



CPED STAFF REPORT
 Prepared for the City Planning Commission

CPC Agenda Item #8
 June 23, 2014
 BZZ-6576

LAND USE APPLICATION SUMMARY

Property Location: 2622 West Lake Street
Project Name: Not applicable
Prepared By: [Hilary Dvorak](#), Principal Planner, (612) 673-2639
Applicant: Greystar
Project Contact: David Graham with ESG
Request: A 90-unit residential building
Required Applications:

Conditional Use Permit	To increase the height of the building located in the SH Shoreland Overlay District from the permitted 2.5 stories/35 feet to 8 stories/100 feet.
Variance	To reduce the required 15-foot front yard setback along West Lake Street to allow for staircases, ground level patios, trellises and other similar features.
Variance	To reduce the required 15-foot corner side yard setback along Thomas Avenue South to allow for staircases, ground level patios, trellises and other similar features.
Variance	To reduce the required 19-foot east interior side yard setback to allow for staircases, ground level patios, trellises and other similar features.
Variance	To reduce the required 19-foot north rear yard setback to allow for staircases, ground level patios, trellises and other similar features.
Site Plan Review	For a new 8-story, 90-unit residential building.

SITE DATA

Existing Zoning	OR2 High Density Office Residence District SH Shoreland Overlay District
Lot Area	56,315 square feet / 1.29 acres
Ward(s)	7
Neighborhood(s)	Cedar Isles Dean
Designated Future Land Use	Urban Neighborhood
Land Use Features	Commercial Corridor (West Lake Street)
Small Area Plan(s)	Midtown Greenway Land Use and Development Plan

Date Application Deemed Complete	May 27, 2014	Date Extension Letter Sent	Not applicable
End of 60-Day Decision Period	July 26, 2014	End of 120-Day Decision Period	Not applicable

BACKGROUND

SITE DESCRIPTION AND PRESENT USE. The property is located in southwest Minneapolis on the northeast corner of West Lake Street and Thomas Avenue South. The site is currently vacant.

SURROUNDING PROPERTIES AND NEIGHBORHOOD. The site is surrounded by a high-density residential building, public park land and the Midtown Greenway. North of the Midtown Greenway is a low-density residential area. The site is located in the Cedar Isles Dean neighborhood.

PROJECT DESCRIPTION. The applicant is proposing to construct an 8-story, 90-unit multiple-family residential building with 160 underground parking spaces. The building is C-shaped with the opening of the building facing the east property line. The building has been designed to step down in height from West Lake Street to the Midtown Greenway. The portion of the building located along West Lake Street is eight stories and the portion along the Midtown Greenway is three stories. The area in the middle of the building will be used as an amenity deck with a club room, an outdoor swimming pool and terrace space. There will also be terrace space on the top of the eight-story portion of the building. There will be two levels of underground parking. There will be different entrances for the two parking levels but they will both be located along Thomas Avenue South.

RELATED APPROVALS. In October of 2005, land use applications (BZZ-2601) were submitted for a 10-story, 70-unit residential building on the site. The City Planning Commission denied the conditional use permit to increase the height of the building and a variance to increase the lot coverage. Ultimately, this application was withdrawn. Then in April of 2006, the City Planning Commission approved land use applications (BZZ-2861) for a seven-story, 46-unit residential building with ground floor commercial space. This development was never constructed. Although development never occurred on the site, the former two-story office building on the site was demolished in 2006.

PUBLIC COMMENTS. The Cedar Isles Dean neighborhood association submitted a resolution not to oppose the development, the Midtown Greenway Coalition submitted a letter of support for the project and the Minneapolis Park & Rec Board submitted a letter opposing the conditional use permit to increase the height of the building. Any additional correspondence received prior to the public meeting will be forwarded on to the Planning Commission for consideration.

ANALYSIS

CONDITIONAL USE PERMIT

The Department of Community Planning and Economic Development has analyzed the application to increase the height of the building located in the SH Shoreland Overlay District from the permitted 2.5 stories/35 feet to 8 stories based on the following [findings](#):

- I. *The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

The height requirement in the OR2 zoning district is four stories or 56 feet, whichever is less. However, the property is also located within 1,000 feet of the ordinary highwater mark of Lake Calhoun and Lake of the Isles and is therefore subject to the height requirements of the SH Shoreland Overlay District, which is two-and-a-half stories or 35 feet, whichever is less.

The purpose of the SH Shoreland Overlay District is established to “preserve and enhance the environmental qualities of the surface waters and the natural and economic values of Shoreland areas within the city, to provide for the efficient and beneficial utilizations of those waters and Shoreland area, to comply with the requirements of state law regarding the management of Shoreland areas, and to protect the public health, safety and welfare.”

Increasing the height of the building from two-and-a-half stories/35 feet to eight stories/100 feet would not be detrimental to or endanger the public health, safety, comfort or general welfare. The height of the building is compatible with the adjacent buildings to the west and has been designed to step down in height as it approaches the Midtown Greenway to the north.

2. *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

Increasing the height of the building from two-and-a-half stories/35 feet to eight stories/100 feet would not be injurious to the use and enjoyment of other property in the area. The site is a block onto itself as it is bordered by two streets, the Midtown Greenway and public park land. The tallest portion of the development is eight stories however it steps down to three stories as it approaches the Midtown Greenway. The building immediately to the west of the site is 12 stories in height and the 20-story Lake Point Condominiums is located two blocks to the west.

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

Increasing the height of the building will have no impact on utilities, access roads or drainage.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

Increasing the height of the building will have no impact on traffic congestion in the public streets.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

The proposed development would be consistent with the following policies of *The Minneapolis Plan for Sustainable Growth*:

Land Use Policy 1.1: Establish land use regulations to achieve the highest possible development standards, enhance the environment, protect public health, support a vital mix of land uses, and promote flexible approaches to carry out the comprehensive plan.

1.1.4 Support context-sensitive regulations for development and land use, such as overlay districts, in order to promote additional land use objectives.

1.1.5 Ensure that land use regulations continue to promote development that is compatible with nearby properties, neighborhood character, and natural features; minimizes pedestrian and vehicular conflict; promotes street life and activity; reinforces public spaces; and visually enhances development.

Land Use Policy 1.2: Ensure appropriate transitions between uses with different size, scale, and intensity.

1.2.1 Promote quality design in new development, as well as building orientation, scale, massing, buffering, and setbacks that are appropriate with the context of the surrounding area.

Land Use Policy 1.10: Support development along Commercial Corridors that enhances the street’s character, fosters pedestrian movement, expands the range of goods and services available, and improves the ability to accommodate automobile traffic.

1.10.1 Support a mix of uses – such as retail sales, office, institutional, high-density residential and clean low-impact light industrial – where compatible with the existing and desired character.

1.10.4 Encourage a height of at least two stories for new buildings along Commercial Corridors, in keeping with neighborhood character.

1.10.5 Encourage the development of high-density housing on Commercial Corridors.

6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

If the requested land use applications are approved, the proposal will comply with all provisions of the OR2 High Density Office Residence District.

ADDITIONAL STANDARDS TO INCREASE MAXIMUM HEIGHT

In addition to the conditional use standards, the city planning commission shall consider, but not be limited to, the following factors when determining the maximum height:

1. *Access to light and air of surrounding properties.*

The proposed development will not diminish access to light and air for surrounding properties given that the building will be constructed on a block that is onto itself. The closest property is located across the street from the site. There is at least 75 feet of open space between the two buildings.

2. *Shadowing of residential properties, significant public spaces, or existing solar energy systems.*

The applicant submitted two shadow studies. One of the studies depicts the shadowing difference between the previously approved project from 2006 and the proposed development at 12 noon in December, March/September and June and the other study depicts shadowing impacts in the morning, at 12 noon and in the afternoon in June, March/September and December. According to the shadow studies that were submitted by the applicant, the residential property to the west and a portion of the Midtown Greenway to the north will be shadowed by this building at various times throughout the year. Although there will be shadows cast on surrounding properties they won't be cast for the entire day. In addition, the building has been designed step down in height from West Lake Street to the Midtown Greenway which minimizes the shadowing impacts on surrounding properties.

3. *The scale and character of surrounding uses.*

An eight-story building would not be out of scale or character with the surrounding area given the height of the buildings to the west. The development may be considered to be out of scale and character with the one and two-story single family dwellings to the north, however given the context of the immediate properties the proposed development is appropriately scaled. The development site is located approximately 100 feet from the nearest residential property to the north.

4. *Preservation of views of landmark buildings, significant open spaces or water bodies.*

Constructing a building that is either two-and-a-half stories in height or eight stories in height would block views of Lake Calhoun and the open space around it from the properties to the north given the height of those properties. An eight-story building will block views of the public park land on the east side of the site from the residential building to the west much more than a two-and-a-half-story

building would, however, the way that the building has been designed maximizes views in this direction.

CONDITIONAL USES AND VARIANCES

In addition to the conditional use and variance standards contained in Chapter 525, Administration and Enforcement, the city planning commission and board of adjustment shall consider the following:

1. *The prevention of soil erosion or other possible pollution of public waters, both during and after construction.*

The applicant will be working closely with CPED, the Public Works Department and the various utility companies during the duration of the development to ensure that all procedures are followed in order to comply with city and other applicable requirements.

2. *Limiting the visibility of structures and other development from protected waters.*

The development site is located across the street from Lake Calhoun and several hundred feet from Lake of the Isles. The requested height increase would significantly increase the visibility of the development from both water bodies compared to a development built to comply with the SH Shoreland Overlay District. However, in comparison to several other buildings in the vicinity, the proposed building height is relatively modest and steps down from taller buildings to the west.

3. *The suitability of the protected water to safely accommodate the types, uses and numbers of watercraft that the development may generate.*

The proposed development will not generate watercraft activity on Lake Calhoun or Lake of the Isles.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance to reduce the required 15-foot front yard setback along West Lake Street to allow for staircases, ground level patios, trellises and other similar features based on the following [findings](#):

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

The applicant is seeking a variance to reduce the required 15-foot front yard setback along West Lake Street to allow for staircases, ground level patios, trellises and other similar features. The building itself is not located in the required front yard. All of the building features that need variances are permitted obstructions but they exceed the zoning code dimensional allowances that would allow them to be constructed as of right. All of the building features that require variances are located at grade level except for a stack of balconies on the southeast corner of the building that extends from the second floor through the seventh floor. There are other stacks of balconies on the front of the building but they are not located within the required front yard.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The applicant is proposing to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance. Wider staircases, deeper ground level patios, tall trellises and

other similar features are all reasonable building features and will complement the overall size of the building.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

The staircases, ground level patios, trellises and other similar features will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. In addition, the variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties. The building features will break up the ground level of the building and provide architectural articulation along the building wall.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance to reduce the required 15-foot corner side yard setback along Thomas Avenue South to allow for staircases, ground level patios, trellises and other similar features based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

The applicant is seeking a variance to reduce the required 15-foot corner side yard setback along Thomas Avenue South to allow for staircases, ground level patios, trellises and other similar features. In addition to these building features the applicant is proposing to locate an electrical transformer in the required corner side yard towards the north end of the property. The building itself is not located in the required corner side yard. All of the building features that need variances are permitted obstructions but they exceed the zoning code dimensional allowances that would allow them to be constructed as of right. All of the building features that require variances are located at grade level except for four stacks of balconies distributed along the building wall. The electrical transformer needs to be accessible from a public street. Given this requirement the applicant would prefer to locate the electrical transformer along Thomas Avenue South instead of along West Lake Street.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The applicant is proposing to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance. Wider staircases, deeper ground level patios, tall trellises and other similar features are all reasonable building features and will complement the overall size of the building. The electrical transformer is necessary for the development to occur. The northwest corner of the property is accessible from a public street and is the least visible from the public realm.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

The staircases, ground level patios, trellises and other similar features will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. In addition, the variance will not be detrimental to the health, safety, or welfare of the general public

or of those utilizing the property or nearby properties. The building features will break up the ground level of the building and provide architectural articulation along the building wall. The electrical transformer will be screened from the public street and sidewalk with landscaping.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance to reduce the required 19-foot east interior side yard setback to allow for staircases, ground level patios, trellises and other similar features based on the following [findings](#):

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

The applicant is seeking a variance to reduce the required 19-foot east interior side yard setback to allow for staircases, ground level patios, trellises and other similar features. The building itself is not located in the required east interior side yard. All of the building features that need variances are permitted obstructions but they exceed the zoning code dimensional allowances that would allow them to be constructed as of right. All of the building features that require variances are located at grade level except for four stacks of balconies distributed along the building wall.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The applicant is proposing to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance. Wider staircases, deeper ground level patios, tall trellises and other similar features are all reasonable building features and will complement the overall size of the building.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

The staircases, ground level patios, trellises and other similar features will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. In addition, the variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties. The building features will break up the ground level of the building and provide architectural articulation along the building wall.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance to reduce the required 19-foot north rear yard setback to allow for staircases, ground level patios, trellises and other similar features based on the following [findings](#):

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

The applicant is seeking a variance to reduce the required 19-foot north rear yard setback to allow for staircases, ground level patios, trellises and other similar features. The building itself is not located in the required rear yard. All of the building features that need variances are permitted

obstructions but they exceed the zoning code dimensional allowances that would allow them to be constructed as of right. Specifically, the building features that require variances are three stacks of balconies distributed along the building wall.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The applicant is proposing to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance. Balconies that extend three feet into the required rear yard are reasonable building features and will complement the overall size of the building.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

The deeper balconies will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. In addition, the variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties. The deeper balconies will provide architectural articulation along the building wall.

SITE PLAN REVIEW

The Department of Community Planning and Economic Development has analyzed the application based on the required [findings](#) and [applicable standards](#) in the site plan review chapter:

I. Conformance to all applicable standards of Chapter 530, Site Plan Review.

Building Placement and Design – Requires alternative compliance

- The building reinforces the street wall, facilitates pedestrian access and maximizes natural surveillance. The building is located at the front yard setback line along West Lake Street and the corner side yard setback line along Thomas Avenue South, there are entrances and exits at street level that can be accessed by residents and guests and there are large windows and balconies on all sides of the development that maximize the opportunities for people to observe adjacent spaces and the public sidewalks.
- The building walls fronting along both West Lake Street and Thomas Avenue South are being built 15 feet from the property lines. This is the required front and corner side yard setback required in the OR2 Zoning District.
- Where the building is setback from the property lines there are staircases, ground level patios, trellises and other similar features and landscaping.
- The main entrance to the building will be located along West Lake Street and the ground floor units facing the east interior property line will have individual entrances at street level.
- Parking for the proposed development will be located in two levels of enclosed parking beneath the building.
- The building is articulated with recesses and projections, varying window patterns, recessed and projecting balconies and different materials.
- There are areas of the building that are over 25 feet in length and void of windows, entries, recesses or projections, or other architectural elements. Those areas are located on the ground level facing Thomas Avenue South, the Midtown Greenway and the east interior property line. Alternative compliance is required.

- The exterior materials include brick, stone, metal and stucco. The sides and rear of the building are similar to and compatible with the front of the building.
- Plain face concrete block is not being proposed as an exterior building material.
- The development meets the minimum window percentages required by the zoning code. The windows in the development are vertical in nature and are evenly distributed along the building walls. See **Table I**.
- The entire building frontage along West Lake Street contains active functions. However, only 20 percent of the building frontage along Thomas Avenue South contains active functions. Alternative compliance is required.
- The principal roof line of the building will be flat. There is a mixture of flat and pitched roofed buildings in the area.

Table I. Percentage of Windows Required for Elevations Facing a Public Street, Sidewalk, Pathway, or On-Site Parking

Residential Uses	Code Requirement		Proposed	
	1 st Floor facing West Lake Street	20% minimum	266 sq. ft.	57 %
2 nd Floor and Above facing West Lake Street	10% minimum	183 sq. ft.	At least 40%	At least 745 sq. ft.
1 st Floor facing Thomas Avenue South	20% minimum	453 sq. ft.	24%	542 sq. ft.
2 nd Floor through and Above facing Thomas Avenue South	10% minimum	311 sq. ft.	At least 10%	At least 311 sq. ft.
1 st Floor facing the Midtown Greenway	20% minimum	266 sq. ft.	24%	316 sq. ft.
2 nd Floor and Above facing the Midtown Greenway	10% minimum	183 sq. ft.	At least 27%	At least 497 sq. ft.

Access and Circulation – Meets requirements

- All of the entrances leading into the building are directly connected to a public sidewalk.
- No integrated transit shelters are proposed as part of this development. Prior to the former building on the site being demolished the transit bench that was located along West Lake Street, in front of the site, was removed. The bus bench was an artistic vintage bus transit bench and a City of Minneapolis public art piece. The City’s Public Arts Administrator has expressed an interest in returning the artistic vintage bus transit bench back to this location once the development is complete. CPED is recommending that the applicant work with the City’s Public Arts Administrator to ensure that there is room in the right-of-way to accommodate and protect the artistic vintage bus transit bench.
- Parking for the proposed development will be located in two levels of enclosed parking beneath the building.
- There is no public alley on this block.
- The maximum impervious surface requirement in the OR2 zoning district is 70 percent. According to the materials submitted by the applicant 70 percent of the site will be impervious.

Landscaping and Screening – Meets requirements

- The applicant is meeting the landscaping and screening requirements of the zoning code. In addition to providing the required number of canopy trees and shrubs on the site, additional ornamental trees, grasses, perennials and annuals will be planted on the site. The applicant is also proposing to landscape the boulevard along West Lake Street with trees, perennials and annuals.

