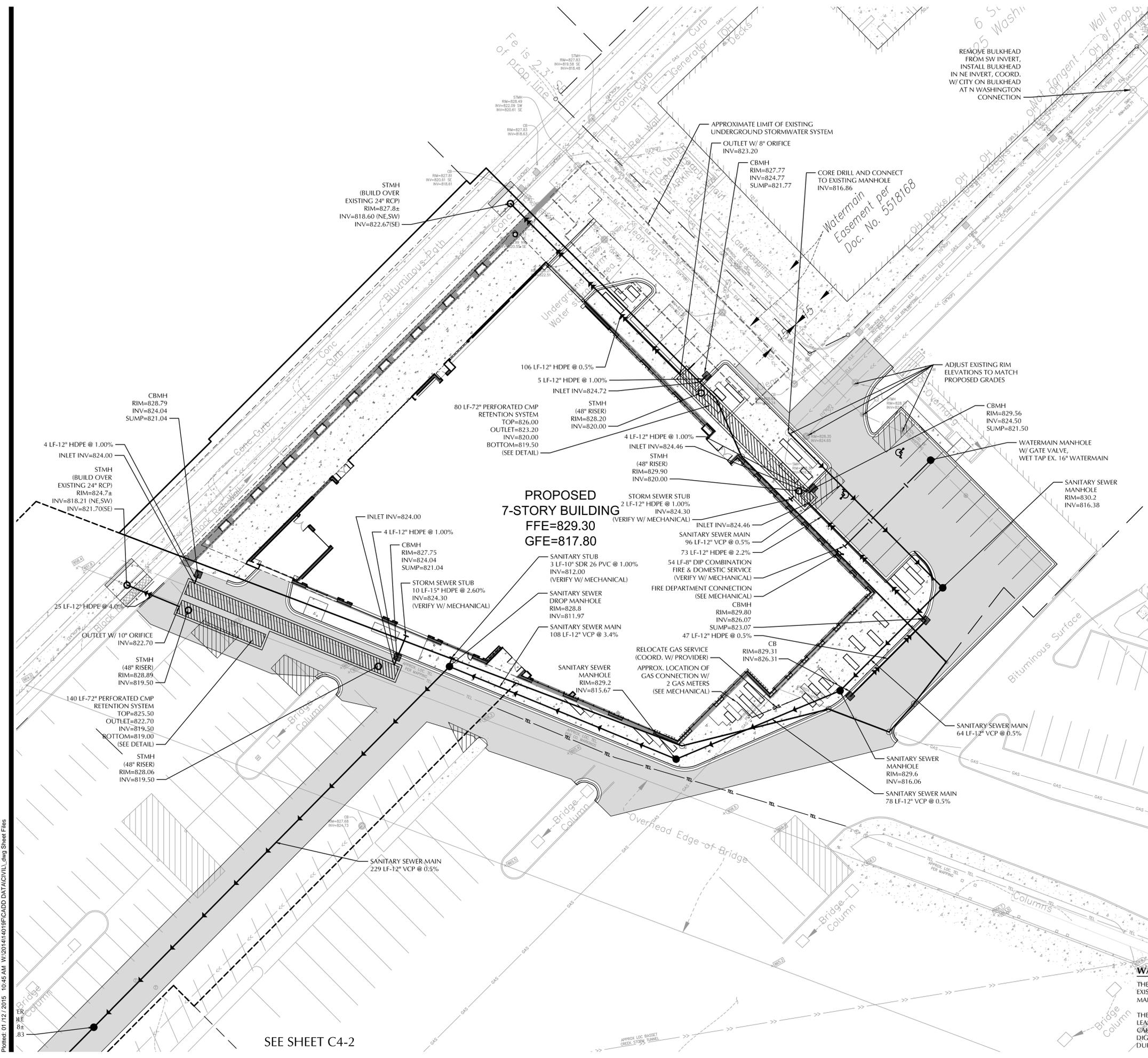
UTILITY PLAN

Project No.:

14019.OF

Sheet No.:

C4-1



SEE SHEET C4-2

Plotted: 01/12/2015 10:45 AM W:\2014\14019\CADD\DATA\CIVIL.dwg Sheet Files



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Gopher State One Call

TWIN CITY AREA: 651-454-0002  
TOLL FREE: 1-800-252-1166

Project Name:

T3  
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Minneapolis, Minnesota

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14019E C4-1.DWG/C4-2

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Nicholas M. Mammel - PE  
45861 License No. Date

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CHL Checked By: 01/12/15 Review Date:

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C4-1	UTILITY PLAN NORTH
C4-2	UTILITY PLAN SOUTH
CB-1	DETAIL SHEET
CB-2	DETAIL SHEET

Sheet Title:

