



CPED STAFF REPORT
 Prepared for the City Planning Commission

CPC Agenda Item #5
 November 10, 2014
 BZZ-6884

LAND USE APPLICATION SUMMARY

Property Location: 3524 42nd Avenue South
Project Name: Sanford Middle School Expansion and Renovation
Prepared By: [Mei-Ling Anderson](#), City Planner, (612) 673-5342
Applicant: Minneapolis Public Schools
Project Contact: Paul G. May, Miller Dunwiddie Architecture
Request: To allow a 39,000 square foot addition to an existing building.
Required Applications:

| | |
|-------------------------------|---|
| Conditional Use Permit | To allow an addition to an existing school for students in grades 6 through 8 in the RIA Single-Family District. |
| Conditional Use Permit | To increase the maximum allowed height from the permitted 2.5 stories/35 feet to 3 stories/56 feet. |
| Variance | To reduce the east front yard setback from 20 feet to 0 feet for an accessible ramp exceeding four feet in width. |
| Variance | To reduce the west front yard setback from 20 feet to 0 feet for an accessible ramp exceeding four feet in width. The variance for the reduction in the west front yard setback from 20 to 14 feet for the building wall has been returned. |
| Variance | To increase the maximum allowed impervious surfaces from 60 percent of the site to 68 percent of the site in the RIA Single-Family District. |
| Variance | To increase the maximum allowed floor area ratio above 0.5 to 0.72. |
| Site Plan Review | For a 39,000 square foot addition to an existing building. |

SITE DATA

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|-----------------------------------|----------------------------------|
| Existing Zoning | RIA Single-Family District |
| Lot Area | 159,395 square feet / 3.65 acres |
| Ward(s) | 12 |
| Neighborhood(s) | Longfellow Community Council |
| Designated Future Land Use | Urban Neighborhood |
| Land Use Features | Not applicable |
| Small Area Plan(s) | Not applicable |

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|---|-------------------|---------------------------------------|----------------|
| Date Application Deemed Complete | October 17, 2014 | Date Extension Letter Sent | Not applicable |
| End of 60-Day Decision Period | December 16, 2014 | End of 120-Day Decision Period | Not applicable |

BACKGROUND

SITE DESCRIPTION AND PRESENT USE. The subject site is 159,395 square feet (3.65 acres) and occupies the city block bounded by E 35th Street (north), 42nd Avenue S (east), E 36th Street (south), and 41st Avenue S (west). The site contains an existing, three-story building that has been in operation as a school since 1924. There are currently 760 students enrolled at the school in grades 6, 7, and 8. The main entrance is currently located along E 36th Street. The topography of the site is generally flat, with the exception of the east portion of the site, which is at a higher grade relative to the public street and sidewalk, and the northeast portion of the site, which has varied topography and currently contains landscaping.

SURROUNDING PROPERTIES AND NEIGHBORHOOD. The surrounding area contains primarily low-density residential housing. There is a restaurant to the north of the subject site, on the opposite side of E 35th Street (zoned C1), and another commercially zoned property to the south. There are also other small-scale, neighborhood-serving commercial uses throughout the neighborhood, as well as several parks and other schools, such as Howe School and Dowling Elementary, Minnehaha Academy, and Hiawatha Elementary. The site is located a half-mile to the west of the Mississippi.

PROJECT DESCRIPTION. The applicant is proposing a 39,000 square foot expansion to the existing building in order to accommodate 200 additional students in grades 6 through 8, as well as 10 staff members, for a total of approximately 960 students and 110 staff members. The northeast portion of the existing building would be demolished as part of the project. The renovation would include upgrading the main east and west school entrances, installing an air conditioning system, adding additional on-site parking spaces in the existing surface parking area, constructing a new gymnasium, implementing several mechanical upgrades, and installing a stormwater filtration pond.

The applications required include two conditional use permits in order to expand the existing school building: one to allow a school, grades K-12, to operate in the building addition in the RIA Single-Family District, and one increase the allowed height in the RIA Single-Family District from 2.5 stories/35 feet to 3 stories/56 feet. The building addition would be designed to match the height of the existing building.

The applicant is requesting four variances: 1) the proposed accessibility ramp along 41st Avenue S exceeding four feet in width is located within the 20-foot front yard setback along the west property line; 2) the proposed accessibility ramps along 42nd Avenue S are located within the 20-foot front yard setback along the east property line; 3) the project would increase the maximum allowed impervious surfaces from 60 to 68 percent; and 4) the maximum floor area ratio would be increased from 0.5 to 0.72. The variance to reduce the west front yard setback from 20 feet to 14 feet for the building wall has been returned.

The site is also subject to site plan review.

RELATED APPROVALS. The school has received two land use approvals since 2003 for a telecommunications tower. There have also been various remodeling permits approved since 1990, however, this would be the largest expansion since 1960.

| Planning Case # | Application | Description | Action |
|------------------------|-----------------------|--------------------------|---------------|
| BZZ-1314 (2003) | Administrative review | Telecommunications tower | Approved |
| BZZ-4876 (2010) | Administrative | Telecommunications | Approved |

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|--|--------|-------|--|
| | review | tower | |
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PUBLIC COMMENTS. As of the printing of this report, staff has not received any comments on this project. Any correspondence received prior to the public meeting will be forwarded on to the Planning Commission for consideration.

ANALYSIS

CONDITIONAL USE PERMIT

The Department of Community Planning and Economic Development has analyzed the application to allow an addition to an existing school for students in grades 6 through 8 in the RIA Single-Family District based on the following [findings](#):

1. *The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

The addition to the northeast side of the existing school will not be detrimental to or endanger the public health, safety, comfort, or general welfare. Sanford Middle School has operated in this location since it was built in 1924, and there were a series of expansions in 1960. The project will allow the school to add two teacher teams to each grade level (6, 7, and 8), as well as allow each grade to operate on its own level. The applicant is also proposing to add a new gymnasium; construct more prominent and accessible entrances along 41st Avenue South and 42nd Avenue South; make site, landscaping, and stormwater improvements; and provide air conditioning throughout the entire building. The proposed site improvements – including 39 additional trees and 301 additional shrubs, new pedestrian walkways, the installation of 68 bicycle parking spaces, a stormwater retention and filtration system, and an enclosed trash enclosure – will greatly improve upon the public realm in and around the site.

2. *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

The proposal to expand the existing school use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal or orderly development and improvement of surrounding property. The existing building was constructed as school and has been used as one for the past 90 years, so the expansion would be in keeping with the site’s intended use. The scale of the addition will be compatible with the rest of the building given the size of the overall property. In addition, the property’s use as a school is compatible with the surrounding residential uses. The recommended landscaping and site improvements will bring the site into compliance with zoning code standards and improve upon the appearance of the site.

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

The site is currently served by existing utility infrastructure, and the project will expand the stormwater management system on the site. In addition, the Public Works Department has reviewed the preliminary plan and will review the final plan for compliance with standards related to access and circulation, drainage, and sewer/water connections. The applicant will be required to continue to work closely with the Public Works Department, the Plan Review Section of CPED and the various utility companies during the duration of the development should the applications be approved. This would be required to ensure that all procedures are followed and that the

development complies with all city and other applicable requirements. The applicant is aware that the final plans are expected to incorporate any applicable comments or modifications as required by the Public Works Department.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

A discretionary TDMP was completed for this development. The TDMP focused on vehicle and pedestrian circulation, as well as vehicle and bicycle parking. The study concludes that there will continue to be a need for on-street parking after the site improvements result in the addition of 21 vehicle parking spaces (for a total of 51) and an additional 76 bicycle parking spaces (for a total of 146). However, the results of the TDMP conclude that circulation and safety conditions can be improved, relative to today, through additional bicycle parking, designated bus and parent drop-off and pick-up areas, ongoing education for parents, staff, and students, and increasing school patrol.

The proposed use is not expected to contribute to traffic congestion in the public streets. There are currently 30 vehicle spaces on the site and the applicant is proposing to add 19 standard spaces and three accessible spaces, for a total of 51, which exceeds the minimum vehicle parking requirement of one per classroom plus 15 grandfathered spaces, for a total of 47 spaces minimum. As conditioned, all proposed parking stalls will meet the City's dimension and design requirements.

There are currently 70 bicycle parking spaces on-site and the City requires three per classroom. With the addition, the bicycle parking must be within 50 feet of the principal entrance to meet the standards for short-term bicycle parking. MPS is proposing a total of 146 bicycle parking spaces, which would be located near the principal entrances on 41st Avenue S and 42nd Avenue S.

The school is required to have 2 small loading spaces per the zoning code. The applicant has designated a place for two small loading spaces near the building adjacent to the surface parking area along 41st Avenue S.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

The site is designated as Urban Neighborhood in *The Minneapolis Plan for Sustainable Growth*. The Urban Neighborhood designation is described as predominantly residential. It also includes other, neighborhood-serving semi-public uses, such as schools, community centers, religious institutions, and public safety facilities. As conditioned, the proposed development would be consistent with the following general land use policies of *The Minneapolis Plan for Sustainable Growth*:

Land Use Policy 1.1: Establish land use regulations to achieve the highest possible development standards, enhance the environment, protect public health, support a vital mix of land uses, and promote flexible approaches to carry out the comprehensive plan.

- 1.1.5 Ensure that land use regulations continue to promote development that is compatible with nearby properties, neighborhood character, and natural features; minimizes pedestrian and vehicular conflict; promotes street life and activity; reinforces public spaces; and visually enhances development.

Land Use Policy 1.2: Ensure appropriate transitions between uses with different size, scale, and intensity.

- 1.2.1 Promote quality design in new development, as well as building orientation, scale, massing, buffering, and setbacks that are appropriate with the context of the surrounding area.

- 1.2.2 Ensure that lighting and signage associated with non-residential uses do not create negative impacts for residential properties.

Land Use Policy 1.3: Ensure that development plans incorporate appropriate transportation access and facilities, particularly for bicycle, pedestrian, and transit.

- 1.3.1 Require safe, convenient, and direct pedestrian connections between principal building entrances and the public right-of-way in all new development and, where practical, in conjunction with renovation and expansion of existing buildings.
- 1.3.2 Ensure the provision of high quality transit, bicycle, and pedestrian access to and within designated land use features.

Land Use Policy 1.8: Preserve the stability and diversity of the city's neighborhoods while allowing for increased density in order to attract and retain long-term residents and businesses.

- 1.8.2 Advance land use regulations that retain and strengthen neighborhood character, including direction for neighborhood serving commercial uses, open space and parks, and campus and institutional uses.

Public Services and Facilities Policy 5.1: Coordinate facility planning among city departments and public institutions.

- 5.1.1 Encourage communication and coordination among city departments, Hennepin County, Minneapolis Park and Recreation Board, and Minneapolis Public Schools to share use of facilities.
- 5.1.2 Explore opportunities for co-location of public services where appropriate.
- 5.1.3 Work with all partner agencies, including City departments, to ensure that facility planning is consistent with the land use policies of The Minneapolis Plan.

Public Services and Facilities Policy 5.2: Support the efforts of public and private institutions to provide a wide range of educational choices for Minneapolis students and residents throughout the city.

- 5.2.1 Work with institutions to ensure that school facilities are safe, accessible, and functionally appropriate for a diverse array of educational programs.
- 5.2.8 Provide infrastructure (sidewalks, crosswalks, signage, etc), education, and enforcement to ensure safe routes to neighborhood schools.

Heritage Preservation Policy 8.7: Create a regulatory framework and consider implementing incentives to support the ethic of “reduce, reuse, and recycle” and revitalization for buildings and neighborhoods.

- 8.7.1 Protect historic resources from demolition and explore alternatives to demolition.

Urban Design Policy 10.8: Strengthen the character and desirability of the city's urban neighborhood residential areas while accommodating reinvestment through infill development.

- 10.8.8 Appropriate non-residential land uses, such as institutional, public and suitable commercial uses, should be integrated into low density residential areas through proper building location and design, landscaping, and other site improvements.

Urban Design Policy 10.18: Reduce the visual impact of automobile parking facilities.

10.18.1 Require that parking lots meet or exceed the landscaping and screening requirements of the zoning code, especially along transit corridors, adjacent to residential areas, and areas of transition between land uses.

10.18.4 Provide walkways within parking lots in order to guide pedestrians through the site.

6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

If the requested land use/zoning applications are approved, the proposal will comply with all provisions of the RIA Single-Family District.

CONDITIONAL USE PERMIT

The Department of Community Planning and Economic Development has analyzed the application to allow an increase the maximum allowed height from the permitted 2.5 stories/35 feet to 3 stories/56 feet based on the following [findings](#):

1. *The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

The height limitation in the RIA zoning district is two-and-a-half stories or 35 feet. The proposed addition will be three stories or 56 feet in height. The proposal to increase the height of the proposed addition to Sanford Middle School will match the floor levels and height of the existing building, and will not be detrimental to or endanger the public health, safety, comfort or general welfare.

2. *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

The proposal to increase the height of the proposed addition to Sanford Middle School will not be injurious to the use and enjoyment of other property in the vicinity, nor will it impede the normal or orderly development and improvement of surrounding property. The school occupies an entire city block. The addition will be built in the northeast portion of the side, where a one-story and three-story portion of the building will be removed (containing the band, choir, drama, gymnasium, and some offices and classrooms). The addition will be setback 14 feet from the east property line and 136 feet from the north property line. Since the school occupies an entire block and is surrounded on all sides by public streets, the public streets would serve as an additional buffer between the addition and adjacent uses.

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

Increasing the height of the building will have no impact on utilities, access roads or drainage.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

Increasing the height of the building will have no impact on traffic congestion in the public streets.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

The proposed development would be consistent with the following general land use policies of *The Minneapolis Plan for Sustainable Growth*:

Land Use Policy 1.1: Establish land use regulations to achieve the highest possible development standards, enhance the environment, protect public health, support a vital mix of land uses, and promote flexible approaches to carry out the comprehensive plan.

- 1.1.4 Support context-sensitive regulations for development and land use, such as overlay districts, in order to promote additional land use objectives.
- 1.1.5 Ensure that land use regulations continue to promote development that is compatible with nearby properties, neighborhood character, and natural features; minimizes pedestrian and vehicular conflict; promotes street life and activity; reinforces public spaces; and visually enhances development.

Land Use Policy 1.2: Ensure appropriate transitions between uses with different size, scale, and intensity.

- 1.2.1 Promote quality design in new development, as well as building orientation, scale, massing, buffering, and setbacks that are appropriate with the context of the surrounding area.

Urban Design Policy 10.13: Work with institutional and public partners to assure that the scale and form of new development or expansion will occur in a manner most compatible with the surrounding area.

- 10.13.1 Concentrate the greatest density and height in the interior of institutional campuses with stepped-down building design as it transitions to the neighborhood.
- 10.13.2 Develop building forms on the edges of institutional property which are most reflective of neighboring properties as the preferred option, while recognizing that in certain circumstances greater bulk and density may be preferable to expansion beyond existing campus boundaries.
- 10.13.3 Encourage institutional uses and public buildings and facilities to incorporate architectural and site design that is reflective of their civic importance and that identifies their role as focal points for the community.
- 10.13.4 Promote active uses at the ground floor level.

- 6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

If the requested land use/zoning applications are approved, the proposal will comply with all provisions of the RIA Single-Family District.

Additional Standards to Increase Maximum Height

In addition to the conditional use permit standards, the Planning Commission shall consider, but not be limited to, the following [factors](#) when determining the maximum height of principal structures in commercial districts:

- 1. *Access to light and air of surrounding properties.*

The proposed addition will not diminish access to light and air for surrounding properties. The school occupies an entire block. The addition will be setback 14 feet from the east property line and 136 feet from the north property line. Since the school occupies an entire block and is surrounded on all sides by public streets, the public streets would serve as an additional buffer

between the addition and adjacent uses. The closest properties to the addition would be approximately 120 feet away on the opposite side of 42nd Avenue S.

2. *Shadowing of residential properties, significant public spaces, or existing solar energy systems.*

The applicant submitted a shadow study depicting shadowing impacts at one hour before sunrise, noon, and 1 hour before sunset in March, June, and December. According to the shadow study that was submitted by the applicant, a few of the residential properties along 42nd Avenue South could experience a minimal amount of shadowing by the addition during the late afternoon hours in December, but it would not last the entire day.

3. *The scale and character of surrounding uses.*

The proposed addition will be three stories or 56 feet at its tallest point. The addition may be considered to be out of scale and character with the one and two-story single-family dwellings to the north and east, however, the overall height of the addition is as tall as the existing portion of the school.

4. *Preservation of views of landmark buildings, significant open spaces or water bodies.*

The building addition will not block views of landmark buildings, significant open spaces or water bodies.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance to reduce the east front yard setback from 20 feet to 0 feet for an accessible ramp exceeding four feet in width based on the following [findings](#):

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

There are entrances currently located along 42nd Avenue; however, due to the elevated grade of the school relative to the public sidewalk, visitors must use stairs in order to access these entrances. The change in elevation is approximately 5.5 feet between the street level and the main floor of the building. According to the applicant, this grade change requires a sidewalk of significant length in order to provide access to those with mobility impairments. The proposed accessible ramp would encroach upon the 20-foot setback along the east property line because it exceeds four feet in width.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The existing building currently uses the entrance located on E 36th Street as its main entrance, which creates traffic circulation issues in this location. Creating an accessible, main building entrance on 42nd Avenue S will alleviate traffic pressures near the south of the site while bringing the school into greater compliance with the school and City's goals in terms of providing school facilities that are fully accessible. The location and width of the ramp are reasonable.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. The east side of the school is currently not ADA accessible. Locating the ramps in this location will allow the site to be accessible on the east side of the property, where there is likely to be a substantial amount of activity since the gym is also proposed for this location. Granting the variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance to reduce the west front yard setback from 20 feet to 0 feet for an accessible ramp exceeding four feet in width based on the following [findings](#):

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

The existing building is already located within the west front yard setback along 41st Avenue S. The entrance from the building to the parking lot is not fully accessible, given the 2.5-foot change in elevation from the street level to the main floor of the building. The applicant is proposing to construct an accessible ramp between the public sidewalk and building in order to allow visitors, staff, and students to access this side of the building. The applicant is proposing a ramp that exceeds four feet, which is the maximum allowed width to not be considered a permitted encroachment.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The existing building currently uses the entrance located on E 36th Street as its main entrance, which creates traffic circulation issues in this location. Creating an accessible entrance on 41nd Avenue S will alleviate traffic pressures near the south of the site while bringing the school into greater compliance with the school and City's goals in terms of providing school facilities that are fully accessible. The location and width of the ramp, and the placement of the proposed building wall, are reasonable.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. The entrance on the west side of the property, which is connected to the existing on-site parking lot, is not fully accessible due to the elevation of the entrance being approximately 2.5 feet above the site. Locating the ramps in this location will allow the site to be accessible on the west side of the property. The variance is necessary, as the width of the ramp exceeds four feet. However, granting the variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance to increase the maximum allowed impervious surfaces from 60 percent of the site to 68 percent of the site in the RIA Single-Family District based on the following [findings](#):

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

The maximum impervious surface percentage of the lot that is allowed in the RIA zoning district is 65 percent. The proposed impervious surface area is 68 percent of the lot, while the existing site is 59 percent impervious surfaces. The amount of impervious surface that would be added to the site is approximately 13,626 square feet. The expansion to the school is being done in anticipation of increased enrollment; the current student enrollment is 760 and the projected student enrollment is 960. The number of classrooms will increase from 45 to 62 and the number of teachers will increase from 100 to 110. In order to accommodate the additional students, classrooms and teachers the building footprint will increase from 41,750 square feet to 59,750 square feet. In addition, the impervious surfaces will be increased in order to accommodate new accessible ramps and sidewalks leading to building entrances, as well as a new building entrance on 42nd Avenue S. The number of bicycle and vehicle parking spaces will also be increased as part of this development through the addition of new bicycle corrals and through the rearrangement of the existing parking areas.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The variance request is reasonable and in keeping with the spirit and intent of the ordinance and the comprehensive plan. In general, lot controls are established to provide for the orderly development and use of land and to minimize conflicts among land uses by regulating the use of lots and lot area in order to provide adequate light, air, open space and separation of uses. In order to accommodate a larger student population, the building and site changes are necessary.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

While the amount of impervious surface on the site is increasing, the applicant is proposing to capture stormwater runoff in a filtration basin and detention pond. Expanding the stormwater management system will help with rate control and water quality. In addition, staff is recommending that the applicant install additional landscaped area on the west side of the parking lot. Granting the variance would not adversely alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity, nor would it be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance to increase the maximum allowed floor area ratio above 0.5 to 0.72 based on the following [findings](#):

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

The maximum floor area ratio (FAR) allowed in the RIA zoning district for a school, K-12 is 0.5. The proposed FAR is 0.72 (the existing FAR is 0.47). In the RIA zoning district, the minimum lot size requirement for a school, K-12 is 20,000 square feet. The school's lot size is 3.65 acres. The perceived scale and massing of a building with a 0.5 FAR on a 20,000 square foot lot versus a 159,395 square foot (3.65 acre) site is much different. Given the size of the site and the scale of the existing building the proposed addition is proportional to the size of the lot.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The variance request is reasonable and in keeping with the spirit and intent of the ordinance and the comprehensive plan. In general, building bulk regulations are established in order to assure that the scale and form of new development or expansion will occur in a manner most compatible with the surrounding area. The site is 3.65 acres in size and is separated from surrounding development by public streets on all four sides. The massing of the building is spread out over the majority of the site. The remainder of the site is used for surface parking. The scale of the building and the addition is proportional to the size of the lot.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

The site is 3.65 acres in size and is separated from surrounding development by public streets on all sides. The scale of the building and the addition is proportional to the size of the lot. Granting the variance would not adversely alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity, nor would it be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

SITE PLAN REVIEW

The Department of Community Planning and Economic Development has analyzed the application based on the required [findings](#) and [applicable standards](#) in the site plan review chapter:

I. Conformance to all applicable standards of Chapter 530, Site Plan Review.

Building Placement and Design – Requires alternative compliance

- The placement of the addition would reinforce the street wall, facilitate pedestrian access, and maximize natural surveillance. The addition will fill in gaps in the existing street wall along 42nd Avenue S, the east side of the property, which will contain the building's primary entrance. The addition will also bring the building closer to the north side of the property, which runs along E 35th Street.
- The first floor of the addition will be located 20 feet from the eastern front lot line along 42nd Avenue S, and would be built to the district setback requirement.
- The area between the building addition and the front lot line contains amenities, including bicycle racks, landscaping, stairways, and accessible ramps.
- The building addition will contain the building's new main entrance, which faces 42nd Avenue S and is adjacent to one of two of the property's front lot lines.

- The site’s accessory parking is currently located along the western portion of the site, with approximately 101 feet of frontage along the north property line (E 35th Street). The project would result in the addition of 22 parking spaces through restriping the existing area rather than through expanding the surface parking lot.
- The walls of the addition provide architectural detail and contain windows to create visual interest, increase security of adjacent outdoor spaces, and to divide the building into smaller, identifiable sections.
- There are no areas of the development that are over 25 feet in length and void of windows, entries, recesses or projects, or other architectural elements.
- The exterior materials that the applicant is proposing would be durable and compatible with all sides of the existing building. Brick would be used for the majority of the exterior, and cast stone, metal panel, and glass would also be used. Plain face concrete block is not proposed.
- The windows are evenly distributed and vertical in nature.
- The building elevations facing the public street or on-site parking are required to contain 30 percent windows on the first floor and 10 percent windows on each level above the first floor. As shown in **Table I**, the applicant is meeting this requirement on some of the levels of the addition, but is requesting alternative compliance for the window requirement on the west elevation facing the parking lot, the first and third floors of the north elevation facing E 35th Street, and the first floor of the east elevation facing 42nd Avenue S.
- The ground floor of the addition will contain more than 70 percent active functions on all street-facing sides.
- The roof line of the addition will be flat to match the existing building. However, most of the buildings in the vicinity are residential structures that have pitched roofs.

Table I. Percentage of Windows Required for Elevations Facing a Public Street, Sidewalk, Pathway, or On-Site Parking

| | Code Requirement | | Proposed | |
|---|------------------|-------------|----------|---------------|
| 41st Avenue S (West) | | | | |
| 1 st Floor | 30% minimum | 180 sq. ft. | 0% | 0 sq. ft. |
| 2 nd Floor | 10% minimum | 100 sq. ft. | 0% | 0 sq. ft. |
| E 35th Street (North) | | | | |
| 1 st Floor | 30% minimum | 255 sq. ft. | 0% | 0 sq. ft. |
| 2 nd Floor | 10% minimum | 142 sq. ft. | 21% | 296 sq. ft. |
| 3 rd Floor | 10% minimum | 131 sq. ft. | 4% | 55 sq. ft. |
| 42nd Avenue S (East) | | | | |
| 1 st Floor | 30% minimum | 633 sq. ft. | 19% | 407 sq. ft. |
| 2 nd Floor | 10% minimum | 316 sq. ft. | 40% | 1,267 sq. ft. |
| 3 rd Floor | 10% minimum | 242 sq. ft. | 49% | 1,195 sq. ft. |

Access and Circulation – Meets requirements

- All of the entrances leading into the building are directly connected to a public sidewalk.
- There are no transit shelters proposed as part of this development.
- Vehicular access and circulation has been designed to minimize conflicts with pedestrians and surrounding residential uses. The main entrance will be moved to 42nd Avenue S in order to provide a more efficient drop-off and pick-up area. The two curb cuts that provide access to the existing parking area along 41st Avenue S will not be altered as part of this project.
- There are no public alleys adjacent to this property.
- The maximum impervious surface requirement in the RIA Single-Family District is 60 percent. The proposed addition would increase the impervious surfaces to 68 percent of the site. The

applicant has applied for a variance to increase the maximum allowed impervious surfaces on the site.

Landscaping and Screening – *Requires alternative compliance*

- The composition and location of landscaped areas complement the scale of the development and its surroundings.
- The zoning code requires that at least 20 percent of the site not occupied by buildings be landscaped. The lot area of the site is 159,395 square feet and the building footprint is 59,750 square feet. The difference is 99,645 square feet, and 20 percent of this number is 19,929 square feet. The applicant is proposing approximately 52,272 square feet of landscaped area on the site, or approximately 33 percent of the site not occupied by the building, which exceeds site plan review minimum standards.
- The zoning code requires that the site contain at least one canopy tree per 500 square feet of required green space and at least 1 shrub for each 100 square feet of required green space. The tree requirement for this site is 40 and the shrub requirement is 200. The applicant is proposing a total of 39 new canopy trees and 300 new shrubs on-site, as well as a variety of perennials and grasses, although the landscaping plan provided does not clearly identify the species and location of the existing landscaping materials. Approximately 38 of the 65 existing trees are located in the adjacent public right-of-way, so there are approximately 27 existing canopy trees on-site. The proposed landscaping materials would exceed the minimum tree and shrub requirement, however, staff would like to see the landscaping materials rearranged in order to bring the site into more compliance with other site plan review landscaping standards.
- All areas not occupied by buildings, parking, and loading areas contain landscaping.
- The site contains a surface parking area with 51 spaces. The parking lot frontages along the west and north property lines (41st Avenue S and E 35th Street) are required to contain a landscaped yard of at least seven feet in width. The applicant is exceeding the minimum landscaped yard requirement along E 35th Street, as they are proposing to retain the 10-foot wide landscaped yard in this location. However, the applicant is proposing to provide an on-site landscaped yard that would range between zero and five feet along 41st Avenue S. Because the landscaped yard does not total seven feet or more in width along the west parking lot frontage, alternative compliance is requested.
- The zoning code requires that a six-foot screen that is at least 95 percent opaque be provided between parking areas and adjacent residential uses, and a three foot tall screen that is 60 percent opaque be located between the parking area and an adjacent street. A three-foot screen is required between the parking area and the public right-of-way along 41st Avenue S and E 35th Street. There are existing shrubs located in the required landscaped buffers in the northwest portion of the surface parking area; however, they do not provide a continuous, three-foot tall screen that is 60 percent opaque. There is no screen provided for the southernmost 180 feet of the west parking lot frontage. Alternative compliance is requested for the parking lot screening requirement.
- The corners of the surface parking area contain landscaping.
- The parking area would contain 51 vehicle parking spaces and is subject to the requirement that no parking space be located more than 50 feet from an on-site deciduous tree. The development does not meet this requirement and requires alternative compliance.
- Surface parking areas are required to have no less than one tree per each 25 linear feet of parking or loading area frontage. This requirement applies to the west and north parking lot frontages. The west parking lot frontage would be required to have 14 canopy trees and five are provided. The north parking lot frontage would be required to have four canopy trees and four are provided. Alternative compliance is requested for the linear tree requirement.

Table 2. Landscaping and Screening Requirements

| | Code Requirement | Proposed |
|--------------------------------------|-------------------------|---|
| Lot Area | -- | 159,395 sq. ft. |
| Building footprint | -- | 59,750 sq. ft. |
| Remaining Lot Area | -- | 99,645 sq. ft. |
| Landscaping Required | 19,929 sq. ft. | 52,272 sq. ft. |
| Canopy Trees (1: 500 sq. ft.) | 40 trees | 66 trees (plus approximately 38 trees in the public right-of-way) |
| Shrubs (1: 100 sq. ft.) | 200 shrubs | 300 shrubs (503 with existing) |

Additional Standards – Meets requirements

- There is an existing surface parking area located along 41st Avenue S, which is the western portion of the site. The parking spaces located adjacent to the sidewalk and right-of-way on 41st Avenue S have wheel stops. The applicant is also proposing to reconstruct the existing detention pond and turn it into an above-ground filtration basin that overflows into a detention pond. The expansion of the stormwater facilities will help to control runoff and accommodate the additional impervious surfaces on the site.
- The building addition will not block views of important elements of the city.
- According to the shadow study submitted by the applicant, a few of the residential properties along 42nd Avenue South could experience a minimal amount of shadowing by the addition during the late afternoon hours in December, but it would not last the entire day
- The addition should have minimal wind effects on the surrounding areas.
- The site plan complies with crime prevention design elements. There are walkways that direct people to building entrances, the addition would bring the building wall closer to the property line, and the addition will contain large windows facing the public street to increase surveillance.
- This building is neither historically designated nor is the site located in a historic district. In 2005, a historic context study of all Minneapolis Public Schools was prepared for the Minneapolis Heritage Preservation Commission. The study did not evaluate the schools for designation, but does recommend that they be further studied.

2. Conformance with all applicable regulations of the zoning ordinance.

The proposed use is *conditional* in the RIA District.

Off-street Parking and Loading – Meets requirements with Conditions of Approval

- The parking requirement for the school is based on the number of classrooms proposed (62) and the number students of legal driving age enrolled at the school (0). Since there are no students of legal driving age, the minimum requirement is one space per classroom (62), while the maximum allowed is two per classroom (124).
- The school is currently legally nonconforming as to the minimum off-street parking requirement. The current school has 45 classrooms, which requires 45 off-street parking spaces. Thirty off-street parking spaces are currently provided. Therefore, the school is grandfathered for 15 spaces. The applicant is proposing to increase the number of classrooms by 17 and increase the number of off-street parking spaces by 21, for a total of 51 spaces. Eighteen of the 21 new spaces would be standard spaces and three would be accessible. The 51-space surface parking area will be under the maximum of 124 spaces.

- Of the 51 spaces, three spaces would be accessible. However, the accessible spaces do not meet the minimum drive aisle width requirement of 22 feet; the distance between the accessible space and the concrete walkway is 18 feet. Staff is recommending a condition that all parking spaces and drive aisles meet the minimum design standards in Chapter 541.
- The applicant is proposing to add 76 bicycle parking spaces, for a total of 146 including the 70 spaces that are currently installed. The zoning code requirement is three per classroom, with at least 50 percent meeting the standards for short-term bicycle parking. The applicant is required to provide a minimum of 51 spaces based on the increase in classrooms from 45 to 62 (17 classrooms added). Of these, at least 26 must meet the standards for short-term bicycle parking. Staff is recommending a condition that would require the applicant to provide a minimum of 51 additional bicycle parking spaces, of which at least 61 would meet the standards for short-term bicycle parking as described in Chapter 541; they must be within 50 feet of a principal entrance.
- The school use has “Low” loading requirement. The school is required to provide two small loading spaces based on its square footage, and two are proposed in the site plan.

Table 3. Vehicle Parking Requirements Per Use (Chapter 541)

| | Minimum Parking Requirement | Applicable Reductions | Total Minimum Requirement | Maximum Parking Allowed | Proposed |
|----------------------------|------------------------------------|------------------------------|----------------------------------|--------------------------------|-----------------|
| School, grades K-12 | 62 | 15 (grandfather rights) | 47 | 124 | 51 |

Table 4. Bicycle Parking and Loading Requirements (Chapter 541)

| | Minimum Bicycle Parking | Minimum Short-Term | Minimum Long-Term | Proposed | Loading Requirement | Proposed |
|----------------------------|--------------------------------|---------------------------|--------------------------|-----------------|----------------------------|-----------------|
| School, grades K-12 | 121 | 61 | Not less than 50% | 146 | 2 small | 2 small |

Building Bulk and Height – Requires conditional use permit and variance

- After the 39,000 square foot addition, the building will have 114,700 square feet in gross floor area. The existing floor area ratio (FAR) of the site is 0.47, and the addition would result in an FAR of 0.72, which exceeds the 0.5 district maximum and requires a variance.
- The applicant has also applied for a conditional use permit to increase the height of the building addition from the allowed 2.5 stories / 35 feet to 3 stories / 56 feet. The building addition would match the height of the existing building.

Table 5. Building Bulk and Height Requirements

| | Code Requirement | Proposed |
|--|--|----------------------------------|
| Lot Area | -- | 159,395 square feet / 3.65 acres |
| Gross Floor Area (GFA) | -- | 114,700 sq. ft. |
| Maximum Floor Area Ratio (GFA/Lot Area) | 0.50 | 0.72 |
| Maximum Building Height | 2.5 stories or 35 ft., whichever is less | 3 stories / 56 ft. |

Residential Lot Requirements – *Not applicable*

Yard Requirements – *Requires variance(s)*

- The applicant is proposing to reduce the front yards on both the east and west sides of the property from 20 feet to 0 feet. The applicant has applied for variances for these setbacks to allow for accessible ramps. Much of the parking lot is legally nonconforming as to the required yards. Staff is recommending that landscaped yards be provided as required by Chapter 530.
- The addition would exceed the minimum setback requirements for the corner side yards along E 35th Street and E 36th Street.

Table 6. Minimum Yard Requirements

| | Zoning District | Proposed |
|---|------------------------|-----------------|
| Front (42nd Ave S) | 20 ft. | 0 ft. |
| Front (41st Ave S) | 20 ft. | 0 ft. |
| Corner Side (E 35th St) | 12 ft. | 140 ft. |
| Corner Side (E 36th St) | 12 ft. | 45 ft. |

Signs – *Meets requirements with Conditions of Approval*

- Signs are subject to Chapter [543](#) of the Zoning Code. The applicant is proposing two new signs as part of the project. Both signs would require a variance for height. However, a sign variance in a residential district is not an authorized variance, so staff is recommending that the applicant submit separate plans for the new signage that is in compliance with Chapter 543 of the zoning code.
- There would be one, three by five foot sign (15 square feet) mounted near the new main entrance on 42nd Avenue S. It would be internally illuminated with dynamic text. The height of the sign would be nine feet above grade. The zoning code allows the dynamic changeable copy portion of the sign to be up to 16 square feet, and either the monument or wall sign, but not both, may be illuminated.
- The other sign would be attached to the north (E 35th Street) building wall and would consist of individual, one-foot tall letters spanning 15 feet in width, for a total of 15 square feet. The letters would be made of stainless steel. The sign would be approximately 21 feet above the adjacent grade at its highest point.

Table 7. Signage Summary

| | Number Allowed Per Zoning Lot | Proposed Number | Maximum Area Per Sign | Proposed Area | Maximum Allowed Height | Proposed Height |
|--------------|--------------------------------------|------------------------|------------------------------|----------------------|-------------------------------|------------------------|
| Monument | 1 | 1 | 32 sq. ft. | 15 sq. ft. | 8 ft. | 9 ft. |
| Attached | 1 | 1 | 32 sq. ft. per block face | 15 sq. ft. | 14 ft. | 21 ft. |
| Total | 2 | 2 | | 30 sq. ft. | | |

Dumpster Screening – *Meets requirements with Conditions of Approval*

- There will be a trash and recycling storage area provided on the west side of the building, near the center of the 41st Avenue S block. The storage area would be enclosed structure. CPED is recommending that elevations of the enclosure be provided with the final plans.

Screening of Mechanical Equipment – *Meets requirements with Conditions of Approval*

- The mechanical equipment associated with the addition will be located on the rooftop. The applicant has provided a photo demonstrating how the equipment would be screened. CPED is recommending that the screening materials be indicated on the final elevations.

Lighting – *Meets requirements with Conditions of Approval*

- A lighting plan showing footcandles was not submitted as part of the application materials. CPED is recommending that a lighting plan that meets the requirements of Chapter [535](#) of the Zoning Code be met.

Impervious Surface Area – *Requires variance(s)*

- The maximum impervious surface requirement in the RIA zoning district is 65 percent. Currently 59 percent of the site is impervious. The applicant has applied for a variance to increase this to 68 percent.

Specific Development Standards – *Meets requirements*

- The specific development standards for schools, grades K—12 are:
 - (1) The use shall include a regular course of study accredited by the State of Minnesota.
 - (2) To the extent practical, all new construction or additions to existing buildings shall be compatible with the scale and character of the surroundings, and exterior building materials shall be harmonious with other buildings in the neighborhood.
 - (3) An appropriate transition area between the facility and adjacent property shall be provided by landscaping, screening and other site improvements consistent with the character of the neighborhood.

Overlay District Standards – Not applicable**3. Conformance with the applicable policies of *The Minneapolis Plan for Sustainable Growth*.**

The Minneapolis Plan for Sustainable Growth identifies the site as Urban Neighborhood the future land use map. The proposed development is consistent with the following principles and policies outlined in the comprehensive plan:

Policy 10.8: Strengthen the character and desirability of the city's urban neighborhood residential areas while accommodating reinvestment through infill development.

10.8.8 Appropriate non-residential land uses, such as institutional, public and suitable commercial uses, should be integrated into low density residential areas through proper building location and design, landscaping, and other site improvements.

Policy 10.13: Work with institutional and public partners to assure that the scale and form of new development or expansion will occur in a manner most compatible with the surrounding area.

10.13.1 Concentrate the greatest density and height in the interior of institutional campuses with stepped-down building design as it transitions to the neighborhood.

10.13.2 Develop building forms on the edges of institutional property which are most reflective of neighboring properties as the preferred option, while recognizing that in certain circumstances greater bulk and density may be preferable to expansion beyond existing campus boundaries.

10.13.3 Encourage institutional uses and public buildings and facilities to incorporate architectural and site design that is reflective of their civic importance and that identifies their role as focal points for the community.

10.13.4 Promote active uses at the ground floor level.

4. Conformance with applicable development plans or objectives adopted by the City Council.

There are no adopted small area plans that apply to this particular parcel at this time.