Table 2. Landscaping and Screening Requirements

	Code Requirement	Proposed
Lot Area	--	56,315 sq. ft.
Building footprint	--	39,369 sq. ft.
Remaining Lot Area	--	16,946 sq. ft.
Landscaping Required	3,389 sq. ft.	13,104 sq. ft.
Canopy Trees (1: 500 sq. ft.)	7 trees	19 trees
Shrubs (1: 100 sq. ft.)	34 shrubs	198 shrubs

Additional Standards – Meets requirements

- Stormwater runoff will be accommodated in the open green spaces on the site and in an underground retention system.
- The proposed building will block views of Lake Calhoun and the open space around it from the properties to the north given the height of those properties. In addition, an eight-story building will block views of the public park land on the east side of the site from the residential building to the west, however, the way that the building has been designed maximizes views in this direction.
- The applicant submitted two shadow studies. One of the studies depicts the shadowing difference between the previously approved project from 2006 and the proposed development at 12 noon in December, March/September and June and the other study depicts shadowing impacts in the morning, at 12 noon and in the afternoon in June, March/September and December. According to the shadow studies that were submitted by the applicant, the residential property to the west and a portion of the Midtown Greenway to the north will be shadowed by this building at various times throughout the year. Although there will be shadows cast on surrounding properties they won't be cast for the entire day. In addition, the building has been designed step down in height from West Lake Street to the Midtown Greenway which minimizes the shadowing impacts on surrounding properties.
- The building has been designed with different setbacks at different levels of the building which should help minimize wind effects on the surrounding area.
- The site plan complies with crime prevention design elements as there are walkways that direct people to the building entrances, there are large windows and balconies on all sides of the building that maximize the opportunities for people to observe adjacent spaces and the public sidewalks and there are lights located near all of the pedestrian entrances and along the pedestrian alley.
- The site is not located in a historic district.

2. Conformance with all applicable regulations of the zoning ordinance.

The proposed use is *permitted* in the OR2 High Density Office Residence District.

Off-street Parking and Loading – Meets requirements

Table 3. Vehicle Parking Requirements Per Use (Chapter 54I)

	Minimum Parking Requirement	Applicable Reductions	Total Minimum Requirement	Maximum Parking Allowed	Proposed
Residential dwellings	90	OR2 and OR3 zoning districts (9)	81	None if located in an enclosed structure	160
Total	90	81	81	--	160

Table 4. Bicycle Parking and Loading Requirements (Chapter 54I)

	Minimum Bicycle Parking	Minimum Short-Term	Minimum Long-Term	Proposed	Loading Requirement	Proposed
Residential dwellings	45	--	Not less than 90%	90	None	None
Total	45	--	41	90	--	--

Building Bulk and Height – Requires conditional use permit

- The development meets the maximum gross floor area requirement of the zoning code but a conditional use permit is required to increase the height of the building. The proposed height of the building exceeds both the district height limitation of 4 stories or 56 feet and the height limitation in the SH Shoreland Overlay District limitation of two-and-a-half stories or 35 feet.

Table 5. Building Bulk and Height Requirements

	Code Requirement	Density Bonus	Proposed
Lot Area	--	--	56,315 sq. ft. / 1.29 acres
Gross Floor Area (GFA)	--	--	168,945 sq. ft.
Maximum Floor Area Ratio (GFA/Lot Area)	2.5	20% enclosed parking density bonus 3.0	3.0
Maximum Building Height	4 stories or 56 feet, whichever is less		8 stories/100 feet

Residential Lot Requirements – Meets requirements

Table 6. Residential Lot Summary

	Code Requirement	Proposed
Dwelling Units (DU)	--	90 DUs
Density (DU/acre)	--	70 DU/acre
Minimum Lot Area	5,000 sq. ft.	56,315 sq. ft.
Minimum Lot Width	--	--

Yard Requirements – *Requires variance(s)*

Table 7. Minimum Yard Requirements

	Zoning District	Proposed Building Setback	Permitted Obstructions
Front	15 ft.	15 ft.	As close as 0 ft.
Corner Side	15 ft.	15 ft.	As close as 0 ft.
Interior Side (East)	19 ft.	19 ft.	As close as 15 ft.
Rear	19 ft.	20 ft.	As close as 17 ft.

Signs – *Meets requirements*

Table 8. Signage Summary

	Number Allowed Per Zoning Lot	Proposed Number	Maximum Size Allocation	Maximum Area Per Sign	Proposed Area	Maximum Allowed Height	Proposed Height
Attached	No limit	1	255 sq. ft.	180 sq. ft.	48 sq. ft.	14 ft.	13 ft.

Dumpster Screening – *Meets requirements*

- There will be a trash and recycling storage area provided on the first floor of the building.

Screening of Mechanical Equipment – *Meets requirements*

- An electrical generator will be located along Thomas Avenue South between the building and the property line. The generator will be screened with landscaping. The mechanical equipment that will be located on the building will be located in an enclosed mechanical penthouse which has been designed to blend in with the exterior of the building.

Lighting – *Meets requirements*

- A lighting plan showing footcandles was submitted as part of the application materials. The lighting plan meets the requirements of Chapter [535](#) of the Zoning Code.

Impervious Surface Area – *Meets requirements*

- The maximum impervious surface requirement in the OR2 zoning district is 70 percent. According to the materials submitted by the applicant 70 percent of the site will be impervious.

Specific Development Standards – *Not applicable*

SH Shoreland Overlay District Standards – *Requires conditional use permit*

- The applicant has applied for a conditional use permit to increase the height of the building.

3. Conformance with the applicable policies of *The Minneapolis Plan for Sustainable Growth*.

The Minneapolis Plan for Sustainable Growth identifies the site as Urban Neighborhood on the future land use map. The proposed development is consistent with the following principles and policies outlined in the comprehensive plan:

Urban Design Policy 10.4: Support the development of residential dwellings that are of high quality design and compatible with surrounding development.

10.4.1 Maintain and strengthen the architectural character of the city's various residential neighborhoods.

10.4.2 Promote the development of new housing that is compatible with existing development in the area and the best of the city's existing housing stock.

Urban Design Policy 10.5: Support the development of multi-family residential dwellings of appropriate form and scale.

Policy 10.6: New multi-family development or renovation should be designed in terms of traditional urban building form with pedestrian scale design features at the street level.

10.6.1 Design buildings to fulfill light, privacy, and view requirements for the subject building as well as for adjacent properties by building within required setbacks.

10.6.2 Promote the preservation and enhancement of view corridors that focus attention on natural or built features, such as the Downtown skyline, landmark buildings, significant open spaces or bodies of water.

10.6.3 Provide appropriate physical transition and separation using green space, setbacks or orientation, stepped down height, or ornamental fencing to improve the compatibility between higher density and lower density residential uses.

10.6.4 Orient buildings and building entrances to the street with pedestrian amenities like wider sidewalks and green spaces.

10.6.5 Street-level building walls should include an adequate distribution of windows and architectural features in order to create visual interest at the pedestrian level.4. Conformance with applicable development plans or objectives adopted by the City Council.

4. Conformance with applicable development plans or objectives adopted by the City Council.

The *Midtown Greenway Land Use and Development Plan* was approved by the City Council in February of 2007. The plan identifies this site as high density housing. Buildings should be designed so solar access to the Midtown Greenway is maintained and developments should promote security on the greenway by having doors, windows and balconies facing the greenway itself.

5. Alternative compliance.

The Planning Commission or zoning administrator may approve alternatives to any site plan review requirement upon finding that the project meets one of three criteria required for [alternative compliance](#). Alternative compliance is requested for the following requirements:

- **Blank Walls.** There are areas of the building that are over 25 feet in length and void of windows, entries, recesses or projections, or other architectural elements. Those areas are located on the ground level facing Thomas Avenue South, the Midtown Greenway and the east interior property line. Given the grade changes on the site the first level of the underground parking garage is exposed for a portion of the building on the ground level. CPED is recommending that alternative compliance be granted. The applicant is proposing to plant landscape materials along the perimeter of the building which will help break up the building wall.

- **Active Functions.** The entire building frontage along West Lake Street contains active functions. However, only 20 percent of the building frontage along Thomas Avenue South contains active functions. Given the grade changes on the site the first level of the underground parking garage is exposed for a portion of the building on the ground level. CPED is recommending that alternative compliance be granted. The applicant is proposing to plant landscape materials along the perimeter of the building which will help break up the building wall.

RECOMMENDATIONS

Recommendation of the Department of Community Planning and Economic Development for the Conditional Use Permit:

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the application for a conditional use permit to increase the height of the building located in the SH Shoreland Overlay District from the permitted 2.5 stories/35 feet to 8 stories/100 feet at the property located at 2622 West Lake Street, subject to the following conditions:

- I. The conditional use permit shall be recorded with Hennepin County as required by Minn. Stat. 462.3595, subd. 4 before building permits may be issued or before the use or activity requiring a conditional use permit may commence. Unless extended by the zoning administrator, the conditional use permit shall expire if it is not recorded within two years of approval.

Recommendation of the Department of Community Planning and Economic Development for the Variance:

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the application for a variance to reduce the required 15-foot front yard setback along West Lake Street to allow for staircases, ground level patios, trellises and other similar features at the property located at 2622 West Lake Street.

Recommendation of the Department of Community Planning and Economic Development for the Variance:

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the application for a variance to reduce the required 15-foot corner side yard setback along Thomas Avenue South to allow for staircases, ground level patios, trellises and other similar features and an electrical transformer at the property located at 2622 West Lake Street.

Recommendation of the Department of Community Planning and Economic Development for the Variance:

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the application for a variance to reduce the required 19-foot east interior side yard setback to allow for staircases, ground level patios, trellises and other similar features at the property located at 2622 West Lake Street.

Recommendation of the Department of Community Planning and Economic Development for the Variance:

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the application for a variance to reduce the required 19-foot north rear yard setback to allow for staircases, ground level patios, trellises and other similar features at the property located at 2622 West Lake Street.

Recommendation of the Department of Community Planning and Economic Development for the Site Plan Review:

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the site plan review application to allow a new 8-story, 90-unit residential building at the property located at 2622 West Lake Street, subject to the following conditions:

1. Approval of the final site plan, landscaping plan and elevations by the Department of Community Planning and Economic Development.
2. All site improvements shall be completed by June 23, 2016, unless extended by the Zoning Administrator, or the permit may be revoked for non-compliance.
3. The applicant shall work with the City's Public Arts Administrator to ensure that there is room in the right-of-way to accommodate and protect the artistic vintage bus transit bench.

ATTACHMENTS

1. PDR report
2. Written description and findings submitted by applicant
3. Zoning map
4. Civil plans, landscape plans, site photos, renderings, shadow studies, floor plans, building elevations, lighting plan
5. Correspondence



Minneapolis Development Review
250 South 4th Street
Room 300
Minneapolis, MN 55415

Preliminary Development Review Report

Development Coordinator Assigned: **PATRICIA MURZYN**
(612) 673-5827
patricia.murzyn@minneapolismn.gov

Status *
RESUBMISSION REQUIRED

Tracking Number:	PDR 1001201
Applicant:	GREYSTAR DEVELOPMENT 730 BERING DRIVE, SUITE 400 HOUSTON, TX 77057
Site Address:	2622 LAKE ST W
Date Submitted:	20-MAY-2014
Date Reviewed:	28-MAY-2014

Purpose

The purpose of the Preliminary Development Review (PDR) is to provide Customers with comments about their proposed development. City personnel, who specialize in various disciplines, review site plans to identify issues and provide feedback to the Customers to assist them in developing their final site plans.

The City of Minneapolis encourages the use of green building techniques. For additional information please check out our green building web page at: http://www.ci.minneapolis.mn.us/mdr/GreenBuildingOptions_home.asp.

DISCLAIMER: *The information in this review is based solely on the preliminary site plan submitted. The comments contained in this report are preliminary ONLY and are subject to modification.*

Project Scope

New 90 unit apartment building with at grade and underground parking.

Review Findings (by Discipline)

Zoning - Planning

- Land use applications have been submitted for this site. The project is tentatively scheduled for the June 23, 2014, CPC meeting pending submittal of the incomplete items.
- Verify that the alignment of the sidewalk along Thomas Avenue South meets PW and ADA standards.

Historical Preservation Committee

- There is no HPC flag on this property. HPC review is not required at this time.

*Approved: You may continue to the next phase of developing your project.
*Resubmission Required: You cannot move forward or obtain permits until your plans have been resubmitted and approved.

□ Addressing

- Per City of Minneapolis Street Naming and Address Standard V1.22, the City of Minneapolis holds authority for assignment of all addresses, verification, change, and/or additions. Each assigned address number uses the street that provides the best/direct access for life safety equipment and best/direct access to the occupants.
- The address for the proposed apartment building will be 2622 West Lake Street . This address meets the City of Minneapolis Street Naming and Address Standard requirements.
- When assigning suite sequences the following guidelines are as follows:
- The first one to two digits of the suite sequence number will designate the floor number of the site.
- The last two digits of the suite sequence number will designate the unique ID for the unit (condo, suite, unit, or apartment).
- Suite sequence digit numbers will be assigned to dwelling, commercial and retail units, not common areas. For example, laundry rooms, saunas, workout rooms, etc., would not be assigned numbers.
- Please provide each condo, suite, unit or apartment number.
- This building is also considered to have a parking ramp per MCO Chapter 108. As such, within 5 years of the date of the certificate of occupancy being issued, the parking ramp will be required to have annual inspections and apply for a Ramp Operating Certificate.

□ Parks - Forestry

- Contact Paul Martinson (612-499-9209) regarding any questions related to adding, removal or the process for protection of trees during construction in the city right of way.
- Effective January 1, 2014, the City of Minneapolis and the Minneapolis Park and Recreation Board adopted an update to the existing Parkland Dedication Ordinance. The adopted City of Minneapolis Parkland Dedication ordinance is located in Section 598.340 of the City's Land Subdivision ordinance:
 - <http://library.municode.com/index.aspx?clientId=11490>.
- As adopted, the fee in lieu of dedication for new residential units is \$1,500 per unit (affordable units excluded per ordinance) and for commercial and industrial development it is \$200 per development employee (as defined in ordinance). Any dedication fee (if required) must be paid at the time of building permit issuance.
- There is also an administration fee that is 5% of the calculated park dedication fee.
- As proposed, for your PROJECT, the calculated dedication fee is as follows:
- Park Dedication Fee Calculation =
- Residential (90 units x \$1,500 per unit) = \$135,000
- 5% of \$135,000 (Administration Fee) = \$ 6,750
- Total Park Dedication Administrative Fee: \$141,750
- This is a preliminary calculation based on your current proposal; a final calculation will be made at the time of building permit submittal.
- For further information, please contact Patty Murzyn 612-673-5827.

□ Construction Code Services

- A soils report should be provided to review groundwater levels and address any potential impacts to the below grade parking level.
- Since the highest occupied level is more than 75' above fire department access, verify that all of the requirements for High Rise buildings have been addressed (MBC section 403)
- 2 means of egress will be required from the second level patio.
- Depending on the final design, uses and layout of the first level lobby, 2 exits may be required. A required exit may not pass thru a garage.
- Where exit enclosures egress thru the level of exit discharge, a clear, unobstructed, readily visible and identifiable way to the discharge must be provided (MBC 1024.1 exception 1)
- Verify that the exits in the tower portion of the structure meet the separation requirements of MBC section 1015.2.1.
- Where individual dwelling units exceed an occupant load of 16, 2 means of egress are required. 3 of the 5 units on the 8th level exceed 3,200 s.f.

❑ Right of Way

- An encroachment permit shall be required for all streetscape elements in the Public right-of-way such as: plants & shrubs, planters, tree grates and other landscaping elements, sidewalk furniture (including bike racks and bollards), and sidewalk elements other than standard concrete walkways such as pavers, stairs, raised landings, retaining walls, access ramps, and railings (NOTE: railings may not extend into the sidewalk pedestrian area). Please contact Bob Boblett at (612) 673-2428 for further information.
- Note to the Applicant: Any elements of an earth retention system and related operations (such as construction crane boom swings) that fall within the Public right-of-way will require an encroachment permit application. If there are to be any earth retention systems which will extend outside the property line of the development then a plan must be submitted showing details of the system. All such elements shall be removed from the Public right-of-way following construction with the exception of tie-backs which may remain but must be uncoupled and de-tensioned. Please contact Bob Boblett at (612) 673-2428 for further information.
- In addition, any elements of an earth retention system and related excavations that fall within the Public right-of-way will require a "Right-of-Way Excavation Permit". This permit is typically issued to the General Contractor just prior to the start of construction. However, it is the Applicant's responsibility to insure that all required permits have been acquired by its consultants, contractors, sub-contractor's prior to the start of work.
- Public sidewalks located on private property may require the property owner to grant sidewalk easements to the City.
- The highway easement along Lake St. W. will serve as a sidewalk easement. No further action is necessary.

❑ Street Design

- The layout of the sidewalk and driveway aprons along Thomas Ave. S. does not meet ADA requirements; a 6' unobstructed clear pedestrian zone needs to be provided (in a straight line) for the extent of the Project.

❑ Sidewalk

- A 6' minimum dimension is required at the corner on Thomas Avenue South and Lake Street between the proposed planter and the existing traffic light base.
- An ADA compliant pedestrian ramp is required at the crosswalk at the intersection of Thomas Ave. S. and W. Lake St. Construct one (1) ADA compliant pedestrian ramp crossing Thomas Ave. S. at this location. Include the appropriate details and standard plates in the site plan, refer Mn/DOT Standard Plan 5-297.250 Pedestrian Curb Ramp Details at: <http://standardplans.dot.state.mn.us/stdplan.aspx>. Public Works recommends open planting beds rather than raised planters along W. Lake St.. Given the high salt environment, sod is not recommended, rather the Applicant should consider salt resistant trees and other plant species. The Applicant should consult with Paul Martinson of the Minneapolis Park Board on the selection of salt resistant tree & plant species. The Applicant should also consider widening the planted boulevard space as much as possible, the current width is considered the absolute minimum necessary to sustain plant growth.
- Tree planting details shall be included in the plans. The Applicant shall provide some form of engineered/structured soil in the form of a tree trench or tree pit for all proposed street trees. Sidewalk layouts and landscaping in the Public right-of-way shall follow established design standards; refer to the following:
 - City of Minneapolis Urban Forest Policy (http://www.minneapolismn.gov/www/groups/public/@cped/documents/webcontent/convert_282934.pdf.)
 - Access Minneapolis - Design Guidelines for Streets and Sidewalks (http://www.minneapolismn.gov/publicworks/transplan/comp/public-works_trans-plan_designguidelines.)