SCHEMATIC  
UTILITY PLAN

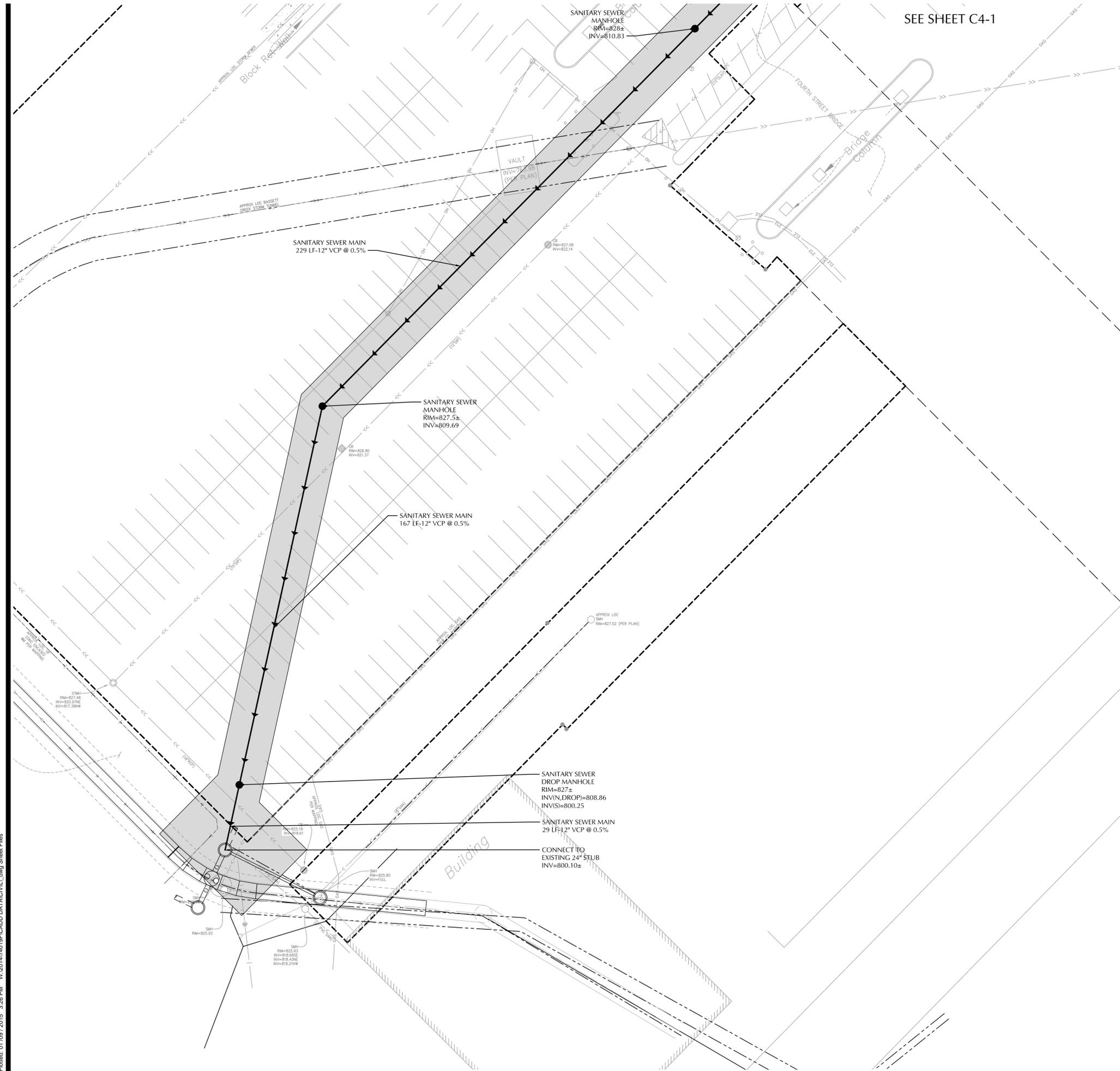
Project No.:

14019.OF

Sheet No.:

C4-2

SEE SHEET C4-1



UTILITY PLAN NOTES

- ALL SANITARY SEWER, STORM SEWER AND WATERMAIN UTILITIES SHALL BE FURNISHED AND INSTALLED PER THE REQUIREMENTS OF THE SPECIFICATIONS, THE CITY AND THE STANDARD UTILITIES SPECIFICATION OF THE CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM), 1999 EDITION.
- SEE DETAIL SHEET AND THE CONTRACT SPECIFICATIONS FOR SPECIFIC UTILITY DETAILS.
- ALL CONNECTIONS TO EXISTING UTILITIES SHALL BE PERFORMED PER THE REQUIREMENTS OF THE CITY, THE CITY DEPARTMENT OF ENGINEERING AND BUILDING INSPECTIONS DEPARTMENT AND THE CONSTRUCTION ENGINEER MUST BE NOTIFIED AT LEAST 48 HOURS PRIOR TO ANY WORK WITHIN THE PUBLIC RIGHT OF WAY, OR WORK IMPACTING PUBLIC UTILITIES.
- THE CONTRACTOR SHALL NOTIFY GOPHER STATE ONE CALL AT 651-454-0002 AT LEAST 48 HOURS PRIOR TO PERFORMING ANY EXCAVATION OR UNDERGROUND WORK.
- THE CONTRACTOR SHALL FIELD ADJUST WATERMAIN TO AVOID CONFLICTS WITH SANITARY SEWER, STORM SEWER, AND SERVICES AS REQUIRED.
- ALL STREET REPAIRS AND PATCHING SHALL BE PERFORMED PER THE REQUIREMENTS OF THE CITY. ALL TRAFFIC CONTROL SHALL BE PROVIDED BY THE CONTRACTOR AND SHALL BE ESTABLISHED PER THE REQUIREMENTS OF THE MINNESOTA MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MMUTCD) AND THE CITY. THIS SHALL INCLUDE ALL SIGNAGE, BARRICADES, FLASHERS AND FLAGGERS AS NEEDED. ALL PUBLIC STREETS SHALL BE OPEN TO TRAFFIC AT ALL TIMES. NO ROAD CLOSURES SHALL BE PERMITTED WITHOUT THE EXPRESSED AUTHORITY OF THE CITY.
- ALL UTILITY PIPE BEDDING SHALL BE COMPACTED SAND OR FINE GRANULAR MATERIAL PER THE REQUIREMENTS OF THE CITY. ALL COMPACTION SHALL BE PERFORMED PER THE REQUIREMENTS OF THE CEAM SPECIFICATION.
- ADJUST ALL EXISTING STRUCTURES, BOTH PUBLIC AND PRIVATE TO THE PROPOSED GRADES WHERE DISTURBED AND COMPLY WITH ALL REQUIREMENTS OF THE UTILITY OWNERS, STRUCTURES BEING RESET TO PAVED AREAS MUST MEET OWNERS REQUIREMENTS FOR TRAFFIC LOADING.
- MAINTAIN 8 FEET OF COVER OVER ALL WATERMAINS AND SERVICES.
- GROUNDWATER WAS ENCOUNTERED BETWEEN 30' - 36' IN DEPTH.
- THE CONTRACTOR, PROPERTY OWNER OR RESPONSIBLE PARTY SHALL CONTACT MINNEAPOLIS SURFACE WATERS AND SEWERS 48 HOURS PRIOR TO ANY EXCAVATION OR CONSTRUCTION RELATED TO OR IN THE LOCATION OF THE PROPOSED STORMWATER MANAGEMENT BMP (CONTACT PAUL CHELSEN, 673-2406 OR PAUL.CHELSEN@MINNEAPOLISMN.GOV)
- UPON THE PROJECT'S COMPLETION THE GENERAL CONTRACTOR, PROPERTY OWNER OR RESPONSIBLE PARTY SHALL PROVIDE TO THE DEPARTMENT OF PUBLIC WORKS A FINAL STORMWATER MANAGEMENT REPORT INCLUDING RECORD DRAWINGS. THIS REPORT WILL SERVE AS A MEANS OF VERIFICATION THAT THE INTENT OF THE APPROVED STORMWATER MANAGEMENT DESIGN HAS BEEN MET. THIS FINAL REPORT SHALL SUBSTANTIATE THAT ALL ASPECTS OF THE ORIGINAL DESIGN HAVE BEEN ADEQUATELY PROVIDED FOR BY THE CONSTRUCTION OF THE PROJECT.
- PROPOSED PIPE MATERIALS:
 

STORM SEWER	ALUMINIZED CSP	72" DIAMETER
STORM SEWER	ALUMINIZED CSP OR DUAL WALL HDPE	12" TO 15" DIAMETER
STORM SEWER	RCP	12" DIAMETER
WATER SERVICE	DIP CLASS 53	8" DIAMETER
SANITARY SERVICE	VCP	8" DIAMETER



BENCHMARKS

TOP NUT HYDRANT ALONG NORTHWEST SIDE OF 3RD AVE. N., ELEVATION = 841.21 (NGVD1929)  
MINNEAPOLIS SURVEY MONUMENT: CENTER OF 2ND AVE. N. AND 4TH ST. N. ELEVATION = 848.98 (NGVD1929)

Plotted: 01/09/2015 3:28 PM W:\2014\14019\CADD\DATA\CIVIL.dwg Sheet Files

Table with 2 columns: Date and Description of CADD qualification (e.g., 1/18/14 SCHEMATIC UTILITY, 11/12/14 PDR SUBMITTAL).

Table with 2 columns: Date and Description of submittals (e.g., 1/18/14 SCHEMATIC UTILITY, 11/12/14 PDR SUBMITTAL).