5. Alternative compliance.

The Planning Commission or zoning administrator may approve alternatives to any site plan review requirement upon finding that the project meets one of three criteria required for [alternative compliance](#). Alternative compliance is requested for the following requirements:

- **Windows.** The building elevations facing the public street or on-site parking are required to contain 30 percent windows on the first floor and 10 percent windows on each level above the first floor. As shown in Table I, the applicant is meeting this requirement on some of the levels of the addition, but is requesting alternative compliance for the window requirement on the west elevation facing the parking lot, the first and third floors of the north elevation facing E 35th Street, and the first floor of the east elevation facing 42nd Avenue S. Staff recommends granting alternative compliance for this standard. Because the building faces a public street on all four sides and, due to the placement of the existing building, it is not practical to locate the addition, which includes a gymnasium, to the rear or interior of the lot. The west side elevation that does not contain windows is not directly adjacent to the surface parking area; so the impact

of not having windows would not have a negative impact on the surface parking area. Finally, it would not be practical to install windows on the first floor of the addition facing 42nd Avenue South due to the grade change. The 30 percent minimum for ground floor non-residential uses is meant to increase surveillance and activity. As an alternative, the applicant is greatly exceeding the window requirement on the floors with active classrooms that are situated above where the ground floor measurement is taken for the ordinance.

- **Landscaped yard.** The site contains a surface parking area with 51 spaces. The parking lot frontages along the west and north property lines (41st Avenue S and E 35th Street) are required to contain a landscaped yard of at least seven feet in width. The applicant is exceeding the minimum landscaped yard requirement along E 35th Street, as they are proposing to retain the 10-foot wide landscaped yard in this location. However, the applicant is proposing to provide an on-site landscaped yard that would range between zero and five feet along 41st Avenue S. Because the landscaped yard does not total seven feet or more in width along the west parking lot frontage, alternative compliance is requested. Staff finds that it would be practical to require the applicant to provide a seven-foot wide landscaped yard, of which three feet would be located in the public right-of-way, to provide a buffer between the parking area and public walkway along the west property line. Since some of the parking spaces proposed by the applicant do not currently comply with the minimum drive aisle dimensions, the applicant will also need to revise the parking area design near the west property line in order to accommodate the same number of spaces.
- **Screening of parking areas.** The zoning code requires that a six-foot screen that is at least 95 percent opaque be provided between parking areas and adjacent residential uses, and a three foot tall screen that is 60 percent opaque be located between the parking area and an adjacent street. A three-foot screen is required between the parking area and the public right-of-way along 41st Avenue S and E 35th Street. There are existing shrubs located in the required landscaped buffers in the northwest portion of the surface parking area; however, they do not provide a continuous, three-foot tall screen that is 60 percent opaque. There is no screen provided for the southernmost 180 feet of the west parking lot frontage. Staff is recommending that the applicant shall comply with the screening requirements for all new parking lot frontage areas, as recommended by staff. Staff would additionally recommend refreshing the wood mulch in the existing locations.
- **Parking spaces within 50 feet of a canopy tree.** The parking area would contain 51 vehicle parking spaces and is subject to the requirement that no parking space be located more than 50 feet from an on-site deciduous tree. The development does not meet this requirement and requires alternative compliance. The applicant has indicated that it is not practical to add additional trees in or near the surface parking area, as they would like to maintain the existing snow storage and outdoor recreation area. Staff finds that it would be practical to require compliance with this standard through restriping and the installation of tree islands.
- **Linear tree requirement.** Surface parking areas are required to have no less than one tree per each 25 linear feet of parking or loading area frontage. This requirement applies to the west and north parking lot frontages. The west parking lot frontage would be required to have 14 canopy trees and five are provided. The north parking lot frontage would be required to have four canopy trees and four are provided. Staff finds that it would be practical to require the applicant to provide no fewer than three canopy trees along the west parking lot frontage in the area where the new seven-foot landscaped area would be installed, as recommended by staff.

RECOMMENDATIONS

Recommendation of the Department of Community Planning and Economic Development for the Conditional Use Permit:

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the application for a conditional use permit to allow an addition to an existing school for students in grades 6 through 8 in the RIA Single-Family District at the property located at 3524 42nd Avenue South, subject to the following conditions:

- I. The conditional use permit shall be recorded with Hennepin County as required by Minn. Stat. 462.3595, subd. 4 before building permits may be issued or before the use or activity requiring a conditional use permit may commence. Unless extended by the zoning administrator, the conditional use permit shall expire if it is not recorded within two years of approval.

Recommendation of the Department of Community Planning and Economic Development for the Conditional Use Permit:

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the application for a conditional use permit to allow an increase in the maximum allowed height from the permitted 2.5 stories/35 feet to 3 stories/56 feet at the property located at 3524 42nd Avenue South, subject to the following conditions:

- I. The conditional use permit shall be recorded with Hennepin County as required by Minn. Stat. 462.3595, subd. 4 before building permits may be issued or before the use or activity requiring a conditional use permit may commence. Unless extended by the zoning administrator, the conditional use permit shall expire if it is not recorded within two years of approval.

Recommendation of the Department of Community Planning and Economic Development for the Variance:

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the application for a variance to reduce the east front yard setback from 20 feet to 0 feet for an accessible ramp exceeding four feet in width at the property located at 3524 42nd Avenue South.

Recommendation of the Department of Community Planning and Economic Development for the Variance:

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the application for a variance to reduce the west front yard setback from 20 feet to 0 feet for an accessible ramp exceeding four feet in width at the property located at 3524 42nd Avenue South.

Recommendation of the Department of Community Planning and Economic Development for the Variance:

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the application for a variance to increase the maximum allowed impervious surfaces from 60 percent of the site to 68 percent of the site in the RIA Single-Family District at the property located at 3524 42nd Avenue South.

Recommendation of the Department of Community Planning and Economic Development for the Variance:

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the application for a variance to increase the maximum allowed floor area ratio above 0.5 to 0.72 at the property located at 3524 42nd Avenue South.

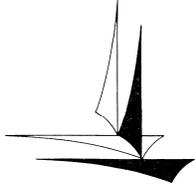
Recommendation of the Department of Community Planning and Economic Development for the Site Plan Review:

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the site plan review application for a 39,000 square foot addition to an existing building at the property located at 3524 42nd Avenue South, subject to the following conditions:

1. Approval of the final site plan, landscaping plan, elevations, trash enclosure and lighting plan by the Department of Community Planning and Economic Development.
2. All site improvements shall be completed by November 10, 2016, unless extended by the Zoning Administrator, or the permit may be revoked for non-compliance.
3. All signs shall comply with Chapter 54I of the zoning code. All signage requires a separate permit from CPED.
4. All on-site vehicle parking spaces and drive aisles shall comply with the minimum dimensions in Chapter 54I of the zoning code.
5. The applicant shall provide elevations for the proposed refuse storage enclosure and rooftop mechanical equipment.
6. The applicant shall submit revised landscaping and site plans that show a seven-foot landscaped and screened yard for the parking lot frontage along the west property line, of which three feet would be located in the public right-of-way, extending approximately to the west building entrance, and to demonstrate compliance with the requirement that all surface parking spaces shall be within 50 feet of an on-site canopy tree.
7. The applicant shall provide no fewer than three canopy trees along the west parking lot frontage.

ATTACHMENTS

1. PDR report
2. Written description and findings submitted by applicant
3. Zoning map
4. Site survey
5. Plans
6. Building elevations
7. Renderings
8. Shadow study
9. Photos
10. TDMP



Minneapolis Development Review
250 South 4th Street
Room 300
Minneapolis, MN 55415

Preliminary Development Review Report

Development Coordinator Assigned: **PATRICIA MURZYN**
(612) 673-5827
patricia.murzyn@minneapolismn.gov

| |
|----------------------------------|
| Status * |
| RESUBMISSION REQUIRED |

| | |
|-------------------------|---|
| Tracking Number: | PDR 1001237 |
| Applicant: | MINNEAPOLIS PUBLIC SCHOOLS 1250 WEST BROADWAY MINNEAPOLIS, MN 55411 |
| Site Address: | 3524 42ND AVE S |
| Date Submitted: | 27-AUG-2014 |
| Date Reviewed: | 03-SEP-2014 |

Purpose

The purpose of the Preliminary Development Review (PDR) is to provide Customers with comments about their proposed development. City personnel, who specialize in various disciplines, review site plans to identify issues and provide feedback to the Customers to assist them in developing their final site plans.

The City of Minneapolis encourages the use of green building techniques. For additional information please check out our green building web page at: http://www.ci.minneapolis.mn.us/mdr/GreenBuildingOptions_home.asp.

DISCLAIMER: *The information in this review is based solely on the preliminary site plan submitted. The comments contained in this report are preliminary ONLY and are subject to modification.*

Project Scope

Review Findings (by Discipline)

Business Licensing

- There is no Minneapolis Business Licensing review required for the proposed project.

Fire Safety

- Maintain automatic fire suppression and fire detection systems throughout the new renovation and expansion of school.
- Maintain fire department apparatus access at all times.
- For Fire Safety questions please contact Mike Raeker @ 612-673-2624 or Mike.Raeker@minneapolismn.gov.

*Approved: You may continue to the next phase of developing your project.

*Resubmission Required: You cannot move forward or obtain permits until your plans have been resubmitted and approved.

□ Zoning - Planning

- The following land use applications have been identified for this preliminary proposal. Additional (or fewer) applications may be required based on additional information or changes in future versions of the plans.
 - Conditional use permit to allow a school for students in grades 6 through 8 in an existing building in the R1A Single-Family District.
 - Conditional use permit to increase the maximum allowed height from the permitted 2.5 stories/35 feet to 3 stories/59 feet.
 - Variance to reduce the east front yard setback from 20 feet to 0 feet for a handicap ramp exceeding four feet in width.
 - Variance to reduce the west front yard setback from 20 feet to 0 feet for a handicap ramp exceeding four feet in width, and from 20 to 14.5 feet for the building wall (see Table 535-1 Permitted Obstructions in Required Yards).
 - Variance to increase the maximum allowed impervious surfaces from 60 percent of the site to 67 percent of the site in the R1A Single-Family District.
 - Variance to increase the maximum allowed floor area ratio.
 - Variance to reduce the minimum vehicle parking requirement.
 - Site plan review for a 39,000 square foot addition to existing building, including alternative compliance for the landscaping and screening requirements, the restriction on blank walls exceeding 25 feet, and the minimum window requirement which applies to any elevation that faces a public street or on-site surface parking areas.
- Please provide or clarify the following items.
 - What is the gross floor area of the building now versus proposed (not including the basement)?
 - Chapter 530, Site Plan Review, applies to the entire site and not just the areas affected by the addition. For instance, the landscaping and screening standards apply to the 41st Avenue S parking lot frontage and the plans should demonstrate compliance with these standards (or, the applicant shall request alternative compliance). As such, please provide details and dimensions for the entire site.
 - Please provide an existing tree and shrub survey (may be combined with proposed landscaping plan).
 - Please provide the square feet (in addition to or in lieu of acres) for any area dimensions required for the application.
 - How many classrooms will be in the school after the addition?
 - Please indicate the number of bicycle parking spaces provided (no less than 3 per classroom without a variance). At least 50 percent of the minimum bicycle parking requirement shall meet the standards for short-term bicycle parking, per section 541.180 of the zoning code.
 - Two small (10' by 25') loading spaces are required for this use based on its size. Please demonstrate compliance with this requirement on the site plan.
 - Please clarify the status of the proposed telecommunications building.
 - Please provide a sign plan for any proposed signage, indicating dimensions, materials, and illumination.
 - Please provide a table on the elevations that indicate the approximate final percentage of each materials proposed

□ Construction Code Services

- The plan as submitted meets the Preliminary Development Review requirements for Construction Code Services.
- A Service Availability Charge (SAC) determination will need to be submitted to the Metropolitan Council for the proposed project. Please refer to this link for more information or Contact Karon Cappaert at 651-602-1118 or karon.cappaert@metc.state.mn.us.
http://www.ci.minneapolis.mn.us/mdr/docs/sac_availability_charge.pdf.

❑ Parks - Forestry

- Contact Craig Pinkalla (612-499-9233 cpinkalla@minneapolisparcs.org) regarding any questions related to planting, removal or the process for protection of trees during construction in the city right of way.
- Effective January 1, 2014, the City of Minneapolis and the Minneapolis Park and Recreation Board adopted an update to the existing Parkland Dedication Ordinance. The adopted City of Minneapolis Parkland Dedication ordinance is located in Section 598.340 of the City's Land Subdivision ordinance:
- <http://library.municode.com/index.aspx?clientId=11490>
- As adopted, the fee in lieu of dedication for new residential units is \$1,500 per unit (affordable units excluded per ordinance) and for commercial and industrial development it is \$200 per development employee (as defined in ordinance). Any dedication fee (if required) must be paid at the time of building permit issuance.
- There is also an administration fee that is 5% of the calculated park dedication fee.
- As proposed, for your PROJECT, the calculated dedication fee is as follows:
- Park Dedication Fee Calculation =
 - Non-Residential Commercial Space = \$14,400
 - 5% of \$XX (Administration Fee) = \$ 720
 - Total Park Dedication Administrative Fee: \$15,120
- This is a preliminary calculation based on your current proposal; a final calculation will be made at the time of building permit submittal.
- For further information, please contact Patty Murzyn 612-673-5827.

❑ Right of Way

- An encroachment permit shall be required for all streetscape elements in the Public right-of-way such as: plants & shrubs, planters, tree grates and other landscaping elements (NOTE: crushed rock is not allowed in the Public right-of-way), sidewalk furniture (including bike racks and bollards), and sidewalk elements other than standard concrete walkways such as pavers, stairs, raised landings, retaining walls, access ramps, and railings (NOTE: railings may not extend into the sidewalk pedestrian area). Please contact Bob Boblett at (612) 673-2428 for further information.
- Note to the Applicant: Any elements of an earth retention system and related operations (such as construction crane boom swings) that fall within the Public right-of-way will require an encroachment permit application. If there are to be any earth retention systems which will extend outside the property line of the development then a plan must be submitted showing details of the system. All such elements shall be removed from the Public right-of-way following construction with the exception of tie-backs which may remain but must be uncoupled and de-tensioned. Please contact Bob Boblett at (612) 673-2428 for further information.

❑ Street Design

- All curb & gutter in the Public right-of-way shall be designed and constructed to City standards, curb & gutter to be City standard B624 Curb and Gutter. Please refer to the following: http://www.minneapolismn.gov/publicworks/plates/public-works_road. Add the appropriate details from the ROAD-1000 Series - Curbs and Gutters (ROAD-1003, and ROAD-1010) to the plans.

❑ Sidewalk

- The maximum width of any proposed ADA Pedestrian ramp is 6'-0"; proposed pedestrian ramps along 41st Ave. S. and 42nd Ave. S., exceed the maximum width and shall be revised accordingly.
- It is recommended to remove the mid-block pedestrian ramps
- Note to the Applicant: Any currently defective sidewalks or other concrete infrastructure within the public right of way, or any concrete infrastructure damaged during construction, must be removed and replaced.

□ Traffic and Parking

- Submit a copy of the TDMP.
 - Note to the Applicant: Please add the following notes to the site plan (Sheet C100):
 - An obstruction permit is required anytime construction work is performed in the Public right-of-way. Please contact Scott Kramer at (612) 673-2383 regarding details of sidewalk and lane closures. Log on to <http://minneapolis.mn.roway.net>. for a permit.
 - All costs for relocation and/or repair of City Traffic facilities shall be borne by the Contractor and/or Property Owner.
 - Contact Doug Maday at (612) 673-5755 prior to construction for the removal of any City of Minneapolis right of way signs that may be in the way of construction.
 - Revise Note #68 on Sheet C100 to read:
 - Contact Allan Klugman at (612) 673-2743 prior to construction for the temporary removal/temporary relocation of any City of Minneapolis signal system that may be in the way of construction.

□ Water

- The plan as submitted meets the requirements of the Public Works Water Maintenance & Distribution Division.

□ Sewer Design

- Stormwater Management: Please provide a copy of any geotechnical reports for the site. In the absence of field measured infiltration rates, the design infiltration rates from the MN Stormwater Manual (http://stormwater.pca.state.mn.us/index.php/Design_infiltration_rates.) should be used for the design of the infiltration practice.
- It is recommended that some form of pre-treatment, such as a plunge pool, sump pit, filter strip, sedimentation basin, grass channel, or a combination of these practices, be installed upstream of the filtration basin. If possible, perhaps only the low flows could be directed to the filtration basin, with the larger flows bypassing to the detention pond. This may reduce sediment buildup within the filtration basin, therefore increasing the lifespan and reducing necessary maintenance.
- The Water Quality Treatment Requirements table on Page 3 of the Stormwater Calculations does not satisfy the requirements demonstrating 70% TSS is being achieved on the site. Only a portion of the site is being directed to the filtration basin therefore only a portion of the site is being treated in the volume. Please provide water quality modeling or additional analysis demonstrating 70% TSS removal.
- It appears the site currently receives a Stormwater Utility Fee Credit. Please note with the proposed changes to the stormwater system this credit may be evaluated.
- Utility Connections: The Site Survey does not indicate the 10" storm sewer outlet pipe for the pond area that is included on the Demolition Plan and stormwater modeling. Please include on the pipe on the survey with the relevant pipe details and invert elevations. Records also indicate an existing storm sewer on the south side of the property that is now shown, please also include this and any other existing storm sewer infrastructure on the plans.
- The proposed storm sewer service connection to the City main should be core-drilled and a saddle tee fitting installed, per City of Minneapolis Standard Supplemental Specifications. Please note this on the plan.
- Non Stormwater Discharges: Detail all mechanical and non-stormwater discharges. Non-stormwater discharges are not permitted unless approved by the City of Minneapolis. Non-stormwater discharges not declared and approved will not be permitted. If there currently are none and nothing is proposed declare this status on the plans.
- For comments or questions on Public Works Surface Water & Sewers Division related requirements please contact Jeremy Strehlo, (Professional Engineer) at (612) 673-3973, or jeremy.strehlo@minneapolismn.gov.

□ Environmental Health

- In 1995 a petroleum release was detected when an underground storage tank (UST) for fuel oil was removed; see Minnesota Pollution Control Agency Leak # 8607. If the project will be disturbing soils in the area of the former UST screening of the soils should be performed to ensure no contaminated soils are encountered on the site. If impacted soil is encountered during site activities work will need to stop and notification provided to the MN State Duty officer at (615) 649-5451. Approval for removal, disposal and/or reuse of impacted soils must be must occur from the MCPA and the City of Minneapolis prior to continuing excavation activities.
- Site construction should not restricted access for maintenance, service and removal of the existing UST.
- If dewatering is required during site construction see below for city permit requirements. Subgrade structures should be designed to prevent infiltration of groundwater without the need for a permanent dewatering system being installed. If a continuously operating permanent dewatering system is needed it must be approved as part of the sanitary sewer and storm drain site plan approval prior to construction beginning.
- No construction, demolition or commercial power maintenance equipment shall be operated within the city between the hours of 6:00 p.m. and 7:00 a.m. on weekdays or during any hours on Saturdays, Sundays and state and federal holidays, except under permit. Contact Environmental Services at 612-673-3867 for permit information.
- Permits and approval are required from Environmental Services for the following activities: Temporary storage of impacted soils on site prior to disposal or reuse; Reuse of impacted soils on site; Dewatering and discharge of accumulated storm water or ground water, underground or aboveground tank installation or removal, well construction or sealing. Contact Tom Frame at 612-673-5807 for permit applications and approvals.
- A review of the project, permits issued and an inspection from Environmental Service for identification of equipment and site operations that require annual registration with the City of Minneapolis will occur for this project.

□ Historical Preservation Committee

- There is no HPC flag on this property. HPC review is not required at this time.

END OF REPORT

Site Plan Review Information

Sanford Middle School Expansion & Renovation

3524 – 42nd Avenue South
Minneapolis, MN 55406

The Marie Sanford Middle School is a 4-level building built in 1924, with a series of single level expansions in 1960. It is the intent of Minneapolis Public Schools to renovate and expand Sanford over the next 18 months, with completion anticipated for the Fall 2016 school year. A portion of the existing building will be removed; enabling a more efficient expansion to occur which better meets the needs of 21st Century learning methods. The school will be occupied during the construction period, which requires a phased sequencing plan. The majority of the renovation work within the existing building (mainly air conditioning) will occur during the summer months. The student population is grades 6, 7 & 8, with a current enrollment of 760 students. The expansion will increase the enrollment capacity to 960 students. The new expanded building plan will enable each of the floors 1, 2 & 3 to house a single grade level, with 2 teacher teams for each grade level. The school is strongly linked to the local neighborhood, and the new gymnasium will also provide needed space for community activities.



View from 42nd Avenue showing proposed new entry and addition

Project Rationale – Conditional Use Permit: Middle School within an R1A zoning district

1) The site has always been a middle school since it initially opened as Marie Sanford Middle School in 1924. The project will remove a series of inefficient spaces within the building, and the expansion will provide learning spaces reflective of current educational design standards. Changes proposed include:

- Air conditioning throughout the new and existing building.
- Additional classroom space to enable enrollment growth
- New gymnasium with separate entrance to enable off-hours community use.
- New identifiable main entrance
- New accessible entries along 41st Avenue and 42nd Avenue
- Increase storm water management of the site

- Separation of bus drop off from automobile drop off.
- 2) The school expansion and renovation will increase the enjoyment of the neighborhood by providing better access to the building, greater security features, and increased community use spaces. It will not impede the adjacent properties.
 - 3) The site has adequate utilities and other infrastructure to serve the school. The project will expand the storm water management of the site.
 - 4) Local traffic, and traffic associated with the school buses and parent pick up / drop off have been distributed to the east and west sides of the property for safety and efficiency.
 - 5) The school use is consistent with the City of Minneapolis comprehensive plan.
 - 6) The school use will conform to the regulations of the area it is located.

Project Rationale – Conditional Use Permit: Increased Height within an R1A zoning district

- 1) The existing height of the school is 52 feet and 3 stories above grade, with a basement level. The expansion will be consistent with and match these same floor levels. The proposed expansions will not increase the height of the building, or impact the adjacent properties.
- 2) Shadows caused by the school expansion will not increase the shadowing of adjacent properties.
- 3) The proposed expansion and renovation is consistent with the height and character of the existing school building.
- 4) The proposed expansion and renovation retains key elements of the existing building, most notably the distinguished entry doorways. The building itself is one of the locally identifiable landmarks for orientation within the neighborhood. The expansion will not impact views to other buildings, open spaces or water bodies.

Rationale for Variance Request – Accessibility ramp within the east (42nd Avenue) 20 foot front yard setback

The 42nd Avenue entries for the main building entrance and for the gymnasium activities area are not fully accessible. Since the building face is 20 feet from the property line (meeting the setback requirements), the ramp leading up to these entrance falls within the setback area. There is an approximate 5.5 feet of elevation change from the street level to the main floor level of the building. This change in elevation requires a significant length of sloped sidewalk to provide a smooth and easy access for those with mobility impairments.

- 1) Accessibility is a critical component of the school, and the design for accessibility is meant to be both functional and beautiful, without creating a second class circulation system for those needing assistance. Placing the sloped sidewalk within the setback area, such that it is integrated into the land contours, provides an asset and visual invitation that this school facility is fully accessible.
- 2) The placement of the accessible ramps in this area is in keeping with the intent and use of a school within the City of Minneapolis comprehensive plan.
- 3) The impact of granting the variance is minimal; it improves the enjoyment, safety and accessibility of the building to the neighborhood.

Rationale for Variance Request – Accessibility ramp and building within the west (41st Avenue) 20 foot front yard setback

The existing building (the single level 1960 addition) already is within the setback area along 41st Avenue. The existing entry to the building from the existing parking lot is not fully accessible. Since the building face at this entry is 20 feet from the property line (meeting the setback requirements), the ramp leading up to this entrance falls within the setback area. There is an approximate 2.5 feet of elevation change from the street level to the main floor level of the building. This change in elevation requires a significant length of sloped sidewalk to provide a smooth and easy access for those with mobility impairments.

- 1) The area of the building that falls within the setback area cannot be removed without significant impact to the educational use of the facility. This element of the variance should be considered a “grandfather” condition. Accessibility is a critical component of the school, and the design for accessibility is meant to be both functional and beautiful, without creating a second class circulation system for those needing assistance. Placing the sloped sidewalk within the setback area, such that it is integrated into the land contours, provides an asset and visual invitation that this school facility is fully accessible.
- 2) The placement of the accessible ramps in this area is in keeping with the intent and use of a school within the City of Minneapolis comprehensive plan.
- 3) The impact of granting the variance is minimal; it improves the enjoyment, safety and accessibility of the building to the neighborhood.

Rationale for Variance Request – Increase maximum allowable impervious surface from 60 % of the site to 67% of the site.

The impervious surface area of the existing school building and parking area will be slightly increased to provide the needed educational space, accessible sidewalks, and site circulation systems needed for a safe, effective school site.

- 1) Since the school is an urban school and the site area is not expandable, the impervious area has been increased. The on-site storm water management plan accommodates this increase.
- 2) The increase in impervious surface area does not alter the site’s intent and use as a school within the City of Minneapolis comprehensive plan.
- 3) The impact of granting the variance is minimal; it improves the operational use, the safety and the accessibility of the building to the neighborhood.

Rationale for Variance Request – Increase maximum allowable floor area ratio (FAR) of the site.

The allowed FAR on the site is 0.50. The existing building has an FAR of 0.76, which this renovation and expansion project will increase to 0.92. The full block site cannot be expanded to increase the site area. The project has removed several inefficient areas of the existing building, which are predominantly single level areas. In replacing these areas, and expanding the footprint slightly, the FAR is increased.

- 1) The FAR of the building is already greater than permitted within the residential zone of the site. This element of the variance should be considered a “grandfather” condition. Schools are generally multi-story buildings that have greater floor areas. Sanford is no different. In seeking to expand the only middle school in this area of the City, rather than construct a new middle school on another (unknown) site, the FAR has been increased on this site.
- 2) Permitting an increase in the FAR still complies with the intent and use of a school within the City of Minneapolis comprehensive plan.
- 3) The impact of granting the variance is minimal; it improves the operational use, the safety and the accessibility of the building to the neighborhood.

Rationale for Variance Request – Reduce the minimum vehicle parking requirement.

The existing site parking lot does not provide sufficient parking for the current middle school building. The parking lot, located in the northwest quadrant of the site, will be re-stripped and the geometry of the parking areas improved for safety and circulation purposes. The parking quantity will be increased by 19 spaces, from 32 spaces to 51 spaces. This increase exceeds the amount needed for the proposed expansion area, but does not provide enough additional parking to make up the shortfall that currently exists.

- 1) The parking shortfall will be reduced by the proposed expansion and renovation project. This variance should be considered a “grandfather” condition, as the shortfall existed prior the start of the project. School staff and visitors are able to park on the perimeter of the school site, which assists in the distribution of the parking. Given its daytime use, street parking can be used for residential and neighborhood use during non-school hours.
- 2) The impact of the parking variance is reduced, as this project is improving the existing condition. This is a middle school, so the students are not driving to the school, meaning that those parking are adult staff, parents and visitors. This is a reasonable accommodation with the intent and use of a school within the City of Minneapolis comprehensive plan.
- 3) The impact of granting the variance is minimal; as the school has worked hard and deliberately to cooperate with the neighborhood regarding cooperative use of its on-site and street parking areas.



Aerial photo of Sanford School site from South.



East Elevation - current



Current Main entry – South Elevation – 35th Street



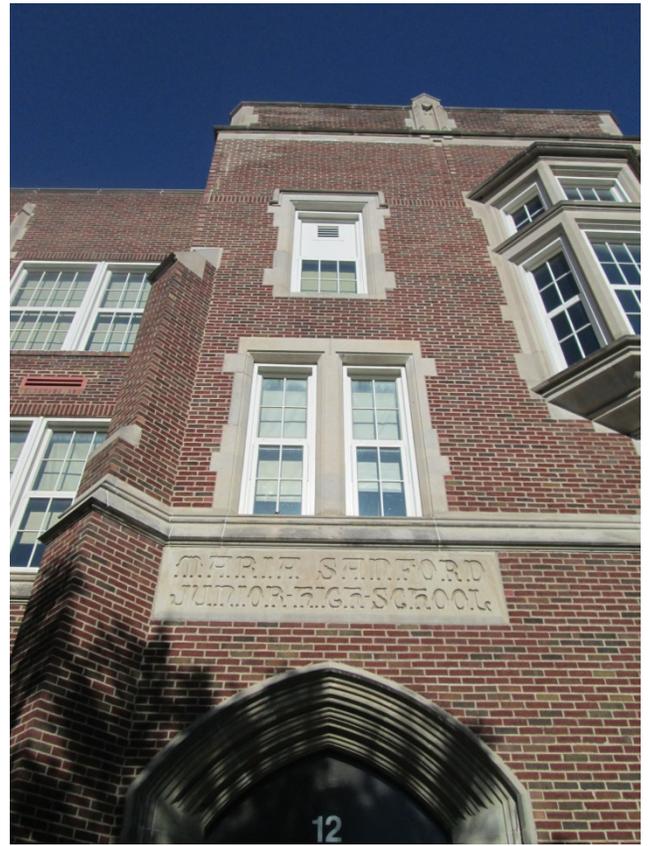
Sanford School, circa 1926.



Current North Elevation – 36th Street



Current South Elevation – from corner of 35th & 41st



Signage above existing entries on East & West doors.



Current West elevation – 41st Avenue - partial



View from Northeast showing proposed gymnasium and new classroom addition

October 21, 2014

Sanford School – Landscape Plan Response

General

Parking lot Screening.

1. 41st Ave S Parking Lot frontage - Existing parking lot has a 7' landscape area with existing trees and shrubs to remain as the required parking lot screen. See photo #1
2. 41st Ave S Parking Lot frontage- In front of loading dock and handicap parking. This is an existing condition where no landscape buffer is present due to safety and trash/delivery truck movement requirements that prevent the creation of a 7 foot landscape area.
3. 35th Street Parking lot Frontage has a 19" landscape zone and the existing trees and shrubs will remain as the required parking lot screen. See Photo #2
4. Interior: There are no interior planning islands proposed to accommodate snow plow efforts, maximize parking and accommodate (3) existing basketball hoops.
5. Trees have been added along the eastern edge of the Parking lot to provide the 50' minimum distance for the eastern most row of parking. The trees are located to not interfere with basketball hoops and snow storage.

Site Landscape Area: 51,248 SF

1 Tree/ 500 SF = 102.5 Trees

Existing Trees to Remain: 64 Proposed Trees: 40

1 Shrub/ 100 SF = 512 shrubs

Existing Shrubs to Remain: 202 Proposed Shrubs: 301

Submitted by:
Joan MacLeod, RLA
Damon Farber Associates

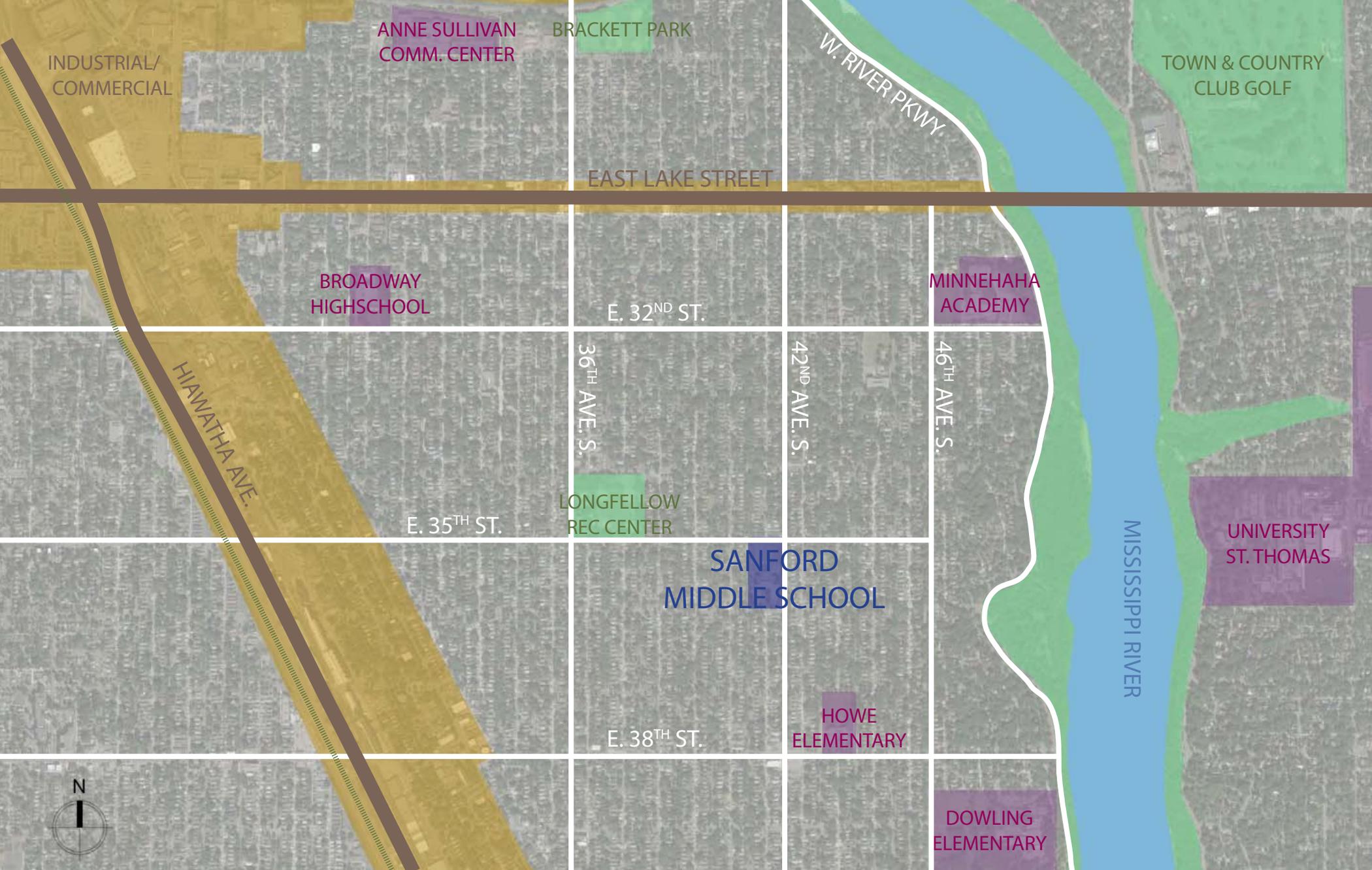


Photo #1
35th Street – 19' Landscape Strip. Parking Lot Screening to remain



Photo #2

41 st Ave S – Parking Lot Screening – 8' Landscape Planting Strip. Existing shrubs to remain



CONTEXT





EXISTING BUILDING:

- MECHANICAL UPGRADES
- RENOVATION OF EXISTING SPACES FOR RELOCATED CLASSROOM PROGRAMS
- IMPROVED LIGHTING IN CORRIDORS

DEMOLITION OF BAND, CHOIR AND DRAMA ROOMS (1 STORY BUILDING)

DEMOLITION OF OFFICES, CLASSROOMS AND GYMNASIUM (3 STORY BUILDING)

MAIN ENTRANCE (ACCESSIBLE)



EXISTING PLAN



MINNEAPOLIS PUBLIC SCHOOLS
Urban Education. Global Citizens.



miller dunwiddie
ARCHITECTURE

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.

Signature
xxx
Name
Date License #

PROJECT:

Sanford Middle School
Expansion &
Renovations

Minneapolis Public Schools,
OP# 2767
3524 42nd Ave S, Minneapolis
MN 55406

MDA Project #MPS1401

REVISED:

Mark Date Issue

PDR SUBMITTAL

NOT FOR
CONSTRUCTION

AUGUST 18, 2014

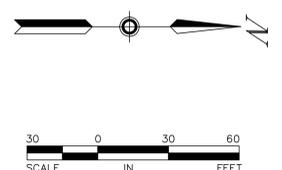
I hereby certify that this survey, plan, or report was prepared by me or under my direct supervision and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota.

Dated this 1st day of April, 2014.

SUNDE LAND SURVEYING, LLC.

By: *Mark S. Hanson*
Mark S. Hanson, P.L.S. Minn. Lic. No. 15480

| | | |
|---|----------------|------------------|
| Revision | By | Date |
| 04/01/2014 | CWJ | |
| Drawing Title: | | |
| LOCATION, TOPOGRAPHIC AND UTILITY SURVEY FOR: MILLER DUNWIDDIE ARCHITECTS (SANFORD MIDDLE SCHOOL) | | |
|  | | |
| Main Office: 9001 East Bloomington Freeway (139W) • Suite 118 Bloomington, Minnesota 55420-3435 952-881-2405 (Fax: 952-888-9526) West Office: Mankato, North Dakota 701-663-2562 | | |
| Project: 2005-139-a | Blk/Pg: 704/19 | Date: 04/01/2014 |
| Township: 28 | Range: 23 | Section: 6 |
| File: 2005139A001.dwg | | Sheet: 1 of 1 |



BENCHMARKS
DATUM NGVD 1929

- 1.) Top of top nut of fire hydrant northwest quadrant of 35th Street E. and 41st Avenue S.
Elevation = 826.50 feet
- 2.) Top of top nut of fire hydrant northwest quadrant of 35th Street E. and 42nd Avenue S.
Elevation = 826.06 feet
- 3.) Top of top nut of fire hydrant northwest quadrant of 36th Street E. and 41st Avenue S.
Elevation = 826.66 feet
- 4.) Top of top nut of fire hydrant northwest quadrant of 36th Street E. and 42nd Avenue S.
Elevation = 823.90 feet

LEGEND

- O Denotes iron monument set marked with P.L.S. No. 15480
- AC Denotes air conditioner
- AGP Denotes above ground pipe
- SBH Denotes basketball hoop
- BE Denotes building entrance
- BR Denotes bike rack
- CB Denotes catch basin
- CDS Denotes civil defense siren
- CM Denotes city monument
- COL Denotes building column
- FH Denotes fire hookup
- FP Denotes flag pole
- G Denotes gutter elevation
- GM Denotes gas meter
- GP Denotes guard post
- GW Denotes guy wire
- HCR Denotes handicap ramp
- HYD Denotes fire hydrant
- INV Denotes structure invert elevation
- MC Denotes metal cover
- MH Denotes manhole
- OHE Denotes overhead electric line
- OHTV Denotes overhead cable tv line
- PP Denotes power pole
- PPLP Denotes power and light pole
- PVC Denotes plastic pipe
- RCP Denotes reinforced concrete pipe
- SAN Denotes sanitary manhole
- SAN S Denotes sanitary sewer
- SMH Denotes storm manhole
- ST S Denotes storm sewer
- STA Denotes survey control station
- TC Denotes top of concrete curb elevation
- TCS Denotes traffic control sign
- UGC Denotes underground communication line
- UGE Denotes underground electric line
- VCP Denotes vitrified clay pipe
- W Denotes water line
- WMH Denotes water manhole
- WV Denotes water valve
- BAS Denotes Basswood tree
- BIR Denotes Birch tree
- BOX Denotes Boxelder tree
- CED Denotes Cedar tree
- GINK Denotes Ginkgo tree
- LOC Denotes Locust tree
- MPL Denotes Maple tree
- PINR Denotes Red Pine tree
- PINW Denotes White Pine tree
- SHB Denotes shrub
- SPCG Denotes Colorado Green Spruce tree

DESCRIPTION OF PROPERTY SURVEYED

Lot 178, Block, 8, AUDITOR'S SUBDIVISION NUMBER 236, Hennepin County, Minnesota.