❑ Water

- The plan as submitted meets the requirements of the Public Works Water Maintenance & Distribution Division.

□ Traffic and Parking

- Please contact Bill Prince at (612) 673-3901 regarding existing and proposed street lighting. All street lighting (existing and proposed) shall be shown clearly on the site plan.
- Contact Allan Klugman (612) 673-5750 prior to construction for the temporary removal/temporary relocation of any City of Minneapolis signal system that maybe near the construction limits as the construction project will assume all costs.
- It is recommended that the vehicle access points be combined and the ramp designed to have internal circulation. Current ordinance states that all maneuvers associated with loading, parking or sanitation pick up for a private development shall occur on private property. Please provide a narrative explaining the trash removal operations and show turning maneuvers for all truck type vehicles that will be using the loading and parking entrance areas.
- Vehicle ramps into parking structures shall have no more than a 4% slope for the first 20 feet from the right of way (please indicate slope on the drawings).
- The plans indicate an "enhanced bus stop" at the intersection of Thomas Ave. S. and W. Lake St.; the Applicant shall provide verification that Metro Transit has been notified and approvals given for the bus stop proposal. Please contact Robert Rimstad at (612) 349-7768 for further information.

□ Sewer Design

- Groundwater: The submitted materials indicate additional piezometer readings were to be continued after the geotechnical report, dated March 16, 2014. If copies of these readings are available, please provide them.
- It must be adequately demonstrated that excessive pumping of groundwater will not be necessary in order to keep the below grade areas dry. Please provide calculations demonstrating the peak groundwater discharge rate and yearly volume. These calculations should be based on the seasonal high groundwater elevation.
- The geotechnical report recommends a drain tile system be installed below the basement floor slab. This is not indicated on the plans. Please verify if a drain tile system or foundation drains are proposed and show these on the plans, including the proposed invert elevations and proposed discharge locations.
- Stormwater Management: The water quality requirements of the City's Stormwater Management Ordinance (Ch. 54 MCO) requires both 70% TSS removal and 30% phosphorous load reduction, as the site discharges to Lake Calhoun. Please provide documentation that the 30% phosphorous load reduction is also being met.
- The water quality requirements of the City's Stormwater Management Ordinance (Ch. 54 MCO) must be evaluated based on the NURP particle size distribution. Please provide documentation demonstrating the proposed site will meet the water quality requirements for a NURP particle size distribution.
- The City's Stormwater Management Ordinance (Ch. 54 MCO) does not require that the proposed stormwater runoff volume not exceed the existing runoff volume. Please clarify the requirement that is referenced on page 3 of the Final Stormwater Management Report.
- In the Final Stormwater Management Report, please use the term "pre-settlement" when referring to native conditions. In the City's terminology "pre-development" refers to the condition before the proposed development.
- The proposed project is located within the Minnehaha Creek Watershed District, which has a separate review process from that of the City. Please note it may be necessary to obtain approvals or permits from Minnehaha Creek Watershed District.
- Utility Connections: Please note on the plans if the existing sanitary sewer services are proposed to be removed or left in place. Please note these service connections are the responsibility of the property owner. If any damage occurs due to failure of any service the property owner is liable for the damage. It also appears that the proposed sanitary sewer services and proposed private storm sewer are being shown on the Existing Conditions and Removals Plan. To eliminate confusion, these should not be shown on this plan sheet.
- It appears that the proposed discharge from the 10-year storm event greatly exceeds the capacity of the 9" storm sewer catch basin lead proposed to be tied into; this will likely result in flooding of the street at this location and possibly within the property. The stormwater modeling should take into account the restricted discharge pipe to determine the impacts and possible necessary mitigation.

- Non Stormwater Discharges: Detail all mechanical and non-stormwater discharges. Non-stormwater discharges are not permitted unless approved by the City of Minneapolis. Non-stormwater discharges not declared and approved will not be permitted. If there currently are none and nothing is proposed declare this status on the plans.
- For comments or questions on Public Works Surface Water & Sewers Division related requirements please contact Jeremy Strehlo, (Professional Engineer) at (612) 673-3973, or jeremy.strehlo@minneapolismn.gov.

❑ Fire Safety

- Provide required automatic fire suppression system throughout building.
- Fire department connection must be located on the address side of building and within 150 feet of a fire hydrant.
- Provide required fire alarm system throughout building.
- Provide and maintain fire apparatus access roads at all times.

❑ Business Licensing

- There is no Minneapolis Business Licensing review required for the proposed project.

❑ Environmental Health

- The site had an underground fuel tank of 2000 gallon removed around June 1991. The condition of the tank when removed was not documented. Since then the site has been involved in the Voluntary Investigation and Cleanup Program (VIC Site VP20980) and the Petroleum Brownfield Program (Site 3592). The developer should contact the Minnesota Pollution Control Agency to review these records and determine if the project will be disturbing impacted soils. If such conditions exist the developer should enter the VIC or Petroleum Brownfield program and request a letter of no determination from the MPCA. If it is the project will be disturbing identified impacted soils on the site preapproval for removal, disposal and/or reuse of must occur from the MCPA and the City of Minneapolis prior to beginning excavation activities. If impacted soil is encountered during site activities work will need to stop and notification provided to the MN State Duty officer at (615) 649-5451.
- The project scope identifies underground parking and the project site is located near Cedar Lake, Lake of the Isles and Lake Calhoun. Determination of the high static water level should be determined. If dewatering is required during site construction see below for city permit requirements. Subgrade structures should be designed to prevent infiltration of groundwater without the need for a permanent dewatering system being installed. If a continuously operating permanent dewatering system is needed it must be approved as part of the sanitary sewer and storm drain site plan approval prior to construction beginning.
- No construction, demolition or commercial power maintenance equipment shall be operated within the city between the hours of 6:00 p.m. and 7:00 a.m. on weekdays or during any hours on Saturdays, Sundays and state and federal holidays, except under permit. Contact Environmental Services at 612-673-3867 for permit information.
- Permits and approval are required from Environmental Services for the following activities: Temporary storage of impacted soils on site prior to disposal or reuse; Reuse of impacted soils on site; Dewatering and discharge of accumulated storm water or ground water, underground or aboveground tank installation or removal, well construction or sealing. Contact Tom Frame at 612-673-5807 for permit applications and approvals.

END OF REPORT

**STATEMENT OF PROPOSED USE,
PROJECT DESCRIPTION, AND PROPOSED FINDINGS**
Revised May 21, 2014 (Changes shown in underlined italics)
**Submitted in Support of Land Use Applications
For a Multiple Family Dwelling
At 2622 West Lake Street**

Greystar Real Estate Partners, Applicant

The Applicant, Greystar Real Estate Partners, submits this Statement of Proposed Use, Project Description, and Proposed Findings, in support of its application for the following land use approvals for a proposed 90 unit multiple family project:

- **Conditional use permit:** To increase the height of the building located in the SH (Shoreland) Overlay District from 2.5 stories/35 feet to 89 feet high (8 stories) from West Lake Street (plus an approximately 2-foot high parapet and two appurtenant elevator and stairway penthouses)
- **Variance of the 15-foot front yard setback along West Lake Street for balconies, patios, exterior stairs, an entry trellis and similar features.**
- **Variance of the 15-foot corner side yard setback along Thomas Avenue South for balconies, patios, entry trellis and similar features.**
- **Variance of the 19-foot east interior side yard setback for balconies, patios, entry trellis and similar features.**
- **Variance of the 19-foot north rear yard setback for balconies, patios, entry trellis and similar features.**
- **Site Plan Review.**

Zoning: OR2, High Density Office Residence District and Shoreland Overlay District (Zoning Plate 23)

Ward: **Ward 10;** Council Member Lisa Goodman; CIDNA Neighborhood Organization

I. GENERAL LAND USE APPLICATION

A. Statement of Proposed Use and Project Description.

Proposed Use. Greystar Real Estate Partners proposes to develop a multi-family dwelling with 90 dwelling units with below grade parking. The Project will have outdoor common areas at entry level and roof top.

Project Site. The Project site is bounded by West Lake Street on the south, Thomas Avenue on the west, the Midtown Greenway pedestrian and bicycle trail on the north, and Minneapolis Park and Recreation Board (“MPRB”) land on the east. West Lake Street is a 6-lane Class “A” arterial that The Minneapolis Plan designates as a high density Commercial Corridor. Thomas Avenue provides pedestrian and motor vehicle access to the front door and parking garage for the Calhoun Beach Club Apartments and is a dead-end street. There is no direct access from the Project site to the Midtown Greenway, which is elevated as it passes the site. The park land on the east side of the Project site is currently unimproved. The site is within 1,000 feet of the high-water mark of Lake Calhoun and Lake of the Isles and is therefore within a SH Shoreland Overlay District. The site is immediately east of the 12-story Calhoun Beach Club Apartments and the historic Calhoun Beach Club. The 20-story Lake Point Condominium is two blocks west of the site. The Midtown Greenway Land Use and Development Plan guides the site for “High Density Housing (over 120 DU/acre).”

Site History. The Project site is currently vacant. The property was the site of a two-story office building formerly occupied by Weisman Enterprises. Coldwell Banker and JC Penny occupied that building before Weisman Enterprises. The building was demolished in 2006 in anticipation of development of a 7 story, 84 feet tall, 46-unit residential development that included a 2,000 square foot commercial space. The City approved the proposal (BZZ-2861) (including a conditional use permit for an 84 foot tall building in the Shoreland Overlay District), but the developer, The Lander Group, abandoned the project when the economy changed in 2006.

Design and Construction. As described in more detail in the description of the project incorporated in the architectural submittals, the project consists of a C-shaped building, with a trapezoidal 8-story tower and an L-shaped 3-story component, constructed over a two-level parking podium. One level of the parking podium is below grade. The tower is located at the south end of the site, with the longest side along the West Lake Street frontage, overlooking Lake Calhoun, and in line with taller Calhoun Beach Club Apartments. The shorter sides of the tower face east and west, with the shortest side facing east toward the park land. The 3-story portion of the building faces the entire Midtown Greenway frontage. Most of the park land frontage consists of either the 3-story component or the open end of the C, which creates a second story courtyard.

The tower is 89 feet high (8 stories) from West Lake Street to the roof deck. A parapet will extend approximately 2-feet above the roof. Two appurtenant elevator and stairway penthouses will extend approximately 12 to 15 feet above the parapet near the center of the roof, with openings on the roof level, including a vestibule that provides access to the rooftop deck.

As viewed from Lake Street and Lake Calhoun to the south, the building provides a transition from the taller Lake Point Condominium, Calhoun Beach Club and Calhoun Beach Club Apartments to the west to the park land on the east. The building, which steps from 8-stories at West Street to three stories along the Greenway, is designed to minimize shadowing on the Midtown Greenway, the single family residences north of the Greenway, and the parkland to the east of the Project site.

The building will be concrete “Type-1” construction, with an exterior of high quality glass, stone, brick and natural metal elements. The lower level of parking will be constructed completely below grade, above the relatively high ground water table. The upper level of parking will be constructed on grade level along Lake Street but will be concealed from public view by the first level entry, lobby, fitness center and units along Lake Street and most of the park side of the building. The natural slope of the site, and some berming, will conceal the upper level of the garage along the Greenway and the northerly portions of the Thomas Avenue and park facades. Both levels of the garage will be accessed from Thomas Avenue South. The lower level of the garage will be used only by residents of the building. The upper level of the garage be used by residents, guests and service people.

Sustainability. The site is the key sustainable element of the Project. The West Lake Street Corridor and the Midtown Greenway currently provide residents with the opportunity to walk, bike and take buses from the site. The Project will also include indoor parking for at least 90 bicycles (twice as many as the Code requires) with easy access to the street and Greenway. In the near future, the site will have nearby access to LRT. In the longer range, the site may be served by streetcars. The Project will incorporate Best Management Practices for stormwater management, including a boulevard along West Lake Street, featuring a permeable landscape zone with trees, lower plants, and ground cover. The building is designed in accordance with the Minneapolis Plan sustainable design principles, including green construction practices, energy efficient building systems, and preservation of natural resources. The Project will include thermal high-efficiency window and exterior envelope systems, the pursuit of LEED equivalent performance and participation in the Xcel Energy Design Assistance Program. The Project also includes dedicated space for recycling and composting.

Noise and odors. To minimize noise impacts, air conditioning and heating will be accomplished with individual room units with rooftop condensers. Noise from these units is not expected to be noticeable beyond the Project site boundaries. There be no central compressor for air conditioning. The emergency generator exhaust will be muffled to city zoning standards and located on Thomas Avenue South near the parking entrance.

B. Site Characteristics

Ward:	Ward 7; Council Member Lisa Goodman
Neighborhood:	Cedar Isles Dean Neighborhood Association www.cidna.org
Zoning District:	OR-2 (Plate 23—Shoreland Overlay)
Guidance:	The Comprehensive Plan guides the Property for mixed use development (retail, office and housing). The Property is in the Midtown Land Use Development Plan. The Property is not within a “small area plan” or any other specific planning guidelines.
Lot Area	Total area=57,190 s.f. or 1.313 Acres including Lake Street Right-of-Way Easement area= 875 s.f. or 0.020 Acres; Net Area=56,315 s.f. or 1.293 Acres
Permitted uses:	Multiple-family dwelling with 5 or more units. (2012 Amendments)
Height in Shoreland Overlay:	2.5 stories or 35 feet, whichever is less, subject to increase by conditional use permit.
FAR/ Building Area	FAR = 2.5
Of right w/ bonus for below grade parking	x 1.20 =168,945 GSF of right with below grade parking bonus (Gross area, excluding parking)
Parking :	1 per dwelling unit = 90 required (160 proposed)
Bicycle Parking	45 required (at least 90 proposed)
Setbacks	15 feet front; 5 feet + 2 x number of stories above first floor (5+(2 x 7) = 19)

C. Zoning Analysis

1. Proposed Zoning. The Applicant does not propose to rezone the property from the current OR2-Shoreland Overlay.

2. Shoreland Overlay District. The Project is subject to the height restrictions of the Shoreland Overlay because of its location. The project site is not on a steep slope or within 50 feet of the ordinary high water mark and is therefore not subject to the requirements related to those conditions. The project will comply with all applicable requirements related to grading, fill, and storm water management. The Project is separated from Lake Calhoun to the south by the six-lane West Lake Street and from Lake of the Isles to the north by the elevated Midtown Greenway, MPRB soccer practice fields, and West Lake of the Isles Parkway.

3. Calculations.

a. Gross Floor Area. The OR-2 High Density Office Residence District allows multi-family housing with a maximum floor area ratio (FAR) of 2.5, provided pursuant to Section 546.130, the maximum floor area ratio of multiple-family dwellings may be increased by twenty (20) percent if all required parking is provided within the building, entirely below grade, or in a parking garage of at least two (2) levels. With the increase for the two (2) level parking garage, the gross floor area of the Project of 168,945 (excluding parking) meets the maximum allowable area, as follows:

$$\text{Maximum Gross Building Area} = 56,315 \times 2.5 \times 1.20 = 168,945$$

b. Parking. 1 car per unit required and provided; 1 bicycle per two units required; 1 bicycle per unit provided.

c. Maximum lot coverage. Under section 546.140, the maximum allowable lot coverage is 70 percent. The proposed building foot print of 39,369 is less than the maximum allowable lot coverage:

Maximum Allowable Lot Coverage: $0.70 \times 56,315 = 39,420.50$

d. Impervious surface. Under section 546.150, the maximum allowable impervious surface coverage is 85%: The proposed total impervious surface for the entire Project Site of 44,086 is less than the maximum allowed:

Maximum Allowable Impervious Surface Coverage: $0.85 \times 56,315 = 47,867$ square feet

Proposed Impervious Surface = $44,086 / 56,315 \times 100 = 78.8\%$

e. Yard requirements. The following are required and proposed yard requirements, pursuant to Table 547-2

Required	Proposed
Front: 15 feet (Lake Street and Thomas)	15 feet, with encroachments for canopies and trellises
Rear: $5 + 2X$ stories = $5 + (2 \times 7) = 19$ feet	20 feet
Side: $5 + 2X$ stories = $5 + (2 \times 7) = 19$ feet	19 feet

Yard obstructions. The Project includes several obstructions that project into required yards. All of the elements would be permitted obstructions but some exceed the zoning code dimensional requirements that would allow them to be constructed without a variance. The obstructions consist of planters, handicap entrance ramps and landing, porches, balconies, patios, exterior stairs, canopies, trellises and similar features. The obstructions that require variances are shown, with dimensions, on a separate “Encroachments” drawing included in the architectural submittals. The obstructions for which variances are required specifically include the following:

- Canopies over the entry and over individual ground level patios extend more than 2.5 feet into required front and side yards.
- Balconies that exceed 50 square feet project into required front, rear and interior side yards (although no balcony encroaches more than 4 feet into a required front or rear yard or is located less than 10 feet from any interior side lot line).
- Ground level patios that exceed 50 square feet project into the front yard and interior side yard (although no ground level patio encroaches more than 4 feet into a required front yard).
- Electrical transformer located in required yard, on west side of building, near northeast corner.