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Nicholas M. Mannel - PE 45861 License No. Date

NMM TRG

Project Lead: CHL Drawn By: TRG

Checked By: CHL Review Date: 01/12/15

Table listing sheet titles and numbers: C1-1 EXISTING CONDITIONS PLAN, C1-2 DEMOLITION PLAN, C2-1 SITE PLAN, C3-1 GRADING & DRAINAGE PLAN, C3-2 STORM WATER POLLUTION PREVENTION PLAN - SWPPP, C4-1 UTILITY PLAN NORTH, C4-2 UTILITY PLAN SOUTH, C8-1 DETAIL SHEET, C8-2 DETAIL SHEET.

MINNEAPOLIS STANDARD TYPICAL MANHOLE CONSTRUCTION. Includes diagrams of manhole sections and detailed notes on construction requirements, materials, and standards.

MINNEAPOLIS STANDARD SEDIMENT CONTROL - ROCK ENTRANCE. Includes diagrams of rock entrance structures and detailed notes on construction and maintenance.

MINNEAPOLIS STANDARD INLET PROTECTION TYPE A & C. Includes diagrams of inlet protection types and detailed notes on materials and installation.

MINNEAPOLIS STANDARD EROSION AND SEDIMENT CONTROL NOTES. Includes detailed text notes regarding erosion control measures, sedimentation, and site stabilization.

MINNEAPOLIS STANDARD WATER TIGHT PIPE CONNECTION. Includes diagrams of pipe connection details and detailed notes on gaskets, seals, and materials.

MINNEAPOLIS STANDARD B-624 CURB AND GUTTER. Includes diagrams of curb and gutter profiles and detailed notes on dimensions and materials.

MINNEAPOLIS STANDARD D-12 CURB AND GUTTER MODIFIED. Includes diagrams of modified curb and gutter profiles and detailed notes on dimensions and materials.

MINNEAPOLIS STANDARD B612 CURB AND GUTTER. Includes diagrams of curb and gutter profiles and detailed notes on dimensions and materials.

MINNEAPOLIS STANDARD SAW CUT AT CURB AND GUTTER REMOVAL. Includes diagrams showing curb removal details and detailed notes on procedures.

MINNEAPOLIS STANDARD PVC PIPE STANDARDS. Includes a table of pipe specifications and detailed notes on standards and materials.

MINNEAPOLIS STANDARD CATCH BASIN INSTALLATION. Includes diagrams of catch basin installation and detailed notes on construction and materials.

MINNEAPOLIS STANDARD CATCH BASIN GRATE TYPE 'Y'. Includes diagrams of grate installation and detailed notes on materials and standards.

MINNEAPOLIS STANDARD SPECIFICATION NOTES. Includes detailed text notes regarding pipe materials, standards, and construction requirements.

**12" Heavy Duty Municipal Core cast base w/ 4" deep fluted non tapered shaft**

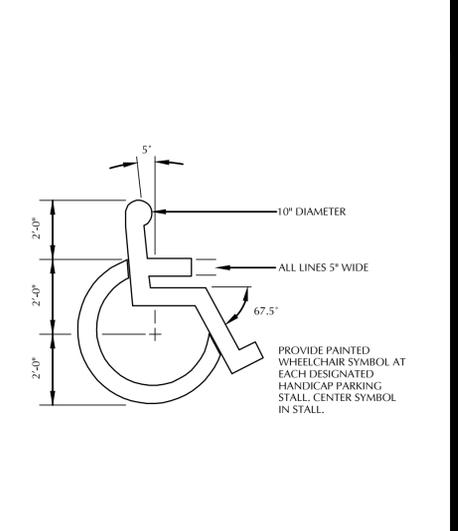
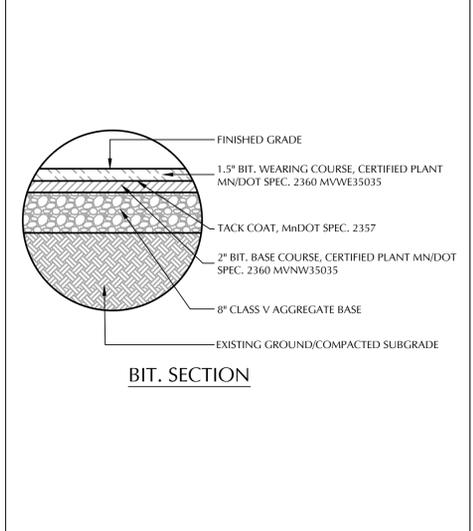
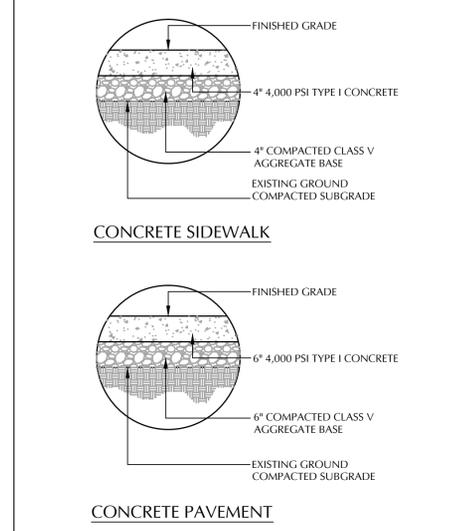
Brand: Castrol Series  
 Stock Number: 7832  
 mwp: 58699  
 size: 12"  
 Base Diameter: 17"  
 Height: 12"  
 Pole Finish: Powder Coat Finish  
 Colors: Black  
 Dimensions: 19"  
 Luminaire Dims: 23 x 12  
 Post Material: Fluted extruded aluminum 4  
 Globe Material: conical main lens - aluminum lantern  
 Lighting: Standard the Socket Only  
 Optional: HID, HPS or MH