PLAT RECORDING INFORMATION

The plot of Auditor's Subdivision Number 236 was filed of record on June 24, 1929, as Document No. 1546169.

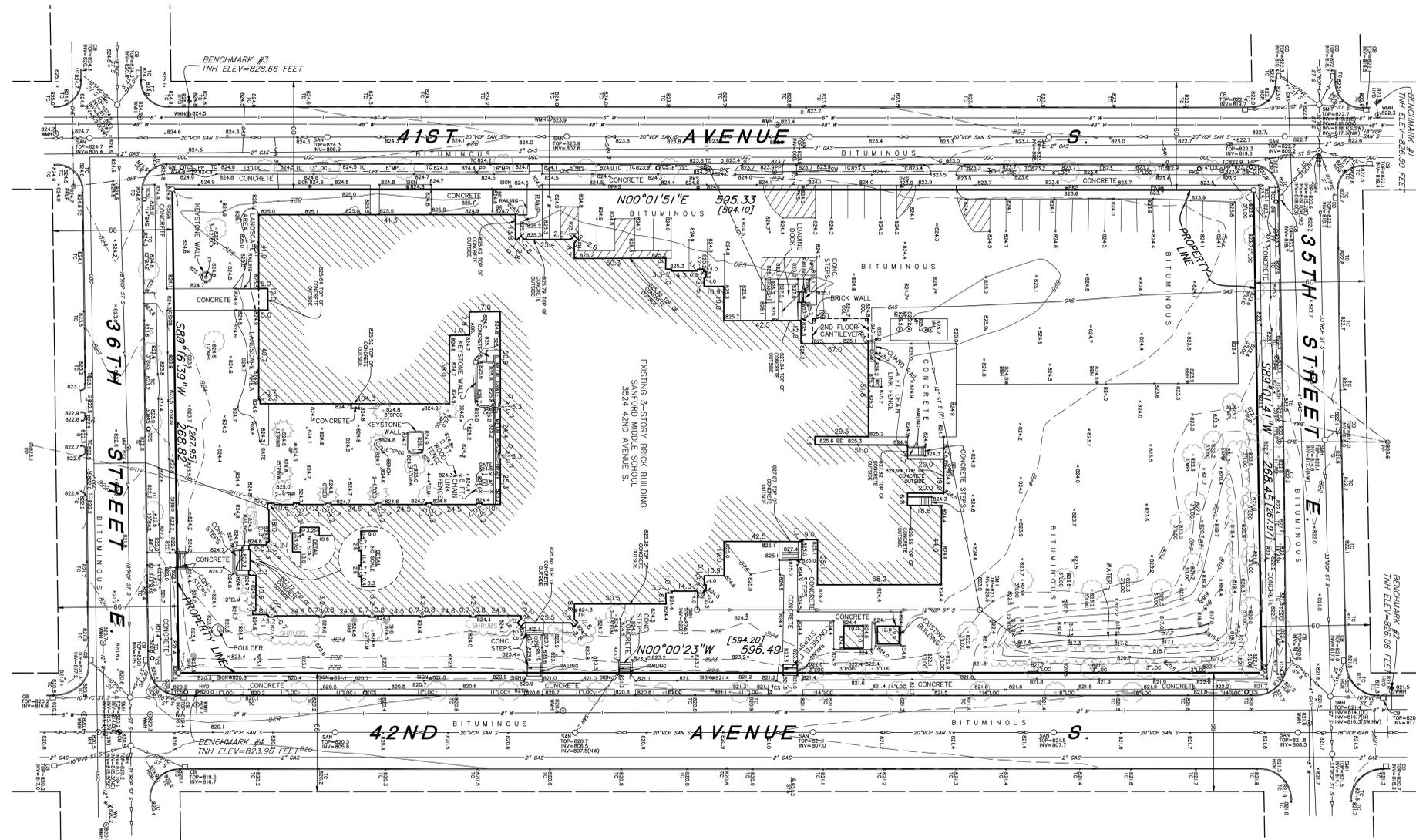
[] Bearings and/or dimensions listed within brackets are per plat or record documents.

GENERAL NOTES

- 1.) Survey coordinate basis: Hennepin County coordinates.

UTILITY NOTES

- 1.) Utility information from plans and markings was combined with observed evidence of utilities to develop a view of the underground utilities shown hereon. However, lacking excavation, the exact location of underground features cannot be accurately, completely and reliably depicted. Where additional or more detailed information is required, excavation may be necessary.
- 2.) Other underground utilities of which we are unaware may exist. Verify all utilities critical to construction or design.
- 3.) Some underground utility locations are shown as marked onsite by those utility companies whose locators responded to our Gopher State One Call, ticket numbers 50483658 and 140790096.
- 4.) Contact GOPHER STATE ONE CALL at 651-454-0002 (800-252-1166) for precise onsite location of utilities prior to any excavation.



DRAWING NUMBER

G002

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Signature: Rhonda S. Pierce

Name: Rhonda S. Pierce

DATE: OCTOBER 1, 2014

PROJECT: Sanford Middle School Expansion & Renovations

PROJECT: Sanford Middle School Expansion & Renovations

Sanford Middle School Expansion & Renovations
Minneapolis Public Schools
MPS1401

3524 42nd Ave S,
Minneapolis MN 55406

REVISIONS:

95% CONSTRUCTION DOCUMENTS

NOT FOR CONSTRUCTION

COMM. NO.: 14-005

Date: OCTOBER 1, 2014

Drawn: ALA

CHECKED: RSP

© 2014 Miller Dunwiddie Architecture, Inc.

DRAWING TITLE:

SITE LAYOUT AND PAVING PLAN

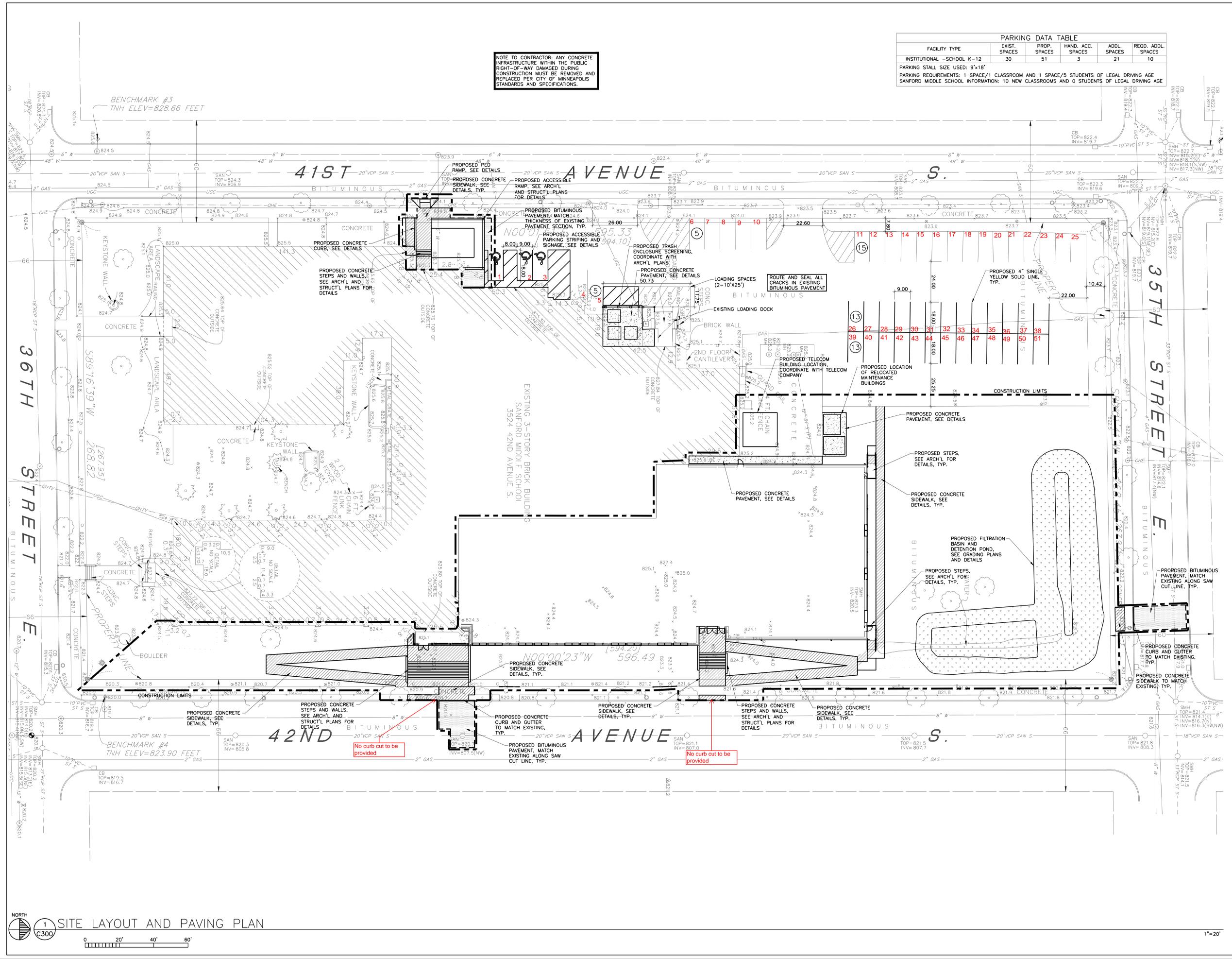
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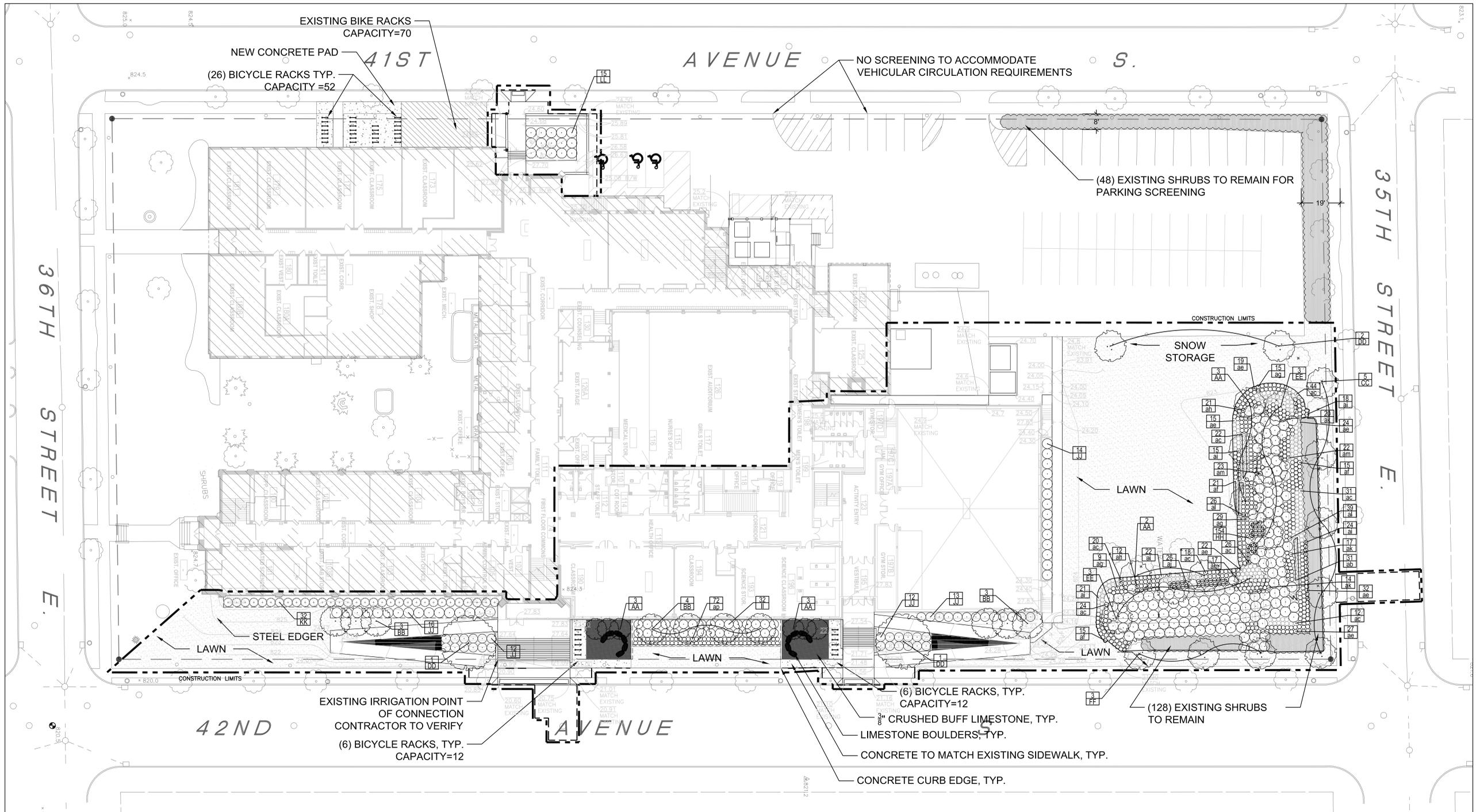
C300

| PARKING DATA TABLE | | | | | |
|-----------------------------|---------------|--------------|-------------------|--------------|--------------------|
| FACILITY TYPE | EXIST. SPACES | PROP. SPACES | HAND. ACC. SPACES | ADDL. SPACES | RECD. ADDL. SPACES |
| INSTITUTIONAL - SCHOOL K-12 | 30 | 51 | 3 | 21 | 10 |

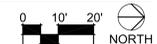
PARKING STALL SIZE USED: 9'x18'
PARKING REQUIREMENTS: 1 SPACE/1 CLASSROOM AND 1 SPACE/5 STUDENTS OF LEGAL DRIVING AGE
SANFORD MIDDLE SCHOOL INFORMATION: 10 NEW CLASSROOMS AND 0 STUDENTS OF LEGAL DRIVING AGE

NOTE TO CONTRACTOR: ANY CONCRETE INFRASTRUCTURE WITHIN THE PUBLIC RIGHT-OF-WAY DAMAGED DURING CONSTRUCTION MUST BE REMOVED AND REPLACED PER CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS.





LANDSCAPE LAYOUT PLAN
1" = 20'-0"



ALL PLANTED AREAS TO HAVE AN AUTOMATIC UNDERGROUND IRRIGATION SYSTEM. CONTRACTOR TO SUBMIT IRRIGATION PLAN.

PLAN LEGEND

- EXISTING TREE TO REMAIN SEE CIVIL SHEET
- SHADE/ORNAMENTAL TREE
- DECIDUOUS SHRUB
- CONIFEROUS SHRUB
- PERENNIAL
- TURF
- LIMESTONE BOULDER ON CRUSHED GRANITE
- I BIKE RACK - CAPACITY
- - - CONSTRUCTION LIMITS
- PROPERTY LINE

| ALT. SYM | QTY | COMMON NAME | SCIENTIFIC NAME | SIZE | ROOT COND. | ROOT BALL / CONT. SIZE | COMMENTS |
|------------------------------------|-----|------------------------------|---|----------|------------|-------------------------------|----------|
| (39) DECIDUOUS TREES | | | | | | | |
| AA | 11 | ASPEN QUAKING | <i>Populus tremuloides</i> | 2.5" CAL | B&B | 28" MIN. DIA. STRAIGHT LEADER | |
| BB | 10 | BIRCH RENAISSANCE REFLECTION | <i>Betula papyrifera 'Oscor'</i> | 2" CAL | B&B | 28" MIN. DIA. CLUMP | |
| CC | 5 | MAPLE RED SUNSET | <i>Acer rubrum 'Franksred'</i> | 2.5" CAL | B&B | 28" MIN. DIA. STRAIGHT LEADER | |
| DD | 4 | KENTUCKY ESPRESSO COFFEETREE | <i>Gymnocladus dioica 'Espresso'</i> | 2.5" CAL | B&B | 28" MIN. DIA. MALE SELECTION | |
| EE | 6 | OAK SWAMP WHITE | <i>Quercus bicolor</i> | 2.5" CAL | B&B | 28" MIN. DIA. STRAIGHT LEADER | |
| FF | 3 | HACKBERRY | <i>Celtis occidentalis</i> | 2.5" CAL | B&B | 28" MIN. DIA. STRAIGHT LEADER | |
| (300) DECIDUOUS SHRUBS | | | | | | | |
| HH | 154 | DOGWOOD ISANTI | <i>Cornus sericea 'Isanti'</i> | 30" HGT | POT | #2 CONT. PLANT 4' O.C. | |
| IJ | 32 | WINTERBERRY JIM DANDY | <i>Ilex verticillata 'Atergroup'</i> | 30" HGT | POT | #2 CONT. PLANT 4' O.C. | |
| JJ | 67 | WINTERBERRY RED SPRITE | <i>Ilex verticillata 'Red Sprite'</i> | 30" HGT | POT | #2 CONT. PLANT 4' O.C. | |
| KK | 32 | JUNIPER MINT JULEP | <i>Juniperus chinensis 'Mint Julep'</i> | 30" HGT | POT | #2 CONT. PLANT 4' O.C. | |
| LL | 15 | HONEYSUCKLE DWARF BUSH | <i>Dierilla lonicera</i> | 30" HGT | POT | #2 CONT. PLANT 3' O.C. | |
| (808) RAINGARDEN PERENNIALS | | | | | | | |
| ab | 31 | KARL FOERSTER REED GRASS | <i>Calamagrostis x acutiflora 'Karl Foerster'</i> | | POT | #3 CONT. PLANT 24" O.C. | |
| ac | 197 | SWITCH GRASS RED | <i>Panicum virgatum 'Shenandoah'</i> | | POT | #1 CONT. PLANT 24" O.C. | |
| ag | 139 | SNEEZEWEED MARDI GRAS | <i>Helenium 'Mardi Gras'</i> | | POT | #1 CONT. PLANT 24" O.C. | |
| af | 51 | VERVAIN BLUE | <i>Verbena hastata</i> | | POT | #1 CONT. PLANT 18" O.C. | |
| aq | 53 | CONEFLOWER GOLDSTRUM | <i>Rudbeckia 'Goldstrum'</i> | | POT | #1 CONT. PLANT 18" O.C. | |
| ah | 33 | CONEFLOWER PURPLE | <i>Echinacea purpurea 'Rubinstem'</i> | | POT | #1 CONT. PLANT 18" O.C. | |
| aj | 100 | IRIS BLUEFLAG | <i>Iris versicolor</i> | | POT | #3 CONT. PLANT 18" O.C. | |
| ak | 26 | IRONWEED | <i>Vernonia fasciculata</i> | | POT | #1 CONT. PLANT 18" O.C. | |
| al | 51 | JOE PYE WEED | <i>Eupatorium 'Phantom'</i> | | POT | #1 CONT. PLANT 18" O.C. | |
| am | 65 | MILKWEED SWAMP | <i>Asclepias incarnata</i> | | POT | #3 CONT. PLANT 24" O.C. | |
| an | 62 | RUE TALL MEADOW | <i>Thalictrum pubescens</i> | | POT | #3 CONT. PLANT 24" O.C. | |
| (72) GRASSES | | | | | | | |
| ap | 72 | TUFTED HAIR GRASS | <i>Deschampsia cespitosa 'Schottland'</i> | | POT | #1 CONT. PLANT 24" O.C. | |

PLANT SCHEDULE

GENERAL NOTES

- CONTRACTOR SHALL INSPECT THE SITE AND BECOME FAMILIAR WITH EXISTING CONDITIONS RELATING TO THE NATURE AND SCOPE OF WORK.
- CONTRACTOR SHALL VERIFY PLAN LAYOUT AND BRING TO THE ATTENTION OF THE LANDSCAPE ARCHITECT DISCREPANCIES WHICH MAY COMPROMISE THE DESIGN OR INTENT OF THE LAYOUT.
- CONTRACTOR SHALL ASSURE COMPLIANCE WITH APPLICABLE CODES AND REGULATIONS GOVERNING THE WORK AND MATERIALS SUPPLIED.
- CONTRACTOR SHALL PROTECT EXISTING ROADS, CURBS/GUTTERS, TRAILS, TREES, LAWNS AND SITE ELEMENTS DURING CONSTRUCTION OPERATIONS. DAMAGE TO SAME SHALL BE REPAIRED AT NO ADDITIONAL COST TO THE OWNER.
- CONTRACTOR SHALL VERIFY ALIGNMENT AND LOCATION OF UNDERGROUND AND ABOVE GRADE UTILITIES AND PROVIDE THE NECESSARY PROTECTION FOR SAME BEFORE CONSTRUCTION BEGINS (MINIMUM 10' CLEARANCE).
- CONTRACTOR SHALL COORDINATE THE PHASES OF CONSTRUCTION AND PLANTING INSTALLATION WITH OTHER CONTRACTORS WORKING ON SITE.
- UNDERGROUND UTILITIES SHALL BE INSTALLED SO THAT TRENCHES DO NOT CUT THROUGH ROOT SYSTEMS OF EXISTING TREES TO REMAIN.
- EXISTING CONTOURS, TRAILS, VEGETATION, CURB/GUTTER AND OTHER ELEMENTS ARE BASED UPON INFORMATION SUPPLIED TO THE LANDSCAPE ARCHITECT BY OTHERS. CONTRACTOR SHALL VERIFY DISCREPANCIES PRIOR TO CONSTRUCTION AND NOTIFY LANDSCAPE ARCHITECT OF SAME.
- HORIZONTAL AND VERTICAL ALIGNMENT OF PROPOSED WALKS, TRAILS OR ROADWAYS ARE SUBJECT TO FIELD ADJUSTMENT REQUIRED TO CONFORM TO LOCALIZED TOPOGRAPHIC CONDITIONS AND TO MINIMIZE TREE REMOVAL AND GRADING. CHANGES IN ALIGNMENT AND GRADES MUST BE APPROVED BY THE LANDSCAPE ARCHITECT PRIOR TO IMPLEMENTATION.
- CONTRACTOR SHALL REVIEW THE SITE FOR DEFICIENCIES IN SITE CONDITIONS WHICH MIGHT NEGATIVELY AFFECT PLANT ESTABLISHMENT, SURVIVAL OR WARRANTY. UNDESIRABLE SITE CONDITIONS SHALL BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT PRIOR TO COMMENCEMENT OF WORK.
- CONTRACTOR IS RESPONSIBLE FOR ONGOING MAINTENANCE OF NEWLY INSTALLED MATERIALS UNTIL TIME OF SUBSTANTIAL COMPLETION. REPAIR OF ACTS OF VANDALISM OR DAMAGE WHICH MAY OCCUR PRIOR TO SUBSTANTIAL COMPLETION SHALL BE THE RESPONSIBILITY OF THE LANDSCAPE CONTRACTOR.
- EXISTING TREES OR SIGNIFICANT SHRUB MASSINGS FOUND ON SITE SHALL BE PROTECTED AND SAVED UNLESS NOTED TO BE REMOVED OR ARE LOCATED IN AN AREA TO BE GRADED. QUESTIONS REGARDING EXISTING PLANT MATERIAL SHALL BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT PRIOR TO REMOVAL.
- EXISTING TREES TO REMAIN, UPON DIRECTION OF LANDSCAPE ARCHITECT, SHALL BE FERTILIZED AND PRUNED TO REMOVE DEAD WOOD, DAMAGED AND RUBBING BRANCHES.
- CONTRACTOR SHALL PREPARE AND SUBMIT A WRITTEN REQUEST FOR THE SUBSTANTIAL COMPLETION INSPECTION OF LANDSCAPE AND SITE IMPROVEMENTS PRIOR TO SUBMITTING FINAL PAY REQUEST.
- CONTRACTOR SHALL PREPARE AND SUBMIT REPRODUCIBLE AS-BUILT DRAWING(S) OF

LANDSCAPE INSTALLATION, IRRIGATION AND SITE IMPROVEMENTS UPON COMPLETION OF CONSTRUCTION INSTALLATION AND PRIOR TO SUBSTANTIAL COMPLETION.

GRADING NOTES

- GRADING LIMITS ARE DEFINED AS THE JUNCTURE OF PROPOSED GRADE WITH EXISTING GRADE UNLESS NOTED OTHERWISE.
- GRADING LIMITS AND LIMITS OF WORK SHOWN ON PLAN ARE ONLY APPROXIMATE AND MAY BE ADJUSTED IN FIELD BY LANDSCAPE ARCHITECT. WORK OUTSIDE OF THESE LIMITS WILL BE DONE AT LANDSCAPE CONTRACTORS EXPENSE UNLESS DIRECTED BY LANDSCAPE ARCHITECT OR OWNER IN WRITING.

PLANTING NOTES

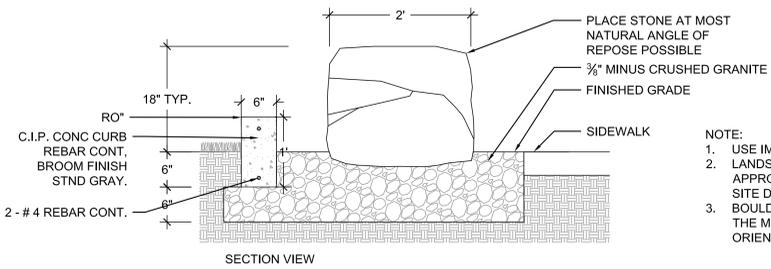
- STAKE PROPOSED PLANTING LOCATIONS PER PLAN FOR REVIEW AND APPROVAL BY LANDSCAPE ARCHITECT PRIOR TO INSTALL.
- PLANT MATERIAL SHALL COMPLY WITH THE CURRENT EDITION OF THE AMERICAN STANDARD FOR NURSERY STOCK, ANSI Z60.1. UNLESS NOTED OTHERWISE, DECIDUOUS SHRUBS SHALL HAVE AT LEAST 5 CANES AT THE SPECIFIED HEIGHT. ORNAMENTAL TREES SHALL HAVE NO "V" CROTCHES AND SHALL BEGIN BRANCHING NO LOWER THAN 3' FEET ABOVE THE ROOT BALL. STREET AND BOULEVARD TREES SHALL BEGIN BRANCHING NO LOWER THAN 6' ABOVE PAVED SURFACE.
- INSTALL PLANT MATERIAL ONCE FINAL GRADING AND CONSTRUCTION HAS BEEN COMPLETED IN THE IMMEDIATE AREA.
- INSTALL PLANT MATERIALS PER PLANTING DETAILS.
- SUBSTITUTION REQUESTS FOR PLANT MATERIAL TYPE & SIZE SHALL BE SUBMITTED TO THE LANDSCAPE ARCHITECT FOR CONSIDERATION PRIOR TO BIDDING. ALL SUBSTITUTIONS AFTER BIDDING MUST BE APPROVED BY LANDSCAPE ARCHITECT AND ARE SUBJECT TO CONTRACT ADJUSTMENTS.
- ADJUSTMENTS IN LOCATION OF PROPOSED PLANT MATERIALS MAY BE NEEDED IN FIELD. LANDSCAPE ARCHITECT MUST BE NOTIFIED PRIOR TO ADJUSTMENT OF PLANTS.
- FERTILIZE PLANT MATERIAL UPON INSTALLATION WITH DRIED BONE MEAL, OTHER APPROVED FERTILIZER MIXED IN WITH THE PLANTING SOIL PER THE MANUFACTURER'S INSTRUCTIONS OR MAY BE TREATED FOR SUMMER AND FALL INSTALLATION WITH AN APPLICATION OF GRANULAR 10-0-5 OF 12 OZ. PER 2.5" CALIPER TREE AND 8 OZ. PER SHRUB WITH AN ADDITIONAL APPLICATION OF 10-0-10 THE FOLLOWING SPRING IN THE TREE SAUCER.
- INSTALL 18" DEPTH OF PLANTING SOIL IN AREAS RECEIVING GROUND COVER, PERENNIALS, & ANNUALS. PLANTING SOIL SHALL CONSIST OF MDDOT 3877-C MODIFIED TO CONTAIN A MAXIMUM OF 30% SAND, A PH OF 7.1 MAX, OR AS OTHERWISE SPECIFIED IN THE PROJECT MANUAL.
- TREE WRAPPING MATERIAL SHALL BE TWO-WALLED PLASTIC SHEETING APPLIED FROM TRUNK FLARE TO FIRST BRANCH. WRAP SMOOTH-BARKED DECIDUOUS TREES

PLANTED IN THE FALL PRIOR TO DECEMBER 1 AND REMOVE WRAPPING AFTER MAY 1.

- 3/16" THICK STEEL EDGER TO BE USED TO CONTAIN SHRUBS, PERENNIALS AND ANNUALS WHERE PLANTING BED MEETS SOD UNLESS OTHERWISE NOTED.
- APPLY PRE-EMERGENT HERBICIDE (PREEN OR APPROVED EQUAL) IN ANNUAL, PERENNIAL AND SHRUB BEDS FOLLOWED BY SHREDDED HARDWOOD MULCH. REFER TO SPECIFICATIONS FOR ADDITIONAL INFORMATION REGARDING USE OF HERBICIDES.
- INSTALL 3" DEEP SHREDDED HARDWOOD MULCH RINGS AT CONIFEROUS & DECIDUOUS TREES WITH NO MULCH IN DIRECT CONTACT WITH TREE TRUNK.
- INSTALL 2" DEEP SHREDDED HARDWOOD MULCH RINGS AT SHRUB PLANTING AREAS WITH NO MULCH IN DIRECT CONTACT WITH SHRUB STEMS.
- INSTALL 2" DEEP FINELY SHREDDED MULCH OR 1" DEEP SHREDDED HARDWOOD MULCH IN PERENNIAL PLANTING BEDS. REMOVE ALL MULCH FROM STEMS OF PERENNIALS - PLANT STEMS SHOULD NOT BE IN DIRECT CONTACT WITH MULCH.
- WARRANTY NEW PLANT MATERIAL THROUGH ONE CALENDAR YEAR FROM THE DATE OF SUBSTANTIAL COMPLETION. NO PARTIAL ACCEPTANCE WILL BE CONSIDERED.
- SPRING PLANT MATERIAL INSTALLATION IS FROM THE APRIL 15 TO JUNE 15.
- FALL CONIFEROUS PLANTING IS ACCEPTABLE FROM AUGUST 21 TO SEPTEMBER 30.
- FALL DECIDUOUS PLANTING IS ACCEPTABLE FROM AUGUST 15 UNTIL NOVEMBER 15.
- ADJUSTMENTS TO PLANTING DATES MUST BE APPROVED IN WRITING BY THE LANDSCAPE ARCHITECT.

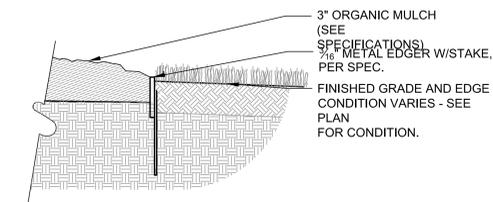
IRRIGATION NOTES

- CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING AN IRRIGATION LAYOUT PLAN AND SPECIFICATION AS PART OF THE SCOPE OF WORK. SUBMIT LAYOUT PLAN AND SPECIFICATIONS FOR APPROVAL BY THE LANDSCAPE ARCHITECT PRIOR TO ORDER AND/OR CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT SODDED/SEEDED AND PLANTED AREAS ARE IRRIGATED PROPERLY, INCLUDING THOSE AREAS DIRECTLY AROUND AND ABUTTING BUILDING FOUNDATION.
- CONTRACTOR SHALL VERIFY (EXISTING) IRRIGATION SYSTEM LAYOUT AND CONFIRM COMPLETE LIMITS OF IRRIGATION PRIOR TO SUPPLYING SHOP DRAWINGS.
- CONTRACTOR SHALL CONTACT LANDSCAPE ARCHITECT FOR INSPECTION AND APPROVAL OF ALL AREAS RECEIVING DRIP IRRIGATION PRIOR TO INSTALLATION OF ANY MULCH.
- CONTRACTOR SHALL PROVIDE THE OWNER WITH AN IRRIGATION SCHEDULE APPROPRIATE TO THE PROJECT SITE CONDITIONS AND TO PLANTED MATERIAL GROWTH REQUIREMENTS.
- CONTRACTOR SHALL ENSURE THAT SOIL CONDITIONS AND COMPACTON ARE ADEQUATE TO ALLOW FOR PROPER DRAINAGE AROUND THE CONSTRUCTION SITE. UNDESIRABLE CONDITIONS SHALL BE BROUGHT TO THE ATTENTION OF THE LANDSCAPE ARCHITECT PRIOR TO BEGINNING OF WORK. IT SHALL BE THE LANDSCAPE CONTRACTOR'S RESPONSIBILITY TO ENSURE PROPER SURFACE AND SUBSURFACE DRAINAGE IN PLANTING AREAS.

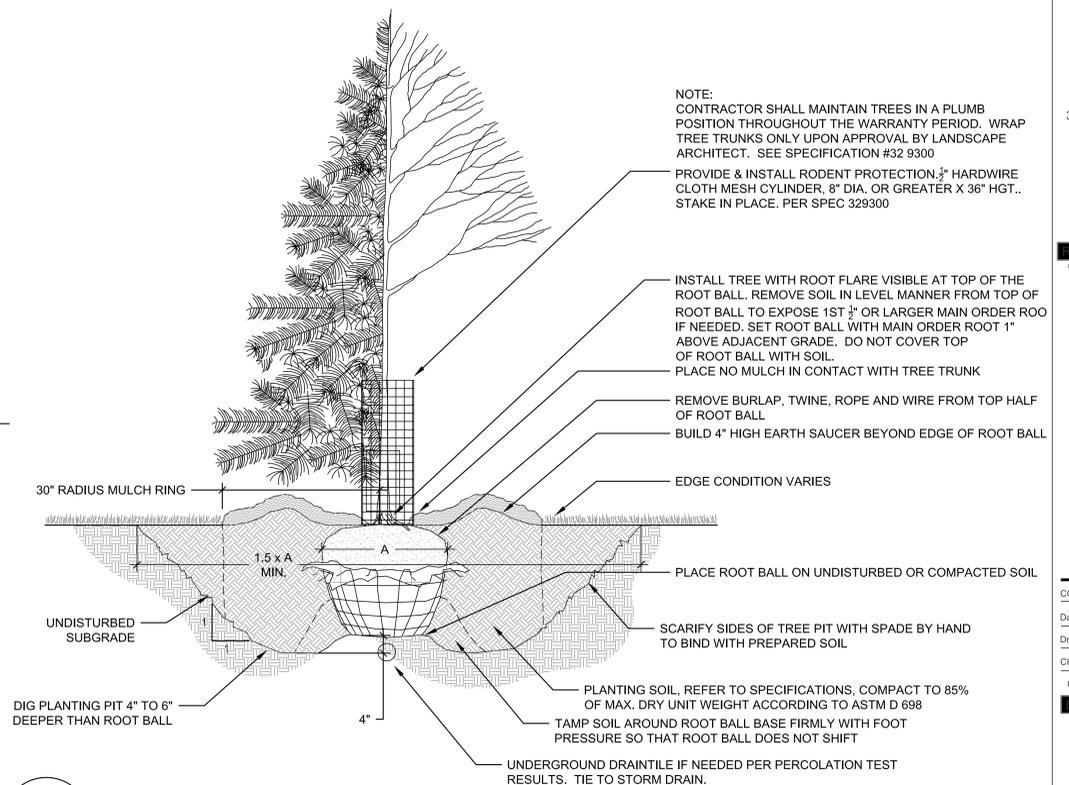


- NOTE:**
- USE IMPORTED, SPECIFIED BOULDERS.
 - LANDSCAPE ARCHITECT SHALL APPROVE BOULDERS IN YARD BEFORE SITE DELIVERY.
 - BOULDERS SHALL BE INSTALLED WITH THE MOST NATURAL APPEARING ORIENTATION.