The yard obstructions, other than the electrical transformer, are architectural features that provide residents with private outdoor space and add variety and texture to the facades. The patios and balconies provide a pedestrian scale and “eyes on the street” (and the park and Greenway). Although the patios and balconies that project into the required yards exceed 50 square feet, and therefore require a variance, the portions that project into the required yards are substantially less than 50 square feet. The electrical transformer is a necessary functional component of the project that must be placed in a location that is accessible by maintenance vehicles. The electrical transformer is located on Thomas Avenue, away from Lake Street, so service vehicles will not obstruct traffic, and away from the Greenway, the park land.

PROPOSED FINDINGS

Conditional Permit for Height

The Applicant respectfully asks the Planning Commission to grant a conditional use permit for height in excess of 2.5 stories or 35 feet based on the following proposed findings under Section 525.340 (general requirements for a conditional use permit) and under Sections 525.340 and 547.110 (specific requirements for increasing maximum height in the OR-2 and Shoreland Overlay District (same findings required for both)).

The proposed development satisfies the six findings Section 525.340 requires the city planning commission to make before granting a conditional use permit as follows:

(1) The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.

Increasing maximum height in the OR-2 district and Shoreland Overlay District to 89 feet and 8 stories will not be detrimental to or endanger the public health, safety, comfort or general welfare. The area is already characterized by high rise residential structures, the site is zoned for the proposed use, and the additional height, and the resulting increase in density are consistent with the general goals of the Minneapolis Plan to increase residential density and the specific designation of Lake Street as a High Density Corridor.

In fact the proposed development will have a positive effect public health, safety, comfort or general welfare as follows:

- The Project will create a pedestrian-friendly streetscape along a stretch of West Lake Street that is not currently comfortable for pedestrians or bicycles, with a wide landscaped boulevard setback shaped by the new building façade, new street trees and public realm streetscape amenities.
- The street front entrance and lobby, the landscaped open space, and the first floor units and fitness room that face West Lake Street and Thomas Avenue South and the low-rise town home units and second level patio deck along the Greenway and the park, will provide activity and eyes on the street, Greenway and park, which will create a safe environment for pedestrians, and enhance public comfort and general welfare.
- As demonstrated by the shadow studies submitted with this application, the building is designed to minimize shadowing on the Greenway, nearby single family homes, and park land to mitigate the potentially detrimental effect of the additional height.
- The parking garage entrances and exits are located mid-block on Thomas Avenue, away from the Lake Street intersection, with spacing and width designed for pedestrian safety.
- The Applicant is committed to collaborating with CIDNA, the Park Board, The Greenway Coalition and the HCRRA to create new and safer pedestrian and bike pathways, both near term and long range, including a possible pedestrian and bicycle path between the building and the Greenway that will connect Thomas Avenue to the park land.

The Applicant will use its best efforts to protect the mature cottonwood trees along the north and east boundaries of the Project site.

(2) The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.

Increasing maximum height in the OR-2 district and Shoreland Overlay District to 89 feet and 8 stories will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district. The proposed development will not only “not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district,” but will in fact enhance and benefit the use and enjoyment of other property in the vicinity, as follows:

- Improved streetscape along Lake Street will improve use and enjoyment and access to the parks and commercial amenities along Lake Street for pedestrians and bicyclists.
- The Project may serve as a catalyst for further streetscape and landscape improvements along West Lake Street and Thomas Avenue South, including improved pedestrian and bicycle circulation from Thomas Avenue to the park land through the north portion of the Project site, and from that park land to a crossing over or under the Greenway to Lake of the Isle and more pedestrian-friendly crossing over West Lake Street.
- The Project massing will fit the scale and character of the taller buildings to the west of the site, and will mitigate the height of those buildings by stepping down to the Greenway and the park land.
- The Project will mitigate the impact of the 6-lane West Lake Street by providing a transition from the taller tower element along West Lake Street and Thomas Avenue to the lower element along the Greenway and the park land.
- The Project height will blend with the top of the existing tree canopy adjacent to the site.
- The emergency generator exhaust will be muffled to city zoning standards and located on Thomas Avenue South near the parking entrance.
- Air conditioning and heating will be accomplished with individual room units with rooftop condensers. Noise from these units is not expected to be noticeable. There be no central cooling tower on the roof.

(3) Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.

Increasing the height of the building will have no impact on utilities, access roads or drainage. The Project will use existing utility services, which are adequate for the Project without burdening existing development. Access, drainage, and driveways will be consistent with all applicable regulations. Vehicle access will be limited to two curb cuts along Thomas Avenue, a dead end street that has and will have no traffic other than residential and service traffic related to the Calhoun Beach Club Apartments and the Project. The garage access points are a safe distance from street intersections. The two driveways will be limited to 20 feet in width each, and will be separated from each other, to minimize pedestrian crossing width. The lower parking level, the lowest portion of the building, will be above the water table so no permanent dewatering will be required. Rainwater runoff will be managed within below-grade chambers within the building setbacks.

(4) Adequate measures have been or will be taken to minimize traffic congestion in the public streets serving the Project.

Increasing the height of the building will have no impact on traffic congestion in the public streets. Vehicle access will be limited to two adjacent locations on the Project Site on Thomas Avenue South, a safe distance from street intersections. The driveways will be limited to 20 feet in width, to minimize pedestrian crossing width. Service and delivery vehicles, including moving vans, will also use Thomas Avenue South for access to the building. Trucks will enter Thomas Avenue South from West Lake Street and return using a three-point turn into the building. Trucks will not stop on West Lake Street.

(5) The conditional use is consistent with the applicable policies of the comprehensive plan.

See Exhibit A. Because the Applicant needs to demonstrate conformance with the Comprehensive Plan to satisfy the requirements for the conditional use permit and variances, we address conformance with the Comprehensive Plan in Exhibit A to this Statement.

(6) The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.

Subject to the variance requested for encroachments into the required yards, the project complies with all applicable regulations.

ADDITIONAL FINDINGS FOR HEIGHT IN EXCESS OF THE MAXIMUM

The proposed Project satisfies the additional considerations required under Sections 525.340 and 547.110 (specific requirements for increasing maximum height in the OR-2 and Shoreland Overlay District (same findings required for both)).

(1) Access to light and air of surrounding properties.

The Project will not affect access to light and air for any of its neighbors. The Project is located on its own block, separated from its closest neighbor, the Calhoun Beach Club Apartments, by the right of way of Thomas Avenue and an additional 15 foot setback. The Project is designed to step down to three stories where the Project faces its more distant single family neighbors across the Greenway to the north.

(2) Shadowing of residential properties or significant public spaces.

As the Applicant's shadow study demonstrates, the Project is designed to protect the Greenway and single family residents to the north and the park land to the east from shadowing. Portions of all of the surrounding properties will, of course, be shadowed at various times during the year, especially when the sun is low in the mornings, evenings and the winter months. Keeping the tower portion of the building at the south end of the Project site, close to West Lake Street however, reduces the shadowing on the Greenway. For example, the shadow studies show that the Project would shade about one half of the width of the Midtown Greenway bicycle trail at noon on winter solstice.

(3) The scale and character of surrounding uses.

The additional height of the Project is consistent with the scale and character of surrounding uses, and in fact mitigates the impact of the existing taller structures to the west of the Project by providing a visual transition from the taller buildings to the Greenway, the park land, and the single family homes

north of the Greenway. The tower portion of the Project creates a “bookend” with the older and taller Calhoun Beach Club on the west and the newer and even taller Calhoun Beach Club Apartments in the middle. The three story town home architectural expression on the north and east facades, facing the Greenway and park land, make the transition from the scale and character of the single family district within the Project site.

(4) Preservation of views of landmark buildings, significant open spaces or water bodies.

The additional height of the Project will not have a significant effect on existing views of landmark buildings, significant open spaces or water bodies. For pedestrians, bicyclists, motorists, and residents of existing single family homes, the additional height will not block views of Lake Calhoun, Lake of the Isles, or the open space around either any more than an “of right” 2.5 story, 35 foot tall building would. The additional height will affect views of Lake Calhoun, Lake of the Isles, the open space around them, and the park land on the east side of the Project site from the upper floors of the existing Calhoun Beach Club Apartments more than a 2.5 story, 35 foot tall building would, but configuration of the proposed tower preserves north and south views of Lake Calhoun, Lake of the Isles, and the open space around them for those upper floor residents of the existing Calhoun Beach Club Apartments.

With respect to views of the landmark Calhoun Beach Club building, the proposed Project will not block any views the newer Calhoun Beach Club Apartments does not already block.

**ADDITIONAL CONSIDERATIONS FOR CONDITIONAL USES AND
VARIANCES IN THE SHORELAND OVERLAY DISTRICT**

Section 551.490 requires the Commission to consider the following additional evaluation criteria before granting a conditional use permit or variance in the Shoreland Overlay District:

(1) The prevention of soil erosion or other possible pollution of public waters, both during and after construction.

The Project will be constructed and maintained in accordance with applicable laws, including laws protecting public waters from pollution. In addition, with respect to specific risks associated with the Project site, the building will be constructed above the water table so there will be no need for pumping ground water. Public Works will review and approve an erosion control plan and a stormwater management plan prior to the issuance of building permits. Further, the risk of pollution from direct run-off from the site is low because the Project site is separated from Lake Calhoun by the 6-lane West Lake Street and from Lake of the Isles by the elevated Midtown Greenway. The proposed stormwater management system will be a significant improvement over current undeveloped site conditions.

(2) Limiting the visibility of structures and other development from protected waters.

The requested additional height will significantly increase the visibility of the development from Lake Calhoun and compared to a development built to “of-right” 35 foot height within the Shoreland Overlay District or the restrictions of the base zoning district. The Project will, however, be constructed at a height that will blend with the top of the canopy of the existing cottonwood trees along Lake Calhoun and Lake of the Isle. Further, the proposed building will block views of the east façade of the existing Calhoun Beach Club Apartments, which may be considered an improvement of the view from the east shore of Lake Calhoun.

(3) The suitability of the protected water to safely accommodate the types, uses and numbers of watercraft that the development may generate.

The Project will not impact watercraft usage on Lake Calhoun. At the CIDNA Land Use Committee meeting, there was some discussion about impact of the building on wind surfers and sail boaters. The unscientific consensus was that, during the part of the year when the winds come out of the northwest, the winds are already deflected by the tree line and the taller buildings to the west of the Project Site, surfers and sailors tend to move to the south end of the Lake to catch the wind, and the Project is therefore not likely to have a noticeable effect.

VARIANCES

The Applicant requests variances from the front and side yard requirements of the Ordinance to accommodate encroachments of balconies, patios, entry trellis and similar features into the required yards, as follows:

- Canopies over the entry and over individual ground level patios extend more than 2.5 feet into required front and side yards.
- Balconies that exceed 50 square feet project into required front, rear and interior side yards (although no balcony encroaches more than 4 feet into a required front or rear yard or is located less than 10 feet from any interior side lot line).
- Ground level patios that exceed 50 square feet project into the front yard and interior side yard (although no ground level patio encroaches more than 4 feet into a required front yard).
- Trellises incorporated in the entrance and the individual ground level patios project into the required yards.

Because these variances are so closely related to each other and the findings for each of the variances are so similar, the Applicant presents its proposed findings in support of the variances together, as follows:

(1) Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.

The location and configuration of the Project site are unique to the Project site, and present the following circumstances that are unique to the Project site and were not created by persons presently having an interest in the Project site:

The site is located within a Commercial Corridor and in a portion of the area covered by the Midtown Greenway Land Use and Development Plan that is guided for High Density Residential development, and within a Shoreland Overlay District in which building height is limited, which together demands a large building footprint. In addition, the natural slope to the site sets the first floor along Lake Street and Thomas Avenue above the elevation of the sidewalk, which requires the Applicant to provide exterior stairs and ramps to provide access to the pedestrian entrances to the lobby, fitness center and parking garage. Site constraints and the configuration of the underground parking structure prevent the Applicant from cutting the stairs and ramps into the building footprint. Similar conditions at the east side of the building require the Applicant to place concrete stoops in the required yard to provide access to the ground level units. The Applicant also intends to place trellises in the required yards to shelter and identify the entrances. Site constraints also provide limited options for locating the electrical transformer.

The site constraints also present practical difficulties with respect to using the balconies to create adequate private outdoor space for each unit *if the balconies did not project into the required yard*.

All of the elements *other than the electrical transformer* are permitted obstructions but exceed the zoning code dimensional requirements that would allow them to be constructed without a variance.

The circumstances are not based on economics alone. The circumstances are based primarily on the challenges of complying with the Code of Ordinances while respecting the goals and desires of the CIDNA neighborhood on a constrained site. The requested variances will allow the Applicant to provide residential scale amenities, such as ground level patios, planters and trellises at the street level and balconies that are partly inset and partly projecting, which will break the mass of the tower façade and provide a more human scale to the building. The requested variances are consistent with Site Plan Review and Minneapolis Plan design guidelines, which call for articulation of facades and features that provide “eyes on the street” to create a safer environment.

(2) The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the Ordinance and the comprehensive plan.

The proposed variances will allow the Applicant to develop the property for high density housing, as permitted by the zoning ordinance and recommended by the Minneapolis Plan and Midtown Greenway design guidelines.

(3) The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

The proposed variance will alter the essential character of the current site and its immediate neighbors, but it will do so in a manner consistent with the guidance for the locality, and in so doing will be beneficial to the use and enjoyment of other property in the vicinity. The Project site is itself is currently vacant. The existing Calhoun Beach Club and Calhoun Beach Club Apartments are consistent with the current zoning and guidance for their respective sites but do not provide for a transition to the open space and single family uses that establish the essential character of other properties in the vicinity. The proposed variance will allow development of the Project site at a density that is consistent with adopted zoning and guidance for the immediate vicinity, with a design that will provide a transition to the open space and single family uses that surround the immediate vicinity.

SITE PLAN REVIEW

The Applicant respectfully requests approval of Site Plan Review on the basis of the following findings, as required by Section 530.70:

(1) The site plan conforms to all applicable standards of chapter 530.

The plans prepared and submitted by ESG Architects on behalf of the Applicant comply with the requirements of Chapter 530. The Applicant is not seeking approval of alternative compliance on any Site Plan Review standard.

The building is set on the required setback lines to reinforce the street grid. The area between the building and the lot line includes amenities such as landscaping and planters. The buildings is oriented so the principal entrance faces Lake Street South, which is the front lot line, rather than the interior of the site or Thomas Avenue South. The building corner at the entrance is a glass storefront with view in and out of resident activity spaces to create a secure and active environment at street level. The exterior courtyard provides views in from and out to the park land..

The facades on all sides of the building are articulated with balconies, entry trellises, ground level patios, and variations in materials. Entrances are clearly defined. Each façade meets or exceeds the minimum requirements for clear glazing. Lighting in the open space, courtyards and along the street front will provide a safe and comfortable environment for pedestrians.

Parking garage entrances and exits are located on Thomas Avenue South to avoid conflict with the more intense traffic on West Lake Street. The parking garage entrances are located a safe distance from the corner of West Lake Street and Thomas Avenue South, and kept to a 22 foot maximum width to reduce conflicts between vehicles and pedestrians.

One hundred percent of the site that is not covered by buildings will be developed as landscaped open space. The courtyard and rooftop patios will also be developed with green roof planting materials and larger plants in planters. The parking will be one hundred percent enclosed, so there will be no required screening. The Project will have decorative fences for privacy and safety at the edges of the raised court yard.

(2) The site plan conforms to all applicable regulations of this zoning ordinance and is consistent with the applicable policies of the comprehensive plan and applicable small area plans adopted by the city council.

The Site Plan is designed in accordance with all requirements of the Minneapolis Code of Ordinances, the Minnesota State Building Code and other applicable law. See Exhibit A for discussion of compliance with the comprehensive plan and small area plans.

Exhibit List

Exhibit A: Compliance with Comprehensive Plan

Exhibit A
Compliance with Comprehensive Plan

Exhibit A Comprehensive Plan

The Project is consistent with the following Principles and Policies of the Minneapolis Plan and the Midtown Greenway Land Use and Development Plan:

The Minneapolis Plan

The site is located on West Lake Street which is a designated Commercial Corridor. In addition, the site is located on the south side of the Midtown Greenway, an existing greenway. The following principles and policies of The Minneapolis Plan apply to this proposal:

1. Land Use. General Land Use Policy

Policy 1.2: Ensure appropriate transitions between uses with different size, scale, and intensity.

1.2.1 Promote quality design in new development, as well as building orientation, scale, massing, buffering, and setbacks that are appropriate with the context of the surrounding area.

- The Project will provide a transition from the taller Lake Point Condominium, Calhoun Beach Club, and Calhoun Beach Club Apartments west of the Project Site to the open space and single family uses east and north of the site.

1.3.1 Require safe, convenient, and direct pedestrian connections between principal building entrances and the public right-of-way in all new development

- The Project will have direct access to the existing transit stop at Thomas and Lake and will accommodate and encourage safe and convenient access for pedestrians and bicyclists between the building and the Greenway and the parkway system.

1.3.2 Ensure the provision of high quality transit, bicycle, and pedestrian access to and within designated land use features and the parkway system.

- The Project encourages safe and convenient access for pedestrians and bicyclists between the building and the Greenway.

Policy 1.8: Preserve the stability and diversity of the city's neighborhoods while allowing for increased density in order to attract and retain long-term residents and businesses.

1.8.1 Promote a range of housing types and residential densities, with highest density development concentrated in and along appropriate land use features.

- The Project will provide high density, high quality luxury rental housing in a neighborhood with few such options, on a Commercial Corridor with convenient access to transit, bicycle paths, retail centers at Hennepin and Lake and at Market Place and Lake, and the park system.