**Installation Instructions**

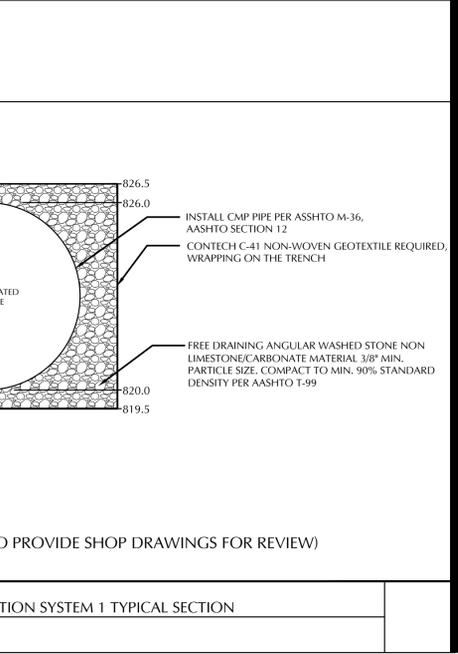
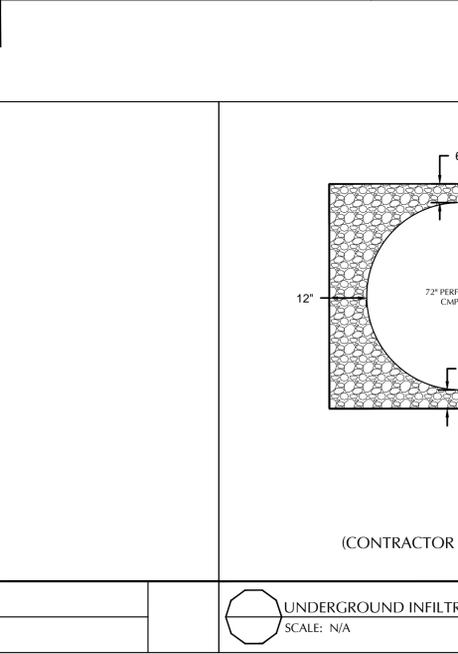
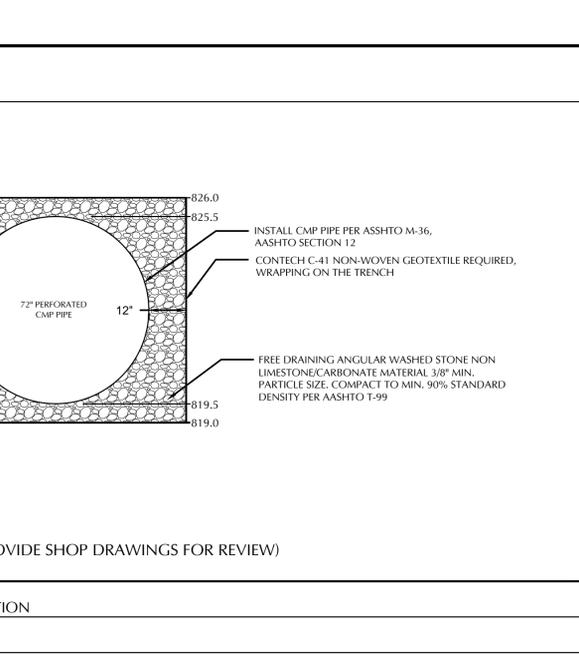
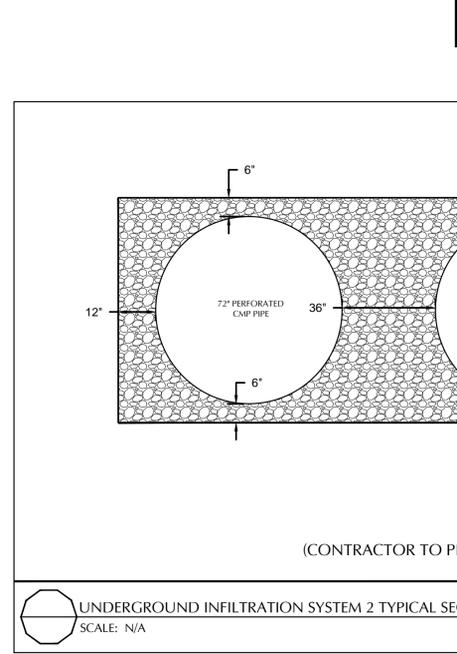
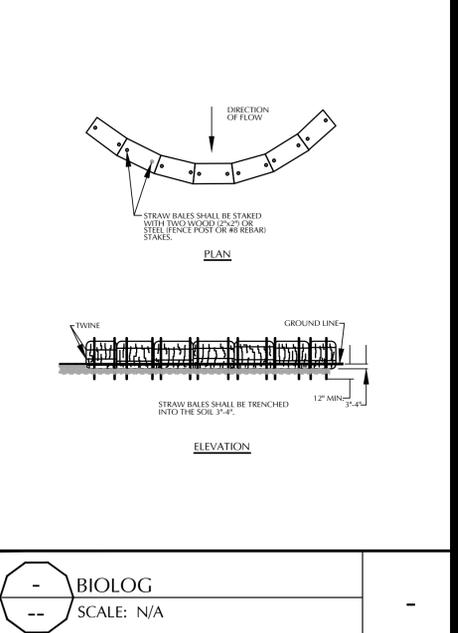
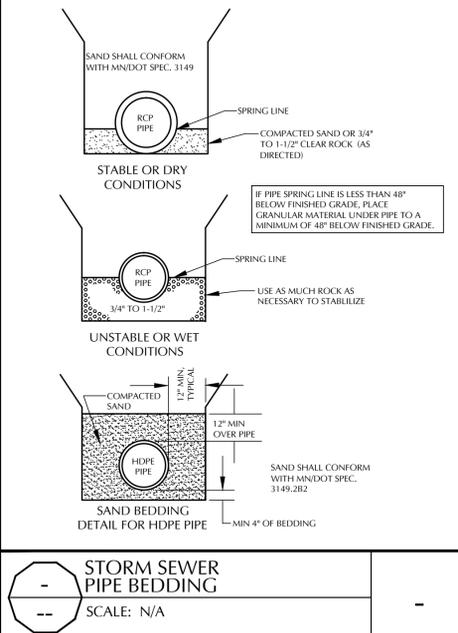
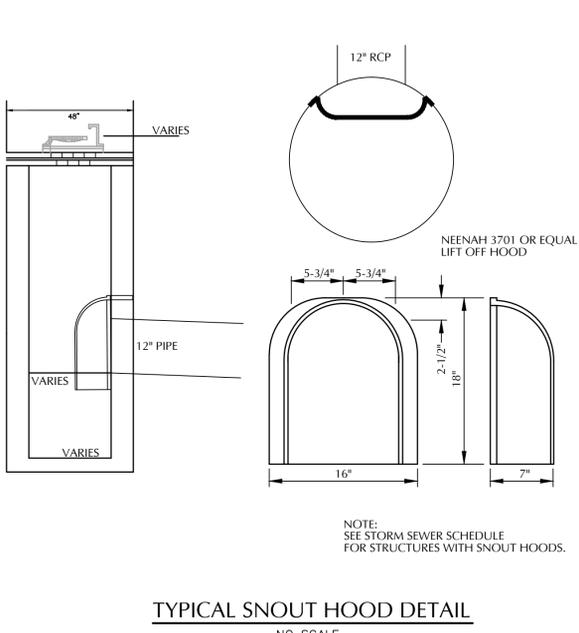
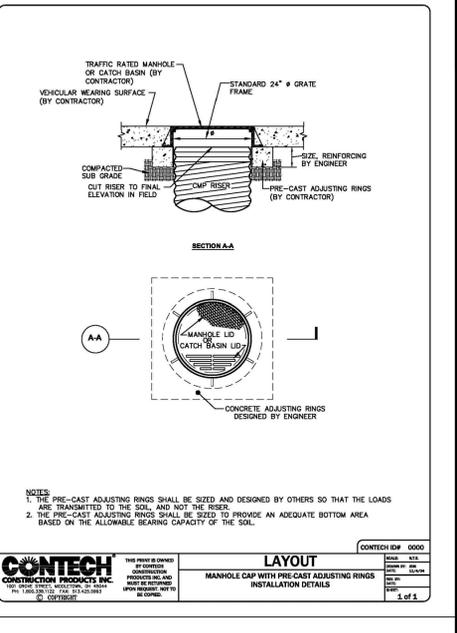
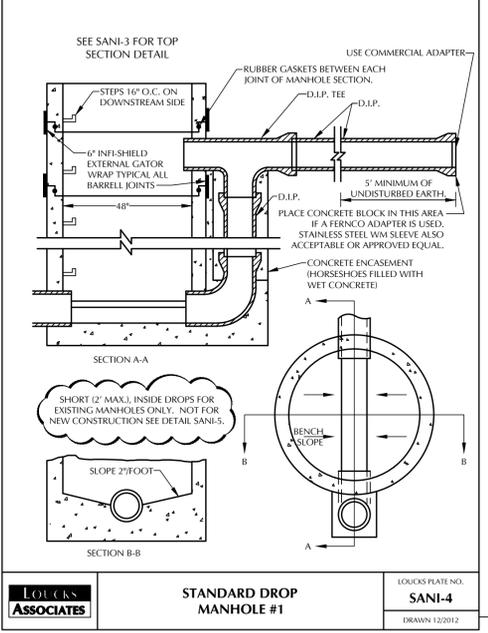
Lights are to be installed onto a cement pad the depth of your local frost line. The base pad should be poured cylindrically with a width of not less than 2" larger than diameter of base. 1/2" x 1/2" to 1/2" L-Shape bolts are necessary for installation (not provided). May obtain an your local hardware store secure four (4) of these bolts, according to the template provided. Electrical conduit containing the main electrical wire should protrude through the center of the cement pad. Connect the base of the light to the cement pad using nuts, and connect the main electrical wire to the leads extending down from the socket assembly (standard) or the ballast/socket assembly (optional). Assembly is now complete.