1
L002 CRUSHED STONE & BOULDER DETAIL & CONCRETE CURB EDGE
SCALE 1"=1'-0"



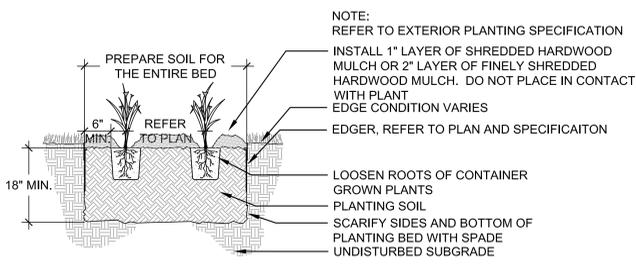
2
L002 STEEL EDGER
SCALE 1-1/2" = 1'-0"



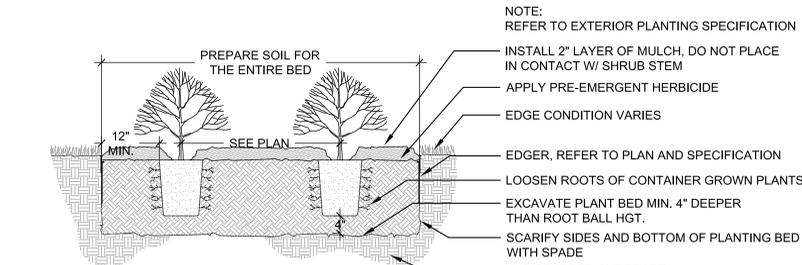
- NOTE:**
- CONTRACTOR SHALL MAINTAIN TREES IN A PLUMB POSITION THROUGHOUT THE WARRANTY PERIOD. WRAP TREE TRUNKS ONLY UPON APPROVAL BY LANDSCAPE ARCHITECT. SEE SPECIFICATION #32 9300
- PROVIDE & INSTALL RODENT PROTECTION 3/4" HARDWARE CLOTH MESH CYLINDER, 8" DIA. OR GREATER X 36" HGT., STAKE IN PLACE. PER SPEC 323900

5
L002 TREE PLANTING DETAIL
SCALE 3/4" = 1'-0"

NOTES



3
L002 PERENNIAL PLANTING DETAIL
SCALE 3/4" = 1'-0"



4
L002 SHRUB PLANTING DETAIL
SCALE 3/4" = 1'-0"

CONSTRUCTION SEQUENCING AND INSPECTION OF EROSION AND SEDIMENT CONTROL PRACTICES FOR STORMWATER MANAGEMENT SYSTEMS

NOTES

- A. APPLY AND ACQUIRE NPDES PERMIT. CONTRACTOR SHALL FOLLOW ALL REQUIREMENTS WITHIN STORM WATER POLLUTION PREVENTION PLAN (SWPPP).
- B. CONTRACTOR, PROPERTY OWNER OR RESPONSIBLE PARTY SHALL CONTACT MINNEAPOLIS SURFACE WATERS AND SEWERS 48 HOURS PRIOR TO ANY EXCAVATION OR CONSTRUCTION RELATED TO OR IN THE LOCATION OF THE PROPOSED STORMWATER MANAGEMENT BMP. CONTACT PAUL CHELSEN 612-673-2406 OR PAUL.CHELSEN@MINNEAPOLIS.MN.GOV.
- C. INSTALL EROSION CONTROL AND TREE PROTECTION ALONG PERIMETER OF SITE. INSTALL EROSION CONTROL INSERTS AT ALL CATCH BASINS AND MANHOLES AND CONSTRUCT ROCK CONSTRUCTION ENTRANCE. SEE EROSION CONTROL NOTES FOR FURTHER INSTRUCTION.
- D. PROCEED WITH SITE DEMOLITION, GRADING AND CONSTRUCTION.
- E. MAINTAIN EROSION CONTROL BMP'S AS OUTLINED ON DRAWING AND WITHIN SWPPP.
- F. ALL SITE WORK SHOULD BE COMPLETE PRIOR TO WORK ON THE INFILTRATION AREAS BEING STARTED TO THE EXTENT POSSIBLE. IF CONSTRUCTION OF THESE AREAS NEEDS TO OCCUR PRIOR TO FINAL SITE STABILIZATION, THEN THE CONSTRUCTED AREA MUST BE PROTECTED AND CONTRIBUTING FLOWS NEED TO BE FILTERED TO PREVENT CLOGGING OF THE SYSTEM OR COMPACTION OF THE INFILTRATION AREA.
- G. SHOP DRAWINGS OF ALL SYSTEMS NEED TO BE APPROVED PRIOR TO CONSTRUCTION. ALL SOIL MATERIAL TESTING SHALL BE DONE PRIOR TO INSTALLATION TO ENSURE SOIL MIXTURE IS ADEQUATE FOR INFILTRATION. TESTS SHOULD BE SUBMITTED TO ENGINEER AND APPROVED PRIOR TO INSTALLATION. SPECIFICATIONS INDICATE MATERIALS REQUIRED FOR EACH SYSTEM.
- H. NOTIFY CIVIL ENGINEER OF WORK BEING DONE ON STORMWATER SYSTEMS AND THE SCHEDULE OF CONSTRUCTION. ALLOW A MINIMUM OF FIVE WORKING DAYS FOR NOTIFICATION SO ENGINEER CAN CONDUCT SITE MEETING TO DISCUSS THE INTENT OF THE SYSTEM AND SO CONSTRUCTION OBSERVATION CAN BE SCHEDULED ACCORDINGLY. SITE ARE TO REMAIN UNDER THE CONTROL OF THE DESIGN AND THE INFILTRATION SYSTEM CONSTRUCTION NEEDS TO OCCUR PRIOR TO STARTING CONSTRUCTION ON THE SYSTEM.
- I. ALL MATERIAL TESTING SHALL BE DONE PRIOR TO INSTALLATION OF THE INFILTRATION SYSTEM TO ENSURE MATERIALS MEET SPECIFICATIONS.
- J. MAINTAIN EROSION AND SEDIMENT CONTROL ON CONTRIBUTING AREAS TO AVOID CLOGGING SYSTEM.
- K. CONSTRUCT STORMWATER SYSTEMS PER DRAWINGS AND SPECIFICATIONS.
- L. COMPLETE CONSTRUCTION OF PARKING AND SIDEWALKS AFTER STORMWATER SYSTEM IS INSTALLED. AFTER PAVEMENT IS INSTALLED, VERIFY THAT INFILTRATION SYSTEMS ARE CLEAR AND FULLY FUNCTIONAL. VACUUM AND CLEAN SYSTEMS SO THEY ARE FULLY FUNCTIONAL AT PROJECT CLOSEOUT.
- M. INSTALL LANDSCAPING AND PLANTING MATERIALS PER LANDSCAPE DRAWINGS AND SPECIFICATIONS.
- N. REMOVE ALL TEMPORARY EROSION CONTROL BMP'S AFTER PAVING AND INFILTRATION AREAS ARE COMPLETE AND AFTER TURF HAS BEEN ESTABLISHED.
- O. CONTRACTOR SHALL TAKE PHOTOGRAPHS AND MEASUREMENTS OF ALL STORMWATER MANAGEMENT SYSTEMS THROUGHOUT CONSTRUCTION. DOCUMENTATION OF CONSTRUCTION SHALL BE SUBMITTED TO THE CIVIL ENGINEER AT THE CLOSEOUT OF THE PROJECT. CLOSEOUT DOCUMENTATION SHALL INCLUDE PHOTOGRAPHS AND MEASUREMENTS OF SYSTEM DURING CONSTRUCTION, TESTING REPORTS AND OBSERVATIONS AND REDLINE DRAWINGS OF ANY FIELD MODIFICATIONS MADE DURING CONSTRUCTION.
- P. A LETTER WRITTEN ON COMPANY LETTERHEAD THAT THE STORMWATER MANAGEMENT PRACTICES HAVE BEEN BUILT PER THE CIVIL PLANS, OR PER REDLINE FIELD DRAWINGS, SHALL BE SUBMITTED TO THE CIVIL ENGINEER AT THE CLOSEOUT OF THE PROJECT.
- Q. THE CONTRACTOR SHALL SUBMIT AN AS-BUILT SURVEY OF THE COMPLETED SITE PREPARED AND SIGNED BY A LICENSED SURVEYOR TO THE CIVIL ENGINEER AT THE END OF THE PROJECT. AS-BUILT SURVEY SHALL INCLUDE ENOUGH INFORMATION TO VERIFY THE CONSTRUCTED TOPOGRAPHY, UTILITY AND SITE ELEMENTS. COORDINATE WITH OWNER AND CIVIL ENGINEER FOR SCHEDULE FOR WHEN THIS SHALL BE COMPLETED.
- R. FILE NOTICE OF TERMINATION (NOT) WITH THE MPCA UPON COMPLETION OF PROJECT. SUBMIT COPY TO OWNER AND ENGINEER.
- S. UPON THE PROJECT'S COMPLETION THE GENERAL CONTRACTOR, PROPERTY OWNER OR RESPONSIBLE PARTY SHALL PROVIDE TO THE DEPARTMENT OF PUBLIC WORKS A FINAL STORMWATER MANAGEMENT REPORT INCLUDING RECORD DRAWINGS. THIS REPORT WILL SERVE AS A MEANS OF VERIFICATION THAT THE INTENT OF THE APPROVED STORMWATER MANAGEMENT DESIGN HAS BEEN MET. THIS FINAL REPORT SHALL SUBstantiate THAT ALL ASPECTS OF THE ORIGINAL DESIGN HAVE BEEN ADEQUATELY PROVIDED FOR BY THE CONSTRUCTION OF THE PROJECT.
1. ALL EXISTING INFORMATION TAKEN FROM SURVEY SUNDE LAND SURVEYING DATED APRIL 1, 2014.
2. SUBSURFACE GEOTECHNICAL INVESTIGATION BY NORTHERN TECHNOLOGIES INC. PROJECT NO. 14.60550.100, DATED MAY 7, 2014.
3. CONTRACTOR TO FIELD VERIFY ALL EXISTING CONDITIONS INCLUDING LOCATIONS OF EXISTING UTILITIES, AND NOTIFY ENGINEER OF ANY DISCREPANCIES PRIOR TO STARTING CONSTRUCTION.
4. CONTRACTOR TO PREVENT DIRT AND/OR DEBRIS FROM ENTERING STORM SEWER OR BEING TRANSPORTED OFF-SITE IN AN UNCONTROLLED MANNER. CONTRACTOR TO VERIFY AT PROJECT CLOSEOUT THAT STORM SEWER SYSTEM IS CLEAR OF SEDIMENT AND/OR DEBRIS AND IS FULLY FUNCTIONAL.
5. ALL AREAS DISTURBED BY CONSTRUCTION WHICH ARE OUTSIDE THE LIMITS OF PAVING ARE TO BE RESTORED AND REVEGETATED.
6. ALL UTILITY DEMOLITION AND/OR ABANDONMENT MUST BE PERFORMED IN ACCORDANCE WITH CITY OF MINNEAPOLIS AND STATE OF MINNESOTA REGULATIONS AND STANDARDS.
7. EXISTING UTILITIES ARE SHOWN IN THEIR APPROXIMATE LOCATIONS. CONTRACTOR TO FIELD VERIFY THE LOCATION OF ALL EXISTING UTILITIES WHICH MAY INCLUDE BUT IS NOT LIMITED TO: ELECTRIC, TELEPHONE, GAS, CABLE TV, COMPUTER CABLE, FIBER OPTIC CABLE, SANITARY SEWER, STORM SEWER AND WATER MAIN. CONTRACTOR TO CONTACT Gopher ONE-CALL BEFORE EXCAVATING.
8. ALL EXISTING UTILITIES AND OTHER IMPROVEMENTS ARE TO REMAIN UNLESS NOTED OTHERWISE.
9. CONTRACTOR TO PROTECT FROM DAMAGE ALL EXISTING IMPROVEMENTS, LANDSCAPING, STRUCTURES AND UTILITIES THAT ARE TO REMAIN. CONTRACTOR TO REPAIR ANY DAMAGE AT OWN EXPENSE.
10. CONTRACTOR IS RESPONSIBLE FOR DESIGN AND CONSTRUCTION OF ALL TEMPORARY UTILITY (SANITARY, STORM, WATER MAIN) CONNECTIONS TO FACILITATE THE PROPOSED CONSTRUCTION. CONTRACTOR SHALL OBTAIN ALL REQUIRED PERMITS FOR THIS WORK.
11. CONTRACTOR TO FIELD VERIFY THE EXACT LOCATION AND ELEVATION OF EXISTING STORM AND SANITARY SEWER PRIOR TO THE START OF CONSTRUCTION.
12. SEE LANDSCAPE PLANS FOR LANDSCAPING. SEE ELECTRICAL PLANS FOR SITE LIGHTING.
13. PROVIDE THE FOLLOWING MINIMUM COVER OVER THE TOP OF PIPE AS FOLLOWS:
 - A. 8' OVER WATER MAIN
 - B. 5' OVER SANITARY SEWER
 - C. 1' OVER STORM SEWER
 - D. 3' OVER UNDERGROUND INFILTRATION SYSTEM
14. ALL WORK TO CONFORM WITH CITY OF MINNEAPOLIS AND STATE OF MINNESOTA STANDARDS AND REGULATIONS.
15. ALL EXCAVATIONS MUST COMPLY WITH THE REQUIREMENTS OF OSHA 29 CFR, PART 1926, SUBPART P "EXCAVATIONS AND TRENCHES". THIS DOCUMENT STATES THAT EXCAVATION SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
16. CATCH BASINS AND MANHOLES ARE SHOWN ON PLAN LARGER THAN ACTUAL SIZE. COORDINATE LOCATION OF MANHOLE COVER AND CASTING SO THAT IS PROPERLY LOCATED AT THE BACK OF CURBLINE FOR THE CURB INLETS OR CENTERED IN THE AREA AS SHOWN ON THE PLAN FOR THE AREA DRAIN AND MANHOLE COVERS.
17. PROVIDE BARRICADES AT STREETS AND SIDEWALKS PER CITY OF MINNEAPOLIS REQUIREMENTS.
18. REMOVE ALL SOILS AND SEDIMENTS TRACKED OR OTHERWISE DEPOSITED ONTO PUBLIC AND PRIVATE PAVEMENT AREAS. REMOVAL SHALL BE ON A DAILY BASIS THROUGHOUT THE DURATION OF THE CONSTRUCTION. CLEAN PAVED ROADWAYS BY SHOVELING OR SWEEPING. STREET WASHING IS ALLOWED ONLY AFTER SHOVELING OR SWEEPING HAS REMOVED SEDIMENT. SEE CITY OF MINNEAPOLIS STANDARDS.
19. ALL ELECTRICAL POLE AND LIGHT REMOVAL SHALL BE COORDINATED WITH THE CITY OF MINNEAPOLIS PRIOR TO DEMOLITION.
20. SITE UTILITY CONTRACTOR TO FURNISH AND INSTALL ALL WATER MAIN, SANITARY SEWER AND STORM SEWER FACILITIES AND APPURTENANCES TO WITHIN FIVE FEET OUTSIDE THE BUILDING. COORDINATE WITH MECHANICAL CONTRACTOR FOR EXACT LOCATION AND DEPTH OF CONNECTION WITHIN BUILDING.
21. SANITARY SEWER PIPE AND FITTINGS TO BE POLYVINYL CHLORIDE (PVC) SDR 26 MINIMUM AND COMPLY WITH EITHER ASTM D3034 OR F679. JOINTS TO BE SOLVENT CEMENT OR FLEXIBLE WATERTIGHT PER SPECIFICATIONS AND AS NOTED ON UTILITY PLAN.
22. STORM SEWER PIPE TO BE REINFORCED CONCRETE PIPE (RCP), ASTM C76, CLASS 5, WITH GASKETED JOINTS. POLYVINYL CHLORIDE (PVC) SDR 26 MINIMUM COMPLYING WITH EITHER ASTM D3034 OR F679 WITH SOLVENT CEMENT OR FLEXIBLE WATERTIGHT JOINTS, OR CORRUGATED STEEL PIPE (CSP) PER SPECIFICATIONS AND AS NOTED ON UTILITY PLAN.
23. WATER MAIN TO BE DUCTILE IRON PIPE (DIP), ANSI A-21.51, 150 PSI WORKING PRESSURE MINIMUM.
24. ALL WATER MAIN AND SERVICES TO BE INSTALLED IN ACCORDANCE WITH CITY OF MINNEAPOLIS WATER STANDARDS.
25. COORDINATE SCHEDULING OF CLOSURES AND REOPENINGS OF EGRESSES, LOADING DOCKS, DUMPSTERS, DRIVEWAYS, SIDEWALKS AND ROADS WITH CITY OF MINNEAPOLIS. TRAFFIC CONTROL SIGNAGE, TEMPORARY WALKWAYS AND TEMPORARY DRIVEWAYS TO CONFORM WITH CITY OF MINNEAPOLIS STANDARDS AND REGULATIONS.
26. ALL SITE FEATURES OUTSIDE THE LIMITS OF CONSTRUCTION TO BE RESTORED TO MATCH EXISTING UNLESS NOTED OTHERWISE.
27. ALL PAVEMENT MARKINGS WITHIN ADJACENT STREETS TO BE RESTORED TO MATCH EXISTING.
28. DRAWINGS DO NOT INDICATE AREAS OF TEMPORARY SUPPORT SYSTEMS. THE CONTRACTOR IS RESPONSIBLE FOR ALL MEANS AND METHODS AND WILL HAVE TOTAL CONTROL OVER THE TYPES AND DESIGN OF ALL SHORING, SHEETING, BRACING, ANCHORAGES, EXCAVATION SUPPORT WALLS, DIRECTIONAL BORING, AUGER JACKING, SOIL STABILIZATION AND OTHER METHODS OF PROTECTING EXISTING IMPROVEMENTS. SEE SPECIFICATIONS FOR SUBMITTAL REQUIREMENTS.
29. STORAGE AND PROTECTION OF EXISTING SITE FEATURES WHICH NEED TO BE REMOVED AND REPLACED FOR CONSTRUCTION OF PROJECT ARE THE RESPONSIBILITY OF THE CONTRACTOR. STORAGE SHALL BE WITHIN THE LIMITS OF STAGING AREA AS SHOWN ON SHEET. CONTRACTOR TO PREVENT DAMAGE OR THEFT OF THESE ITEMS AND TO REPLACE AT OWN EXPENSE.
30. CONTRACTOR TO RECORD EXISTING CONDITIONS AS NEEDED (PHOTOGRAPHS, VIDEO PHOTOGRAPHY, FIELD SURVEYING, ETC.) TO ENABLE RECONSTRUCTION TO MATCH EXISTING CONDITIONS. CONTRACTOR TO DOCUMENT EXISTING CONDITIONS SO THAT RECONSTRUCTED AREAS WILL HAVE POSITIVE DRAWINGS SIMILAR TO EXISTING. SEE SPECIFICATIONS.
31. ANY REDESIGN OF DEMOLISHED STRUCTURAL ELEMENTS WILL NEED TO BE DESIGNED AND CERTIFIED BY A REGISTERED ENGINEER.
32. CONTRACTOR TO CONTACT CITY OF MINNEAPOLIS TO OBSERVE ALL WORK BEFORE ANY BACKFILL OCCURS WITHIN EXCAVATIONS. PROVIDE THREE DAYS NOTICE.
33. WHERE DEMOLITION, EXCAVATION, UNDERPINNING, PILE DRIVING OR SIMILAR WORK IS TO BE PERFORMED ADJACENT TO OR IN THE IMMEDIATE VICINITY OF EXISTING STRUCTURES, THE CONTRACTOR WILL PROVIDE BUILDING SURVEYS AND SEISMIC MONITORING.
34. ALL BUILDING EXITS SHALL REMAIN UNOBSTRUCTED AND USABLE AT ALL TIMES. IF THERE ARE UNAVOIDABLE EXCEPTIONS, THEN CONTRACTOR SHALL PROVIDE TEMPORARY EXIT PLANS TO THE CITY OF MINNEAPOLIS AND THE MINNEAPOLIS PUBLIC SCHOOLS PRIOR TO CONSTRUCTION.
35. SEE SWPPP NARRATIVE FOR FURTHER INFORMATION AND DIRECTION OF EROSION CONTROL MEASURES.
36. CONTRACTOR SHALL COORDINATE WITH ARCHITECT'S DRAWINGS TO VERIFY LOCATION, SIZE, AND QUANTITY OF ALL ROOF DRAINS AND UTILITY CONNECTIONS. LIMITS OF PROPOSED SITE PLUMBING FACILITIES SHALL BE FIVE FEET FROM EDGE OF BUILDING UNLESS OTHERWISE NOTED. COORDINATE CONNECTION WITH MECHANICAL.
37. CONTRACTOR SHALL OBTAIN ALL REQUIRED PERMITS PRIOR TO STARTING CONSTRUCTION.
38. ALL MATERIALS FOR PROPOSED CONSTRUCTION OR REPAIR OF EXISTING FACILITIES SHALL BE NEW PRODUCTS DIRECT FROM THE FACTORY AND FREE FROM DEFECTS.
39. WHEN WORKING AROUND EXISTING UTILITIES, LIGHT POLES, TRAFFIC SIGNALS, TELEPHONE OR POWER POLES, THE CONTRACTOR SHALL PROVIDE SUFFICIENT SUPPORT OR BRACING TO PREVENT EXCESSIVE STRESS ON THE PIPING. THE CONTRACTOR IS FULLY RESPONSIBLE FOR ANY DAMAGES CAUSED BY FAILURE TO EXACTLY LOCATE AND PRESERVE THESE UNDERGROUND FACILITIES.
40. A MINIMUM VERTICAL SEPARATION OF 18 INCHES IS REQUIRED AT ALL WATER MAIN AND SEWER MAIN (STORM, SANITARY, DRAIN TILE, FORCE MAIN) CROSSINGS. ONE FULL LENGTH OF WATER PIPE SHALL BE CENTER AT THE POINT OF CROSSING SO BOTH JOINTS WILL BE EQUIDISTANT AND AS FAR FROM THE SEWER AS POSSIBLE.
41. HYDRANTS AND HYDRANT DRAINS SHALL BE LOCATED A MINIMUM OF 10 FEET SEPARATION FROM ANY SEWER LINE.
42. CADD FILES OF THE SITE LAYOUT PLAN WILL BE PROVIDED TO THE CONTRACTOR AT THE TIME OF CONSTRUCTION FOR STAKING PURPOSES. DIMENSIONS SHOWN HEREIN ARE FOR GENERAL REFERENCES TO TYPICAL LAYOUT OF THE SITE. NOT ALL SITE ELEMENTS ARE DIMENSIONED.
43. CARE MUST BE TAKEN DURING CONSTRUCTION AND EXCAVATION TO PROTECT ANY SURVEY MONUMENTS AND/OR PROPERTY IRONS.
44. ALL STUMPS FROM TREES REMOVED WITHIN PROJECT LIMITS SHALL BE GROUND AND REMOVED IN THEIR ENTIRETY.
45. WASTE MATERIALS INCLUDING PAVEMENT REMOVED DURING CONSTRUCTION, WASTE PIPING AND SUPPLIES, CONSTRUCTION DEBRIS AND EXCESS EXCAVATED MATERIAL SHALL BE REMOVED FROM THE PROPERTY OF THE CONTRACTOR AND DISPOSED OF PROPERLY BY THE CONTRACTOR.
46. MAXIMUM CROSS-SLOPES FOR SIDEWALKS AND ADA ACCESS ROUTES SHALL NOT EXCEED 2.00%. ADA SIDEWALKS SHALL NOT EXCEED 0.00% SLOPE ALONG THE PATH OF TRAVEL.
47. CONTRACTOR SHALL NOT BLOCK DRAINAGE FROM OR DIRECT EXCESS DRAINAGE ONTO ADJACENT PROPERTY.
48. ADEQUATE DRAINAGE SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION AND ANY DRAINAGE STRUCTURE DISTURBED DURING CONSTRUCTION SHALL BE RESTORED TO THE SATISFACTION OF THE OWNING AUTHORITY.
49. ALL CONSTRUCTION STORM RUNOFF SHALL COMPLY WITH THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) REQUIREMENTS. CONTRACTOR MUST FILE NPDES PERMIT A MINIMUM OF TEN DAYS PRIOR TO STARTING WORK ON SITE. SEE STORM WATER POLLUTION PREVENTION PLAN AND DRAWINGS FOR EROSION AND SEDIMENT CONTROL.
50. THE CONTRACTOR SHALL PROVIDE A CONTINUOUS, ACCESSIBLE AND SAFE PEDESTRIAN WALKWAY THAT MEETS ADA AND MINNESOTA MUTCD STANDARDS IF WORKING IN A SIDEWALK AREA, AND TRAFFIC CONTROL PER MINNESOTA MUTCD REQUIREMENTS FOR WORK IN THE PUBLIC RIGHT-OF-WAY.
51. PERMITS AND APPROVAL ARE REQUIRED FROM ENVIRONMENTAL SERVICES FOR THE FOLLOWING ACTIVITIES:
 - AFTER HOURS WORK
 - TEMPORARY STORAGE OF IMPACTED SOILS ON SITE PRIOR TO DISPOSAL OR REUSE
 - REMEDIATION OF CONTAMINATED SOIL AND GROUNDWATER, REUSE OF IMPACTED SOILS ON SITE
 - DEWATERING AND DISCHARGE OF ACCUMULATED STORM WATER OR GROUND WATER TO CITY SEWERS
 - FLAMMABLE WASTE TRAPS, UNDERGROUND OR ABOVE GROUND TANK INSTALLATION OR REMOVAL
 - WELL CONSTRUCTION OR SEALING
 - ON-SITE ROCK CRUSHING
- CONTACT TOM FRAME AT TOM.FRAME@CI.MINNEAPOLIS.MN.US OR 612-673-5807, FOR PERMIT APPLICATIONS AND APPROVALS.
52. NO CONSTRUCTION, DEMOLITION OR COMMERCIAL POWER MAINTENANCE EQUIPMENT SHALL BE OPERATED WITHIN THE CITY BETWEEN THE HOURS OF 6:00 PM AND 7:00 AM ON WEEKDAYS OR DURING ANY HOURS ON SATURDAYS, SUNDAYS AND STATE AND FEDERAL HOLIDAYS, EXCEPT UNDER PERMIT. CONTACT ENVIRONMENTAL SERVICES AT 612-673-3867 FOR PERMIT INFORMATION.
53. A REVIEW OF THE PROJECT, PERMITS ISSUED AND AN INSPECTION FROM ENVIRONMENTAL SERVICES FOR IDENTIFICATIONS OF EQUIPMENT AND SITE OPERATIONS THAT REQUIRE ANNUAL REGISTRATION WITH THE CITY OF MINNEAPOLIS WILL OCCUR FOR THIS PROJECT. IF YOU HAVE QUESTIONS CONCERNING THESE ANNUAL REGISTRATIONS CONTACT TOM FRAME AT 612-673-5807.
54. ANY CONCRETE CONSTRUCTION WORK WITHIN THE PUBLIC RIGHT-OF-WAY MUST BE PERFORMED BY A LICENSED AND BONDED CONCRETE CONTRACTOR.

CITY OF MINNEAPOLIS
EROSION CONTROL NOTES

LEGEND

1. CONTRACTOR MUST CALL THE CITY OF MINNEAPOLIS FOR A PRE-CONSTRUCTION MEETING 48 HOURS PRIOR TO ANY LAND DISTURBANCES 612-673-3867. FAILURE TO DO SO MAY RESULT IN FINES. THE REVOCATION OF PERMITS AND A STOP WORK ORDER ISSUED.
2. INSTALL PERIMETER EROSION CONTROL AT THE LOCATIONS SHOWN ON THE PLANS PRIOR TO THE COMMENCEMENT OF ANY LAND DISTURBANCE OR CONSTRUCTION ACTIVITIES. (HAY BALES ARE NOT AN ACCEPTABLE PERIMETER CONTROL).
3. BEFORE BEGINNING CONSTRUCTION, INSTALL A TEMPORARY ROCK CONSTRUCTION ENTRANCE AT EACH POINT WHERE VEHICLES EXIT THE CONSTRUCTION SITE. USE 2 INCH OR GREATER DIAMETER ROCK IN A LAYER AT LEAST 12 INCHES THICK ACROSS THE ENTIRE WIDTH OF THE ENTRANCE. EXTEND THE ROCK ENTRANCE AT LEAST 50 FEET INTO THE CONSTRUCTION ZONE USING A GEO-TEXTILE FABRIC BENEATH THE AGGREGATE TO PREVENT MIGRATION OF SOIL INTO THE ROCK FROM BELOW.
4. REMOVE ALL SOILS AND SEDIMENTS TRACKED OR OTHERWISE DEPOSITED ONTO PUBLIC AND PRIVATE PAVEMENT AREAS. REMOVAL SHALL BE ON A DAILY BASIS WHEN TRACKING OCCURS AND MAY BE REQUIRED BY MINNEAPOLIS INSPECTORS AT ANY TIME IF CONDITIONS WARRANT. SWEEPING SHALL BE MAINTAINED THROUGHOUT THE DURATION OF THE CONSTRUCTION AND DONE IN A MANNER TO PREVENT DUST BEING BLOWN TO ADJACENT PROPERTIES.
5. INSTALL INLET PROTECTION AT ALL PUBLIC AND PRIVATE CATCH BASIN INLETS WHICH RECEIVE RUNOFF FROM THE DISTURBED AREAS. CATCH BASIN INSERTS OR OTHER APPROVED PRODUCTS ARE REQUIRED UNLESS OTHERWISE NOTED THAT THEY RECEIVE RUNOFF FROM THE PROJECT AREA. HAY BALES OR FILTER FABRIC WRAPPED GRATES ARE NOT ALLOWED FOR INLET PROTECTION.
6. LOCATE SOIL OR DIRT STOCKPILES NO LESS THAN 25 FEET FROM ANY PUBLIC OR PRIVATE ROADWAY OR DRAINAGE CHANNEL. IF REMAINING FOR MORE THAN SEVEN DAYS, STABILIZE THE STOCKPILES WITH GEOTEXTILE VEGETATIVE COVER, TARP, OR OTHER MEANS. CONTROL EROSION FROM ALL STOCKPILES BY PLACING SILT BARRIERS AROUND THE PILES. TEMPORARY STOCKPILES LOCATED ON PAVED SURFACES MUST BE NO LESS THAN TWO FEET FROM THE DRAINAGE/GUTTER LINE AND SHALL BE COVERED IF LEFT MORE THAN 24 HOURS.
7. MAINTAIN ALL TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES IN PLACE UNTIL THE CONTRIBUTING DRAINAGE AREA HAS BEEN STABILIZED. INSPECT TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES ON A DAILY BASIS AND REPLACE DETERIORATED, DAMAGED, OR ROTTED EROSION CONTROL DEVICES IMMEDIATELY.
8. TEMPORARILY OR PERMANENTLY STABILIZE ALL CONSTRUCTION AREAS WHICH HAVE UNDERGONE FINAL GRADING, AND ALL AREAS IN WHICH GRADING OR SITE BUILDING CONSTRUCTION OPERATIONS ARE NOT BEING UNDERGONE AGAINST EROSION. MAINTAIN IN GOOD CONDITION RUNNING WATER WITHIN 7-14 DAYS. USE SEED AND MULCH, EROSION CONTROL MATTING, AND/OR SOODING AND STAKING IN GREEN SPACE AREAS. AN EARLY APPLICATION OF GRANUL BASE ON AREAS TO BE PAVED IS RECOMMENDED TO MINIMIZE EROSION POTENTIAL.
9. REMOVE ALL TEMPORARY, SYNTHETIC, STRUCTURAL, NON-BIODEGRADABLE EROSION AND SEDIMENT CONTROL DEVICES AFTER THE SITE HAS UNDERGONE FINAL STABILIZATION WITH PERMANENT VEGETATION ESTABLISHMENT. FINAL STABILIZATION FOR PURPOSES OF THIS REMOVAL IS 70% ESTABLISHED COVER OVER DEWATERED AREA.
10. REPAIR MIXED CONCRETE AND CONCRETE BATCH PLANTS ARE PROHIBITED WITHIN THE PUBLIC RIGHT-OF-WAY. ALL CONCRETE RELATED PRODUCTION, CLEANING AND MIXING ACTIVITIES SHALL BE DONE IN THE DESIGNATED CONCRETE MIXING/WASHOUT LOCATIONS AS SHOWN IN THE EROSION CONTROL PLAN. UNDER NO CIRCUMSTANCE MAY WASHOUT WATER DRAIN ONTO THE PUBLIC RIGHT-OF-WAY OR INTO DRAINAGE FACILITIES. CONTRACTOR SHALL SET UP A CONCRETE WASHOUT AREA ON-SITE THAT COMPLIES WITH MPCA REQUIREMENTS.
11. CHANGES TO APPROVED EROSION CONTROL PLAN MUST BE APPROVED BY THE EROSION CONTROL INSPECTOR PRIOR TO IMPLEMENTATION. CONTRACTOR TO PROVIDE INSTALLATION AND DETAILS FOR ALL PROPOSED ALTERNATE TYPE DEVICES.
12. ALL EROSION CONTROL ELEMENTS ARE TEMPORARY. CONTRACTOR TO INSTALL EROSION CONTROL ELEMENTS PRIOR TO START OF LAND DISTURBING ACTIVITIES. MAINTAIN IN GOOD CONDITION DURING CONSTRUCTION AND REMOVE FROM THE SITE UPON COMPLETION OF FINAL PAVING AND TURF ESTABLISHMENT.
13. EROSION CONTROL SHALL BE PLACED ALONG THE PERIMETER OF THE SITE EXCAVATION. EROSION CONTROL SHALL BE PLACED SO IT DOES NOT DISTURB THE EXISTING PAVEMENT OR DRIVE LANES THAT ARE TO REMAIN. MANY METHODS OF EROSION CONTROL WILL WORK AND IT IS THE CONTRACTOR'S RESPONSIBILITY TO INSTALL THE MEASURE MOST APPROPRIATE TO THE SITE CONDITIONS AND THAT WHICH MEETS THE CITY OF MINNEAPOLIS STANDARDS. GRAPHICALLY SHOWN ON THE PLANS FOR CLARITY BUT SHALL BE PLACED IN THE MOST APPROPRIATE LOCATIONS IN THE FIELD AND BEHIND THE BACK OF CURB. EXISTING CURB AND GUTTER AND BITUMINOUS ROAD IS TO REMAIN AND SHALL BE PROTECTED FROM DAMAGE. ANY DAMAGED CURB AND GUTTER OR ROADWAY WILL BE REPAIRED BY THE CITY OF MINNEAPOLIS AND PAID FOR SOLELY BY THE CONTRACTOR. SEE DETAILS AND SPECIFICATIONS.
14. SEE SWPPP NARRATIVE FOR FURTHER INFORMATION AND DIRECTION OF EROSION AND SEDIMENT CONTROL MEASURES AND MONITORING.
15. CONTRACTOR TO PREVENT DIRT AND/OR DEBRIS FROM ENTERING STORM SEWER OR BEING TRANSPORTED OFF-SITE IN AN UNCONTROLLED MANNER. CONTRACTOR TO VERIFY PROJECT CLOSEOUT THAT STORM SEWER SYSTEM IS CLEAR OF SEDIMENT AND/OR DEBRIS AND IS FULLY FUNCTIONAL.
16. CONTRACTOR TO PROVIDE TEMPORARY SEED AND MULCH ON ALL NON-PAVED AREAS WITHIN 7 DAYS AFTER ROUGH GRADING IS COMPLETED. SEED WITH ANNUAL RYE SEED AT 60 LBS PER ACRE AND WOOD MULCH FIBER AT 45 LBS PER 1,000 SF.
17. STRAWBALES ARE NOT ALLOWED ON SITE IN ANY CAPACITY.

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EA alert & associates

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Minneapolis, MN 55402
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www.eart.com

ROBERT RIPPE & ASSOCIATES, INC.

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Signature: _____
RHONDA S. PIERCE
Name: _____
Date: OCTOBER 1, 2014
Title: _____

PROJECT:

Sanford Middle School Expansion & Renovations

Minneapolis Public Schools
MPS1401

3524 42nd Ave S,
Minneapolis MN 55406

95% CONSTRUCTION DOCUMENTS

NOT FOR CONSTRUCTION

COMM. NO.: 14-005
Date: OCTOBER 1, 2014
Drawn: ALA
CHECKED: RSP
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DRAWING TITLE:

CIVIL NOTES AND LEGEND

DRAWING NUMBER:

C100

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Signature:
RHONDA S. PIERCE

OCTOBER 1, 2014 4135
14:00

PROJECT:

Sanford Middle School Expansion & Renovations
Minneapolis Public Schools
MPS1401

3524 42nd Ave S,
Minneapolis MN 55406

REVISION:

| Mark | Date | Issue |
|------|------|-------|
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Date: OCTOBER 1, 2014

Drawn: ALA

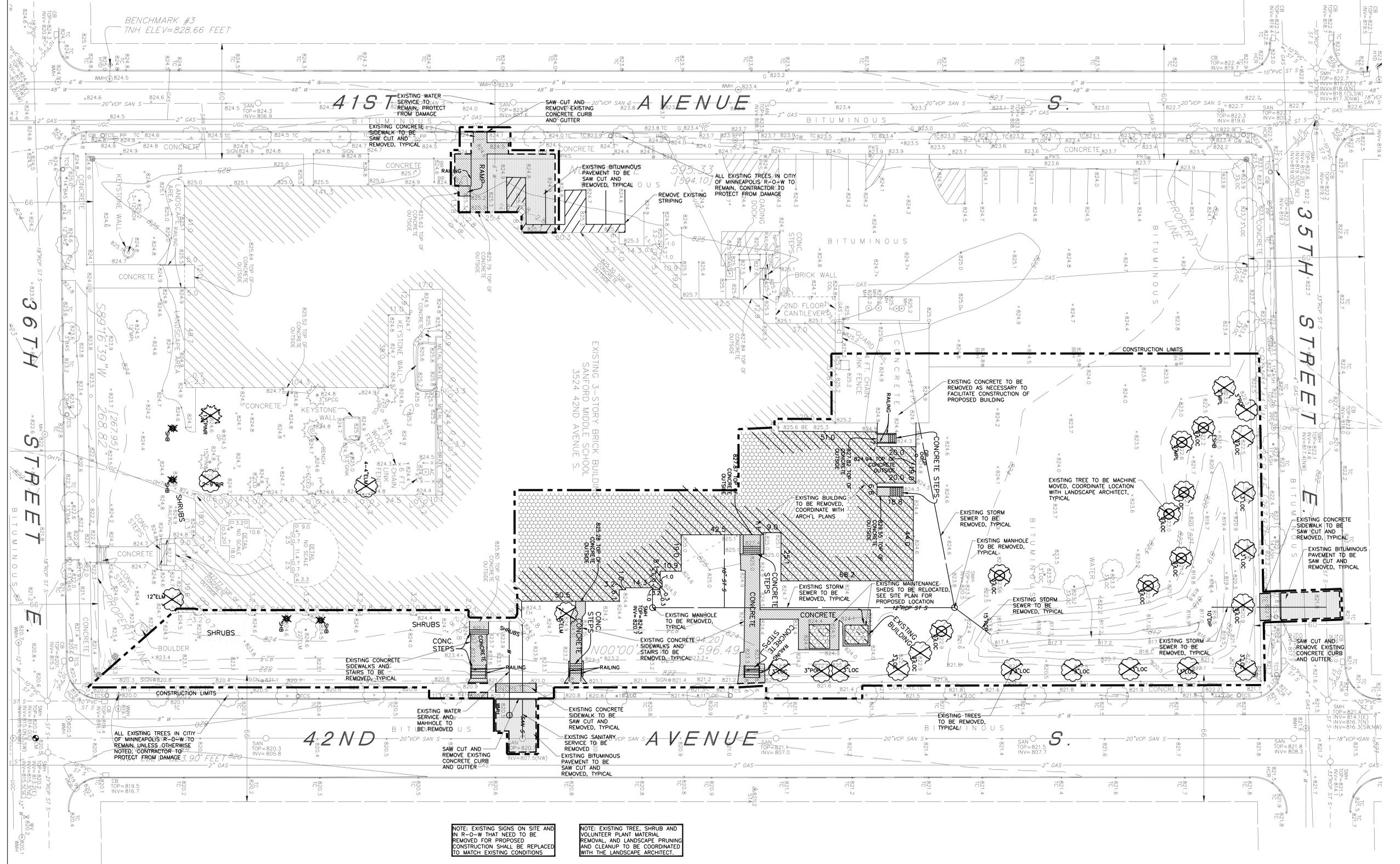
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DRAWING TITLE:
DEMOLITION PLAN

DRAWING NUMBER:

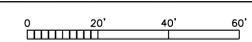
C200



NOTE: EXISTING SIGNS ON SITE AND IN R-O-W THAT NEED TO BE REMOVED FOR PROPOSED CONSTRUCTION SHALL BE REPLACED TO MATCH EXISTING CONDITIONS

NOTE: EXISTING TREE, SHRUB AND VOLUNTEER PLANT MATERIAL REMOVAL AND LANDSCAPE PRUNING AND CLEANUP TO BE COORDINATED WITH THE LANDSCAPE ARCHITECT.