Policy 1.10: Support development along Commercial Corridors that enhances the street's character, fosters pedestrian movement, expands the range of goods and services available, and improves the ability to accommodate automobile traffic.

- The Project will substantially improve the pedestrian environment on Thomas Avenue South and West Lake Street, including the transit stop at Thomas and Lake, with boulevard plantings and landscaping within the front yard setback.

1.10.5 Encourage the development of high-density housing on Commercial Corridors.

- The Project squarely meets this goal by providing 90 new units of high density housing to the West Lake Street Commercial Corridor.

2. Transportation

Policy 2.3: Encourage walking throughout the city by ensuring that routes are safe, comfortable, pleasant, and accessible.

2.3.1 Ensure that there are safe and accessible pedestrian routes to major destinations, including transit corridors, from nearby residential areas.

- The Project will make West Lake Street a safer, more pleasant place to walk with boulevard plantings and landscaping within the front yard setback and with increased activity and visibility generated by the building entrance, fitness room, and apartment units that face West Lake Street.

2.4.3 Encourage higher intensity and transit-oriented development to locate in areas well served by transit.

- The Project will place 90 new units of housing on a bus route, with a stop outside the building entrance.

2.5.1 Complete a network of on- and off-street primary bicycle corridors.

- The Project will promote development of pedestrian and bicycle connection between West Lake Street and the Greenway, through Thomas Avenue South, the Project site, and the park land to the east of the Project site.

3. Housing

Policy 3.1: Grow by increasing the supply of housing.

3.1.1 Support the development of new medium- and high-density housing in appropriate locations throughout the city.

Policy 3.2: Support housing density in locations that are well connected by transit, and are close to commercial, cultural and natural amenities.

3.2.1 Encourage and support housing development along commercial and community corridors, and in and near growth centers, activity centers, retail centers, transit station areas, and neighborhood commercial nodes.

- The Project will serve all of the foregoing housing goals and policies by providing high-density housing, on a transit route in a Commercial Corridor, with commercial and cultural assets of Uptown and the natural amenities of the Minneapolis lakes.

Policy 3.6: Foster complete communities by preserving and increasing high quality housing opportunities suitable for all ages and household types.

3.6.1 Promote the development of housing suitable for people and households in all life stages that can be adapted to accommodate changing housing needs over time.

3.6.2 Promote housing development in all communities that meets the needs of households of different sizes and income levels.

3.6.3 Maintain a healthy supply of multifamily ownership and rental housing, and promote the development of alternative forms of homeownership such as cooperative housing and cohousing.

3.6.4 Provide and maintain moderate and high-density residential areas, as well as areas that are predominantly developed with single and two family structures.

- The Project will provide a moderate- to high-density luxury rental alternative to a neighborhood characterized by high-end single family homes and condominiums.

6. Environment

6.2.4 Endorse the use of alternative modes of transportation such as walking, bicycles, public transit, car and bike share programs, and carpools, as well as promote alternative work schedules.

- The Project will be located on a transit route and at an entrance to the City’s network of bicycle and pedestrian trails.
- The Project will provide residents access to multiple modes of transportation including bike commuting along the Greenway. With the future addition of LRT and long range plans for a Streetcar, the Project has the potential of becoming a truly pedestrian and transit oriented community that allows residents to live, work and play without dependence on daily automobile usage.

6.2.7 Promote the development of sustainable site and building standards.

Policy 6.3: Encourage sustainable design practices in the planning, construction and operations of new developments, large additions and building renovations.

6.3.1 Encourage developments to implement sustainable design practices during programming and design, deconstruction and construction, and operations and maintenance.

6.3.2 Ensure that developments use storm water BMPs (Best Management Practices).

- The Project will feature several green elements, including green construction practices and materials specification, thermal high-efficiency window and exterior envelope systems, the pursuit of LEED equivalent performance and participation in the Xcel Energy Design Assistance Program.
- The Project will provide trash/recycling and composting space in a dedicated room for the development.
- The Project will provide at least 5 electrical receptacles in the parking garage specifically for electric vehicle charging.
- The Project will be designed to avoid the water table and to incorporate storm water BMPs.

6.5.1 Support transit-oriented development, mixed-use projects and other multi-modal development patterns.

- The Project will increase the residential density around an existing bus route and bicycle route, and a potential future LRT or Streetcar route.

6.5.2 Encourage development projects that maximize the development capacity of the site while at the same time reducing non-renewable energy needs.

- The Project will develop the site to the fullest extent acceptable to CIDNA and the City.

6.8.1 Enforce and educate the public on the City’s Urban Forest Policy.

6.8.2 Achieve, at a minimum, no net loss of the urban tree canopy by maintaining and preserving existing trees and planting new trees on public and private property.

6.8.3 The city’s built infrastructure will support a healthy thriving urban tree canopy through street and sidewalk guidelines and other means.

- The existing tree canopy on the adjacent park land is an important feature of the site, which the Applicant will use best effort to protect during construction.
- The proposed design will provide major streetscape improvements within the wide setback along West Lake including a green boulevard between the curb and the new sidewalk featuring permeable ground cover and hearty street trees; a widened pavement area for pedestrians and a permeable landscape zone featuring trees and lower plants between the pedestrian walkway and the building façade.

Policy 6.10: Coordinate and operate waste management programs that focus on reducing, reusing and recycling solid waste prior to disposal.

6.10.1 Operate waste management practices consistent with the state approved waste management hierarchy.

- The Project will provide trash/recycling and composting space in a dedicated room for the development.

10. Urban Design. The Project is consistent with the applicable Urban Design policies and principles of Minneapolis Plan, including in particular the following:

Policy 10.4: Support the development of residential dwellings that are of high quality design and compatible with surrounding development.

10.4.2 Promote the development of new housing that is compatible with existing development in the area and the best of the city’s existing housing stock.

Policy 10.5: Support the development of multi-family residential dwellings of appropriate form and scale.

10.5.2 Medium-scale, multi-family residential development is more appropriate along Commercial Corridors, Activity Centers, Transit Station Areas and Growth Centers outside of Downtown Minneapolis.

Policy 10.6: New multi-family development or renovation should be designed in terms of traditional urban building form with pedestrian scale design features at the street level.

10.6.1 Design buildings to fulfill light, privacy, and view requirements for the subject building as well as for adjacent properties by building within required setbacks.

10.6.2 Promote the preservation and enhancement of view corridors that focus attention on natural or built features, such as the Downtown skyline, landmark buildings, significant open spaces or bodies of water.

10.6.3 Provide appropriate physical transition and separation using green space, setbacks or orientation, stepped down height, or ornamental fencing to improve the compatibility between higher density and lower density residential uses.

10.6.4 Orient buildings and building entrances to the street with pedestrian

10.6.5 Street-level building walls should include an adequate distribution of windows and architectural features in order to create visual interest at the pedestrian level.

10.6.6 Integrate transit facilities and bicycle parking amenities into the site design.

Policy 10.9: Support urban design standards that emphasize traditional urban form with pedestrian scale design features at the street level in mixed-use and transit-oriented development.

10.9.2 Promote building and site design that delineates between public and private spaces.

10.9.3 Provide safe, accessible, convenient, and lighted access and way finding to transit stops and transit stations along the Primary Transit Network bus and rail corridors.

10.9.4 Coordinate site designs and public right-of-way improvements to provide adequate sidewalk space for pedestrian movement, street trees, landscaping, street furniture, sidewalk cafes and other elements of active pedestrian areas.

Policy 10.16: Design streets and sidewalks to ensure safety, pedestrian comfort and aesthetic appeal.

10.16.1 Encourage wider sidewalks in commercial nodes, activity centers, along community and commercial corridors and in growth centers such as Downtown and the University of Minnesota.

10.16.2 Provide streetscape amenities, including street furniture, trees, and landscaping, that buffer pedestrians from auto traffic, parking areas, and winter elements.

10.16.3 Integrate placement of street furniture and fixtures, including landscaping and lighting, to serve a function and not obstruct pedestrian pathways and pedestrian flows.

10.16.4 Employ pedestrian-friendly features along streets, including street trees and landscaped boulevards that add interest and beauty while also managing storm water, appropriate lane widths, raised intersections, and high-visibility crosswalks.

Policy 10.19: Landscaping is encouraged in order to complement the scale of the site and its surroundings, enhance the built environment, create and define public and private spaces, buffer and screen, incorporate crime prevention principles, and provide shade, aesthetic appeal, and environmental benefits.

10.19.1 In general, larger, well-placed, contiguous planting areas that create and define public and private spaces shall be preferred to smaller, disconnected areas.

10.19.2 Plant and tree types should complement the surrounding area and should include a variety of species throughout the site that include seasonal interest. Species should be indigenous or proven adaptable to the local climate and should not be invasive on native species.

10.19.3 Landscaped areas should include plant and tree types that address ecological function, including the interception and filtration of stormwater, reduction of the urban heat island effect, and preservation and restoration of natural amenities.

10.19.4 Landscaped areas should be maintained in accordance with Crime Prevention Through Environmental Design (CPTED) principles, to allow views into and out of the site, to preserve view corridors and to maintain sight lines at vehicular and pedestrian intersections.

10.19.5 Landscaping plans should be designed to facilitate future maintenance including the consideration of irrigation systems, drought and salt-resistant species, ongoing performance of storm water treatment practices, snow storage, access to sun, proximity to buildings, paved surfaces and overhead utilities.

10.19.6 Green roofs, living walls, and porous pavement are encouraged but are not meant to be a substitute for ground-level landscaping of sites as landscaping provides both a natural amenity and aesthetic beauty to the urban landscape.

10.19.7 Boulevard landscaping and improvements, in accordance with applicable city polices, are encouraged.

Policy 10.22: Use Crime Prevention Through Environmental Design (CPTED) principles when designing all projects that impact the public realm, including open spaces and parks, on publicly owned and private land.

10.22.1 Integrate “eyes on the street” into building design through the use of windows to foster safer and more successful commercial areas in the city.

10.22.2 Orient new housing to the street to foster safe neighborhoods.

10.22.3 Design the site, landscaping, and buildings to promote natural observation and maximize the opportunities for people to observe adjacent spaces and public sidewalks.

10.22.4 Provide on-site lighting at all building entrances and along walkways that maintains a minimum acceptable level of security while not creating glare or excessive lighting of the site.

10.22.5 Locate landscaping, sidewalks, lighting, fencing and building features to clearly guide pedestrian movement on or through the site and to control and restrict people to appropriate locations.

10.22.6 Use innovative building designs and landscaping to limit or eliminate the opportunity for graffiti tagging.

10.22.7 Locate entrances, exits, signs, fencing, landscaping, and lighting to distinguish between public and private areas, control access, and to guide people coming to and going from the site.

Midtown Greenway Land Use and Development Plan

Principles of Greenway-Supportive Development

1. Promote a safe, vibrant and active environment with calmed streets and widened sidewalks. Focus investments toward developing an enlivened, pedestrian-friendly public realm.

- The Project will provide an improved pedestrian environment on West Lake Street and, with the cooperation of CIDNA, the Park Board, The Greenway Coalition and the HCRRA will provide an opportunity to create new and safer pedestrian and bike pathways, both near term and long range, including a possible pedestrian and bicycle path between the building and the Greenway that will connect Thomas Avenue to the park land.

2. Encourage redevelopment projects to be transit-supportive by integrating bicycle and pedestrian amenities as well as accessible and visually appealing transit stops into projects.

- The Project will promote proximity to the Greenway as an amenity and provide at least 90 interior bicycle storage areas – twice the minimum the Code requirements. Landscape improvements will enhance the existing bus stop at the southwest corner of the Project site.
3. Promote opportunities for additional public green space, dedicated parks, trail connections and public art along the Greenway edge, especially near transit stops and higher-intensity developments.
- The Applicant will pursue opportunities to collaborate with CIDNA, the Park Board, The Greenway Coalition and the HCRRA to create new and safer pedestrian and bike pathways.
4. Support compact development and promote mixed use in existing commercial areas. Create a more lively and diverse urban environment.
- The Project is a compact, high density project, but, at the request of CIDNA, will not include commercial uses. The project will nevertheless contribute to a more lively and diverse urban environment by providing 90 new housing units and substantial improvements to the existing streetscape.
5. Focus the most intensive development near future transit stops and existing commercial nodes and encourage the provision of open space and active stormwater management in new developments.
- The project is adjacent to an existing transit stop and will incorporate an active stormwater management system. Although the project will take up most the site, the project will include substantial open space improvements along Thomas Avenue South and West Lake Street.
6. Promote development that reinforces appropriate architectural scale and relates to adjacent land uses. Employ development strategies that minimize Greenway and adjacent property shadowing.
- The project massing is designed to complement and improve the appearance of the taller Lake Point Condominiums, Calhoun Beach Club and Calhoun Beach Club Apartments, by matching the massing and height of the historic Calhoun Beach Club and stepping down to the park land and the Greenway. The shadow studies demonstrate that the Project minimizes shadowing of its neighbors.
7. Use new development, the pedestrian environment and open space to promote an integrated relationship between the Greenway floor and the Greenway edge/rim, fostering a sense of place and community.
- The Applicant will promote bicycling and proximity to the Greenway as a project amenity. The Applicant will pursue opportunities to collaborate with CIDNA, the Park Board, The Greenway Coalition and the HCRRA to create new and safer pedestrian and bike pathways.
8. Develop a premier public edge along both sides of the Greenway, including a more pedestrian and bicycle-friendly 29th Street and public promenades.
- The north façade of the Project is oriented to the Greenway. The Applicant will promote development of Thomas Avenue and the north boundary of the Project site as a pedestrian and bicycle path onto and under the Greenway.

9. Promote Greenway safety and comfort through environmental design features such as doors located on the street or Greenway as appropriate, windows facing public space and the relocation of service doors away from the public realm.

- The Project will substantially improve safety and comfort on the Greenway by providing apartments that are oriented to the Greenway and by providing a more comfortable pedestrian and mass-transit environment on the Project site.

10. Promote compatibility of industrial uses with residential areas and the Greenway through landscaping and enhanced urban design.

- Not applicable. There are no existing industrial uses around the Project site and there are no plans to include industrial use in the Project.

11. Promote sustainable site design through the use of ecological landscaping and on-site stormwater management.

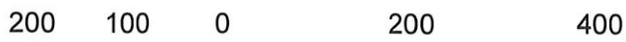
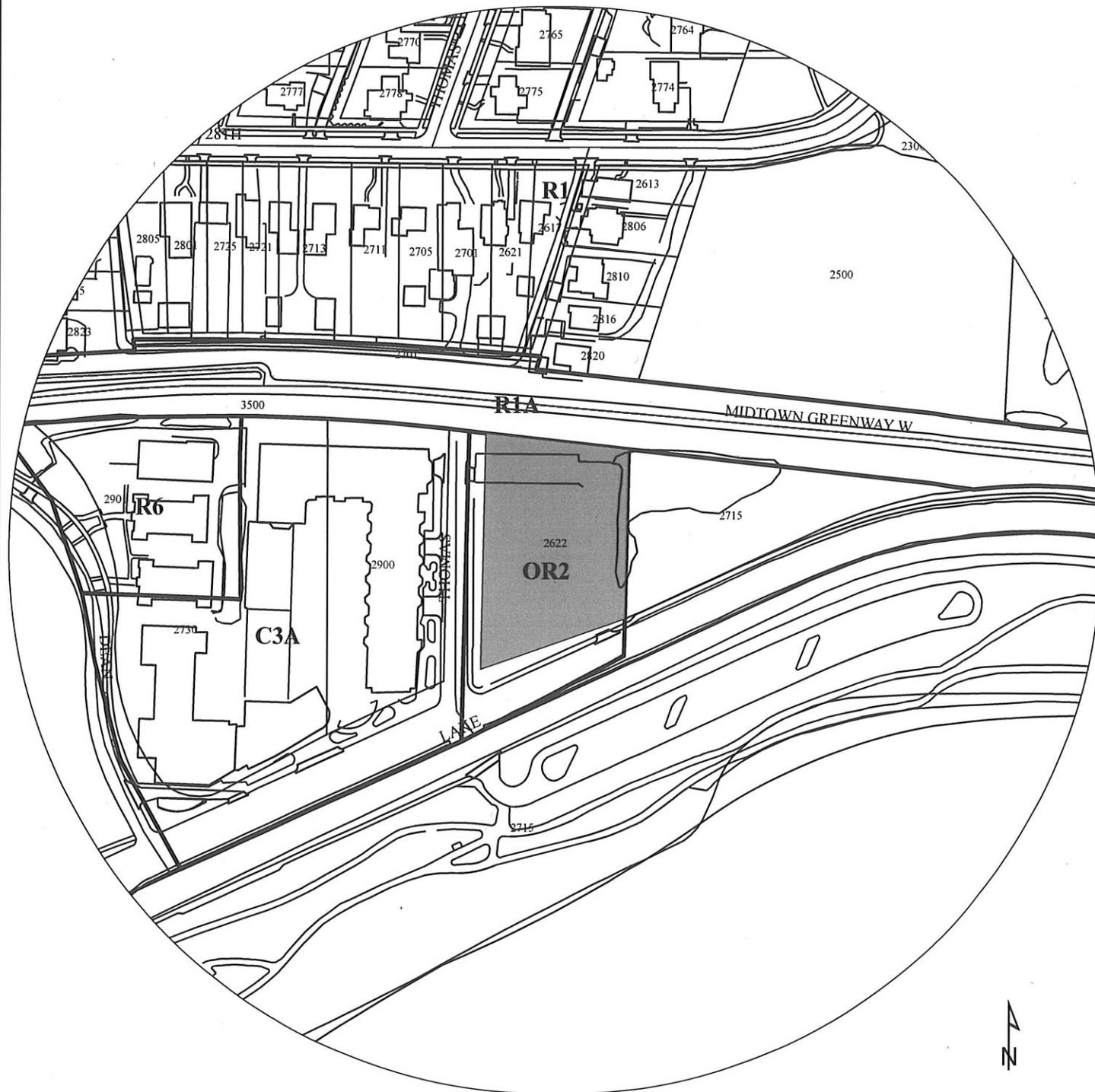
- The Project will incorporate green roof materials in its two roof top patios, ecological principles in its landscaping, and active on-site stormwater management.