**Extended Product Description**

HEAVY DUTY MUNICIPAL QUALITY CORE CAST BASE WITH 4" DEEP FLUTED TUBULAR NON TAPERED SHAFT



 CONCRETE PAVEMENT SECTIONS SCALE: N/A	 TYP. BITUMINOUS PAVEMENT SECTION SCALE: N/A	 HANDICAP SIGNS SCALE: N/A	 WHEELCHAIR SYMBOL SCALE: N/A
--	--	----------------------------------	-------------------------------------



Project Name: T3 MINNEAPOLIS

Minneapolis, Minnesota

Owner/Developer: HINES  
 90 South Seventh Street  
 Suite 150  
 Minneapolis, MN 55402

Professional Services: LOUCKS ASSOCIATES  
 Planning • Civil Engineering • Land Surveying  
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7200 Hennepin Lane - Suite 300  
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14019E C8-2.DWG/Layout1

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Professional Signature: Nicholas M. Mannel - PE

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Checked By: Review Date:

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C4-2	UTILITY PLAN SOUTH
C8-1	DETAIL SHEET
C8-2	DETAIL SHEET

Sheet Title: DETAIL SHEET

Project No.: 14019.OE

Sheet No.: C8-2

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TRUCKS ↑



## **Horgan, Michelle**

---

**From:** Horgan, Michelle  
**Sent:** Thursday, December 18, 2014 2:31 PM  
**To:** heidi.ritchie@minneapolismn.gov  
**Cc:** Pfefferle, Robert  
**Subject:** Hines T3 Office Project - Certificate of Appropriateness Application  
**Attachments:** 12\_18\_14 - T3 - CoA - Council Member Jacob Frey Letter.pdf

Heidi,

Good afternoon, attached is a letter for Council Member Frey letting him know that Hines will be submitting a Certificate of Appropriateness Application to the City of Minneapolis for the T3 office project which is proposed to be built in the North Loop neighborhood

Please let us know if you have any questions.

Thanks.  
Michelle

**Michelle Horgan**

**Hines**

90 South 7th Street | Suite 150 | Minneapolis, MN 55402

P 612.332.9284 | F 612.344.1189 | M 612.562.7750

[michelle.horgan@hines.com](mailto:michelle.horgan@hines.com)

December 18, 2014

Council Member Jacob Frey  
City Hall, Room 307  
350 South 5<sup>th</sup> Street  
Minneapolis, MN 55415

*Via Electronic Mail*

**Re: Certificate of Appropriateness Application  
for Development of T3 Office Project**

Dear Council Member Frey:

On behalf of Hines North Loop Green ("Hines"), I am writing to inform you that we will be submitting the Certificate of Appropriateness application for the T3 Office Project (T3) which is located in the Minneapolis Warehouse Historic District.

Applications for zoning approvals will be submitted later and we will notify you when that submission occurs. Specific zoning applications have not yet been confirmed, but are expected to include amending the existing North Loop Green PUD conditional use permit, subdivision application to re-plat existing North Loop Green parcels affected by the T3 project and to incorporate the reconveyance parcel purchased from MnDOT into the PUD, and site plan review.

T3 will be a seven-story heavy timber office building containing approximately 210,000 rentable square feet. The project will be Phase 2 of the North Loop Green Planned Unit Development (PUD) located at 333 Washington Avenue North in the Rail Yards Area of the Minneapolis Warehouse Historic District. The T3 project will redevelop a site currently used as a surface parking lot into a multi-tenant office development that provides new Class A office space and street level commercial activity in proximity to numerous alternative modes of transportation including light rail, commuter rail, bus, regional trails and bikeways, and the pedestrian skyway system. The T3 project is anticipated to be constructed in 2015 - 2016.