DEMOLITION PLAN
C200



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Signature:
RHONDA S. PIERCE
Name:

OCTOBER 1, 2014 4135
Sheet #

PROJECT:

Sanford Middle School Expansion & Renovations
Minneapolis Public Schools
MPS1401

3524 42nd Ave S,
Minneapolis MN 55406

REVISION:

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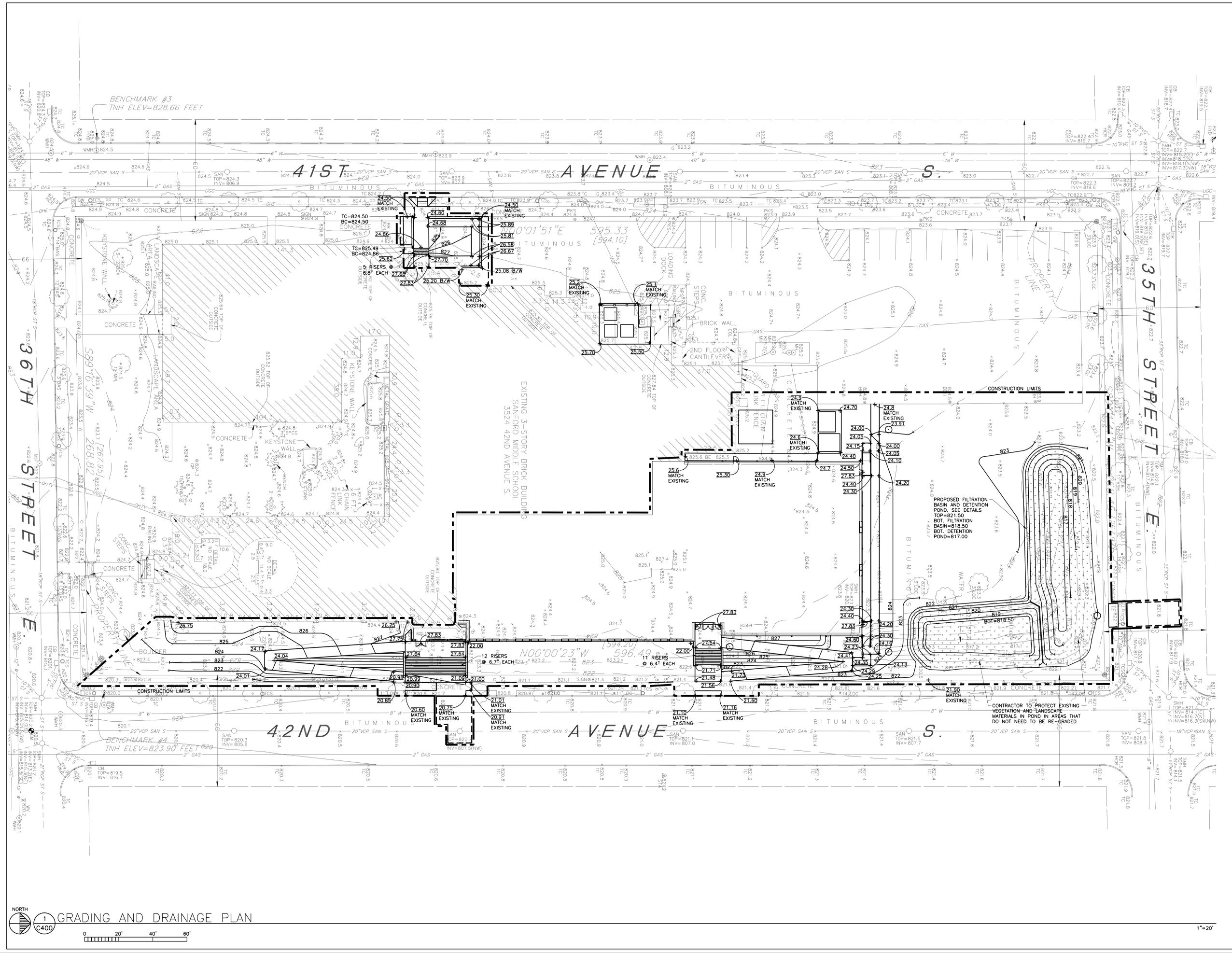
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DRAWING TITLE:

GRADING AND DRAINAGE PLAN

DRAWING NUMBER:

C400



1 GRADING AND DRAINAGE PLAN



1"=20'

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Signature:
RHONDA S. PIERCE
Name:

OCTOBER 1, 2014 4155
Sheet #

PROJECT:

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Minneapolis Public Schools
MPS1401
3524 42nd Ave S,
Minneapolis MN 55406

REVISION:

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Date: OCTOBER 1, 2014

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DRAWING TITLE:

UTILITY PLAN

DRAWING NUMBER:

C500

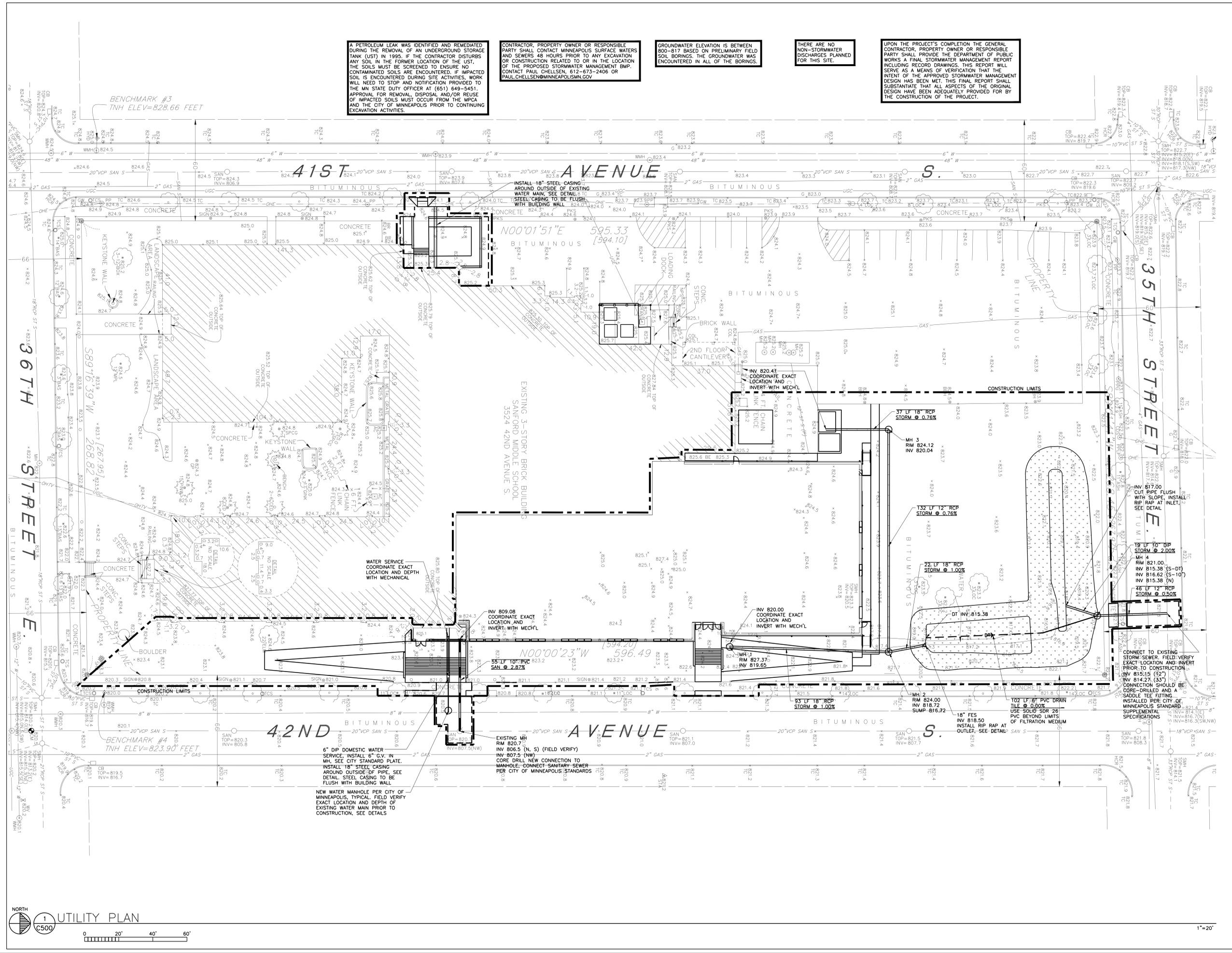
A PETROLEUM LEAK WAS IDENTIFIED AND REMEDIATED DURING THE REMOVAL OF AN UNDERGROUND STORAGE TANK (UST) IN 1995. IF THE CONTRACTOR DISTURBS ANY SOIL IN THE FORMER LOCATION OF THE UST, THE SOILS MUST BE SCREENED TO ENSURE NO CONTAMINATED SOILS ARE ENCOUNTERED. IF IMPACTED SOIL IS ENCOUNTERED DURING SITE ACTIVITIES, WORK WILL NEED TO STOP AND NOTIFICATION PROVIDED TO THE MN STATE DUTY OFFICER AT (651) 649-5451. APPROVAL FOR REMOVAL, DISPOSAL AND/OR REUSE OF IMPACTED SOILS MUST OCCUR FROM THE MPCA AND THE CITY OF MINNEAPOLIS PRIOR TO CONTINUING EXCAVATION ACTIVITIES.

CONTRACTOR, PROPERTY OWNER OR RESPONSIBLE PARTY SHALL CONTACT MINNEAPOLIS SURFACE WATERS AND SEWERS 48 HOURS PRIOR TO ANY EXCAVATION OR CONSTRUCTION RELATED TO OR IN THE LOCATION OF THE PROPOSED STORMWATER MANAGEMENT BMP. CONTACT PAUL CHELSEN, 612-673-2406 OR PAUL.CHELSEN@MINNEAPOLISMN.GOV

GROUNDWATER ELEVATION IS BETWEEN 800-817 BASED ON PRELIMINARY FIELD SOIL BORINGS. THE GROUNDWATER WAS ENCOUNTERED IN ALL OF THE BORINGS.

THERE ARE NO NON-STORMWATER MANAGEMENT REPORTS ON FILE FOR THIS SITE.

UPON THE PROJECT'S COMPLETION THE GENERAL CONTRACTOR, PROPERTY OWNER OR RESPONSIBLE PARTY SHALL PROVIDE THE DEPARTMENT OF PUBLIC WORKS A FINAL STORMWATER MANAGEMENT REPORT INCLUDING RECORD DRAWINGS. THIS REPORT WILL SERVE AS A MEANS OF VERIFICATION THAT THE INTENT OF THE APPROVED STORMWATER MANAGEMENT DESIGN HAS BEEN MET. THIS FINAL REPORT SHALL SUBSTANTIATE THAT ALL ASPECTS OF THE ORIGINAL DESIGN HAVE BEEN ADEQUATELY PROVIDED FOR BY THE CONSTRUCTION OF THE PROJECT.



UTILITY PLAN
C500



1"=20'



USGS VICINITY MAP
NOT TO SCALE



SITE LOCATION MAP
NOT TO SCALE

| ACREAGE SUMMARY | |
|--------------------------|------------|
| TOTAL SITE AREA | 3.67 ACRES |
| ON-SITE DISTURBED AREA | 1.50 ACRES |
| OFF-SITE DISTURBED AREA | 0.1 ACRES |
| TOTAL DISTURBED AREA | 1.6 ACRES |
| EXISTING IMPERVIOUS AREA | 2.2 ACRES |
| PROPOSED IMPERVIOUS AREA | 2.5 ACRES |

| SOIL ASSOCIATION | HYDROLOGIC SOIL GROUP |
|------------------|-----------------------|
| SILTY CLAY | D |

| ESTIMATED BMP QUANTITIES | |
|----------------------------|----------|
| SILT FENCE | 1200 LF |
| FILTER LOGS | 600 LF |
| CATCH BASIN INSERTS | 8 EA |
| ROCK CONSTRUCTION ENTRANCE | 100 CY |
| EROSION CONTROL BLANKET | 15000 SF |

NOTE: QUANTITIES ARE FOR INFORMATIONAL PURPOSES ONLY. CONTRACTOR SHALL DETERMINE FOR THEMSELVES THE EXACT QUANTITIES FOR BIDDING AND CONSTRUCTION. CONTRACTOR SHALL NOT RELY ON THESE QUANTITIES FOR THEIR BID AND CIVIL ENGINEER IS NOT RESPONSIBLE FOR COST ESTIMATES OR ACTUAL CONSTRUCTION COSTS.

| CONTACT INFORMATION | |
|--|-------------------------------------|
| OWNER | |
| BUSINESS: | MINNEAPOLIS PUBLIC SCHOOLS |
| CONTACT PERSON, TITLE: | DON BIELINSKI |
| ADDRESS: | 1250 WEST BROADWAY, MINNEAPOLIS, MN |
| TELEPHONE: | 612.668.5396 |
| EMAIL: | |
| ALTERNATE CONTACT: | MARIAH M. DEAN - 612.668.0060 |
| PARTY RESPONSIBLE FOR LONG TERM MAINTENANCE | |
| BUSINESS: | MINNEAPOLIS PUBLIC SCHOOLS |
| CONTACT PERSON, TITLE: | |
| ADDRESS: | 1250 WEST BROADWAY, MINNEAPOLIS, MN |
| TELEPHONE: | |
| EMAIL: | |
| CONTRACTOR | |
| BUSINESS: | |
| CONTACT PERSON, TITLE: | |
| ADDRESS: | |
| TELEPHONE: | |
| EMAIL: | |
| CONTRACTOR'S ONSITE REPRESENTATIVE | |
| BUSINESS: | |
| CONTACT PERSON, TITLE: | |
| ADDRESS: | |
| TELEPHONE: | |
| EMAIL: | |
| ALTERNATE CONTACT: | |
| PHONE: | |
| EMAIL: | |

NOTE: CONTRACTOR'S ONSITE REPRESENTATIVE SHALL BE COMPLETED ON THE PLAN SET KEPT IN THE CONSTRUCTION TRAILER. THIS INFORMATION SHALL ALSO BE PROVIDED TO THE OWNER AND CIVIL ENGINEER.

SWPPP NARRATIVE

PROJECT DESCRIPTION:
A STORMWATER POLLUTION PREVENTION PLAN (SWPPP) IS REQUIRED AS PART OF THE GENERAL PERMIT AUTHORIZATION TO DISCHARGE STORMWATER ASSOCIATED WITH CONSTRUCTION ACTIVITY UNDER THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES)/STATE DISPOSAL SYSTEM (SDS) CONSTRUCTION STORMWATER PERMIT FOR THE PROJECT. THIS PERMIT IS ISSUED BY THE MINNESOTA POLLUTION CONTROL AGENCY (MPCA).

THE GOAL OF POLLUTION PREVENTION EFFORTS DURING PROJECT CONSTRUCTION IS TO CONTROL SOIL AND POLLUTANTS ON THE SITE AND PREVENT THEM FROM LEAVING THE PROJECT SITE AND FLOWING TO SURFACE WATERS. THE PURPOSE OF THIS SWPPP IS TO PROVIDE GUIDELINES FOR ACHIEVING THAT GOAL. THE SWPPP MUST BE KEPT ONSITE AND UPDATED AS NECESSARY DURING THE COURSE OF CONSTRUCTION TO KEEP IT CURRENT WITH ANY MODIFICATIONS TO THE POLLUTION CONTROL MEASURES BEING UTILIZED.

THE PROJECT CONSISTS OF THE CONSTRUCTION OF A BUILDING ADDITION ONTO THE EXISTING SANFORD MIDDLE SCHOOL. THE PROJECT INCLUDES DEMOLITION OF AN EXISTING BUILDING AND SIDEWALK AREAS, SITE GRADING, STORM SEWER AND UTILITY INSTALLATION, PAVING AND LANDSCAPING FOR THE CONSTRUCTION OF THE SANFORD MIDDLE SCHOOL ADDITION LOCATED EAST OF 41ST AVENUE S, NORTH OF 36TH STREET E, WEST OF 42ND AVENUE S AND SOUTH OF 35TH STREET E IN MINNEAPOLIS, MN.

THE OVERALL PROPOSED STORMWATER MANAGEMENT DESIGN FOR THE SANFORD MIDDLE SCHOOL ADDITION SITE WILL MEET THE REQUIREMENTS OF THE CITY OF MINNEAPOLIS AND THE MPCA NPDES PERMIT. THE STORMWATER MANAGEMENT FEATURES THAT WILL BE UTILIZED AS PART OF THIS PROJECT ARE FILTRATION, PONDING AND DETENTION THAT WILL BE SIZED TO PROVIDE RATE CONTROL FOR THE SITE AND WATER QUALITY TREATMENT DESIGNED TO MEET THE REQUIRED WATER QUALITY TREATMENT VOLUME OF 1.25' OF RUNOFF FROM THE DISTURBED PORTION OF THE SITE.

REGULATORY CONTEXT:
DISCHARGE TO SURFACE OR IMPAIRED WATERS WITHIN ONE MILE OF SITE:
- THIS PROJECT DISCHARGES TO THE MISSISSIPPI RIVER - THIS PORTION OF THE MISSISSIPPI RIVER IS NOT IDENTIFIED AS AN IMPAIRED WATER ON THE MPCA'S 303(D) IMPAIRED WATERS LIST.

PLACEMENT OF FILL IN WATERS OF THE STATE:
- N/A
DRINKING WATER SUPPLY MANAGEMENT AREA:
- N/A

THE PROJECT STORMWATER DISCHARGE IS NOT ANTICIPATED TO IMPACT ANY OF THE FOLLOWING:
- OUTSTANDING RESOURCE VALUE WATERS, TROUT WATERS, WETLANDS, CALCAREOUS FENS, PROPERTIES LISTED BY THE NATIONAL REGISTER OF HISTORIC PLACES OR ARCHAEOLOGICAL SITES

THE PROJECT STORMWATER DISCHARGE IS NOT SUBJECT TO ADDITIONAL REGULATION DUE TO ANY OF THE FOLLOWING:
- OTHER FORMAL ENVIRONMENTAL REVIEWS, ENDANGERED OR THREATENED SPECIES

STORMWATER POLLUTION PREVENTION PLAN (SWPPP) IMPLEMENTATION RESPONSIBILITIES:
1. THE OWNER AND CONTRACTOR ARE PERMITEE(S) AS IDENTIFIED BY THE NPDES PERMIT.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL ON-SITE IMPLEMENTATION OF THE SWPPP, INCLUDING THE ACTIVITIES OF ALL OF THE CONTRACTOR'S SUBCONTRACTORS.
3. CONTRACTOR SHALL PROVIDE A PERSON(S) KNOWLEDGEABLE AND EXPERIENCED IN THE APPLICATION OF EROSION PREVENTION AND SEDIMENT CONTROL BMPs TO OVERSEE ALL INSTALLATION AND MAINTENANCE OF BMPs AND IMPLEMENTATION OF THE SWPPP.
4. CONTRACTOR SHALL PROVIDE PERSON(S) MEETING THE TRAINING REQUIREMENTS OF THE NPDES PERMIT TO CONDUCT INSPECTION AND MAINTENANCE OF ALL EROSION PREVENTION AND SEDIMENT CONTROL BMPs IN ACCORDANCE WITH THE REQUIREMENTS OF THE PERMIT. ONE OF THESE INDIVIDUAL(S) MUST BE AVAILABLE FOR AN ONSITE INSPECTION WITHIN 72 HOURS PRIOR TO THE START OF CONSTRUCTION OR AS SOON AS THE PERSONNEL FOR THE PROJECT HAVE BEEN DETERMINED. DOCUMENTATION SHALL INCLUDE:
4.1. NAMES OF THE PERSONNEL ASSOCIATED WITH THE PROJECT THAT ARE REQUIRED TO BE TRAINED PER PART III.F.1 OF THE PERMIT.
4.2. DATES OF TRAINING AND NAME OF INSTRUCTOR AND ENTITY PROVIDING TRAINING.
4.3. CONTENT OF TRAINING COURSE OR WORKSHOP INCLUDING THE NUMBER OF HOURS OF TRAINING.
4.4. FOLLOWING FINAL STABILIZATION AND THE TERMINATION OF COVERAGE FOR THE NPDES PERMIT, THE OWNER IS EXPECTED TO FURNISH LONG TERM OPERATION AND MAINTENANCE (O & M) OF THE PERMANENT STORM WATER MANAGEMENT SYSTEM.

STORMWATER DISCHARGE DESIGN REQUIREMENTS:
THE FOLLOWING SIZING CRITERIA APPLY TO THE DESIGN OF STORMWATER TREATMENT FACILITIES. N/A INDICATES NOT APPLICABLE OR NOT CONSTRUCTED AS PART OF THIS PROJECT.
1. TEMPORARY SEDIMENTATION BASINS: N/A
2. PERMANENT WET SEDIMENTATION BASINS: N/A
3. PERMANENT INFILTRATION/FILTRATION: N/A
4. PERMANENT REGIONAL PONDS: N/A
5. ALTERNATIVE METHODS: N/A

SEQUENCE OF CONSTRUCTION:
THE FOLLOWING SEQUENCE DESCRIBES, IN GENERAL, THE WORK ON THE SITE:
1. CONTRACTOR SHALL VERIFY THAT ALL PERMITS HAVE BEEN OBTAINED AND/OR OBTAIN THE NECESSARY PERMITS.
2. CONTRACTOR SHALL PERFORM SITE INSPECTIONS, RECORD KEEPING AND RECORD RETENTION IN ACCORDANCE WITH ALL PERMITS.
3. CONTRACTOR SHALL INSTALL ALL PERIMETER AND DOWN-GRADIENT EROSION CONTROL AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES (BMPs), CONSTRUCTION ENTRANCES AND INLET PROTECTION DEVICES PRIOR TO SITE GRADING, EXCAVATION, STOCKPILING OR DISTURBING EXISTING VEGETATIVE COVER.
4. CONTRACTOR SHALL PERFORM SITE GRADING, EXCAVATION, STOCKPILING WORK IN ACCORDANCE WITH THE STORMWATER POLLUTION PREVENTION PLAN (SWPPP).
5. CONTRACTOR SHALL INSTALL, INSPECT, MONITOR AND MAINTAIN TEMPORARY AND PERMANENT EROSION CONTROL BMPs AS SHOWN ON PLANS & IN CONFORMANCE WITH NPDES PERMIT, CONTINUOUSLY DURING THE WORK. CONTRACTOR SHALL STABILIZE ALL EXPOSED SOILS NO LATER THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.
6. CONTRACTOR SHALL REPLACE OR REPAIR EROSION CONTROL AND SEDIMENT CONTROL BMPs THAT ARE NOT FUNCTIONING PROPERLY.
7. CONTRACTOR SHALL PERFORM SITE RESTORATION ACTIVITIES FOR PERMANENT VEGETATIVE ESTABLISHMENT.
8. CONTRACTOR SHALL REMOVE SEDIMENT CONTROL DEVICES PRIOR TO SUBMITTING NOTICE OF TERMINATION (NOT).
9. SUBMIT NOTICE OF TERMINATION TO MPCA WITHIN 30 DAYS OF FINAL STABILIZATION.

1. CONSTRUCTION ACTIVITY FIELD REQUIREMENTS:
ALL FIELD REQUIREMENTS SHALL BE PERFORMED IN ACCORDANCE WITH THE REQUIREMENTS OF THE NPDES PERMIT AND STORMWATER POLLUTION PREVENTION PLAN (SWPPP).
A. THE CONTRACTOR MUST IMPLEMENT THE SWPPP AND PROVIDE BMPs IDENTIFIED IN THE SWPPP IN AN APPROPRIATE AND FUNCTIONAL MANNER.
B. THE CONTRACTOR SHALL RESPOND TO CHANGING SITE CONDITIONS AND IMPLEMENT/SUPPLEMENT EROSION PREVENTION AND SEDIMENT CONTROL MEASURES UTILIZED TO PROVIDE ADEQUATE PROTECTION OF DISTURBED SOILS AND ADEQUATE PREVENTION OF SEDIMENT TRANSPORT OFF-SITE. AT A MINIMUM, THE FOLLOWING STORM WATER POLLUTION PREVENTION CONSTRUCTION ACTIVITY FIELD REQUIREMENTS SHALL BE FURNISHED BY THE CONTRACTOR.
2. EROSION PREVENTION PRACTICES
CONTRACTOR SHALL BE RESPONSIBLE FOR THE FOLLOWING EROSION PREVENTION PRACTICES:
A. THE CONTRACTOR SHALL ATTEMPT TO PHASE ALL WORK TO MINIMIZE EROSION AND MAINTAIN VEGETATIVE COVER TO THE EXTENT POSSIBLE. THE LOCATION OF AREAS NOT TO BE DISTURBED MUST BE DELINEATED ON THE SITE BEFORE CONSTRUCTION BEGINS.
B. STABILIZATION ON ALL EXPOSED SOILS MUST BE INITIATED IMMEDIATELY WHENEVER CONSTRUCTION ACTIVITY HAS PERMANENTLY OR TEMPORARILY CEASED ON ANY PORTION OF THE SITE AND WILL NOT RESUME FOR A PERIOD EXCEEDING 7 CALENDAR DAYS, INCLUDING STOCKPILES WITH SIGNIFICANT SILT, CLAY OR ORGANIC COMPONENTS. STABILIZATION MUST BE COMPLETED WITHIN 7 CALENDAR DAYS.
C. THE NORMAL WETTED PERIMETER OF ANY TEMPORARY OR PERMANENT DRAINAGE DITCH THAT DRAINS WATER FROM A CONSTRUCTION SITE OR DIVERTS WATER AROUND A SITE MUST BE STABILIZED BY CONTRACTOR WITHIN 200 FEET FROM THE PROPERTY EDGE, OR FROM THE POINT OF DISCHARGE TO ANY SURFACE WATER WITHIN 24 HOURS OF CONNECTING TO A SURFACE WATER. TEMPORARY OR PERMANENT DITCH SWALES BEING USED AS A SEDIMENT CONTAINMENT SYSTEM DO NOT NEED TO BE STABILIZED UNTIL THEY ARE NO LONGER USED AS A SEDIMENT CONTAINMENT SYSTEM, AFTER WHICH THEY MUST BE STABILIZED WITHIN 24 HOURS.
D. TEMPORARY OR PERMANENT ENERGY DISSIPATION AT PIPE OUTLETS MUST BE PROVIDED WITHIN 24 HOURS OF CONNECTING TO A SURFACE WATER.
E. THE CONTRACTOR MUST DIRECT DISCHARGES FROM BMPs TO VEGETATED AREAS OF THE SITE IN ORDER TO INCREASE SEDIMENT REMOVAL AND MAXIMIZE INFILTRATION UNLESS INFEASIBLE. THE CONTRACTOR MUST UTILIZE VEGETATED AREAS IF NECESSARY TO PREVENT EROSION WHEN DIRECTING STORMWATER TO VEGETATED AREAS.

3. SEDIMENT CONTROL PRACTICES
CONTRACTOR SHALL BE RESPONSIBLE FOR THE FOLLOWING SEDIMENT CONTROL PRACTICES:
A. CONTRACTOR MUST INSTALL ALL DOWN GRADIENT PERIMETER CONTROLS BEFORE ANY UP GRADIENT DISTURBANCE BEGINS. CONTRACTOR SHALL MAINTAIN PERIMETER CONTROLS UNTIL FINAL STABILIZATION HAS BEEN ESTABLISHED.
B. CONTRACTOR SHALL PROVIDE GRADING AND BMP INSTALLATION TO LIMIT ALL SLOPES OF 3H:1V OR STEEPER TO AN UNBROKEN LENGTH OF 75 FEET OR LESS.
C. IF DOWN GRADIENT SEDIMENT CONTROLS ARE OVERLOADED, THE CONTRACTOR MUST INSTALL ADDITIONAL UPGRADIENT SEDIMENT CONTROL PRACTICES OR REDUNDANT BMPs TO ELIMINATE OVERLOADING. THE SWPPP MUST BE AMENDED TO IDENTIFY THESE ADDITIONAL PRACTICES.
D. TIMING AND INSTALLATION OF SEDIMENT CONTROL DEVICES CAN BE ADJUSTED BY CONTRACTOR TO ACCOMMODATE SHORT-TERM ACTIVITIES SUCH AS CLEARING AND GRUBBING OR VEHICLE PASSAGE. ANY SHORT-TERM ACTIVITY MUST BE COMPLETED AS QUICKLY AS POSSIBLE AND THE SEDIMENT CONTROL PRACTICES MUST BE INSTALLED IMMEDIATELY AFTER THE ACTIVITY IS COMPLETED AND IN ALL CASES PRIOR TO THE NEXT PRECIPITATION EVENT.
E. ALL STORM SEWER INLETS AND OUTLETS SHALL BE PROTECTED BY CONTRACTOR WITH APPROPRIATE BMPs DURING THE WORK. THESE PRACTICES SHALL REMAIN IN PLACE UNTIL THE POTENTIAL SOURCES FOR DISCHARGING SEDIMENT TO INLETS HAVE BEEN STABILIZED BY CONTRACTOR.
F. TEMPORARY SOIL STOCKPILES MUST HAVE SILT FENCE OR OTHER EFFECTIVE SEDIMENT CONTROLS. SOIL STOCKPILES SHALL NOT BE PLACED IN SURFACE WATERS OR STORMWATER CONVEYANCES. ALL SOIL STOCKPILES THAT REMAIN UNDISTURBED FOR A PERIOD GREATER THAN 7 DAYS SHALL BE PROTECTED BY CONTRACTOR WITH COVER OF MULCH, EROSION CONTROL MATS, OR PLASTIC SHEETING.
G. CONTRACTOR SHALL IMPLEMENT MEASURES TO CONTROL VEHICLE TRACKING OFF SITE. ROCK CONSTRUCTION ENTRANCES OR EQUIVALENT SYSTEM MUST BE INSTALLED BY CONTRACTOR TO MINIMIZE TRACKING FROM SITE. CONTRACTOR SHALL PROVIDE STREET SWEEPING AS NECESSARY IF BMPs ARE NOT ADEQUATE TO PREVENT SEDIMENT FROM BEING TRACKED ONTO THE STREET.
H. CONTRACTOR SHALL PROVIDE TEMPORARY SEDIMENTATION BASINS AS REQUIRED BY THE PERMIT.
I. CONTRACTOR MUST MINIMIZE SOIL COMPACTION AND PRESERVE TOPSOIL, UNLESS INFEASIBLE. MINIMIZING SOIL COMPACTION IS NOT REQUIRED WHERE THE FUNCTION OF A SPECIFIC AREA OF THE SITE DICTATES THAT IT BE COMPACTED.
J. THE CONTRACTOR MUST PRESERVE A 50 FOOT NATURAL BUFFER OR PROVIDE REDUNDANT SEDIMENT CONTROLS WHEN A SURFACE WATER IS LOCATED WITHIN 50 FEET OF THE PROJECT DISTURBANCE LIMITS AND STORMWATER FLOWS TO THE SURFACE WATER.
K. IF POLYMERS, FLOCCULANTS, OR OTHER SEDIMENTATION TREATMENT CHEMICALS ARE USED ON SITE, THE CONTRACTOR MUST FOLLOW THE FOLLOWING REQUIREMENTS:
a. THE CONTRACTOR MUST USE CONVENTIONAL EROSION AND SEDIMENT CONTROLS PRIOR TO CHEMICAL ADDITION TO ENSURE EFFECTIVE TREATMENT. CHEMICALS MAY ONLY BE APPLIED WHERE TREATED STORMWATER IS DIRECTED TO A SEDIMENT CONTROL SYSTEM WHICH ALLOWS FOR THE SETTLEMENT OF THE LOAD PRIOR TO DISCHARGE.
b. CHEMICALS MUST BE SELECTED THAT ARE APPROPRIATELY SUITED TO THE TYPES OF SOILS LIKELY TO BE EXPOSED DURING CONSTRUCTION. CHEMICALS MUST BE USED IN ACCORDANCE WITH ACCEPTED ENGINEERING PRACTICES AND WITH DOSING STRATEGIES AND SEDIMENT REMOVAL DESIGN SPECIFICATION PROVIDED BY THE MANUFACTURER.

4. DEWATERING AND BASIN DRAINING
CONTRACTOR SHALL BE RESPONSIBLE FOR THE FOLLOWING DEWATERING REQUIREMENTS:
A. CONTRACTOR'S DEWATERING ACTIVITIES THAT HAVE SEDIMENT-LOADED DISCHARGE WATER MUST BE DISCHARGED INTO A TEMPORARY OR PERMANENT BASIN, WHICHEVER IS POSSIBLE. OTHERWISE IT MUST BE DISCHARGED THROUGH SOME FORM OF BEST MANAGEMENT PRACTICE (BMP) BY CONTRACTOR TO LIMIT SEDIMENT FROM LEAVING THE SITE. PRIOR TO DISCHARGE, THE CONTRACTOR SHALL PERFORM A VISUAL TEST TO ENSURE ADEQUATE TREATMENT IS OBTAINED IN THE BASIN OR BMP AND APPLY ADDITIONAL TREATMENT AS REQUIRED TO ENSURE ADEQUATE TREATMENT.
B. THE CONTRACTOR SHALL DISCHARGE WATER FROM DEWATERING IN A MANNER THAT DOES NOT CAUSE NUISANCE CONDITIONS. THE DISCHARGE WATER SHALL BE DISPERSED OVER AN ACCEPTED ENERGY DISSIPATION MEASURE AND NOT ADVERSELY AFFECT THE RECEIVING WATER OR DOWNSTREAM LANDWEEDS OR WETLANDS.
C. IF CONTRACTOR IS USING FILTERS WITH BACKWASH WATER, THE CONTRACTOR SHALL HAIL THE BACKWASH WATER AWAY FOR DISPOSAL. RETURN THE BACKWASH WATER TO THE BEGINNING OF THE TREATMENT PROCESS, OR INCORPORATE THE BACKWASH WATER INTO THE SITE IN A MANNER THAT DOES NOT CAUSE EROSION.
5. INSPECTIONS AND MAINTENANCE
CONTRACTOR SHALL BE RESPONSIBLE FOR PERFORMING THE FOLLOWING INSPECTIONS AND MAINTENANCE:
A. WHEN INSPECTIONS FIND EROSION PREVENTION AND SEDIMENT CONTROL BMPs THAT ARE NONFUNCTIONAL, ALL NONFUNCTIONAL BMPs MUST BE REPAIRED, REPLACED, OR SUPPLEMENTED WITH FUNCTIONAL BMPs WITHIN 24 HOURS AFTER DISCOVERY OR OTHERWISE IN ACCORDANCE WITH THE NPDES PERMIT REQUIREMENTS. THE CONTRACTOR SHALL ALSO PLACE ANY ADDITIONAL EROSION CONTROL MEASURES DEEMED NECESSARY BY MPCA WITHIN 24 HOURS OF NOTICE FROM MPCA.
B. THE CONTRACTOR MUST ROUTINELY INSPECT THE SITE ONCE EVERY 7 DAYS DURING ACTIVE CONSTRUCTION AND WITHIN 24 HOURS AFTER A RAINFALL EVENT GREATER THAN 0.5 INCHES IN 24 HOURS.
C. ALL INSPECTIONS AND MAINTENANCE CONDUCTED DURING CONSTRUCTION MUST BE RECORDED IN WRITING BY CONTRACTOR AND RETAINED WITH THE SWPPP. CONTRACTOR MAINTENANCE MUST BE COMPLETED BY CONTRACTOR IN CONFORMANCE WITH NPDES PERMIT. CONTRACTOR'S RECORDS MUST INCLUDE:
a. DATE AND TIME OF INSPECTION.
b. NAME OF PERSON CONDUCTING INSPECTION.
c. FINDING OF INSPECTION INCLUDING RECOMMENDATIONS FOR CORRECTIVE ACTION.
d. DETAILS OF CORRECTIVE ACTION TAKEN (DATE, TIME, PARTY COMPLETING MAINTENANCE ACTIVITIES).
e. DATE AND AMOUNT OF RAINFALL GREATER THAN 0.5 INCHES IN 24 HOURS.
f. IF ANY DISCHARGE IS OBSERVED TO BE OCCURRING DURING THE INSPECTION, A RECORD OF ALL POINTS OF THE PROPERTY FROM WHICH THERE IS A DISCHARGE MUST BE MADE, AND THE DISCHARGE SHALL BE DESCRIBED (COLOR, ODOR, FLOATING, SETTLED, OR SUSPENDED SOLIDS, FOAM, OIL SHEEN, AND OTHER INDICATORS) AND PHOTOGRAPHED.
g. DOCUMENTATION OF CHANGES MADE TO SWPPP.
D. IN AREAS OF PROJECT WHERE FINAL STABILIZATION IS COMPLETE INSPECTIONS CAN BE REDUCED TO ONCE A MONTH. THESE AREAS SHALL BE INSPECTED BY CONTRACTOR FOR MINIMUM PERIOD OF 12 NON-WINTER MONTHS AND WITHIN 72 HOURS OF SPRING RAINFALL PRIOR TO RESUMING CONSTRUCTION FOLLOWING ANY WINTER STOPPAGE, WHICHEVER COMES FIRST.
E. THE CONTRACTOR IS RESPONSIBLE FOR THE INSPECTION AND MAINTENANCE OF BMPs UNTIL ANOTHER PERMITEE HAS OBTAINED COVERAGE, OR THE PROJECT HAS UNDERGONE FINAL STABILIZATION AND AN NOT HAS BEEN SUBMITTED TO THE MPCA.
F. ALL EROSION CONTROL MEASURES MUST BE INSTALLED AND MAINTAINED BY CONTRACTOR ACCORDING TO THE DETAILS INCLUDED IN THE CONSTRUCTION DOCUMENTS AND IN ACCORDANCE WITH THE PRODUCT MANUFACTURER'S RECOMMENDATIONS. ALL NONFUNCTIONAL BMPs MUST BE REPAIRED OR REPLACED WITHIN 24 HOURS OF DISCOVERY, OR OTHERWISE AS FIELD CONDITIONS ALLOW.
G. ALL PERIMETER CONTROL DEVICES MUST BE REPAIRED, REPLACED OR SUPPLEMENTED BY THE CONTRACTOR WHEN THEY BECOME NON-FUNCTIONAL OR THE SEDIMENT REACHES ONE-HALF THE HEIGHT OF THE DEVICE. CONTRACTOR SHALL REPAIR OR REPLACE DEVICE THAT IS NONFUNCTIONAL BY THE END OF THE NEXT BUSINESS DAY AFTER DISCOVERY, OR THEREAFTER AS SOON AS FIELD CONDITIONS ALLOW.
H. TEMPORARY AND PERMANENT SEDIMENTATION BASINS MUST BE DRAINED AND SEDIMENT REMOVED BY CONTRACTOR ONCE THE SEDIMENT COLLECTED REACHES ONE HALF THE STORAGE VOLUME WITH 72 HOURS OF DISCOVERY, OR AS SOON AS FIELD CONDITIONS ALLOW.
I. ALL SEDIMENT DEPOSITS WITHIN SURFACE WATERS OR STORMWATER CONVEYANCES MUST BE REMOVED AND RESTABILIZED BY CONTRACTOR WITHIN 7 DAYS OF DISCOVERY, INCLUDING DELTAS AND STORM SEWER SEDIMENT DEPOSITS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL PERMITS REQUIRED, IF NECESSARY, FOR SUCH SEDIMENT REMOVAL.
J. CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING EXISTING PAVED SURFACES CLEAN OF SEDIMENT. CONSTRUCTION ENTRANCES SHALL BE CHECKED DAILY BY CONTRACTOR. IF THE ENTRANCE BECOMES INUNDATED WITH SEDIMENT, THE ENTRANCE WILL BE CLEANED OR REPLACED AS APPROPRIATE BY CONTRACTOR. STREETS LEADING TO AND FROM THE CONSTRUCTION ENTRANCE SHALL BE CHECKED DAILY BY CONTRACTOR FOR OFF-SITE SEDIMENT TRACKING ONTO PAVED SURFACES. THESE AREAS WILL BE SWEEPED CLEAN OF ANY TRACKED MATERIALS BY CONTRACTOR AS SOON AS POSSIBLE AND WITHIN 24 HOURS OF DISCOVERY. CONTRACTOR SHALL EXTEND SWEEPING TO THE EXTREMITY OF ANY SEDIMENT TRACKING THAT OCCURS OFF-SITE.
K. CONTRACTOR SHALL BE RESPONSIBLE TO REMOVE ANY OFF-SITE SEDIMENT ACCUMULATIONS IN A MANNER AND AT A FREQUENCY SUFFICIENT TO MINIMIZE OFF-SITE IMPACTS.
L. ALL INFILTRATION/FILTRATION AREAS MUST BE INSPECTED BY CONTRACTOR TO ENSURE THAT NO SEDIMENT FROM ONGOING CONSTRUCTION IS ACCUMULATING OVER THE INFILTRATION/FILTRATION AREA. SEDIMENT ACCUMULATED OVER INFILTRATION/FILTRATION MUST BE REMOVED BY CONTRACTOR.
M. CONTRACTOR SHALL PROTECT INFILTRATION/FILTRATION AREAS FROM SEDIMENTATION AND OVER-COMPACTION. DURING EXCAVATION, SEDIMENT AND EROSION CONTROL DEVICES MUST BE UTILIZED BY CONTRACTOR TO PREVENT SEDIMENTATION AND THE AREA MUST BE STAKED OFF AND MARKED SO THAT HEAVY CONSTRUCTION EQUIPMENT WILL NOT COMPACT THE SOIL.
N. INSPECTIONS CAN BE SUSPENDED DUE TO FROZEN GROUND CONDITIONS UNTIL FIRST RUNOFF OCCURS OR CONSTRUCTION ACTIVITIES RESUME.