Greystar

NAME OF APPLICANT

7th

WARD



PROPERTY ADDRESS

2622 West Lake Street

FILE NUMBER

BZZ-6576

2622 West Lake Street



elness swenson graham architects
500 WASHINGTON AVENUE SOUTH
MINNEAPOLIS, MINNESOTA 55415
P. 612.339.5508
F. 612.339.5382
WWW.ESGARCH.COM

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2622 West Lake Street Minneapolis, MN 55416



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BICYCLE PARKING SCHEDULE	
Description	Bicycle Count
BP-P1 PARKING PLAN	
In-Ground Bike Rack	34
Wall Mounted Bike Rack	28
	62
LEVEL 1	
In-Ground Bike Rack	12
Wall Mounted Bike Rack	16
	28
Grand total	90

AUTO PARKING SCHEDULE	
Description	Count
BP-P1 PARKING PLAN	
COMPACT STALL	20
STANDARD STALL	80
	100
LEVEL 1	
ACCESSIBLE STALL	6
COMPACT STALL	6
STANDARD STALL	48
	60
Grand total: 160	

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LAND USE APPLICATION
5/16/2014

ORIGINAL ISSUE: May 16, 2014

REVISIONS

No. Description Date

213538

PROJECT NUMBER

ESG

ESG

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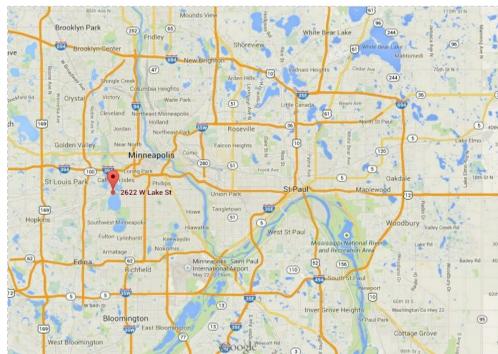
KEY PLAN

2622 W Lake St

TITLE SHEET

T1.1

PROJECT LOCATION



Site Location



Vicinity

PROJECT TEAM

Owner/Developer: Greystar Development
Mr. David Reid
750 Bering Drive, Suite 200
Houston, TX 77507
Ph: 713-479-8943

Architect: Elness Swenson Graham Architects Inc.
500 Washington Ave. South, Suite 1080
Minneapolis, MN 55415
Ph: 612-339-5508
Fx: 612-339-5382

Landscape: Kimley Horn and Associates
Tom Lincoln, PE
2550 University Ave W., Suite 238N
St. Paul, MN 55114
Ph: 651-643-0453

Civil: Kimley Horn and Associates
Tom Lincoln, PE
2550 University Ave W., Suite 238N
St. Paul, MN 55114
Ph: 651-643-0453

UNIT MIX & SQUARE FOOTAGES

Greystar
2622 West Lake Street
Luxury Apartments
8 Stories on Lake and 3 Stories on Greenway

Level	Use	Total GSF	Parking SF	Apt GSF	Units	Parking*
Level P1	Parking	39,756	39,756			100
Level 1	Lobby/Parking	39,290	27,058	12,232	6	60
Level 2	Residential/Amenity	29,558		29,558	16	
Level 3	Residential	29,558		29,558	18	
Level 4	Residential	29,376		29,376	18	
Level 5	Residential	17,110		17,110	9	
Level 6	Residential	17,110		17,110	9	
Level 7	Residential	17,110		17,110	9	
Level 8	Residential	16,284		16,284	5	
Rooftop	Stair/Elev Access	607		607		
Total		235,759	66,814	168,945	90	160



einess swenson graham architects
500 washington avenue south
minneapolis minnesota 55415
p. 6 1 2 . 3 3 9 . 5 5 0 8
f. 6 1 2 . 3 3 9 . 5 3 8 2
www.esgarch.com

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Project Purpose and Vision

The purpose and vision for 2622 West Lake Street is the creation of a landmark quality residential community that is designed around key design principles developed through collaboration with the CIDNA Land Use and Development Committee. The redevelopment will replace an empty parcel which has been fenced off and vacant since a former office structure was demolished in 2006.

- Provide quality residential consistent with the the City classification of West Lake as a High Density Corridor
- Comply with CIDNA parameters, items in the CIDNA Resolution and the Minneapolis Plan for Sustainable Growth
- Preserve access to light and air and minimize shadowing on adjacent parks and the Midtown Greenway
- Fit scale and character of the surrounding context including the 12 story Calhoun Beach Club Apartments, old Calhoun Beach Club, the Midtown Greenway and the single family residences
- Minimally impact the view shed from the shorelines, vehicular ways, walkways, and bikeways of Lake Calhoun, Lake of the Isles, and Cedar Lake.
- Create a step down transitional “ book end” from the massive 12 story Calhoun Beach Club Apartments
- Provide a lower height geometric matched balance to the old Calhoun Beach Club
- Provide a wide landscaped boulevard setback along West Lake shaped by th new building façade ,new street trees and public realm streetscape amenities.
- Overall Height to match urban mature tree line
- Collaborate with CIDNA, the Park Board, The Greenway Coalition and the HCRRA to create new and safer pedestrian and bike pathways, both near term and long range.
- Create landmark quality architecture through the use of high quality materials and Type 1 construction.

Architectural Description

The architectural design and massing of 2622 West Lake Street is the result of a close collaboration with CIDNA based on key urban design and context parameters and the Minneapolis Plan. The shape and geometry of the design is an organic response to the myriad of environmental and contextual forces. The design aesthetic is contemporary featuring a composition of large expanses of top quality glass, stone, brick and natural metal elements.

Public Realm and Streetscape: Key Overall Features

The overall design of 2622 is driven by the goal of enhancing the public realm and streetscapes surrounding the proposed residential building.

- The massing of the design directly corresponds to allowing solar penetration to the public Midtown Greenway throughout the year including the December 21 winter solstice. The

design proposes a high quality townhome architectural definition to the Greenway public realm combined with grading improvements between the Midtown Greenway Bike Trail and the new building facade to allow for a future potential bike and pedestrian pathway and to allow for new landscaping to be placed carefully and within Metropolitan Council within the 20 foot wide Storm Pipe easement.

- As requested by CIDNAs long range planning goals, the design will allow for the creation of a future bike and pedestrian path to be located between the new building and the existing Greenway bike track. This new potential bike and pedestrian path would connect across the midtown Greenway and allow for a connection to south Lake of the Isles and the large dog park.
- The proposed design will convert Thomas Ave S into a beautified pedestrian and bike friendly streetscape. This improved streetscape public realm will connect to the controlled intersection at Thomas Ave S and West Lake. It is desired to get the County to allow a longer pedestrian crossing signal at this key controlled intersection to provide a more pleasant and safe pedestrian and bike crossing experience and also to provide for overall traffic calming at the currently high speed West Lake corridor.
- The proposed design will be provide major streetscape improvements within the wide setback along West Lake including a green boulevard between the curb and the new sidewalk featuring permeable ground cover and hearty street trees; a widened pavement area for pedestrians moving to and from the core of Uptown; an additional landscape and building entry zone between the new sidewalk and the building facade, featuring major expanses of transparent glass and entry ways into the main lobby, community spaces and street level townhome flats.
- The proposed design will feature park side townhome flats in the architectural podium that will shape the west edge of the public Park Board triangular green space to the east. These townhome flats will be set back a full 19 feet from the property line and feature individual door fronts and a rich landscaped layer.

Green and Sustainable Features

A key sustainability strategy for 2622 West Lake Street is the location along the Midtown Greenway which will allow residences to use multiple modes of transportation including bike commuting along the Greenway. With the future addition of LRT and long range plans for a Streetcar, the project has the potential of becoming a truly pedestrian and transit oriented community that allows residents to live, work and play without dependence on daily automobile usage. The development team is committed to the sustainable design principles reflected in the City's comprehensive plan. Our sustainable design mission is to promote livable communities through the use of energy efficient systems, green building practices, reduced dependency on automobiles, creative density, high quality pedestrian and bicycle public realm and preservation of natural resources. The development will feature a series of green elements including green construction practices and materials specification, thermal high-efficiency window and exterior envelope systems, the pursuit of LEED equivalent performance and participation in the Xcel Energy Design Assistance Program.

LAND USE APPLICATION 5 / 16 / 2014		
ORIGINAL ISSUE: 5/16/2014		
REVISIONS		
No.	Description	Date
213538	PROJECT NUMBER	
ESG	ESG	
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KEY PLAN		
2622 W Lake St		
PURPOSE AND VISION		
T1.2		



eIness swenson graham architects
500 Washington Avenue South
Minneapolis, Minnesota 55415
P. 612.339.5508
F. 612.339.5382
www.esgarch.com

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VIEW FROM LAKE SHORE / BOAT LAUNCH



Overall Design Goal - Balance many interests

- Create Sustainable Density; Respect Shoreland, Parks, Trails and Single Family Residences
- Provide quality residential consistent with the the City classification of West Lake as a high density corridor
- Comply with CIDNA parameters and all items in the CIDNA Resolution
- Preserve access to light and air and minimize shadowing on adjacent parks and the Midtown Greenway
- Fit scale and character of the surrounding context including the 12 story Calhoun Beach Club Apartments, old Calhoun Beach Club, the Midtown Greenway and the single family residences
- Minimally impact the view shed from the shorelines, vehicular ways, walkways, and bikeways of Lake Calhoun, Lake of the Isles, and Cedar Lake.
- Create a step down interface transition from the unattractive 12 story Calhoun Beach Club Apartments

- Provide a lower height geometric matched balance to the old Calhoun Beach Club
- Absorb and step down the massive east wall of the Calhoun Beach Club Apartments with a triangular bookend building that narrows down to the East
- Provide a 30 foot, landscaped boulevard setback along West Lake shaped by the new building façade.
- Overall Height to match urban mature tree line
- Collaborate to create new and safer pedestrian and bike pathways



1. Future potential bike and pedestrian path to Isles
2. Convert Thomas into a beautified pedestrian and bike friendly streetscape
3. Provide major streetscape / boulevard improvements within the wide setback along West Lake

LONG-RANGE URBAN DESIGN VISION

LAND USE APPLICATION
5/16/2014

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KEY PLAN

2622 W Lake St

PRINCIPLES AND PROJECT IN CONTEXT

T1.3



einess swenson graham architects
 500 washington avenue south
 minneapolis minnesota 55415
 p. 612.339.5508
 f. 612.339.5382
 www.esgarch.com

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Signature _____

Typed or Printed Name _____

License # Date _____



2550 UNIVERSITY AVE. WEST, SUITE 239N
 ST. PAUL, MINNESOTA 55114
 TEL. NO. (651) 645-4197

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LAND USE APPLICATION
 5/16/2014

ORIGINAL ISSUE: 5/16/2014

REVISIONS

No.	Description	Date

160739000
 PROJECT NUMBER

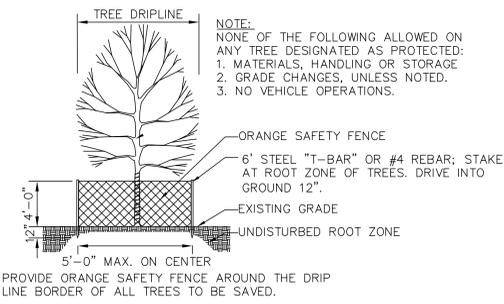
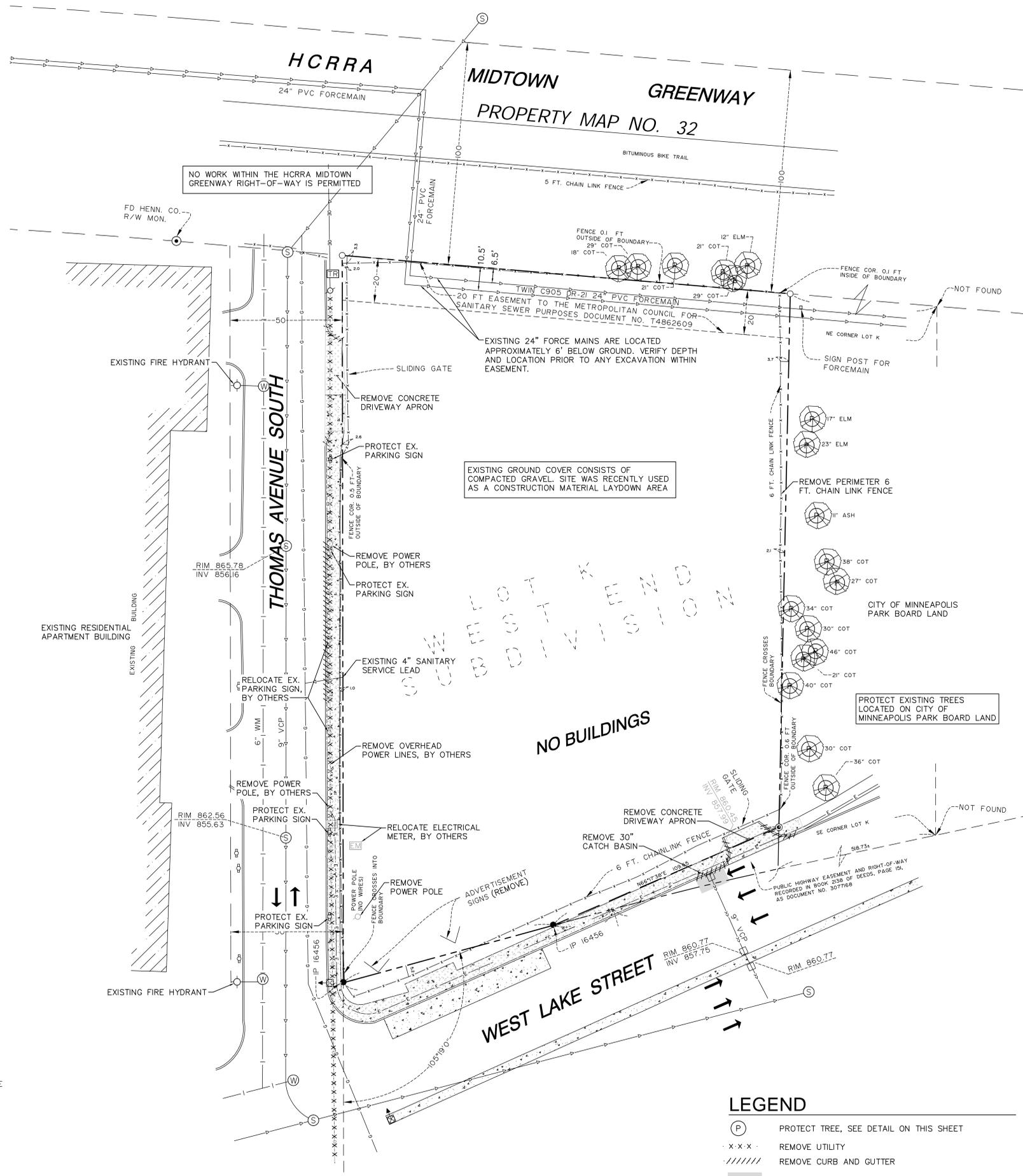


2622 WEST LAKE STREET
 EXISTING CONDITIONS AND REMOVALS PLAN

C1.1

GENERAL REMOVAL NOTES:

- CONTRACTOR SHALL PROTECT ALL EXISTING UTILITY STRUCTURES THAT ARE NOT BEING REMOVED OR RELOCATED.
- TRENCH BOXES OR OTHER TRENCH STABILIZATION METHODS MAY BE REQUIRED DURING CONSTRUCTION TO MINIMIZE REMOVAL LIMITS.
- CONTRACTOR TO VERIFY REMOVAL LIMITS WITH ENGINEER PRIOR TO SAWCUTTING.
- LOCATIONS AND ELEVATIONS OF EXISTING TOPOGRAPHY AND UTILITIES AS SHOWN ON THIS PLAN ARE APPROXIMATE. CONTRACTOR SHALL FIELD VERIFY SITE CONDITIONS AND UTILITY LOCATIONS PRIOR TO EXCAVATION/CONSTRUCTION. THE ENGINEER SHALL BE NOTIFIED IMMEDIATELY IF ANY DISCREPANCIES ARE FOUND.
- CONTRACTOR SHALL COORDINATE LIMITS OF REMOVALS WITH PROPOSED IMPROVEMENTS AND FIELD VERIFY CONDITION OF EXISTING APPURTENANCES TO REMAIN. CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING OR REPLACING MISCELLANEOUS ITEMS (SUCH AS FENCES, SIGNS, IRRIGATION HEADS, ETC.) THAT MAY BE DAMAGED BY CONSTRUCTION.
- AN OBSTRUCTION PERMIT IS REQUIRED FOR WORK PERFORMED IN THE PUBLIC RIGHT-OF-WAY. CONTACT SCOTT KRAMER AT (612) 673-2383 REGARDING DETAILS OF SIDEWALK AND LANE CLOSURES.
- A RIGHT-OF-WAY PERMIT WILL BE REQUIRED FROM HENNEPIN COUNTY FOR WORK WITHIN THE WEST LAKE STREET RIGHT-OF-WAY.
- CONTACT BILL PRINCE AT (612) 673-3901 REGARDING IMPACTS TO THE EXISTING STREET LIGHTING SYSTEM.
- CONTACT DOUG MADAY AT (612) 673-5755 PRIOR TO CONSTRUCTION FOR THE REMOVAL OF ANY CITY OF MINNEAPOLIS SIGNS LOCATED IN THE RIGHT-OF-WAY THAT MAY BE WITHIN THE CONSTRUCTION LIMITS.
- CONTACT RALPH SIEVERT, DIRECTOR OF FORESTRY, AT (612) 313-7735 PRIOR TO THE REMOVAL OF ANY TREES LOCATED WITHIN THE ADJACENT PARK BOARD PROPERTY.