The two-lane private road, Dock Street, that was constructed as part of the PUD Phase 1 Dock Street Apartments project will provide the main access to and from the site. Dock Street was constructed by and is maintained by Hines, but is open to the public. The portion of Dock Street running from Washington Avenue North connecting to the project access drive located within the 3<sup>rd</sup> Street North View Corridor was built during the construction of the Phase 1 project.

The connection to/from the Cedar Lake Trail to the North Loop Neighborhood that was constructed with the Phase 1 Dock Street Apartments project will remain intact. The connection from the trail allows users to travel through the site on sidewalks or shared bicycle access along Dock Street.

I am the contact person for the application and my contact information is as follows:

Bob Pfefferle  
Hines  
90 South 7<sup>th</sup> Street, Suite 150  
Minneapolis, MN 55402  
(612) 247-1269  
robert.pfefferle@hines.com

Please let me know if you have any questions or would like any further information at this time.

Sincerely,

A handwritten signature in black ink, appearing to read 'R. Pfefferle', written over the word 'Sincerely,'.

Robert R. Pfefferle  
Hines

**Horgan, Michelle**

---

**From:** Horgan, Michelle  
**Sent:** Thursday, December 18, 2014 2:37 PM  
**To:** 'David Frank' (david0frank@gmail.com)  
**Cc:** Pfefferle, Robert  
**Subject:** Hines T3 Office Project - Certificate of Appropriateness Application  
**Attachments:** 12\_18\_14 - T3 - CoA - NLNA Letter.pdf

David,

Good afternoon, we wanted to let you and the North Loop Neighborhood Association know that Hines will be submitting a Certificate of Appropriateness Application to the City of Minneapolis for the T3 office project. We anticipate submitting our zoning applications to the City at the end of January.

Please let us know if you have any questions.

Thanks.  
Michelle

**Michelle Horgan**  
**Hines**

90 South 7th Street | Suite 150 | Minneapolis, MN 55402  
P 612.332.9284 | F 612.344.1189 | M 612.562.7750  
[michelle.horgan@hines.com](mailto:michelle.horgan@hines.com)

December 18, 2014

Mr. David Frank  
President  
North Loop Neighborhood Association

*Via Electronic Mail*

**Re: Certificate of Appropriateness Application  
for Development of T3 Office Project**

Dear David:

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T3 will be a seven-story heavy timber office building containing approximately 210,000 rentable square feet. The project will be Phase 2 of the North Loop Green Planned Unit Development (PUD) located at 333 Washington Avenue North in the Rail Yards Area of the Minneapolis Warehouse Historic District. The T3 project will redevelop a site currently used as a surface parking lot into a multi-tenant office development that provides new Class A office space and street level commercial activity in proximity to numerous alternative modes of transportation including light rail, commuter rail, bus, regional trails and bikeways, and the pedestrian skyway system. The T3 project is anticipated to be constructed in 2015 - 2016.

The two-lane private road, Dock Street, that was constructed as part of the PUD Phase 1 Dock Street Apartments project will provide the main access to and from the site. Dock Street was constructed by and is maintained by Hines, but is open to the public. The portion of Dock Street running from Washington Avenue North connecting to the project access drive located within the 3<sup>rd</sup> Street North View Corridor was built during the construction of the Phase 1 project.

The connection to/from the Cedar Lake Trail to the North Loop Neighborhood that was constructed with the Phase 1 Dock Street Apartments project will remain intact. The connection from the trail allows users to travel through the site on sidewalks or shared bicycle access along Dock Street.

I am the contact person for the application and my contact information is as follows:

Bob Pfefferle  
Hines  
90 South 7<sup>th</sup> Street, Suite 150  
Minneapolis, MN 55402  
(612) 247-1269  
robert.pfefferle@hines.com

Please let me know if you have any questions or would like any further information at this time.

Sincerely,



Robert R. Pfefferle  
Hines

**Dvorak, Hilary A.**

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**From:** Karen Lee Rosar <karen.rosar@comcast.net>  
**Sent:** Friday, January 23, 2015 2:51 PM  
**To:** Dvorak, Hilary A.  
**Subject:** 316 Third Ave N

Greetings

I am writing in strong support of Bob Pfefferle and the Hines North Loop Green LLC development located at 316 Third Avenue North. The innovative and unique design fits very well into our neighborhood.

Thank you,

*Karen Lee Rosar*  
111 4<sup>th</sup> Ave N #103  
Minneapolis, MN 55401  
612-220-5390  
[karen.rosar@comcast.net](mailto:karen.rosar@comcast.net)