6. POLLUTION PREVENTION MEASURES
CONTRACTOR SHALL BE RESPONSIBLE FOR IMPLEMENTING THE FOLLOWING POLLUTION PREVENTION MANAGEMENT MEASURES ON THE SITE:
A. THE CONTRACTOR SHALL MINIMIZE THE EXPOSURE OF ALL PRODUCTS, MATERIALS, AND WASTES FROM STORMWATER WHICH MAY BE A SOURCE OF CONTAMINATION TO STORMWATER OR ARE NOT DESIGNED TO BE EXPOSED TO STORMWATER.
B. BUILDING PRODUCTS THAT MAY LEACH POLLUTANTS MUST BE UNDER COVER (PLASTIC SHEETING, TEMPORARY ROOFS, ETC.) TO PREVENT THE DISCHARGE OF POLLUTANTS OR PROTECTED BY A SIMILARLY EFFECTIVE MEANS DESIGNED TO MINIMIZE CONTACT WITH STORMWATER.
C. PESTICIDES, HERBICIDES, INSECTICIDES, FERTILIZERS, TREATMENT CHEMICALS, AND LANDSCAPE MATERIALS MUST BE UNDER COVER (PLASTIC SHEETING, TEMPORARY ROOFS, ETC.) TO PREVENT THE DISCHARGE OF POLLUTANTS OR PROTECTED BY A SIMILARLY EFFECTIVE MEANS DESIGNED TO MINIMIZE CONTACT WITH STORMWATER.
D. HAZARDOUS MATERIALS, TOXIC WASTE, (INCLUDING OIL, DIESEL FUEL, GASOLINE, HYDRAULIC FLUIDS, PAINT SOLVENTS, PETROLEUM-BASED PRODUCTS, WOOD PRESERVATIVES, ADDITIVES, CURING

COMPOUNDS, AND ACIDS) MUST BE STORED IN SEALED CONTAINERS TO PREVENT SPILLS, LEAKS OR OTHER DISCHARGE RESTRICTED ACCESS STORAGE AREAS MUST BE PROVIDED TO PREVENT VAPORUM, STORAGE AND DISPOSAL OF HAZARDOUS MATERIALS MUST COMPLY WITH ALL STATE REQUIREMENTS.
E. SOLID WASTE MUST BE STORED, COLLECTED, AND DISPOSED IN COMPLIANCE WITH ALL STATE REQUIREMENTS.
F. PORTABLE TOILETS MUST BE POSITIONED SO THAT THEY ARE SECURE AND WILL NOT BE TIPPED OVER. SANITARY WASTE MUST BE DISPOSED OF IN ACCORDANCE WITH ALL STATE REQUIREMENTS.
G. THE CONTRACTOR SHALL TAKE REASONABLE STEPS TO PREVENT THE DISCHARGE OF SPILLED OR LEAKED CHEMICALS, INCLUDING FUEL, FROM ALL AREAS WHERE CHEMICALS OR FUEL WILL BE LOADED OR UNLOADED. THE CONTRACTOR MUST CONDUCT FUELING IN A CONTAINED AREA UNLESS INFEASIBLE. THE CONTRACTOR MUST ENSURE ADEQUATE SUPPLIES ARE AVAILABLE AT ALL TIMES TO CLEAN UP DISCHARGED MATERIALS AND THAT AN APPROPRIATE DISPOSAL METHOD IS AVAILABLE FOR RECOVERED SPILLED MATERIALS. SOLID WASTE MUST BE CLEANED UP AND REPROPORTIONED WITH STATE REQUIREMENTS. DRY CLEAN UP MEASURES SHALL BE USED WHERE POSSIBLE.
H. THE CONTRACTOR MUST LIMIT VEHICLE AND EQUIPMENT WASHING TO A DEFINED AREA WHEN COMPLETED ON THE PROJECT SITE. RUNOFF FROM THE WASHING AREA MUST BE CONTAINED IN A SEDIMENT BASIN OR OTHER SIMILARLY EFFECTIVE CONTROLS AND WASTE FROM THE WASHING ACTIVITY MUST BE PROPERLY DISPOSED OF. THE CONTRACTOR MUST PROPERLY USE AND STORE SOAPS, DETERGENTS, OR SOLVENTS. NO ENGINE DEGREASING IS ALLOWED ONSITE.
I. THE CONTRACTOR MUST PROVIDE EFFECTIVE CONTAINMENT FOR ALL LIQUID AND SOLID WASTES GENERATED BY WASHOUT OPERATIONS (CONCRETE, STUCCO, PAINT, FORM RELEASE OILS, CURING COMPOUNDS, AND OTHER CONSTRUCTION MATERIALS) RELATED TO THE PROJECT CONSTRUCTION ACTIVITY. NO WASHOUT WASTES MAY CONTACT THE GROUND, AND THE CONTAINMENT MUST BE DESIGNED SO THAT IT DOES NOT RESULT IN RUNOFF FROM THE WASHOUT OPERATIONS OR AREAS. LIQUID AND SOLID WASTES MUST BE DISPOSED OF PROPERLY AND IN COMPLIANCE WITH ALL MPCA RULES. A SIGN MUST BE INSTALLED ADJACENT TO EACH WASHOUT FACILITY THAT REQUIRES SITE PERSONNEL TO UTILIZE PROPER FACILITIES FOR DISPOSAL OF CONCRETE AND OTHER WASHOUT WASTES.

7. FINAL STABILIZATION
THE CONTRACTOR SHALL ENSURE FINAL STABILIZATION OF THE SITE. FINAL STABILIZATION REQUIRES THE FOLLOWING:
A. ALL SOIL DISTURBING ACTIVITIES ARE COMPLETE AND A UNIFORM PERENNIAL VEGETATIVE COVER WITH A DENSITY OF 70% OVER THE ENTIRE PAVED SURFACE HAS BEEN ACHIEVED, INCLUDING STABILIZATION OF ALL DITCHES AND SWALES.
B. CONTRACTOR SHALL ENSURE THAT ALL PERMANENT STORMWATER TREATMENT SYSTEMS ARE CONSTRUCTED IN ACCORDANCE WITH THE REQUIREMENTS OF THE NPDES PERMIT.
C. CONTRACTOR SHALL REMOVE ALL TEMPORARY SYNTHETIC AND STRIPED BMPs.
D. CONTRACTOR SHALL REMOVE ALL SEDIMENTS FROM STORM WATER CONVEYANCES AND PERMANENT WATER QUALITY BASINS.

RECORD AVAILABILITY AND RETENTION:
- THE CONTRACTOR SHALL MAKE THE SWPPP, INCLUDING ALL CERTIFICATES, REPORTS, RECORDS, OR OTHER INFORMATION OF THE PERMIT, AVAILABLE TO FEDERAL, STATE, OR LOCAL OFFICIALS WITHIN 72 HOURS UPON REQUEST FOR THE DURATION OF THE PERMIT AND FOR THREE YEARS FOLLOWING THE SUBMITTAL OF THE NOTICE OF TERMINATION.

INSPECTION AND ENTRY:
- THE CONTRACTOR MUST ALLOW ACCESS AS REQUIRED BY STATE REGULATIONS FOR REPRESENTATIVES OF THE MPCA OR ANY MEMBER THEREOF WHEN AUTHORIZED BY IT, TO ENTER UPON THE PROJECT SITE FOR THE PURPOSE OF OBTAINING INFORMATION, EXAMINATION OF RECORDS, OR CONDUCTING SURVEYS OR INVESTIGATIONS.

NOTICE OF TERMINATION:
- PERMITEE MUST SUBMIT A NOTICE OF TERMINATION (NOT) WITHIN 30 DAYS IF ONE OR MORE OF THE FOLLOWING CONDITIONS HAVE BEEN MET:
1. FINAL STABILIZATION HAS BEEN ACHIEVED ON ALL PORTIONS OF THE SITE FOR WHICH PERMITEE IS RESPONSIBLE INCLUDING THE REMOVAL OF ALL TEMPORARY MEASURES SUCH AS SILT FENCE.
2. ANOTHER OWNER HAS ASSUMED CONTROL OVER ALL PORTIONS OF THE SITE THAT HAVE NOT ACHIEVED FINAL STABILIZATION.

CHANGES TO SWPPP
- THE PERMITEE MUST AMEND THE SWPPP AS NECESSARY TO INCLUDE ADDITIONAL REQUIREMENTS, SUCH AS ADDITIONAL OR MODIFIED BMPs, DESIGNED TO CORRECT PROBLEMS IDENTIFIED OR ADDRESS SITUATIONS WHENEVER:
1. THERE IS A CHANGE IN DESIGN, CONSTRUCTION, OPERATION OR MAINTENANCE, WEATHER OR SEASONAL CONDITIONS THAT HAVE SIGNIFICANT EFFECT ON DISCHARGE. INSPECTION IS REQUIRED WITHIN 24 HOURS OF A RAINFALL EVENT GREATER THAN ONE-HALF INCH.
2. INSPECTION OR INVESTIGATION BY SITE OPERATORS, LOCAL, STATE OR FEDERAL OFFICIALS INDICATE THE SWPPP IS NOT EFFECTIVE.
3. THE SWPPP IS NOT ACHIEVING THE GENERAL OBJECTIVES OF CONTROLLING POLLUTANTS OR THE SWPPP IS NOT CONSISTENT WITH THE TERMS AND CONDITIONS OF THIS PERMIT.
4. THE MPCA DETERMINES THAT DISCHARGE MAY CAUSE OR CONTRIBUTE TO NON-ATTAINMENT OF ANY APPLICABLE WATER QUALITY STANDARDS OR THE SWPPP DOES NOT INCORPORATE THE REQUIREMENTS RELATED TO AN APPROVED TOTAL MAXIMUM DAILY LOAD (TMDL).

SWPPP CERTIFICATION:
- THIS STORMWATER POLLUTION PREVENTION PLAN WAS PREPARED BY INDIVIDUAL(S) TRAINED IN ACCORDANCE WITH THE PERMIT'S TRAINING REQUIREMENTS FOR PREPARATION OF SWPPPS. INDIVIDUAL(S) PREPARING THIS SWPPP:

PREPARED BY:
DAVID KNEBLE, P.E.
PIERCE PINI AND ASSOCIATES
DAVID@PIERCEPINI.COM
763-537-1311

TRAINING/CERTIFICATION:
DATE OF TRAINING/CERTIFICATION: OCTOBER 22-23, 2012
CERTIFICATION PROGRAM: UNIVERSITY OF MINNESOTA - DESIGN OF CONSTRUCTION SWPPP - ARDEN HILLS, MN
INSTRUCTOR(S): JOHN CHAPMAN
CERTIFICATION EXPIRATION: 2016

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Signature: RICHARD S. PIERCE
Name: RICHARD S. PIERCE

OCTOBER 1, 2014 4133
Revision #

PROJECT:

Sanford Middle School Expansion & Renovations
Minneapolis Public Schools
MPS1401
3524 42nd Ave S,
Minneapolis MN 55406

Marked Date Issue

95% CONSTRUCTION DOCUMENTS

NOT FOR CONSTRUCTION

COMM. NO.: 14-005

Date: OCTOBER 1, 2014

Drawn: ALA

CHECKED: RSP

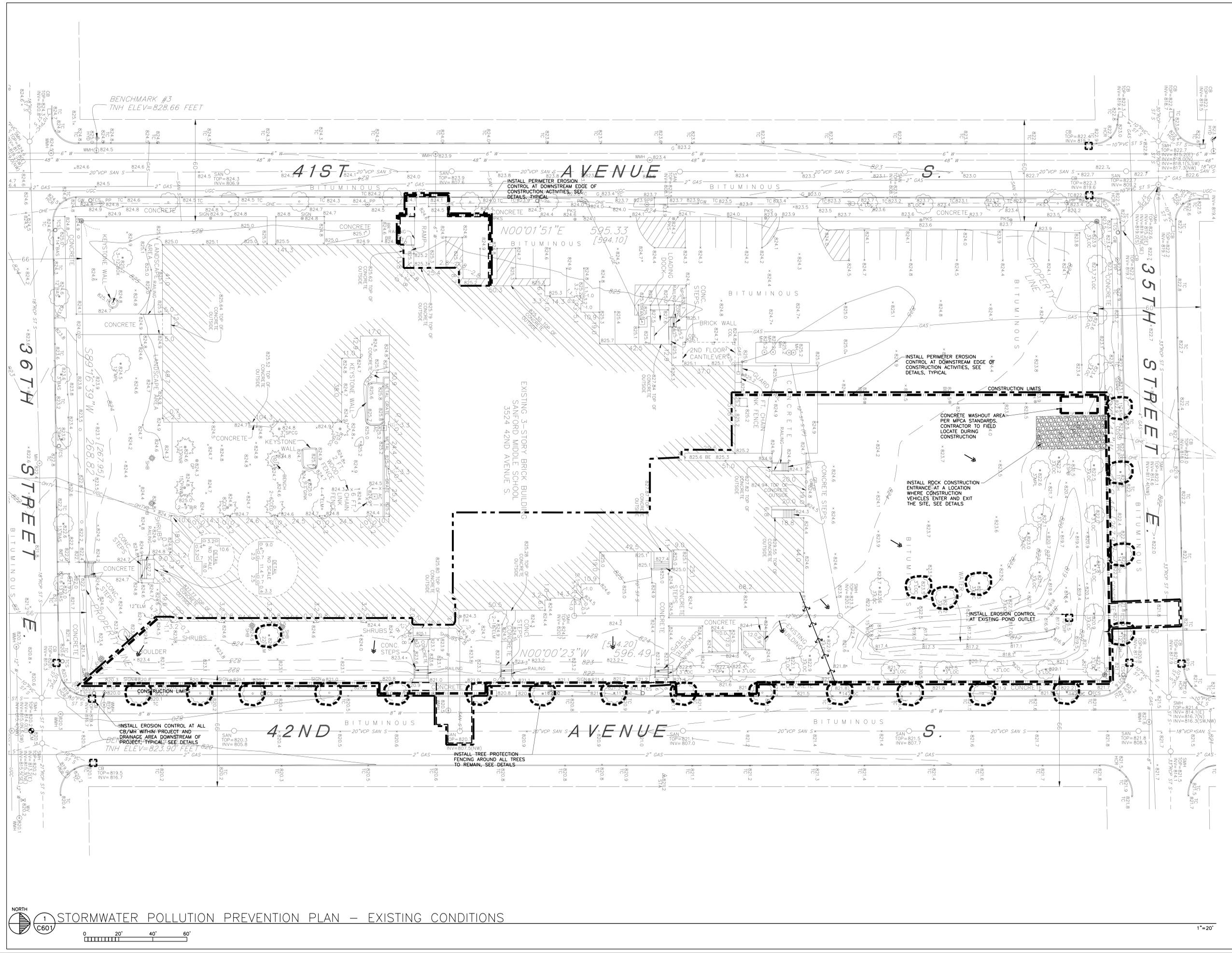
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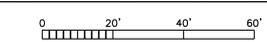
STORMWATER POLLUTION PREVENTION PLAN - EXISTING CONDITIONS

DRAWING NUMBER:

C601



1 STORMWATER POLLUTION PREVENTION PLAN - EXISTING CONDITIONS



1"=20'

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Signature: Rhonda S. Pierce
Name: Rhonda S. Pierce

OCTOBER 1, 2014 4133
Revision #

PROJECT:

Sanford Middle School Expansion & Renovations
Minneapolis Public Schools
MPS1401

3524 42nd Ave S,
Minneapolis MN 55406

REVISIONS:
Mark Date Issue

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COMM. NO.: 14-005

Date: OCTOBER 1, 2014

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CHECKED: RSP

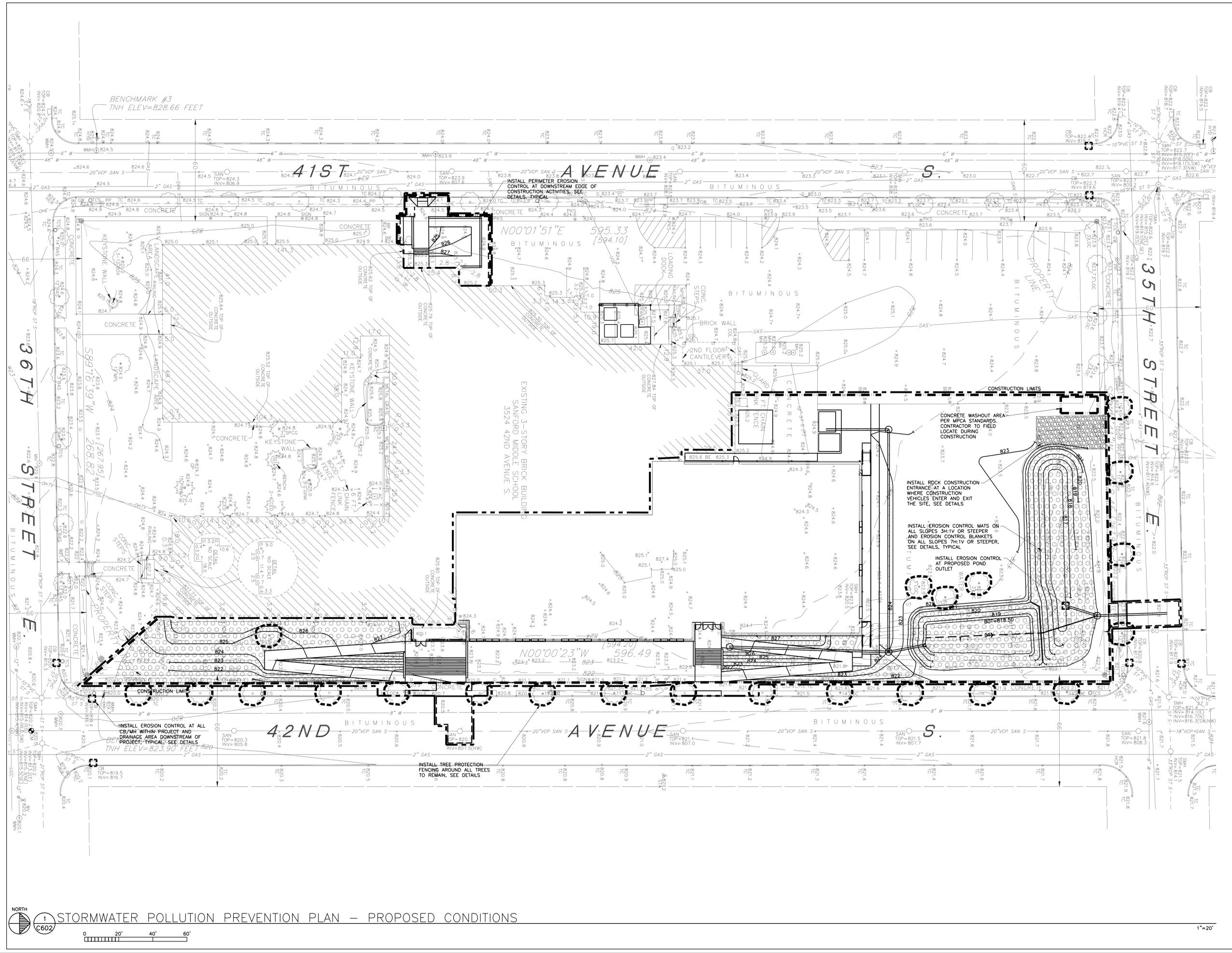
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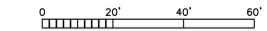
STORMWATER POLLUTION PREVENTION PLAN - PROPOSED CONDITIONS

DRAWING NUMBER:

C602



1 STORMWATER POLLUTION PREVENTION PLAN - PROPOSED CONDITIONS



1"=20'

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Signature
Name
RICHONA S. PIERCE
Date
OCTOBER 1, 2014
Title
Engineer #

PROJECT:

Sanford Middle School Expansion & Renovations
Minneapolis Public Schools
MPS1401

3524 42nd Ave S,
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REVISION:

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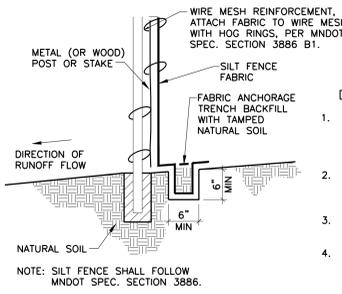
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DRAWING TITLE:

STORMWATER POLLUTION PREVENTION PLAN - DETAILS

DRAWING NUMBER:

C603

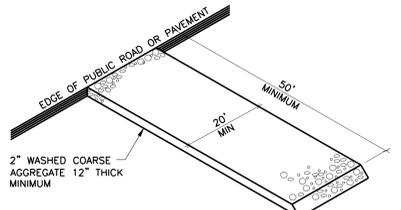


- DESIGN RECOMMENDATIONS**
1. SILT FENCES SHOULD BE INSTALLED ON THE CONTOUR (AS OPPOSED TO UP AND DOWN A HILL) AND CONSTRUCTED SO THAT FLOW CANNOT BYPASS THE ENDS.
 2. ENSURE THAT THE DRAINAGE AREA IS NO GREATER THAN 1/4 ACRE PER 100 FT OF FENCE.
 3. MAKE THE FENCE STABLE FOR THE 10-YEAR PEAK STORM RUNOFF.
 4. WHERE ALL RUNOFF IS TO BE STORED BEHIND THE SILT FENCE, ENSURE THAT THE MAXIMUM SLOPE LENGTH BEHIND THE FENCE DOES NOT EXCEED THE SPECIFICATIONS SHOWN IN TABLE 1.

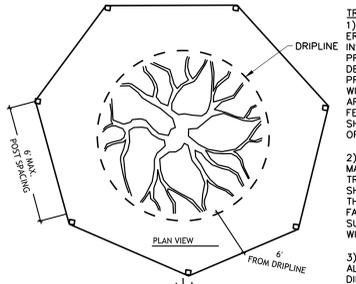
NOTE: SILT FENCE SHALL FOLLOW MNDOT SPEC. SECTION 3886.
FIGURE 1 TYPICAL INSTALLATION FOR SILT FENCE
TABLE 1 MAXIMUM SLOPE LENGTH AND SLOPE FOR WHICH SILT FENCE IS APPLICABLE

| SLOPE H:V | PERCENT | BY CALCULATION | | BY ACCEPTED DESIGN PRACTICES MAXIMUM SLOPE LENGTH |
|-----------|---------|---|--|--|
| | | SILT FENCE STORAGE EQUALS 2 FT FOR A 100-YEAR EVENT | SILT FENCE STORAGE EQUALS 2 FT FOR A 2-YEAR EVENT OR 3 FT FOR A 100-YEAR EVENT | |
| 100:1 | 1% | 400 FT | 900 FT | 100 FT |
| 50:1 | 2% | 200 FT | 450 FT | 75 FT |
| 25:1 | 4% | 100 FT | 225 FT | 75 FT |
| 20:1 | 5% | 80 FT | 180 FT | 75-50 FT |
| 17:1 | 6% | 67 FT | 150 FT | 50 FT |
| 12.5:1 | 8% | 50 FT | 112 FT | 50 FT |
| 10:1 | 10% | 40 FT | 90 FT | 50-25 FT |
| 5:1 | 20% | 20 FT | 45 FT | 25-15 FT |
| 4:1 | 25% | 16 FT | 36 FT | 15 FT |
| 3:1 | 33% | 12 FT | 27 FT | 15 FT |
| 2:1 | 50% | 8 FT | 18 FT | 15 FT |

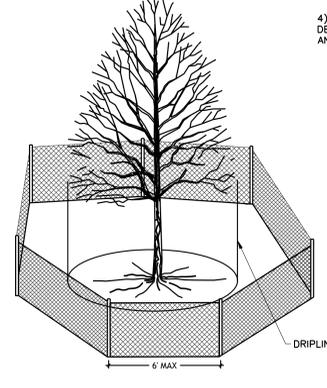
1 HEAVY DUTY SILT FENCE DETAIL
C603 NO SCALE



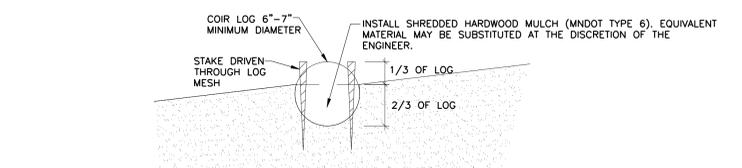
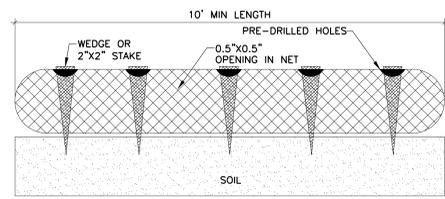
2 GRAVEL CONSTRUCTION ENTRANCE
C603 NO SCALE



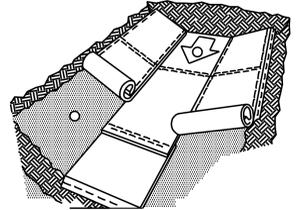
- TREE PROTECTION NOTES:**
- 1) ALL TREE PROTECTION FENCING AND EROSION CONTROL FENCING SHALL BE INSTALLED ACCORDING TO THE PLANS PRIOR TO ANY DEMOLITION. AFTER DEMOLITION OR AS NECESSARY, TREE PROTECTION FENCING MAY BE RELOCATED WITH APPROVAL FROM THE LANDSCAPE ARCHITECT. ALL TREE PROTECTION FENCING AND EROSION CONTROL DEVICES SHALL BE MAINTAINED FOR THE DURATION OF THE CONSTRUCTION PERIOD.
 - 2) CONTRACTOR SHALL NOT STORE ANY MATERIALS OR PARK ANY VEHICLES IN TREE PROTECTION ZONES. THE FENCE SHALL PREVENT TRAFFIC MOVEMENT AND THE PLACEMENT OF TEMPORARY FACILITIES, EQUIPMENT, STOCKPILES AND SUPPLIES FROM HARMING VEGETATION WITHIN THE LIMITS OF PROTECTION.
 - 3) THE CONTRACTOR SHALL CLEANLY CUT ALL ROOTS EXPOSED BY GRADING AS DIRECTED BY THE LANDSCAPE ARCHITECT.
 - 4) THE CONTRACTOR SHALL USE DESIGNATED CONSTRUCTION ENTRANCES AND STAGING AREAS.



3 TREE PROTECTION DETAIL
C603 NO SCALE

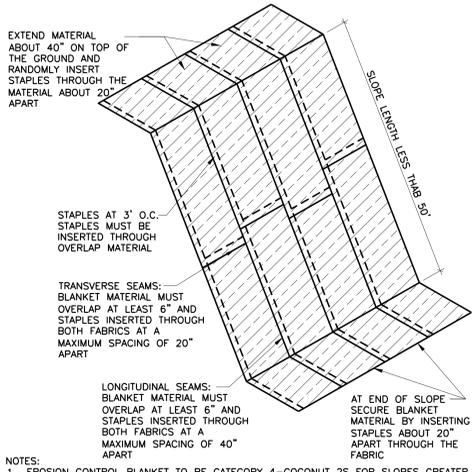


4 COIR LOG
C603 NO SCALE



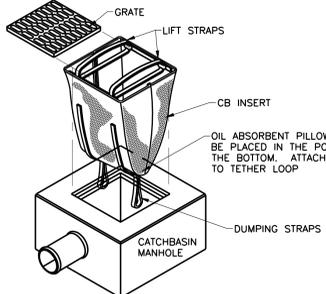
1. BEFORE INSTALLATION APPLY TOPSOIL, FERTILIZER AND SEED TO SURFACE.
2. BEGIN AT THE TOP OF THE CHANNEL. INSTALL MATS BY ANCHORING IN A 6" DEEP BY 6" WIDE TRENCH WITH APPROXIMATELY 12" OF MAT EXTENDED BEYOND THE UP-SLOPE PORTION OF THE TRENCH. ANCHOR WITH A ROW OF STAPLES APPROXIMATELY 12" APART IN THE BOTTOM OF THE TRENCH. BACKFILL AND COMPACT THE TRENCH AFTER STAPLING. APPLY SEED TO COMPACTED SOIL AND FOLD REMAINING 12" PORTION OF MAT BACK OVER SEED AND SOIL. SECURE MATS WITH A WITH A ROW OF STAPLES SPACED APPROXIMATELY 12" APART ACROSS THE WIDTH OF THE MATS.
3. ROLL CENTER MATS IN DIRECTION OF WATER FLOW IN BOTTOM OF CHANNEL.
4. PLACE CONSECUTIVE AND ADJACENT MATS END OVER END (SHINGLE STYLE) WITH A MINIMUM 6" OVERLAP. USE A DOUBLE ROW OF STAPLES STAGGERED 4" APART AND 4" ON CENTER TO SECURE OVERLAPPED MATS.
5. FULL LENGTH EDGE OF MATS AT TOP OF SIDE SLOPES MUST BE ANCHORED WITH A ROW OF STAPLES APPROXIMATELY 12" APART IN A 6" DEEP BY 6" WIDE TRENCH.
6. THE TERMINAL END OF MATS MUST BE ANCHORED WITH A ROW OF STAPLES APPROXIMATELY 12" APART IN A 6" DEEP BY 6" WIDE TRENCH.
7. BACKFILL AND SEED AFTER STAPLING.
8. FOLLOW MANUFACTURER'S RECOMMENDATIONS FOR PROPER INSTALLATION.

5 EROSION STABILIZATION MATS
C603 NO SCALE



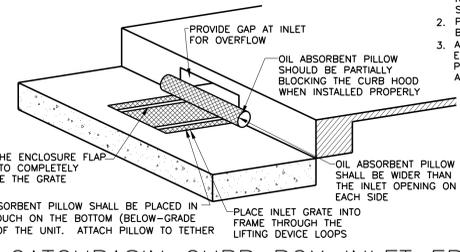
- NOTES:**
1. EROSION CONTROL BLANKET TO BE CATEGORY 4-COCOONUT 2S FOR SLOPES GREATER THAN 5:1 AND SIDES AND BOTTOM OF ALL DRAINAGE SWALES AND PONDING AREAS AND CATEGORY 2-STRAW 2S FOR ALL SLOPES LESS THAN 5:1 PER MNDOT SPEC. SECTION 3885.
 2. INSTALL PER MNDOT SPEC. SECTION 2575

6 EROSION CONTROL BLANKET
C603 NO SCALE

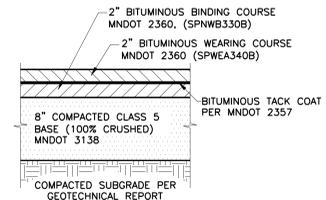


1. OIL ABSORBENT PILLOW SHALL BE REMOVED AND REPLACED WHEN NEAR SATURATION.
2. USE DANDY® BAG II AS MANUFACTURED BY DANDY® PRODUCTS, INC.
3. AN EQUIVALENT CATCHBASIN EROSION CONTROL INSERT METHOD OR PRODUCT MAY BE USED WITH PRIOR APPROVAL FROM ENGINEER.

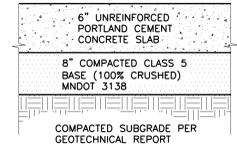
7 CB INSERT EROSION CONTROL
C603 NO SCALE



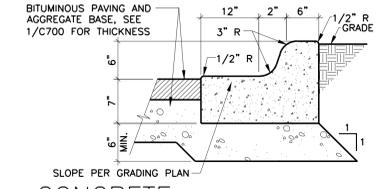
8 CATCHBASIN CURB BOX INLET EROSION CONTROL
C603 NO SCALE



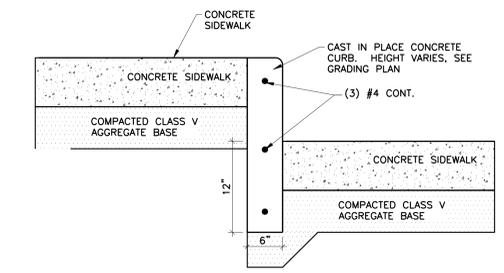
1 BITUMINOUS PAVEMENT DETAIL
 C700 NO SCALE



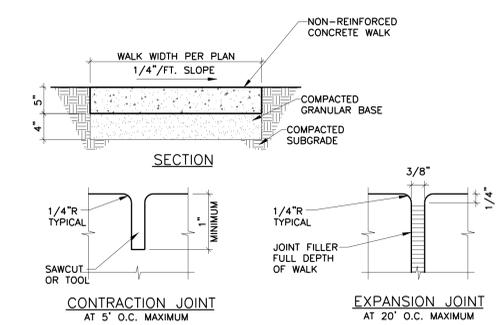
2 CONCRETE PAVEMENT
 C700 NO SCALE



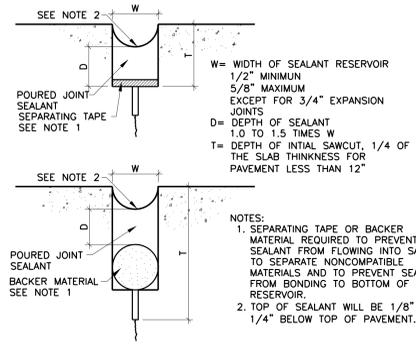
3 CONCRETE CURB & GUTTER (B612)
 C700 NO SCALE



4 CONCRETE CURB DETAIL
 C700 NO SCALE



5 CONCRETE WALK & JOINT DETAILS
 C700 NO SCALE



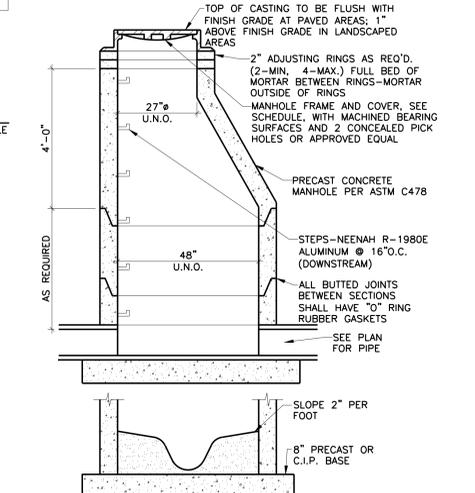
6 JOINT SEALANT DETAILS
 C700 NO SCALE

CASTING SCHEDULE

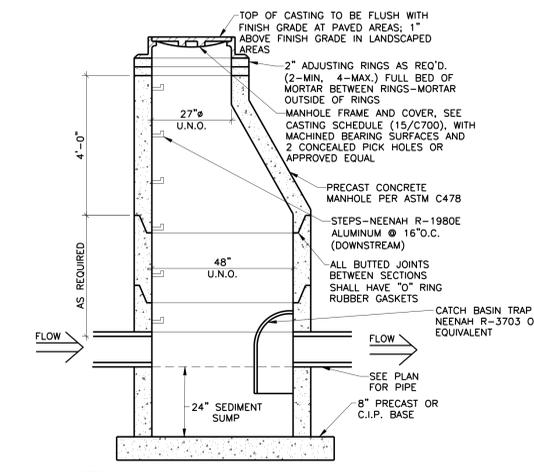
| CATCH BASIN/MANHOLE/RISE | GRATE TYPE (NEENAH) | REMARKS |
|--------------------------|---------------------|---------------|
| 1-4 | R-1642-A | STORM MANHOLE |

NOTE:
 1. CASTINGS SHALL BE NEENAH FOUNDRY CASTINGS OR APPROVED EQUIVALENT.
 2. CASTINGS SHALL BE STAMPED WITH APPROPRIATE UTILITY NAME ("STORM SEWER", "SANITARY SEWER")

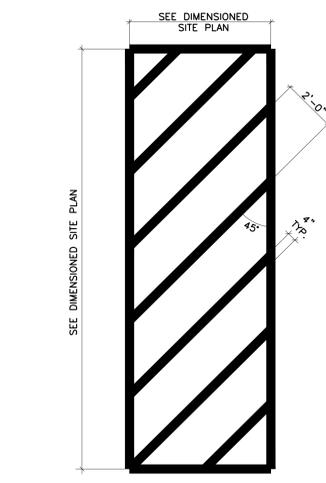
7 CASTING SCHEDULE
 C700 NO SCALE



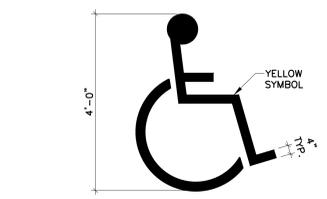
8 STANDARD SEWER MANHOLE
 C700 NO SCALE



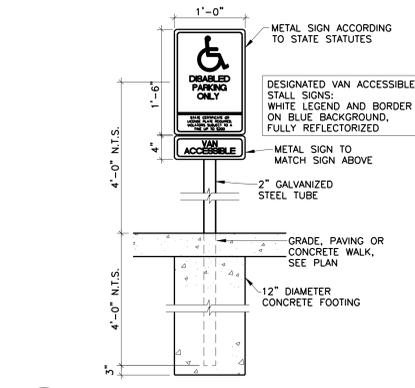
17 MANHOLE WITH SUMP
 C700 NO SCALE



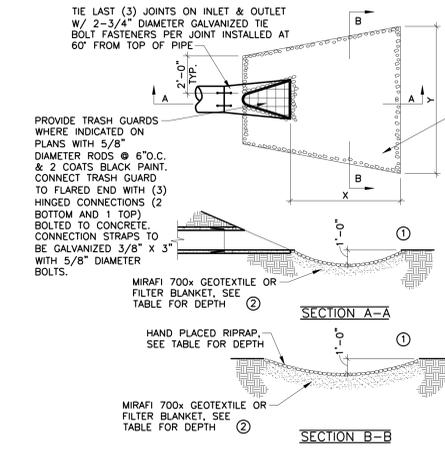
9 ACCESS AISLE STRIPING
 C700 NO SCALE



10 HC PARKING SYMBOL
 C700 NO SCALE



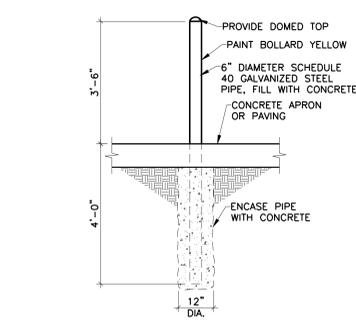
11 ACCESSIBLE PARKING SIGN DETAIL
 C700 NO SCALE