LEGEND

(P)	PROTECT TREE, SEE DETAIL ON THIS SHEET
x-x-x-x	REMOVE UTILITY
////	REMOVE CURB AND GUTTER
■	REMOVE BITUMINOUS PAVEMENT & AGGREGATE BASE
■	REMOVE CONCRETE PAVEMENT & AGGREGATE BASE

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ALL CONTRACTORS MUST CONTACT GOPHER STATE ONE CALL MN TOLL FREE 1-800-252-1166 BEFORE CONSTRUCTION BEGINS TWIN CITY AREA 651-454-0002



eIness swenson graham architects
 500 Washington Avenue South
 Minneapolis, Minnesota 55415
 P. 612.339.5508
 F. 612.339.5382
 www.esgarch.com

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Kimley»Horn

2550 UNIVERSITY AVE. WEST, SUITE 239N
 ST. PAUL, MINNESOTA 55114
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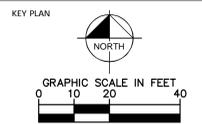
LAND USE APPLICATION
 5/16/2014

ORIGINAL ISSUE: 5/16/2014

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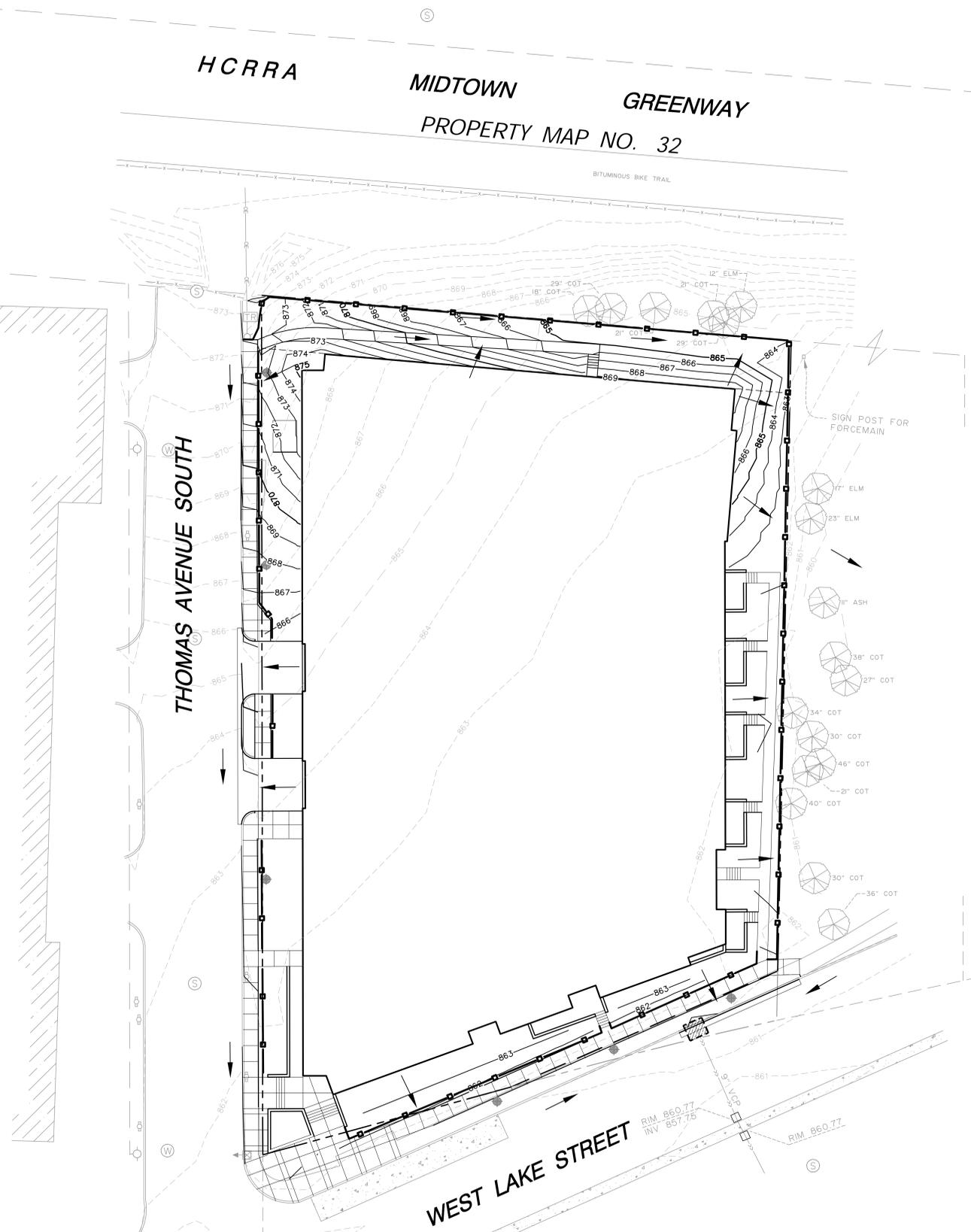
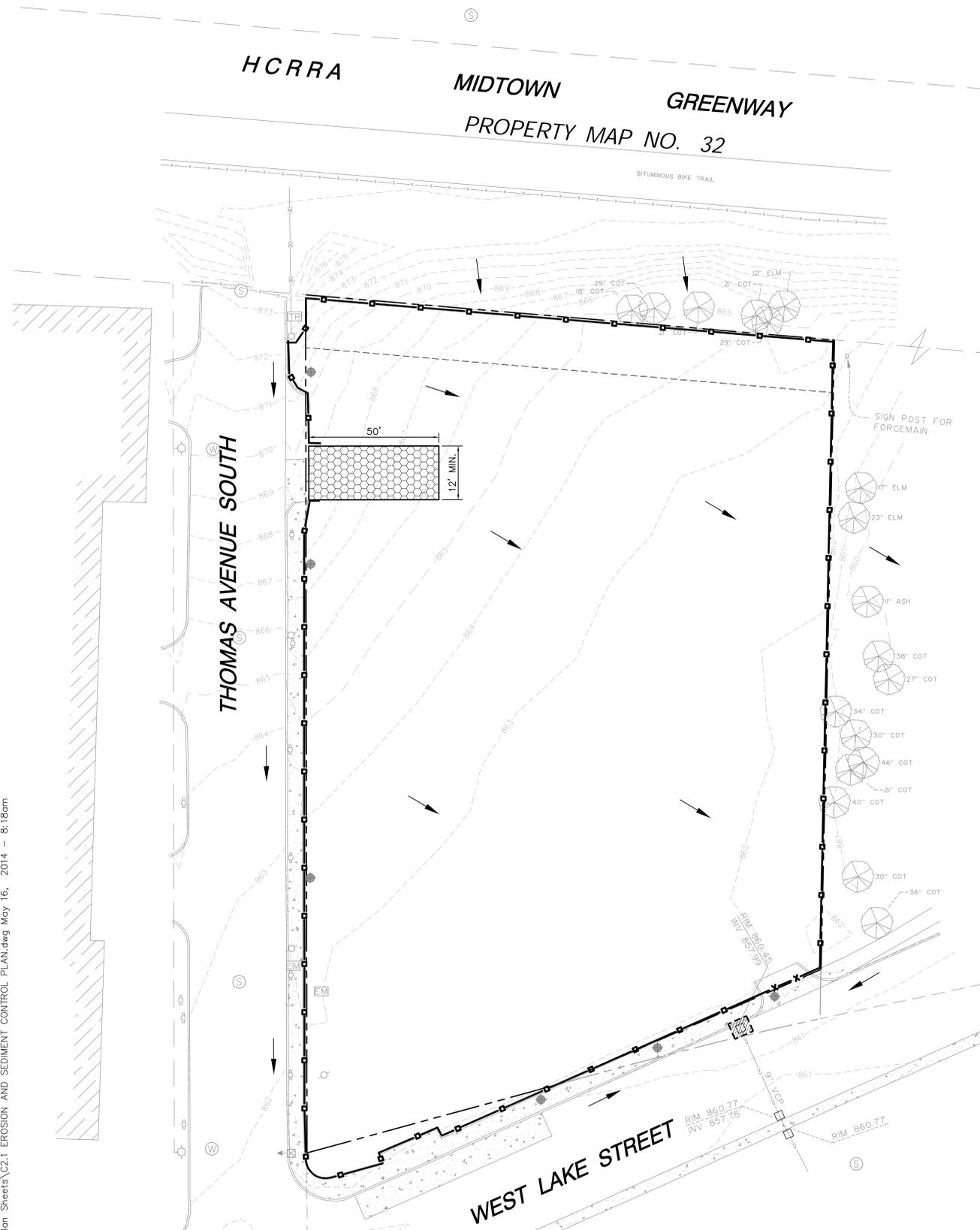
No.	Description	Date

160739000
 PROJECT NUMBER



2622 WEST LAKE STREET
 EROSION AND SEDIMENT CONTROL PLAN

C2.1



PHASE 1 - SITE PREP

PHASE 1 & 2 INSPECTION AND NOTIFICATION NOTES:

- INSPECTION:
 - THE INDIVIDUAL IDENTIFIED AS BEING RESPONSIBLE FOR IMPLEMENTING THE EROSION CONTROL PLAN MUST ROUTINELY INSPECT THE CONSTRUCTION SITE ONCE EVERY SEVEN DAYS DURING ACTIVE CONSTRUCTION AND WITHIN 24 HOURS AFTER A RAINFALL EVENT GREATER THAN 0.5 INCHES IN 24 HOURS.
 - ALL INSPECTIONS AND MAINTENANCE CONDUCTED DURING CONSTRUCTION MUST BE RECORDED IN WRITING AND THESE RECORDS MUST BE RETAINED SO AS TO BE MADE AVAILABLE WITHIN 24 HOURS OF THE CITY OF MINNEAPOLIS' REQUEST.
- NOTIFICATION:

THE CITY OF MINNEAPOLIS SHALL BE NOTIFIED IN WRITING AT THE FOLLOWING POINTS:

 - ON COMPLETING INSTALLATION OF PERIMETER EROSION AND SEDIMENTATION CONTROLS
 - ON COMPLETING LAND-DISTURBING ACTIVITIES AND PUTTING INTO PLACE MEASURES FOR FINAL SOIL STABILIZATION AND REVEGETATION.
 - WHENEVER ANY SITE DEWATERING IS PROPOSED TO TAKE PLACE.
 - WHEN ALL TEMPORARY EROSION AND SEDIMENTATION CONTROLS HAVE BEEN REMOVED FROM THE SITE.

LEGEND

- TEMPORARY ROCK CONSTRUCTION ENTRANCE
- SILT FENCE
- ROCK FILTER LOG
- INLET PROTECTION

OWNER: GREYSTAR REAL ESTATE PARTNERS

CONTRACTOR: TBD

PHASE 2 - PROPOSED

PHASE 1 & 2 GENERAL NOTES:

- REFER TO CONSTRUCTION INSPECTION PLAN IN PROJECT MANUAL FOR INSPECTION REQUIREMENTS DURING CONSTRUCTION.
- ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IN PLACE PRIOR TO DEMOLITION.
- INLET PROTECTION DEVICES AND ALL PERIMETER CONTROL SHALL BE MAINTAINED ONCE SEDIMENT ACCUMULATES TO A DEPTH 1/3 OF THE DESIGNED CAPACITY.

CONSTRUCTION SEQUENCE

- INSTALL SILT FENCE, ROCK LOG, INLET PROTECTION, AND ENTRANCE
- CLEAN AND GRUB SITE. REMOVE PAVEMENT, C&G, SIDEWALK
- MASS EXCAVATION
- CONSTRUCT BUILDING FOOTINGS AND UNDERGROUND PARKING GARAGE
- INSTALL SITE UTILITIES
- INSTALL INLET PROTECTION ON ALL NEW INLETS
- CONSTRUCT BUILDING
- CONSTRUCT CURB & GUTTER AND PAVEMENT
- COMPLETE FINAL GRADING
- COMPLETE FINAL STABILIZATION
- WITH APPROVAL FROM CITY OF MINNEAPOLIS, REMOVE EROSION CONTROL

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CITY OF MINNEAPOLIS STANDARD EROSION CONTROL NOTES:

PER JANUARY 7, 2011 STANDARDS

- CONTRACTOR MUST CALL (612) 673-2738 FOR A PRE-CONSTRUCTION MEETING 48 HOURS PRIOR TO ANY LAND DISTURBANCES. FAILURE TO DO SO MAY RESULT IN FINES, THE REVOCATION OF PERMIT AND A STOP WORK ORDER BEING ISSUED.
- INSTALL PERIMETER EROSION CONTROL AT THE LOCATIONS SHOWN ON THE PLANS PRIOR TO THE COMMENCEMENT OF ANY DISTURBANCE OR CONSTRUCTION ACTIVITIES.
- BEFORE BEGINNING CONSTRUCTION, INSTALL A TEMPORARY ROCK CONSTRUCTION ENTRANCE AT EACH POINT WHERE VEHICLES EXIT THE CONSTRUCTION SITE. USE 2 INCH OR GREATER DIAMETER ROCK IN A LAYER AT LEAST 6 INCHES THICK ACROSS THE ENTIRE WIDTH OF THE ENTRANCE. EXTEND THE ROCK ENTRANCE AT LEAST 50 FEET INTO THE CONSTRUCTION ZONE USING A GEO-TEXTILE FABRIC BENEATH THE AGGREGATE TO PREVENT MIGRATION OF SOIL INTO THE ROCK FROM BELOW.
- REMOVE ALL SOILS AND SEDIMENTS TRACKED OR OTHERWISE DEPOSITED ONTO PUBLIC AND PRIVATE PAVEMENT AREAS. REMOVAL SHALL BE ON A DAILY BASIS WHEN TRACKING OCCURS AND MAY BE ORDERED BY MINNEAPOLIS INSPECTORS AT ANY TIME IF CONDITIONS WARRANT. SWEEPING SHALL BE MAINTAINED THROUGHOUT THE DURATION OF THE CONSTRUCTION AND DONE IN A MANNER TO PREVENT DUST BEING BLOWN TO ADJACENT PROPERTIES.
- INSTALL INLET PROTECTION AT ALL PUBLIC AND PRIVATE CATCH BASIN INLETS, WHICH RECEIVE RUNOFF FROM THE DISTURBED AREAS. CONTRACTOR SHALL CLEAN, REMOVE SEDIMENT OR REPLACE STORM DRAIN INLET PROTECTION DEVICES ON A ROUTINE BASIS SUCH THAT THE DEVICES ARE FULLY FUNCTIONAL FOR THE NEXT RAIN EVENT. SEDIMENT DEPOSITED IN AND/OR PLUGGING DRAINAGE SYSTEMS IS THE RESPONSIBILITY OF THE CONTRACTOR. HAY BALES OR FILTER FABRIC WRAPPED GRATES ARE NOT ALLOWED FOR INLET PROTECTION.
- LOCATE SOIL OR DIRT STOCKPILES NO LESS THAN 25 FEET FROM ANY PUBLIC OR PRIVATE ROADWAY OR DRAINAGE CHANNEL. IF REMAINING FOR MORE THAN SEVEN DAYS, STABILIZE THE STOCKPILES BY MULCHING, VEGETATIVE COVER, TARP, OR OTHER MEANS. CONTROL EROSION FROM ALL STOCKPILES BY PLACING SILT BARRIERS AROUND THE PILES. TEMPORARY STOCKPILES LOCATED ON PAVED SURFACES MUST BE NO LESS THAN TWO FEET FROM THE DRAINAGE/GUTTER LINE AND SHALL BE COVERED IF LEFT MORE THAN 24 HOURS.
- MAINTAIN ALL TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES IN PLACE UNTIL THE CONTRIBUTING DRAINAGE AREA HAS BEEN STABILIZED. INSPECT TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES ON A DAILY BASIS AND REPLACE DETERIORATED, DAMAGED, OR ROTTED EROSION CONTROL DEVICES IMMEDIATELY.
- TEMPORARILY OR PERMANENTLY STABILIZE ALL CONSTRUCTION AREAS WHICH HAVE UNDERGONE FINAL GRADING AND ALL AREAS IN WHICH GRADING OR SITE BUILDING CONSTRUCTION OPERATIONS ARE NOT ACTIVELY UNDERWAY AGAINST EROSION DUE TO RAIN, WIND AND RUNNING WATER WITHIN 7-14 DAYS. USE SEED AND MULCH, EROSION CONTROL MATTING, AND/OR SODDING AND STAKING IN GREEN SPACE AREAS. REMOVE ALL TEMPORARY SYNTHETIC, STRUCTURAL, NON-BIODEGRADABLE EROSION AND SEDIMENT CONTROL DEVICES AFTER THE SITE HAS UNDERGONE FINAL STABILIZATION WITH PERMANENT VEGETATION ESTABLISHMENT. FINAL STABILIZATION FOR PURPOSES OF THIS REMOVAL IS 70% ESTABLISHED COVER OVER DENUDED AREA.
- READY MIXED CONCRETE AND CONCRETE BATCH/MIX PLANTS ARE PROHIBITED WITHIN THE PUBLIC RIGHT OF WAY. ALL CONCRETE RELATED PRODUCTION, CLEANING AND MIXING ACTIVITIES SHALL BE DONE IN THE DESIGNATED CONCRETE MIXING/WASHOUT LOCATIONS AS SHOWN IN THE EROSION CONTROL PLAN. UNDER NO CIRCUMSTANCE MAY WASHOUT WATER DRAIN ONTO THE PUBLIC RIGHT OF WAY OR INTO ANY PUBLIC OR PRIVATE STORM DRAIN CONVEYANCE.
- CHANGES TO APPROVED EROSION CONTROL PLAN MUST BE APPROVED BY THE EROSION CONTROL INSPECTOR PRIOR TO IMPLEMENTATION. CONTRACTOR TO PROVIDE INSTALLATION AND DETAILS FOR ALL PROPOSED ALTERNATE TYPE DEVICES.
- IF DEWATERING OR PUMPING OF WATER IS NECESSARY, THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ANY NECESSARY PERMITS AND/OR APPROVALS PRIOR TO DISCHARGE OF ANY WATER FROM THE SITE. IF THE DISCHARGE FROM THE DEWATERING OR PUMPING PROCESS IS TURBID OR CONTAINS SEDIMENT LADEN WATER, IT MUST BE TREATED THROUGH THE USE OF SEDIMENT TRAPS, VEGETATIVE FILTER STRIPS, OR OTHER SEDIMENT REDUCING MEASURES SUCH THAT THE DISCHARGE IS NOT VISIBLY DIFFERENT FROM THE RECEIVING WATER. ADDITIONAL EROSION CONTROL MEASURES MAY BE REQUIRED AT THE DISCHARGE POINT TO PREVENT SCOUR EROSION. THE CONTRACTOR SHALL PROVIDE A DEWATERING/PUMPING PLAN TO THE EROSION CONTROL INSPECTOR PRIOR TO INITIATING DEWATERING ACTIVITIES.