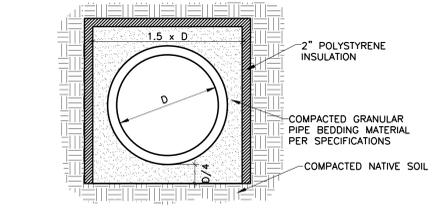
13 RIPRAP AT OUTLET
 C700 NO SCALE

PROVIDE RIPRAP & GRANULAR FILTER (PER MNDOT 2511) AS FOLLOWS

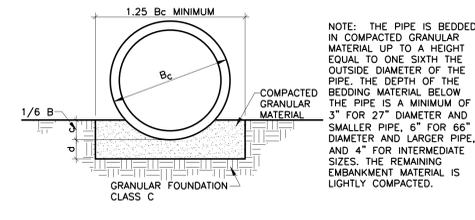
| PIPE DIA. | CLASS II | | | CLASS III | | | CLASS IV | | |
|-----------|----------|-------|-------------------------|-------------------------|-------------------------|-------------------------|-------------------------|-------------------------|-------------------------|
| | X | Y | DEPTH GRAN. RIPRAP (CY) |
| 12" | 8' | 10' | 2.8 | 1.4 | 4.1 | 2.1 | 5.5 | 2.8 | |
| 15" | 8' | 10.5' | 2.9 | 1.5 | 4.4 | 2.2 | 5.8 | 2.9 | |
| 18" | 10' | 12' | 3.9 | 2.0 | 5.9 | 3.0 | 7.8 | 3.9 | |
| 21" | 10' | 12.5' | 4.2 | 2.1 | 6.3 | 3.2 | 8.4 | 4.2 | |
| 24" | 12' | 14' | 5.5 | 2.8 | 8.3 | 4.2 | 11.0 | 5.5 | |
| 27" | 12' | 14.5' | 5.8 | 2.9 | 8.7 | 4.4 | 11.6 | 5.8 | |
| 30" | 14' | 16' | 7.3 | 3.7 | 10.9 | 5.5 | 14.5 | 7.3 | |
| 36" | 16' | 18' | 9.2 | 4.6 | 13.8 | 6.9 | 18.3 | 9.2 | |
| 42" | 18' | 19.5' | 10.9 | 5.5 | 16.3 | 8.2 | 21.7 | 10.9 | |
| 48" | 20' | 21' | 12.9 | 6.5 | 19.4 | 9.7 | 25.8 | 12.9 | |



14 GUARD POST DETAIL
 C700 NO SCALE



15 PIPE INSULATION DETAIL
 C700 NO SCALE



16 CLASS C PIPE BEDDING
 C700 NO SCALE

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Signature: _____
Name: RHONDA S. PIERCE
Date: OCTOBER 1, 2014 License # 41153

PROJECT:
Sanford Middle School Expansion & Renovations
Minneapolis Public Schools
MPS1401
3524 42nd Ave S,
Minneapolis MN 55406

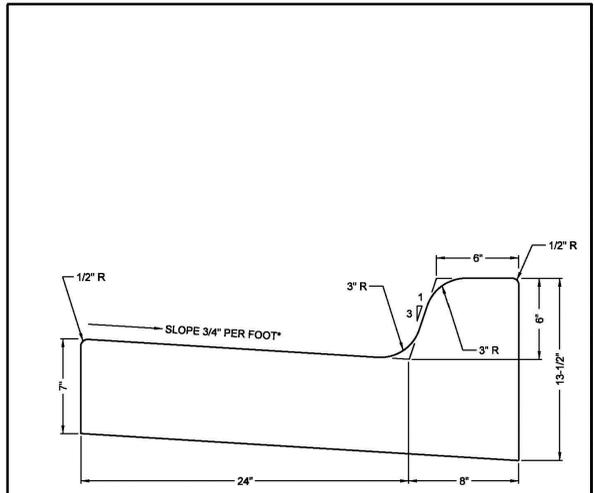
REVISED:
Mark Date Issue

95%
CONSTRUCTION DOCUMENTS
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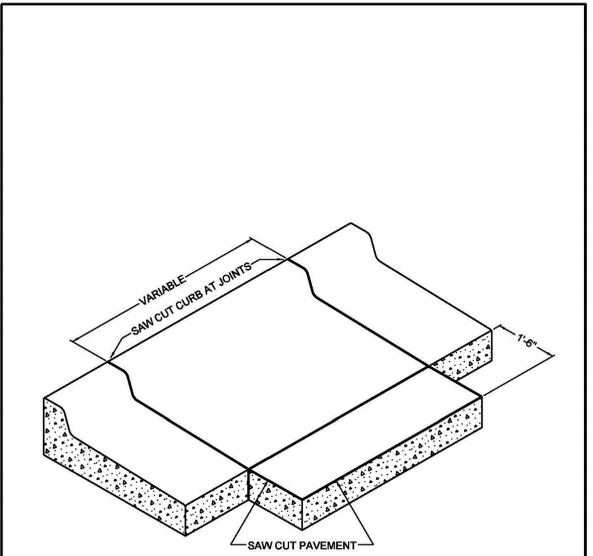
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Date: OCTOBER 1, 2014
Drawn: ALA
CHECKED: RSP

DRAWING TITLE:
CIVIL DETAILS

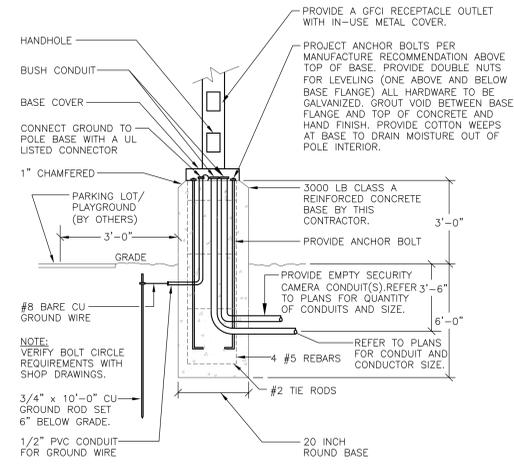
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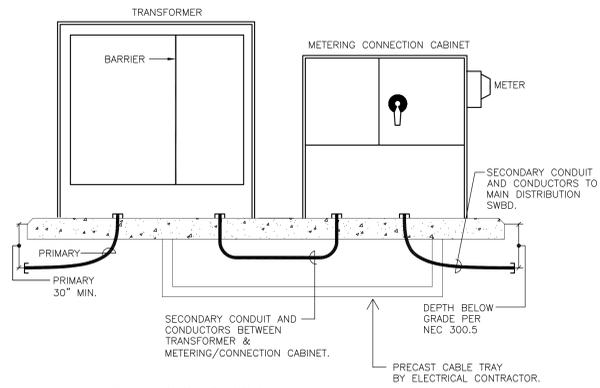
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|--|--|---------------|-----------------------|------------------------------|
| | MINNEAPOLIS DEPARTMENT OF PUBLIC WORKS | | B-624 CURB AND GUTTER | STANDARD PLATE NO. ROAD-1003 |
| | DRAWN: JFC | DATE: 9/10/07 | | |
| | APPROVED: GAS | DATE: 3/18/08 | | |



| | | | | |
|--|--|---------------|------------------------------------|------------------------------|
| | MINNEAPOLIS DEPARTMENT OF PUBLIC WORKS | | SAW CUT AT CURB AND GUTTER REMOVAL | STANDARD PLATE NO. ROAD-1010 |
| | DRAWN: JFC | DATE: 9/10/07 | | |
| | APPROVED: GAS | DATE: 3/18/08 | | |

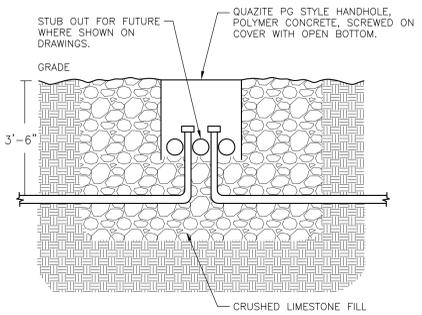


2 TYPE BB POLE BASE DETAIL
NO SCALE

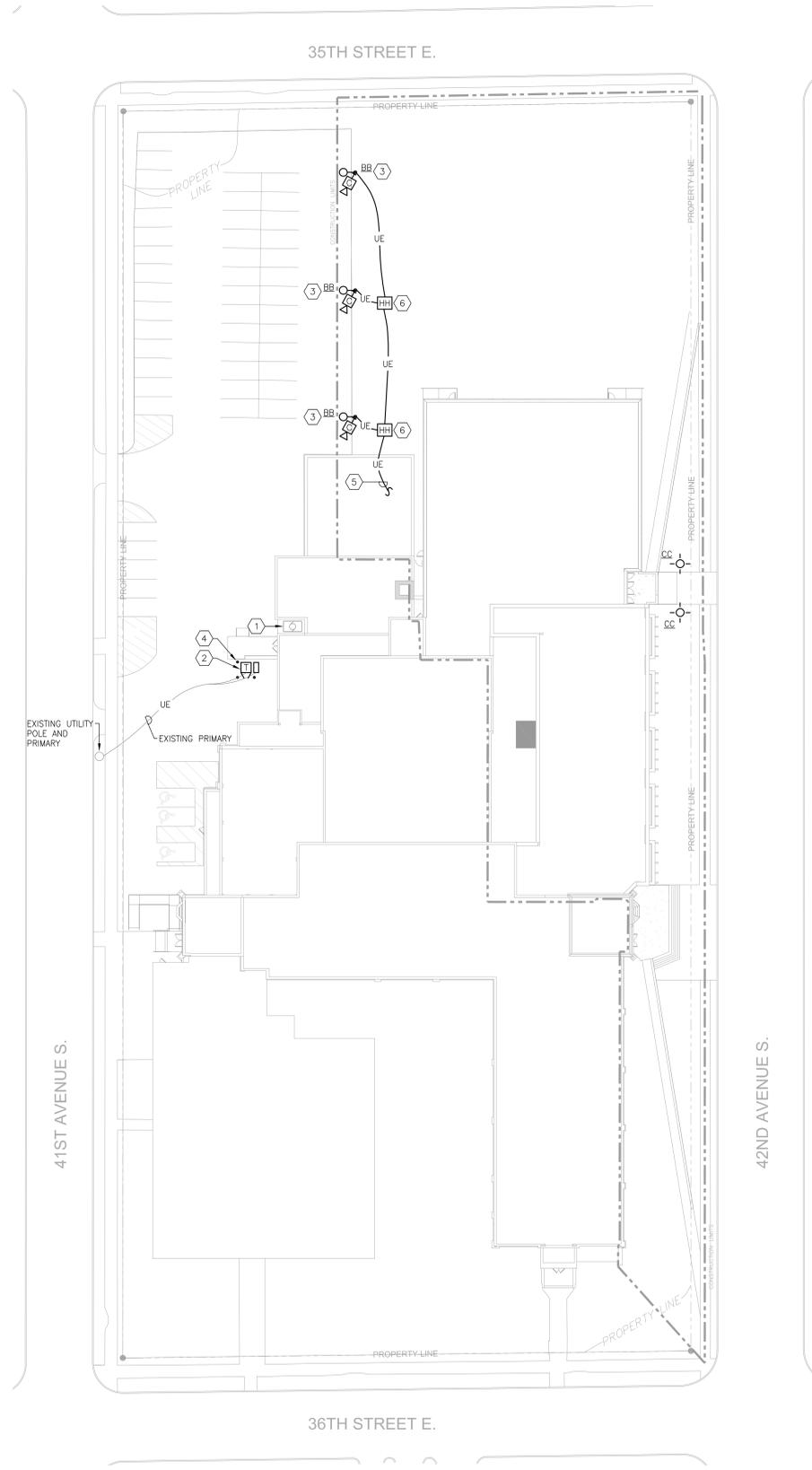


- UTILITY WILL FURNISH & INSTALL:**
- PAD MOUNTED TRANSFORMER.
 - PRIMARY CABLE AND TERMINATIONS.
 - TRANSFORMER GROUNDING SYSTEM.
- CONTRACTOR WILL FURNISH & INSTALL:**
- CONCRETE PAD FOR TRANSFORMER AND METERING CABINET.
 - WIREWAY (SIZE AS REQUIRED PER N.E.C.).
 - SECONDARY CONDUIT AND CONDUIT FROM METERING CABINET TO MAIN DISTRIBUTION SWITCHBOARD.
 - SECONDARY TERMINATION LUGS ON BOTH ENDS OF SECONDARY CONDUCTORS.
 - SECONDARY CONDUCTORS FROM TRANSFORMER TO METERING CABINET.
- UTILITY WILL FURNISH & CUSTOMER WILL INSTALL:**
- METERING CURRENT TRANSFORMERS.
 - STANDARD METER SOCKET.

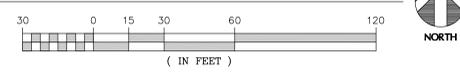
3 MAIN ELECTRICAL SERVICE DETAIL
NO SCALE



4 COMMUNICATIONS HANDHOLE
NO SCALE

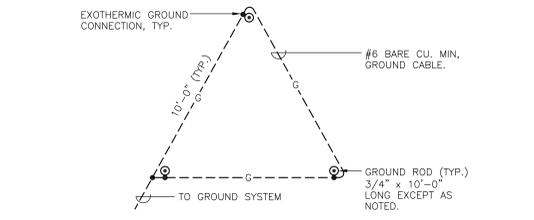


1 ELECTRICAL SITE PLAN



- ELECTRICAL CONSTRUCTION NOTES:**
- WIRING SHALL BE PROVIDED TO LIGHT FIXTURES AND DEVICES SHOWN AS INDICATED. AMPACITY, DERATING, AND CONDUIT FILL SHALL BE AS REQUIRED BY THE NEC.
 - MINIMUM WIRING SIZE SHALL BE #12 AWG, UNLESS OTHERWISE INDICATED. PROVIDE A #12 EQUIPMENT GROUND WIRE IN EACH CONDUIT. PROVIDE MINIMUM 1-1/4" PVC CONDUIT FOR ALL EXTERIOR WIRING UNLESS OTHERWISE NOTED ON THIS DRAWING OR AS REQUIRED BY THE NEC.
 - SHADED IN FIXTURES SHALL BE CONTROLLED PHOTO ON/ PHOTO OFF. NON-SHADED IN FIXTURES SHALL BE PHOTO ON/ SWITCH OFF. ADJUST LIGHT CONTROLLER PANELS ACCORDINGLY.
 - SEE DETAILS THIS SHEET FOR MOUNTING OF LIGHT FIXTURES.
 - AVOID DAMAGE TO TREES FROM UNDERGROUND CONDUIT, STAKE RUNS AND POLE LOCATIONS PRIOR TO EXCAVATION TO VERIFY ACCEPTABILITY TO ARCHITECT AND OWNER.
 - PROVIDE METALLIC STAKE AT STUB LOCATIONS FOR FUTURE LOCATING OF STUB ENDS.
 - DEVICES ON SITE PLAN INDICATED WP SHALL HAVE "IN USE" TYPE WEATHERPROOF COVERS.

- ELECTRICAL KEYED NOTES:**
- EXISTING EMERGENCY GENERATOR TO REMAIN.
 - NEW UTILITY TRANSFORMER. REFER TO MAIN ELECTRIC SERVICE DETAIL THIS SHEET.
 - REFER TO THIS SHEET FOR POLE BASE DETAIL.
 - PROVIDE NEW 6 INCH CONCRETE FILLED STEEL BOLLARDS.
 - ROUTE 1-1/4" SCHEDULE 80 PVC CONDUIT FOR SECURITY CABLING.
 - PROVIDE 17" X 20" QUAZITE HANDHOLE FOR SECURITY CABLING.



5 GROUNDING CENTERPOISE DETAIL
NO SCALE

I HEREBY CERTIFY THAT THIS PLAN SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Signature
JOHN M. KELLEHN
Name
DATE 1/7/13
Date License #

PROJECT:
Sanford Middle School Expansion & Renovations
Minneapolis Public Schools
MPS1401
3524 42nd Ave S.
Minneapolis MN 55406

REVISED:
Mark Date Issue

50% CONSTRUCTION DOCUMENTS
AUGUST 8, 2014

NOT FOR CONSTRUCTION

COMM. NO.: MPS1401
Date: AUGUST 8, 2014
Drawn: TJH
CHECKED: JMK

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DRAWING TITLE:
ELECTRICAL SITE PLAN

LAPS PROJECT NUMBER: 2014.020.00
DRAWING NUMBER

ES100

GENERAL NOTES

- EXISTING IS TO REMAIN UNLESS NOTED OTHERWISE.
- DOORS 4" OFF WALL U.N.O.
- ALL DIMENSION TO CENTER LINE OF NEW WALLS & FINISHED FACE OF EXISTING WALLS.
- AT ALL AREAS WHERE NEW WORK IS DIRECTLY ADJACENT TO EXISTING, PATCH & REPAIR ALL EXISTING. MATCH EXISTING.
- CONTRACTOR SHALL FIELD VERIFY ALL EXISTING CONDITIONS AND DIMENSIONS PRIOR TO BEGINNING WORK.

miller dunwiddie
ARCHITECTURE

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401 - 2nd Avenue North Suite 410, Mpls, MN 55401
www.damonfarber.com 612-332-7522

Alert & Associates
143 Third Street South
St. Paul, MN 55102
651-430-2772 fax 651-430-2661
www.alert.com

ROBERT RIPPE ASSOCIATES, INC.

HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.

Signature _____
Name _____
Date: JULY 1, 2010
License # _____

PROJECT:

**Sanford Middle School
Expansion & Renovations**
Minneapolis Public Schools,
OP# 2767
3524 42nd Ave S, Minneapolis
MN 55406
MDA Project #MPS1401

REVISED:

| Mark | Date | Issue |
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| | | |

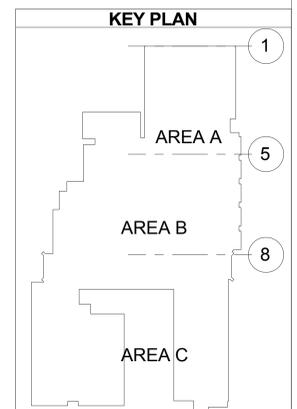
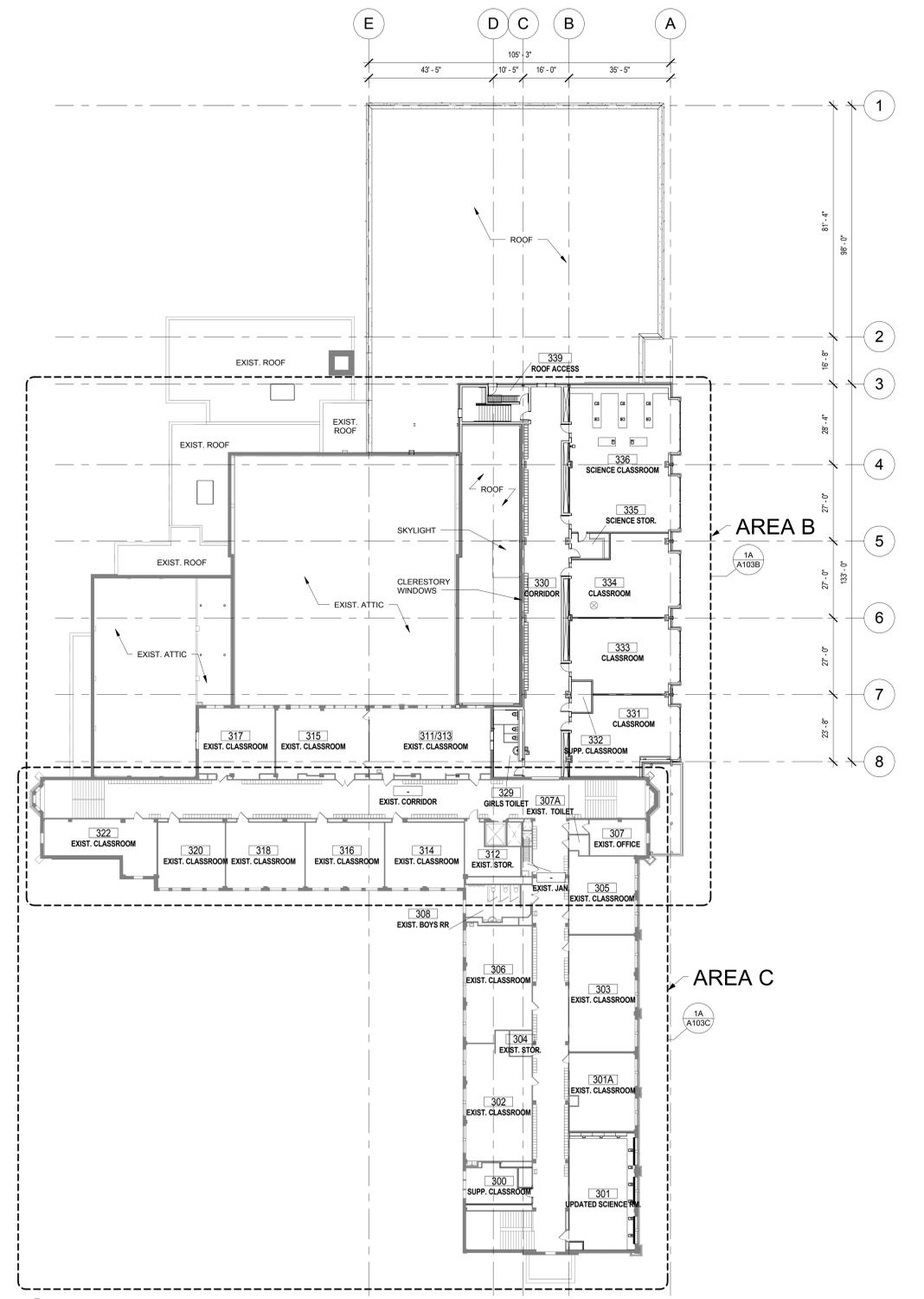
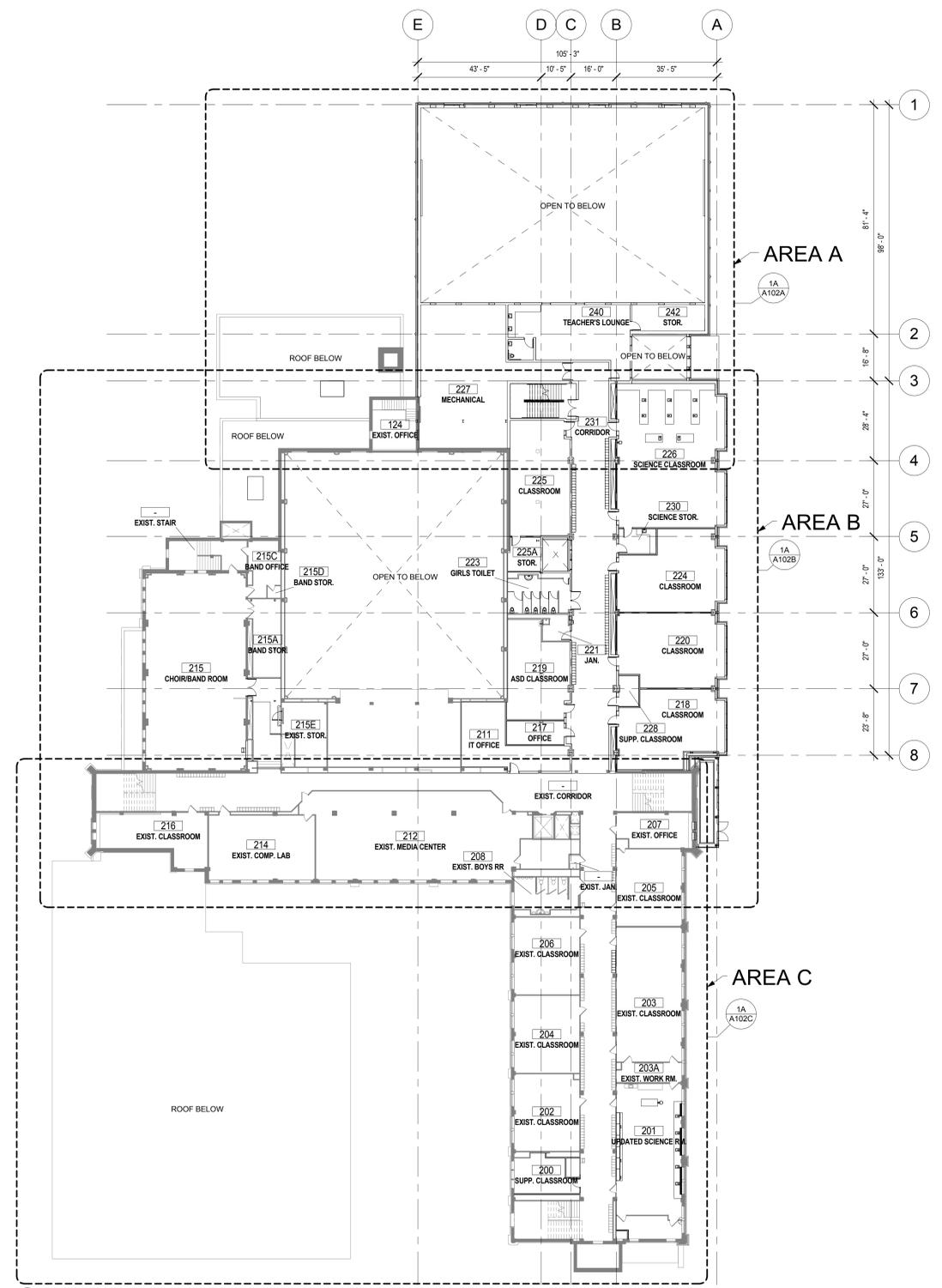
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NOT FOR CONSTRUCTION
AUGUST 18, 2014

COMM. NO.: MPS1401
Date: AUGUST 18, 2014
Drawn: MDR
CHECKED: PGM

DRAWING TITLE:
SECOND AND THIRD LEVEL REFERENCE PLANS

DRAWING NUMBER

A012



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Signature _____
Date _____
Name _____
Date _____ License # _____

PROJECT:

**Sanford Middle School
Expansion & Renovations**

Minneapolis Public Schools,
OP# 2767
3524 42nd Ave S, Minneapolis
MN 55406

MDA Project #MPS1401

REVISED:

| Mark | Date | Issue |
|------|------|-------|
| | | |

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CONSTRUCTION**

AUGUST 18, 2014

COMM. NO.: MPS1401

Date: AUGUST 18, 2014

Drawn: MDR

CHECKED: PGM

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DRAWING TITLE:

**EXTERIOR
ELEVATIONS**

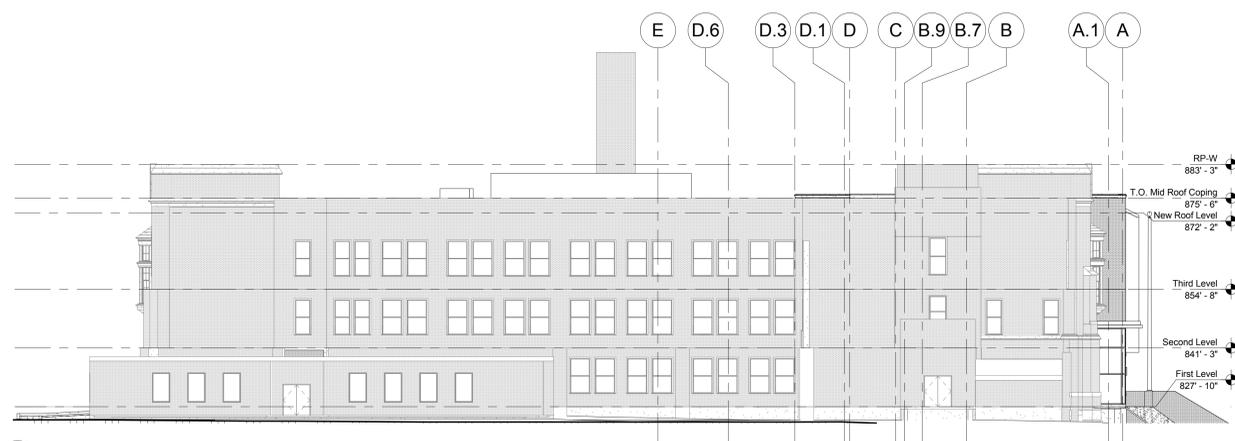
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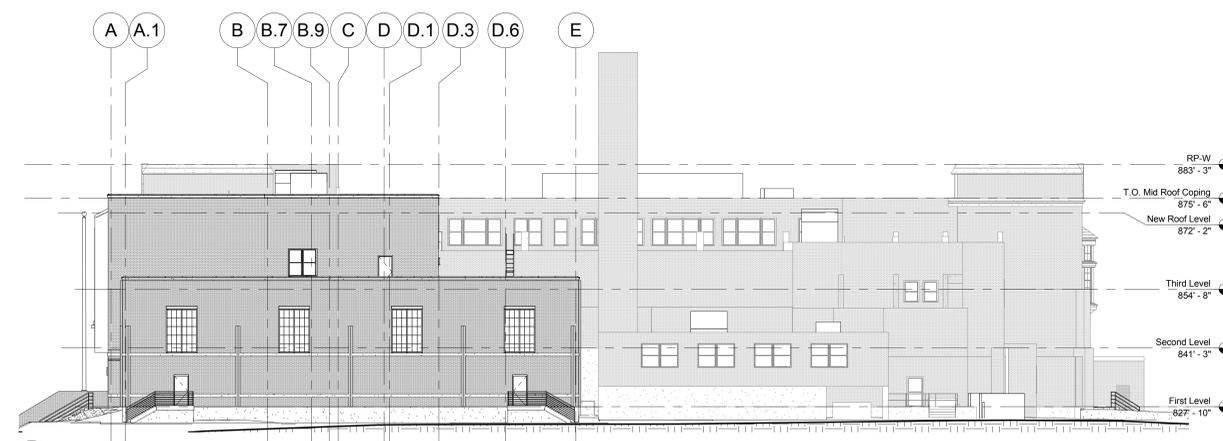
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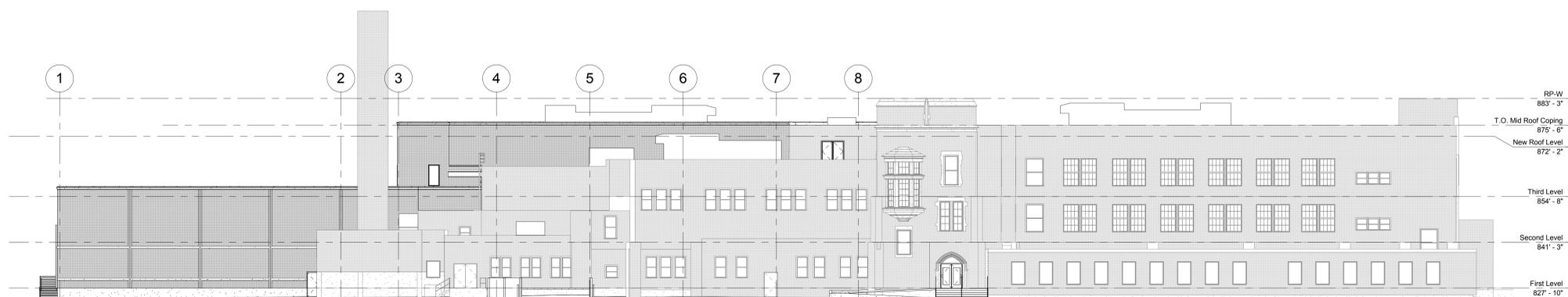
6A
A200 REFERENCE EAST ELEVATION
1/16" = 1'-0"



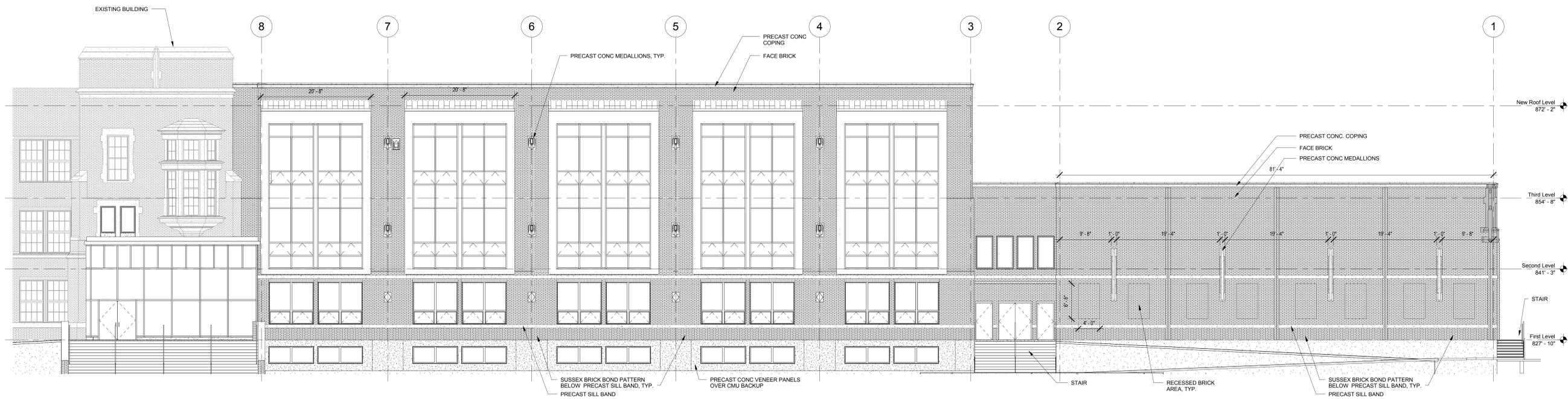
6C
A200 REFERENCE SOUTH ELEVATION
1/16" = 1'-0"



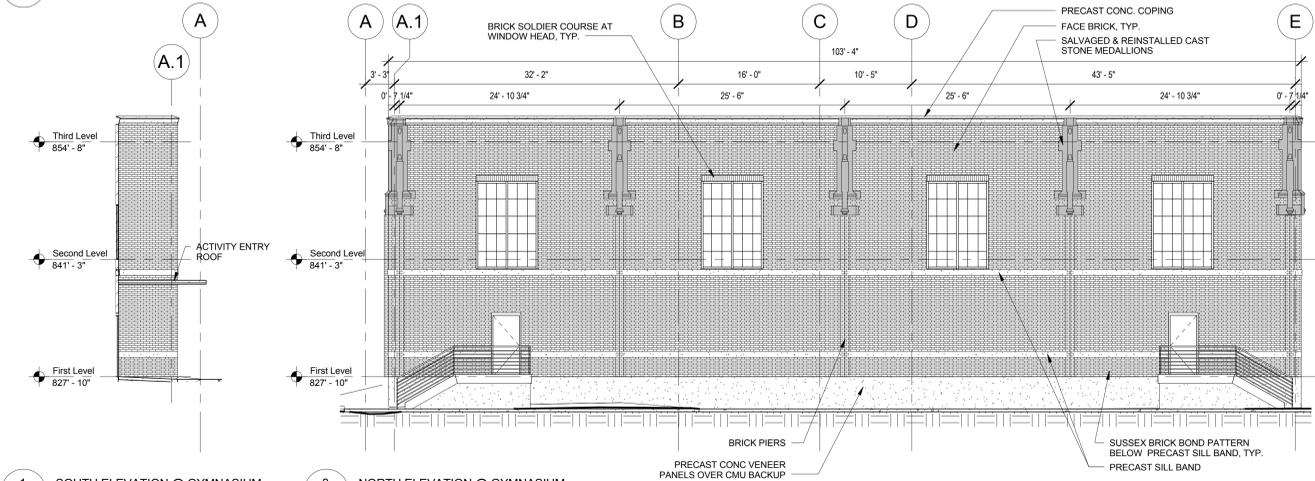
3C
A200 REFERENCE NORTH ELEVATION
1/16" = 1'-0"



6E
A200 REFERENCE WEST ELEVATION
1/16" = 1'-0"

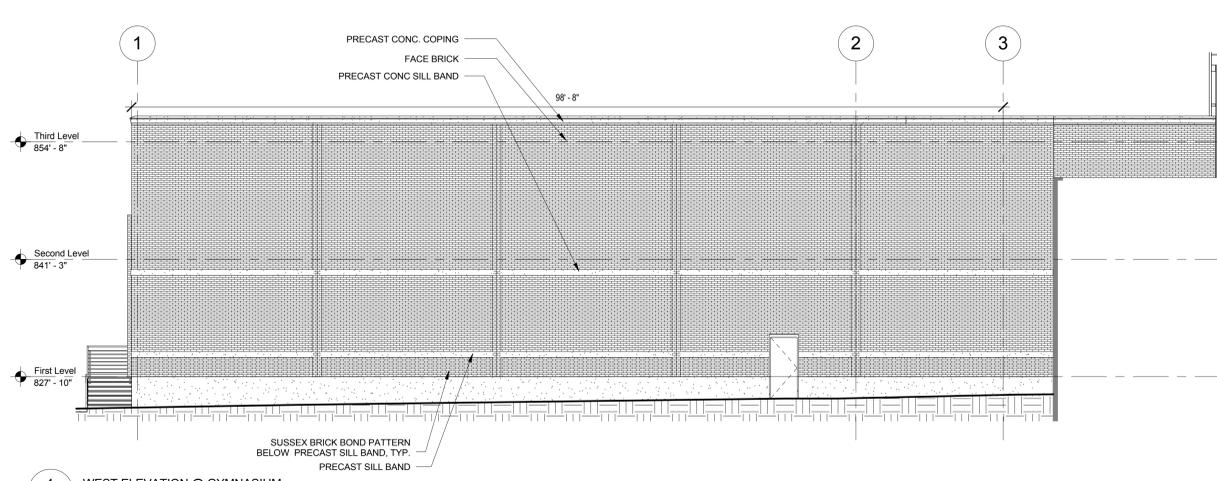


6B EAST ELEVATION
A201
1/8" = 1'-0"

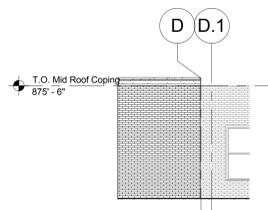


1 SOUTH ELEVATION @ GYMNASIUM
A201
1/8" = 1'-0"

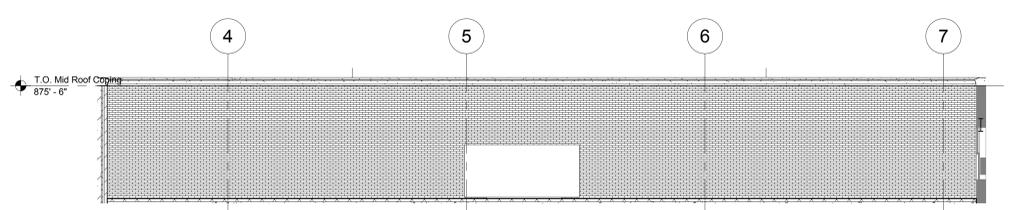
3 NORTH ELEVATION @ GYMNASIUM
A201
1/8" = 1'-0"



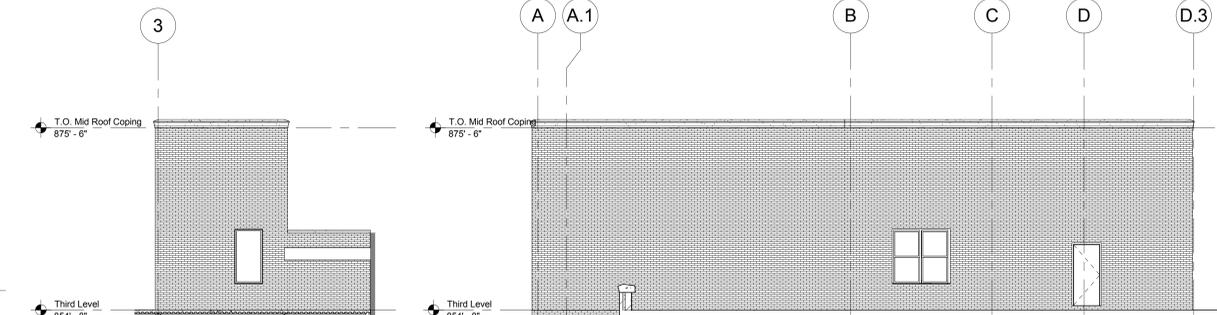
4 WEST ELEVATION @ GYMNASIUM
A201
1/8" = 1'-0"



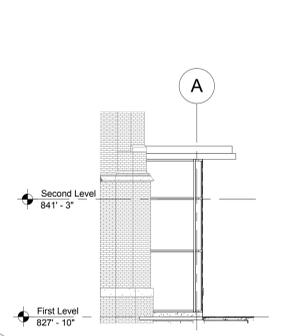
2 NORTH ELEVATION @ GIRLS TOILET
A201
1/8" = 1'-0"



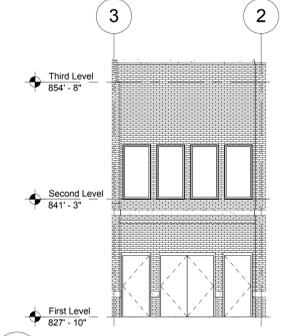
5 WEST ELEVATION @ THIRD FLOOR
A201
1/8" = 1'-0"



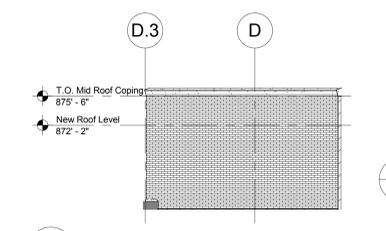
6 WEST ELEVATION @ STAIR TOWER
A201
1/8" = 1'-0"



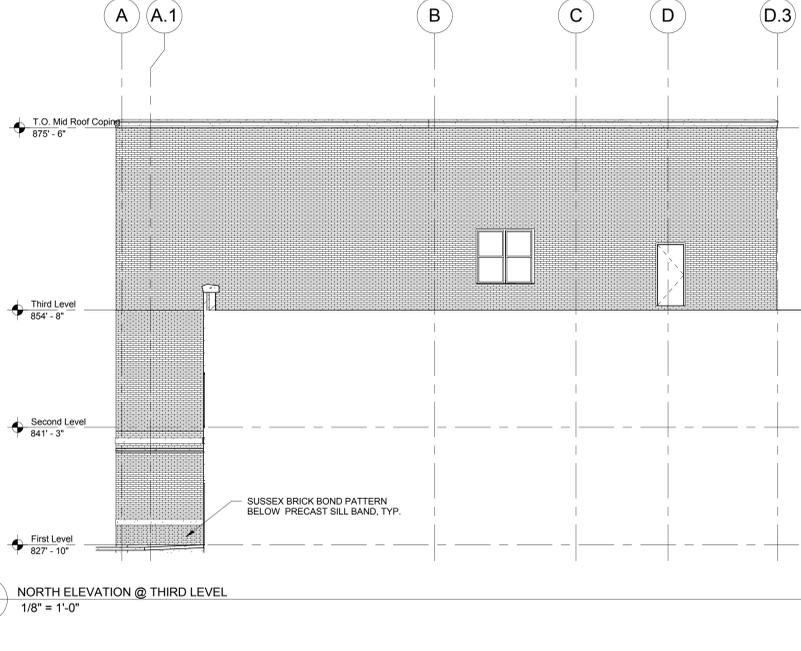
10 SOUTH ELEVATION @ VESTIBULE
A201
1/8" = 1'-0"



9 EAST ELEVATION @ ENTRY
A201
1/8" = 1'-0"



8 SOUTH ELEVATION @ STAIR TOWER
A201
1/8" = 1'-0"



7 NORTH ELEVATION @ THIRD LEVEL
A201
1/8" = 1'-0"

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Signature
Date
JULY 1, 2010

PROJECT:
Sanford Middle School
Expansion &
Renovations
Minneapolis Public Schools,
OP# 2767
3524 42nd Ave S, Minneapolis
MN 55406
MDA Project #MPS1401

REVISED:

PDR SUBMITTAL
NOT FOR
CONSTRUCTION
AUGUST 18, 2014

COMM. NO.: MPS1401
Date: AUGUST 18, 2014
Drawn: MDR
CHECKED: PGM

DRAWING TITLE:
EXTERIOR
ELEVATIONS

DRAWING NUMBER

A201

Percentage for Exterior Materials of Sanford Addition

| Elevation | Brick | Cast Stone | Metal Panel / Coping | Glass / Window | Remarks |
|-----------------|--------|------------|----------------------|----------------|---|
| East Elevation | 45% | 10.50% | 8% | 36.50% | Represents new main entry for school. Brick pattern on gymnasium portion of elevation is animated for variety. |
| North Elevation | 75% | 16.50% | 1% | 7.50% | North elevation is mostly gymnasium function, with some translucent glass for diffused light. Brick pattern is animated for variety. |
| West Elevation | 84.50% | 15% | 0.50% | 0% | West elevation is mostly gymnasium function without glass that causes unequal light distribution to the competition court. Brick pattern is animated for variety. |
| South Elevation | NA | NA | NA | NA | Little or no south facing addition |



NEW MAIN ENTRANCE



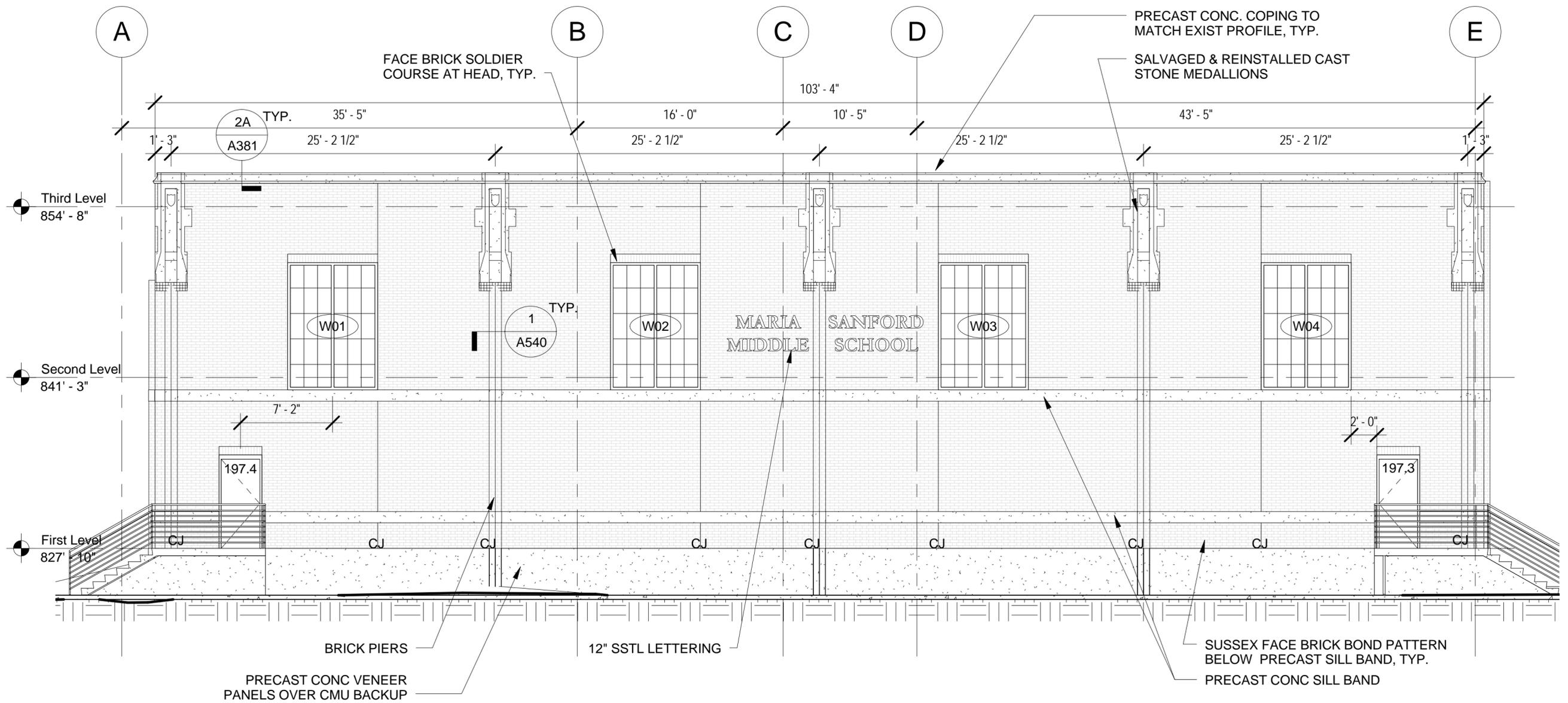
MINNEAPOLIS
PUBLIC SCHOOLS
Urban Education. Global Citizens.

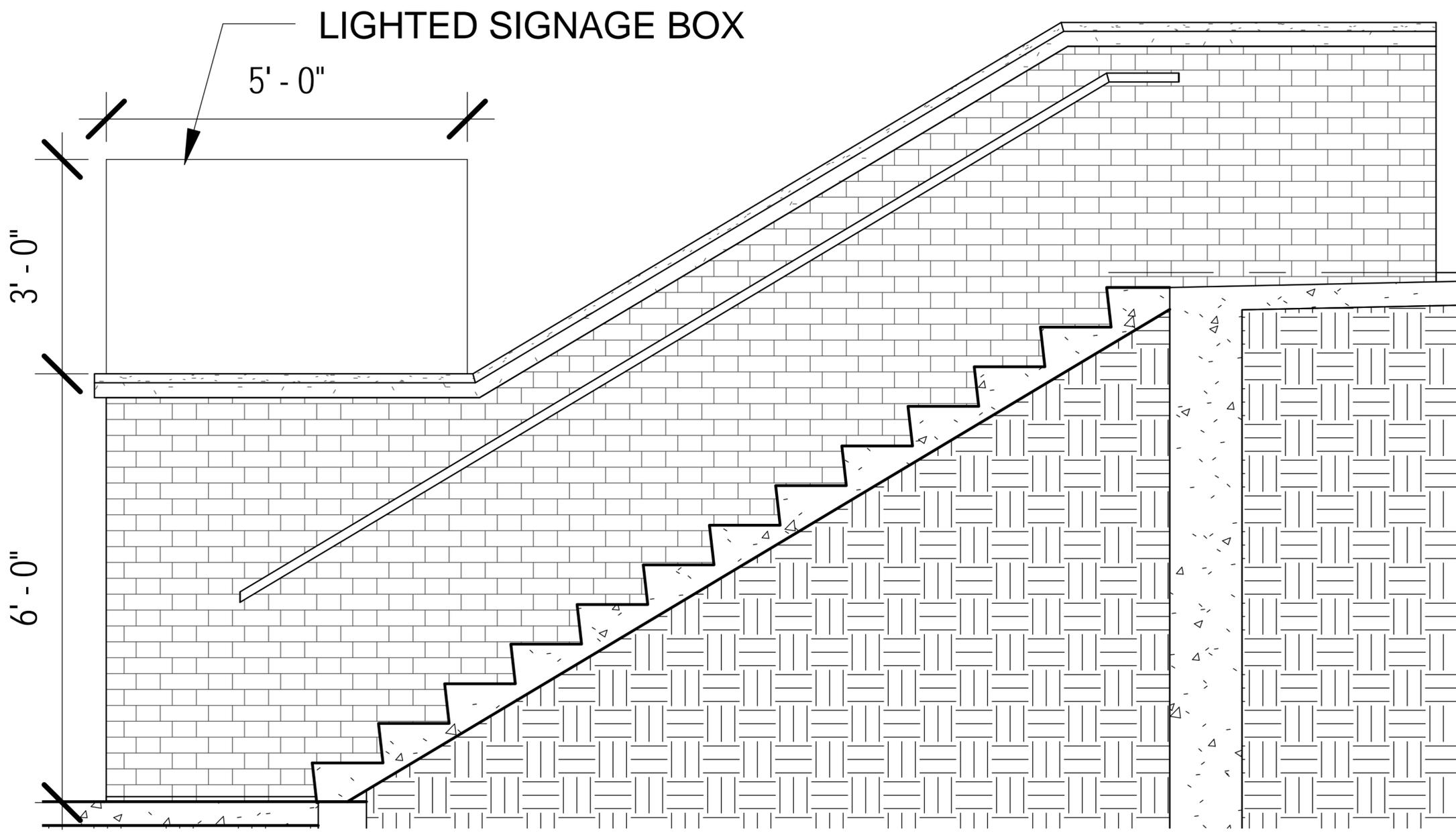


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ARCHITECTURE



NORTH - GYM PERSPECTIVE





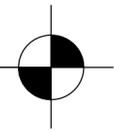
LIGHTED SIGNAGE BOX

5' - 0"

3' - 0"

6' - 0"

First Level
827' - 10"





Sanford Middle School:
Existing Conditions -
Shadow Studies

Sanford Middle School Expansion & Renovations

Minneapolis Public Schools, OP# 2767
MDA Project #MPS1401

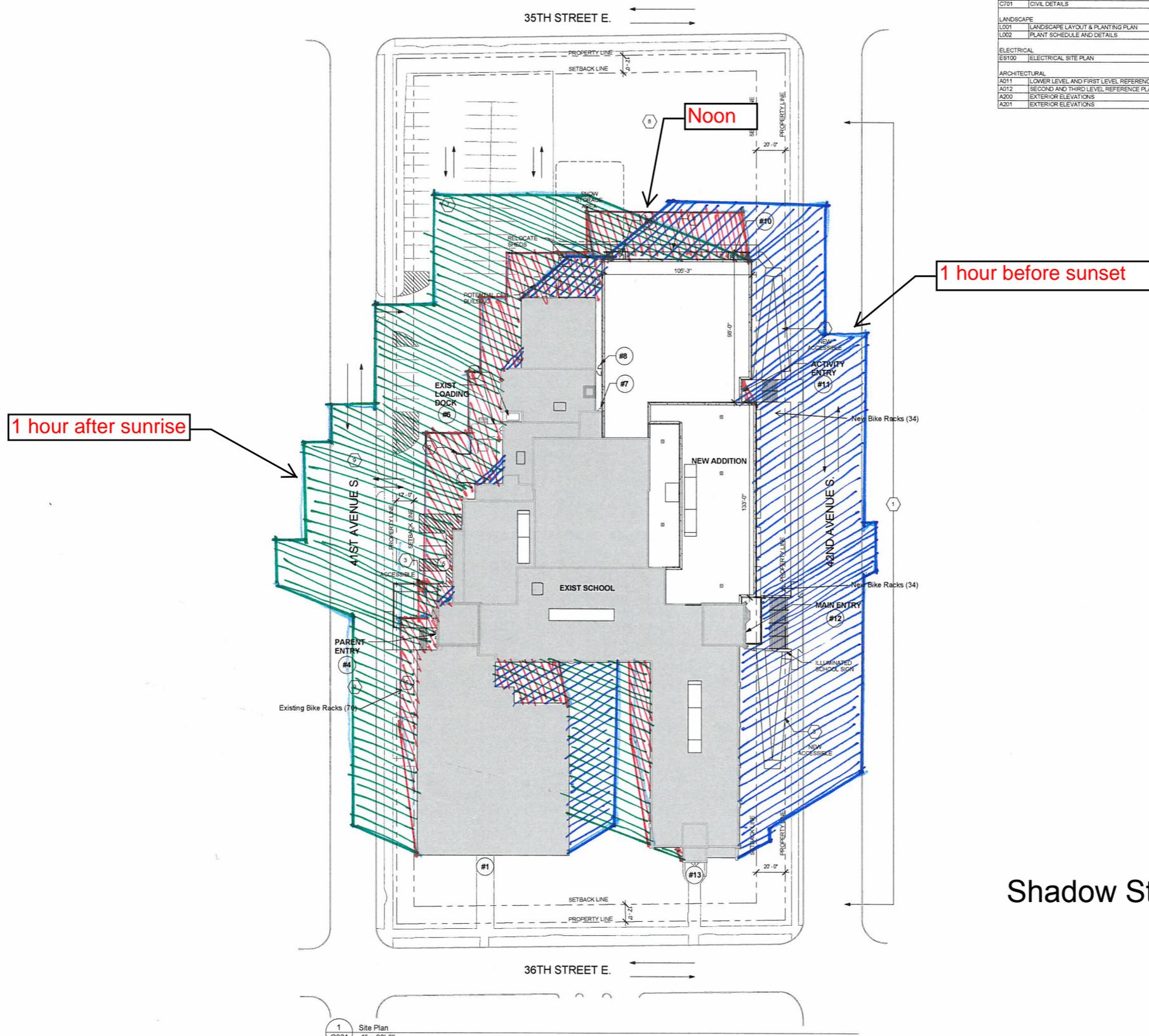
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| [Hatched Box] | EXIST BUILDING |
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| GENERAL INFORMATION | |
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Signature _____
Title _____
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PROJECT:
Sanford Middle School Expansion & Renovations
Minneapolis Public Schools, OP# 2767
3524 42nd Ave S, Minneapolis MN 55406
MDA Project #MPS1401

REVISED:

| Rev | Date | Issue |
|-----|------|-------|
| | | |

Site Plan Review
October 3, 2014

COMM. NO. MPS1401
Date: _____
Drawn: EKH
CHECKED: PGM

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DRAWING TITLE:
SITE PLAN

DRAWING NUMBER

G001

Shadow Study: March 21

1 Site Plan
G001 1" = 30'-0"

Sanford Middle School Expansion & Renovations

Minneapolis Public Schools, OP# 2767
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PROJECT

Sanford Middle School Expansion & Renovations

Minneapolis Public Schools, OP# 2767
3524 42nd Ave S, Minneapolis MN 55406

MDA Project #MPS1401

REVISED:

| NO. | DATE | REVISION |
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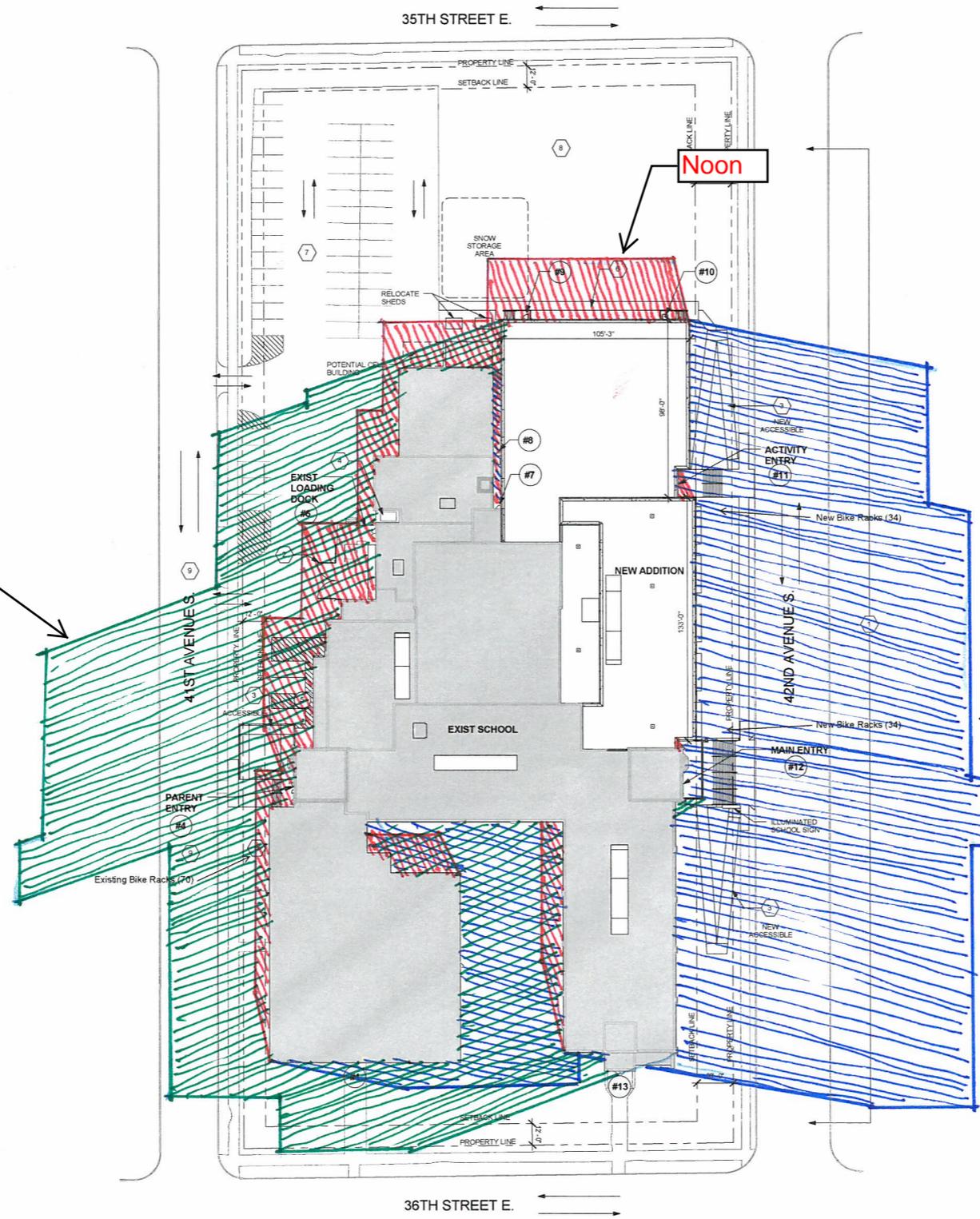
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October 3, 2014

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Drawn: EKH
Checked: PGM

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DRAWING TITLE:
SITE PLAN

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1 hour after sunrise

Noon

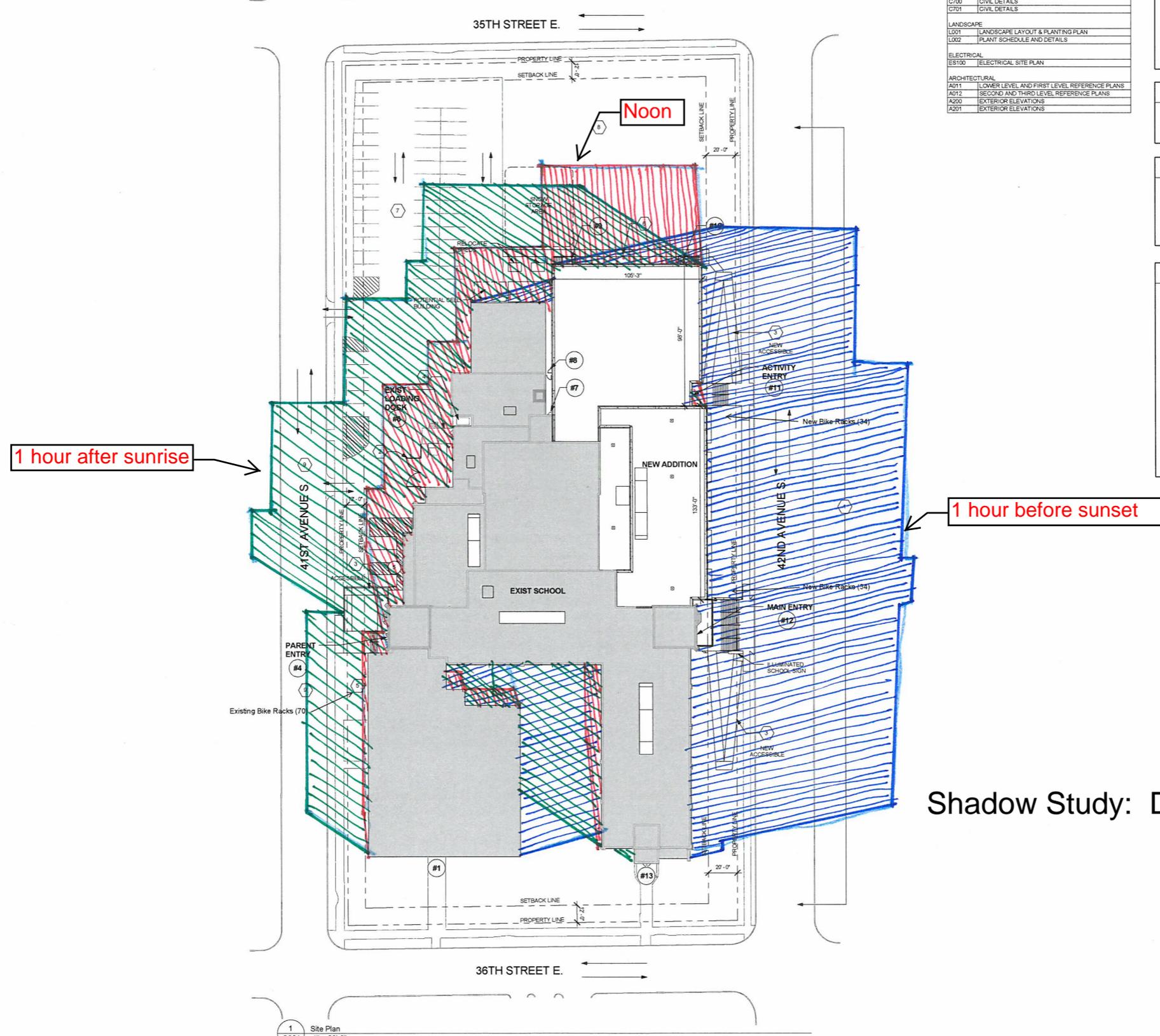
1 hour before sunset

Shadow Study: June 21

1 Site Plan
G001 1" = 30'-0"

Sanford Middle School Expansion & Renovations

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PROJECT: Sanford Middle School Expansion & Renovations
Minneapolis Public Schools, OP# 2767
3524 42nd Ave S, Minneapolis MN 55406
MDA Project #MPS1401

REVISED: _____
Date _____

Shadow Study: December 21

Site Plan Review
October 3, 2014

COMM. NO.: MPS1401
Date: _____
Drawn: EKH
CHECKED: PGM

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DRAWING TITLE: SITE PLAN

DRAWING NUMBER

G001

1 Site Plan
G001 1" = 30'-0"



Travel Demand Management Plan

Sanford Middle School Improvements

Minneapolis, Minnesota

Prepared for:

Minneapolis Public Schools

Prepared by:

Kimley-Horn and Associates, Inc.
St. Paul, Minnesota

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April 2014
160512026



Kimley-Horn
and Associates, Inc.



Travel Demand Management Plan

Sanford Middle School Improvements

Minneapolis, Minnesota

April 30, 2014

Prepared for:

Minneapolis Public Schools

Prepared by:

Kimley-Horn and Associates, Inc.

St. Paul, Minnesota

TRAVEL DEMAND MANAGEMENT PLAN
Sanford Middle School Improvements
Minneapolis, Minnesota

PLAN APPROVAL

Minneapolis Public Schools

By: _____ Dated: _____
David Richards, Director
Capital Planning and Project Development
Minneapolis Public Schools
1250 W Broadway Ave
Minneapolis, Minnesota 55411-2533

Minneapolis Community and Economic Development Department

By: _____ Dated: _____
CPED Planning Director

Minneapolis Public Works Department

By: _____ Dated: _____
Steve Mosing, PE, PTOE
Traffic Operations Engineer

TRAVEL DEMAND MANAGEMENT PLAN
Sanford Middle School Improvements
Minneapolis, Minnesota

REPORT CERTIFICATION

I hereby certify that this report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.



JoNette L. Kuhnau, P.E., PTOE
License No. 44250

4/30/2014

Date

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Background

Minneapolis Public Schools (MPS) is planning to renovate Sanford Middle School. The renovation will increase the enrollment capacity of the facility by about 200 students by adding approximately 30,000 square feet of space and approximately 20 parking spaces as well as relocating the entrance from the southwest corner to the east side of the building. The school's location is bounded by 41st Avenue S on the west, 42nd Avenue S on the east, 35th Street E on the north and 36th Street E on the south. The address is 3524 42nd Avenue S, 55406. **Figure 1** below shows the site location.



Figure 1. Site Location

The school enrollment, staff, building size and parking details are as follows:

Existing:

Students: 777

Staff: 100

Building area: 122,380 sf

On-site parking: 31 spaces (marked and unmarked)

The main entrance is currently on the southwest corner of the building.

In the morning before school, buses drop off students along 36th Street E and 41st Avenue S adjacent to the school. Parents drop off students along both sides of 41st Avenue S and 36th Street E as well as in the parking lot. In the afternoon, buses occupy the curb space adjacent to the school on 36th Street E and 41st Avenue S. Parents pick up students on



both sides of 41st Avenue S, 36th Street E across from the school, and on 42nd Avenue S adjacent to the school.

The existing conditions have several operational issues due to mixing of buses and parent vehicle traffic. This causes vehicle congestion and conflicts between pedestrians and vehicles. During winter months, it is difficult to maintain two-way traffic on 41st Avenue S, 35th Street E, and 36th Street E when there are vehicles parked on both sides of the roadway. Mid-block crossings of pedestrians have been observed in the past, due to insufficient curb space for pick-up/drop-off traffic. Vehicles observed to be parked too close to intersections also reduce sight lines. Issues with vehicular traffic yielding to the school patrols have also been reported.

Proposed:

Students: 960

Staff: 110

Building area: 150,000 sf

On-site parking: 51 spaces

After the renovation, the main entrance will be relocated to the east side of the building (42nd Avenue S). Additional bicycle parking will be added to the site. Buses will be relocated to 42nd Avenue S for pick-up and drop-off. Buses equipped for students with disabilities will pick-up and drop-off at 36th Street E. Parent pick-up and drop-off will be relocated to 41st Avenue S. 42nd Avenue S is a wider street that will reduce traffic impacts resulting from bus loading and unloading operations. 41st Avenue S is a lower volume street that is better suited for parent pick-up and drop-off. The separation of bus and parent pick-up and drop-off locations will improve the overall traffic conditions around the site during peak traffic periods.

The projected increase in enrollment is anticipated to come from within the school's attendance area, consistent with growth in enrollment throughout the MPS district.

The planned project schedule allows for substantial completion of the project and occupancy by the beginning of the 2016-2017 school year. The school will remain open during the 2014-2015 school year.



City of Minneapolis Transportation Goals

The following policies for transportation are included in Chapter 2 of the Minneapolis Plan for Sustainable Growth, adopted by the Minneapolis City Council, October 2, 2009:

Policy 2.1: Encourage growth and reinvestment by sustaining the development of a multi-modal transportation system.

Policy 2.2: Support successful streets and communities by balancing the needs of all modes of transportation with land use policy.

Policy 2.3: Encourage walking throughout the city by ensuring that routes are safe, comfortable, pleasant, and accessible.

Policy 2.4: Make transit a more attractive option for both new and existing riders.

Policy 2.5: Ensure that bicycling throughout the city is safe, comfortable and pleasant.

Policy 2.6: Manage the role and impact of automobiles in a multi-modal transportation system.

Policy 2.7: Ensure that freight movement and facilities throughout the city meet the needs of the local and regional economy while remaining sensitive to impacts on surrounding land uses.

Policy 2.8: Balance the demand for parking with objectives for improving the environment for transit, walking and bicycling, while supporting the city's business community.

Policy 2.9: Promote reliable funding and pricing strategies to manage transportation demand and improve alternative modes.

Policy 2.10: Support the development of a multi-modal Downtown transportation system that encourages an increasingly dense and vibrant regional center.

Policy 2.11: Minneapolis recognizes the economic value of Minneapolis-St. Paul International Airport and encourages its healthy competition to reach global markets in an environmentally responsible manner.



Travel Demand Management Plan Goals

In an effort to work toward these goals, the City of Minneapolis requires the preparation of a Travel Demand Management Plan (TDMP) for all non-residential development or additions over 100,000 square feet or more of gross floor area or any development or redevelopment projects deemed to have a potential substantial traffic impact.

The purpose of the TDMP is to manage the traffic generated by the school and plan for safe and efficient travel to and from the site for people of all ages and for pedestrian, bicycle, automobile and bus traffic. This plan outlines specific mitigation strategies that Minneapolis Public Schools will perform in to manage all modes of travel by students, parents, and staff to and from Sanford School.



Zoning and Land Uses

The existing zoning of the site is R1A, Single-Family District (Low Density). The City of Minneapolis Code of Ordinances describes the R1A district as follows:

“The R1A Single-family District is established to provide for an environment of predominantly low density, single-family dwellings and cluster developments on lots with a minimum of five thousand (5,000) square feet of lot area per dwelling unit. In addition to residential uses, institutional and public uses and public services and utilities may be allowed.”

No zoning changes are needed to accommodate the proposed redevelopment of the site.



Pedestrian, Bicycle, and Transit

Pedestrian

The site is located in the Howe neighborhood of Minneapolis and the greater Longfellow community. The sidewalk network mirrors the gridded roadway network and is highly connective. The location of the school within a single-family residential neighborhood where many of the students live decreases the need for vehicle trips to get to and from the school. Clearly marked crosswalks and crossing patrols increase the safety of the pedestrian network and further incentivize walking trips. As of October 2012, 17.5% of students lived within the 1-mile walk zone and 44% of students lived within two miles to the school. Students that walk or bike to school decrease the vehicle demand at the site. Bicycle and pedestrian infrastructure and crossing patrols make biking and walking a safe option.

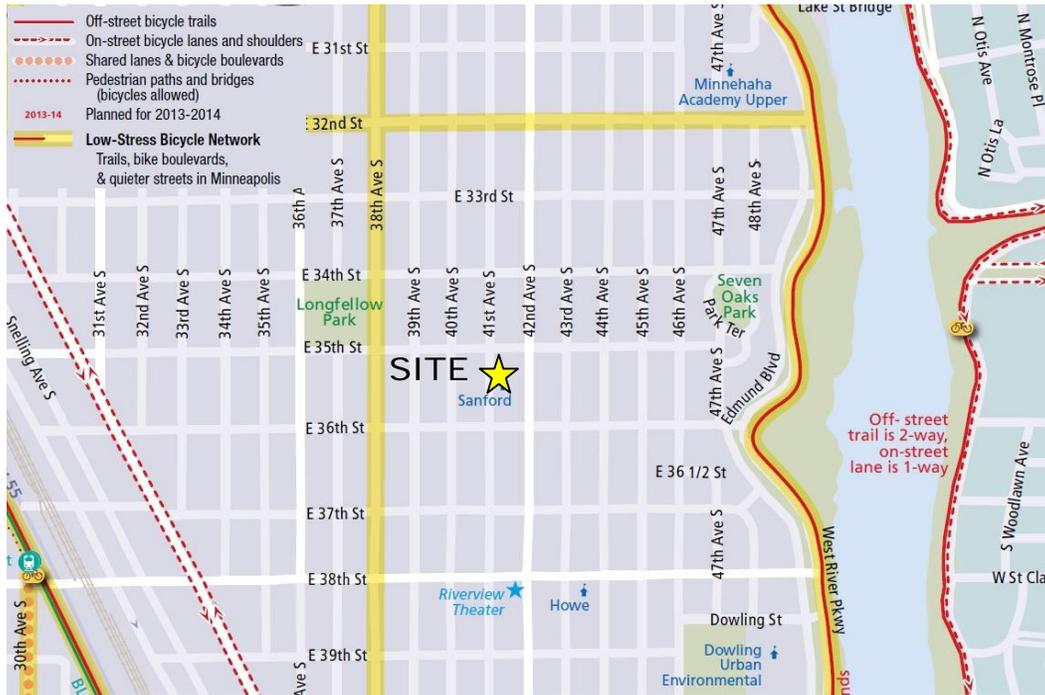
In addition to the sidewalk infrastructure, Sanford School operates school patrols at the following intersections:

- 42nd Avenue S/36th Street E – marked high visibility (zebra) crosswalks and school crossing signing
- 41st Avenue S/ 36th Street E – marked (parallel line) crosswalks and school crossing signing

The 42nd Avenue S/35th Street E has all-way stop control. All other intersections around the school have two-way stop control.

Bicycle

The Minneapolis Bicycle Master Plan does not identify bicycle facilities to be implemented on the roadways immediately adjacent to the site. 46th Avenue S and 38th Street E are planned as shared use bike facilities, 38th Avenue S is a planned signed bike route, and 32nd St E is planned as a bicycle boulevard. The low speed roadways around the site further increase the potential for biking as a mode of transportation to the site. Two bicycle racks are located on the west side of the building along 41st Avenue S and additional racks are proposed as part of the project.



Source: City of Minneapolis

Figure 2. Existing Bike Facilities

Transit

Metro Transit service in the project area consists of the local bus Route 9, which has stops adjacent to the school at 42nd Avenue S/36th Street E and 42nd Avenue S/35th Street E. During periods before and after school, there are a total of 5 buses that pass the school.

The following buses also travel through the area:

- Route 23 (E 38th Street)
- Route 7 (Minnehaha Avenue)
- Blue Line (Hiawatha Avenue)



Source: Metro Transit

Figure 3. Metro Transit Route 9



Parking

Existing Site

Today, Sanford Middle School has two primary sources of parking supply:

- **On-site surface parking lot** - Currently, the parking lot can accommodate a maximum of 31 vehicles in the existing conditions.
- **On-street parking** – Parking is available on all adjacent streets. On-street parking on 42nd Avenue adjacent to the site is restricted to 15-minute parking from 9 AM to 5 PM on school days. All other curb faces adjacent to the site are signed “No Parking 9AM-5PM School Days.”

Existing parking supply is not sufficient to meet demand by staff and students. During peak school traffic periods, parking issues have been observed including vehicles parked too close to intersections, restricted vehicle mobility when vehicles are parked on both sides of 35th Street E, 36th Street E, and 41st Avenue S, and double parking to pick-up and drop off students.

Improved Site

The existing surface parking lot will be expanded to 51 stalls as part of the improvements.

On-street parking will continue to be available adjacent to the site as it is today. Although the surface parking lot will be expanded, the parking supply on the school property will continue to be insufficient to meet demand. Parking can be managed by designating areas for parent pick-up/drop-off, reminding parents and staff about parking restrictions near intersections, and encouraging staff to use alternative modes to commute. In addition, school staff can assist with managing traffic flow and adherence to parking and pick-up/drop-off operations before and after school.



Travel Demand Management Strategies

The purpose of this TDMP is to assist the City of Minneapolis to achieve their overall transportation goals as they relate specifically to the Sanford Middle School site, by managing and minimizing the vehicle trips generated by the school.

This section outlines specific travel demand management strategies to be implemented by Sanford Middle School and the school's students, teachers, and parents. The strategies detail actions to be taken by MPS and Sanford Middle School in addressing the transportation issues cited in this document.

Minneapolis Public Schools, by accepting the responsibility of implementing the items below in conjunction with the remodeling of the Sanford Middle School, desires to help the City of Minneapolis achieve their goal of enhancing the local transportation system and helping to achieve a balance in the needs of all users of the transportation system.

Minneapolis Public Schools and its assigns specifically commit to the implementation of the following measures.

General

1. Minimize vehicle trips by encouraging students within the walk zone to bike and walk to school.
2. Install additional bicycle parking on the school site.
3. Designate 42nd Avenue for school bus pick-up and drop-off of students.
4. Designate 41nd Avenue and 35th Street for parent pick-up and drop-off of students.
5. Educate and periodically remind parents and staff about their role in creating a safe environment around the school site, specifically related to parking restrictions near intersections, crosswalks, and stop signs as well as obeying all school patrols.
6. Educate students on safe walking and biking habits, including crossing only at intersections and with the assistance of the designated school patrols.
7. Increase the number of staff or parent volunteers outside the school during the peak dismissal period to supervise students and assist with the school patrol.