elines swenson graham architects
500 washington avenue south
minneapolis minnesota 55415
p. 612.339.5508
f. 612.339.5382
www.esgar.ch.com

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Typed or Printed Name _____

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Kimley»Horn

2550 UNIVERSITY AVE. WEST, SUITE 238N
ST. PAUL, MINNESOTA 55114
TEL. NO. (651) 645-4197

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LAND USE APPLICATION
5/16/2014

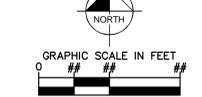
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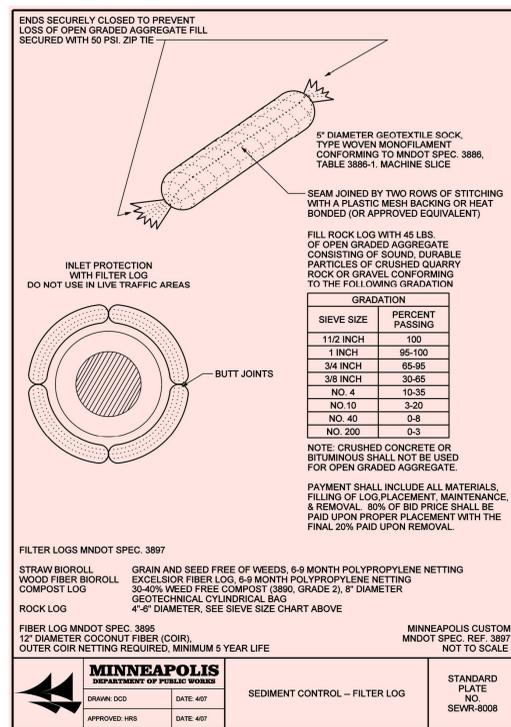
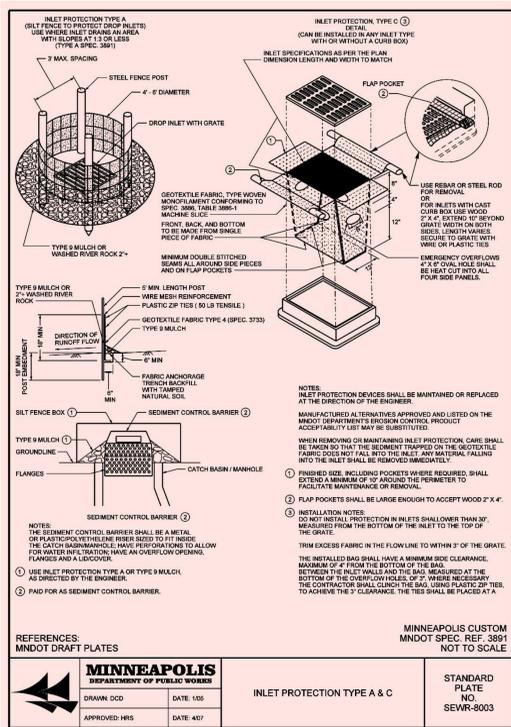
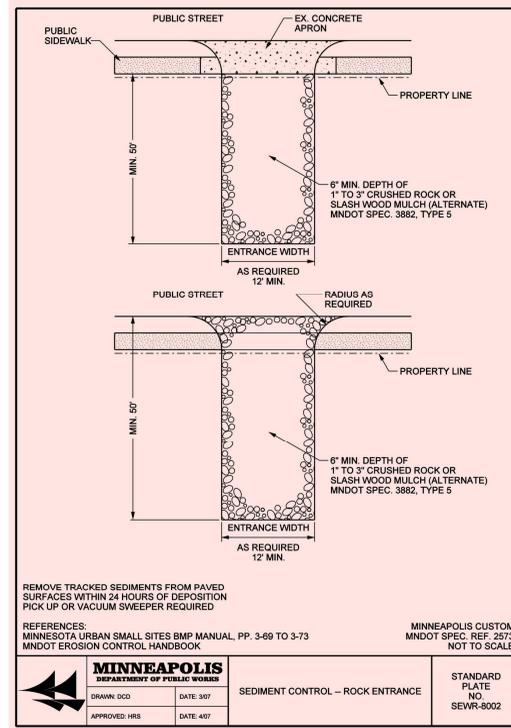
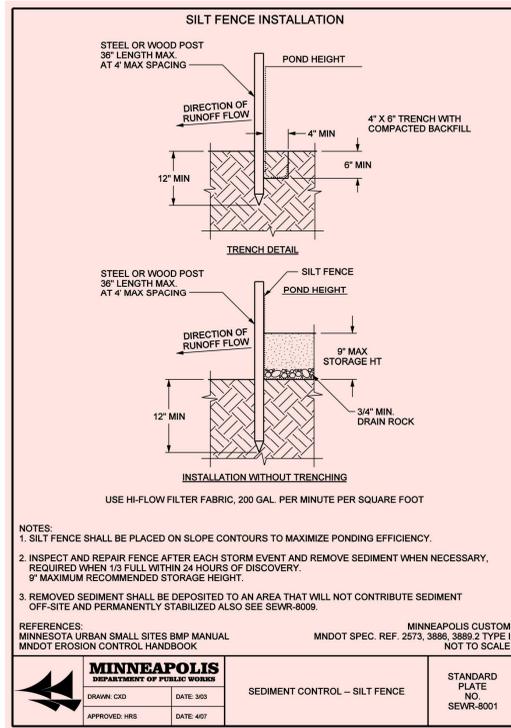
KEY PLAN



2622 WEST LAKE STREET

EROSION AND SEDIMENT CONTROL NOTES AND DETAILS

C2.2





eIness swenson graham architects
 500 Washington Avenue South
 Minneapolis, Minnesota 55415
 P. 612.339.5508
 F. 612.339.5382
 www.esgarch.com

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 ST. PAUL, MINNESOTA 55114
 TEL. NO. (651) 645-4197

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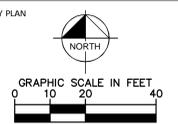
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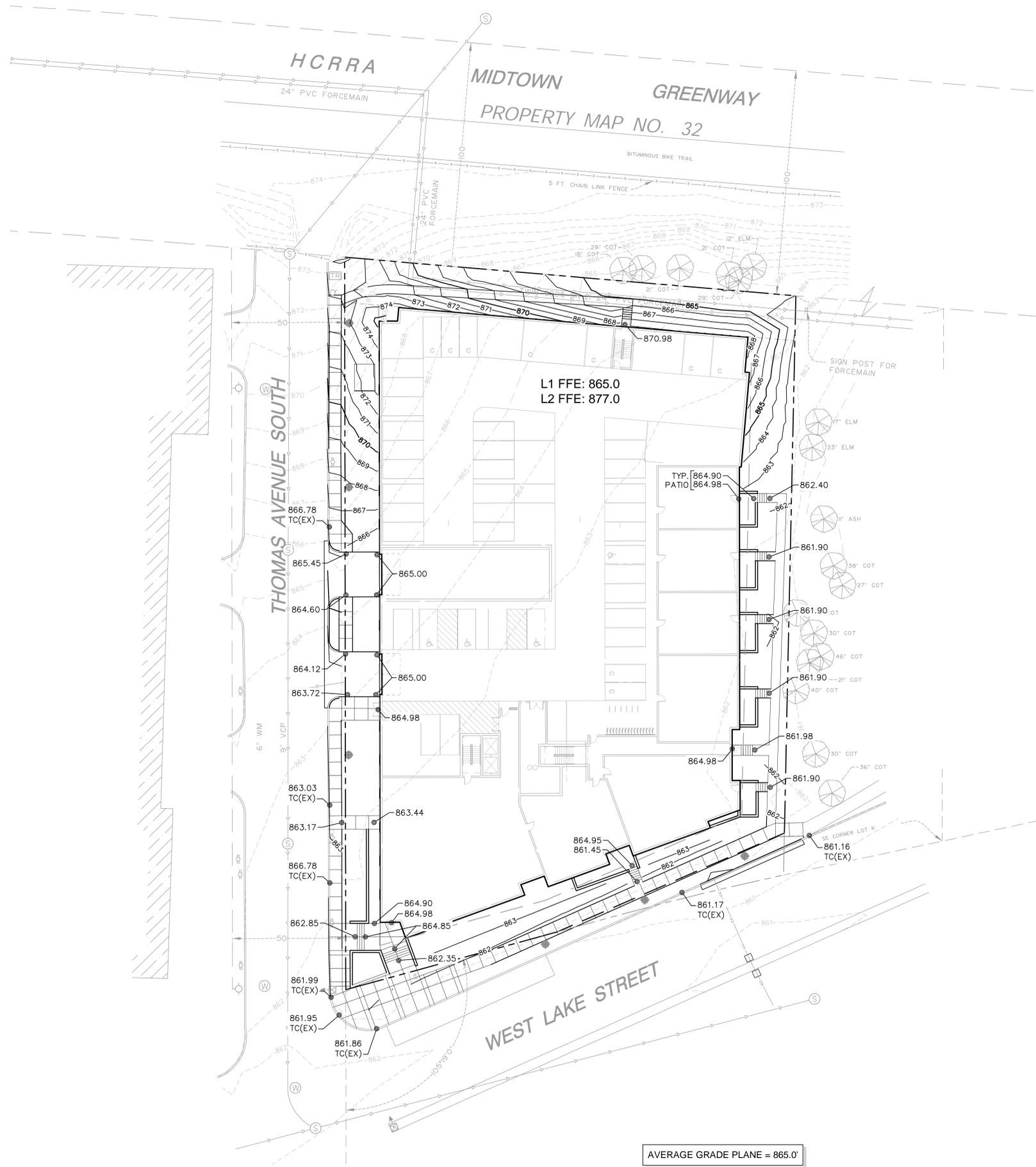
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2622 WEST LAKE STREET
 GRADING PLAN

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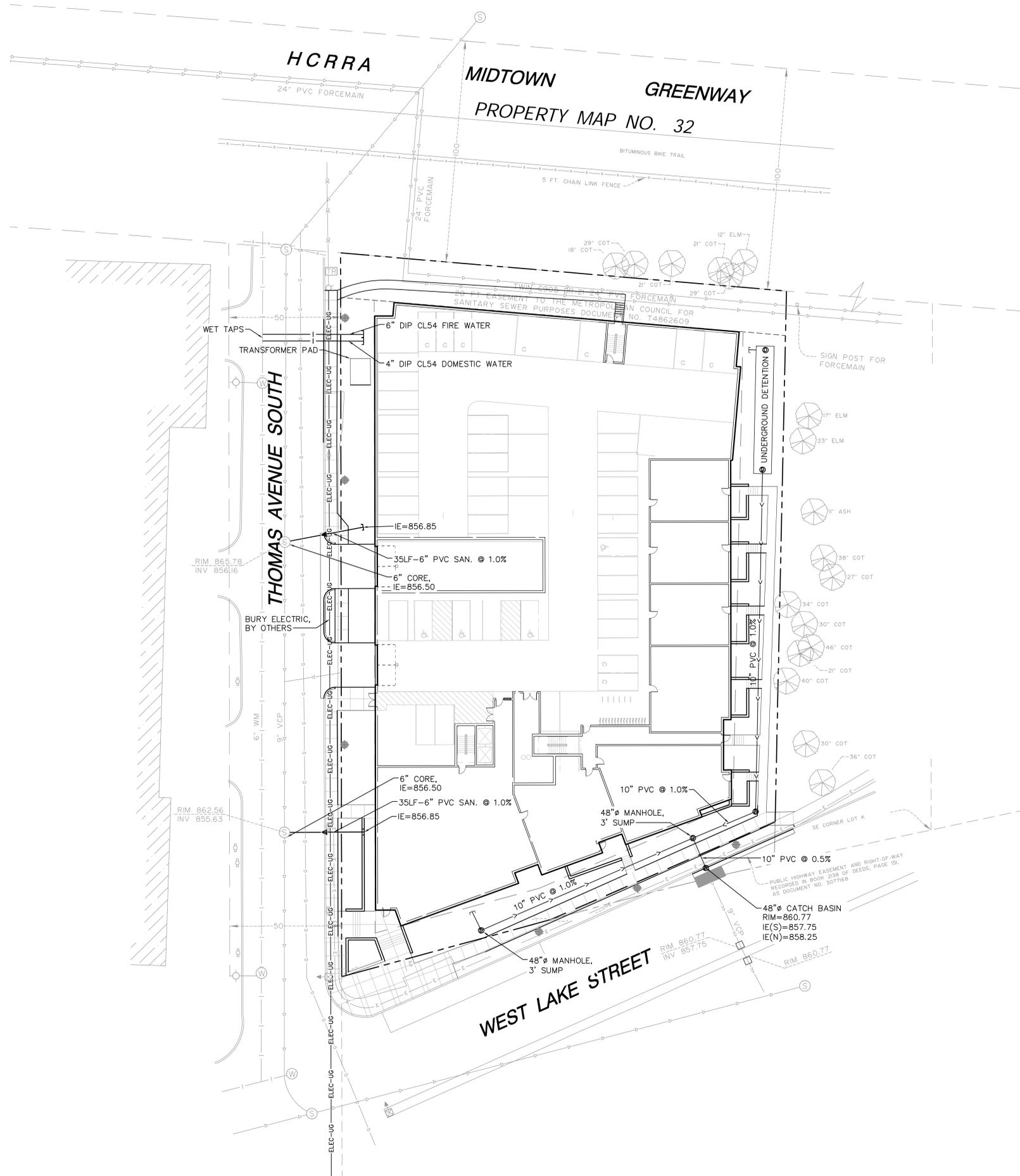


UTILITY NOTES:

1. THE LOCATION OF EXISTING UTILITIES, STORM DRAINAGE STRUCTURES AND OTHER ABOVE AND BELOW-GRADE IMPROVEMENTS ARE APPROXIMATE AS SHOWN. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE EXACT LOCATION, SIZE AND INVERT ELEVATIONS OF SUCH PRIOR TO THE START OF CONSTRUCTION.
2. ALL SANITARY SEWER, STORM SEWER AND WATER MAIN INSTALLATIONS SHALL BE PER MINNESOTA PLUMBING CODE AND IN ACCORDANCE WITH THE CURRENT EDITION OF "STANDARD SPECIFICATIONS FOR WATER MAIN AND SERVICE LINE INSTALLATION AND SANITARY SEWER AND STORM SEWER INSTALLATION" AS PREPARED BY THE CITY ENGINEERS ASSOCIATION OF AMERICA.
3. A MINIMUM VERTICAL SEPARATION OF 18 INCHES IS REQUIRED AT ALL WATER MAIN CROSSINGS WITH SANITARY SEWER OR STORM SEWER.
4. UTILITY SERVICES TYPICALLY TERMINATE FIVE (5) FEET OUTSIDE BUILDING WALL UNLESS OTHERWISE SHOWN OR NOTED.
5. ALL MATERIALS SHALL COMPLY WITH THE REQUIREMENTS OF THE CITY.
6. CONTRACTOR SHALL COORDINATE WITH THE CITY FOR PERMIT AND CITY TAP OF WATER MAIN.
7. CONTRACTOR SHALL CONFIRM LOCATION OF SERVICE ENTRY AT BUILDING.
8. MINIMUM WATER MAIN BURY DEPTH = 7.5 FEET

SYMBOL LEGEND

-  PROPOSED STORM SEWER
-  PROPOSED STORM MANHOLE
-  PROPOSED SANITARY SEWER
-  PROPOSED WATER MAIN/SERVICE



einess swenson graham architects
 500 WASHINGTON AVENUE SOUTH
 MINNEAPOLIS MINNESOTA 55415
 P. 612.339.5508
 F. 612.339.5382
 WWW.ESGARCH.COM

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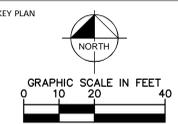
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2622 WEST LAKE STREET
 UTILITY PLAN

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eIness swenson graham architects
500 washington avenue south
minneapolis minnesota 55415
p. 612.339.5508
f. 612.339.5382
www.esgarch.com

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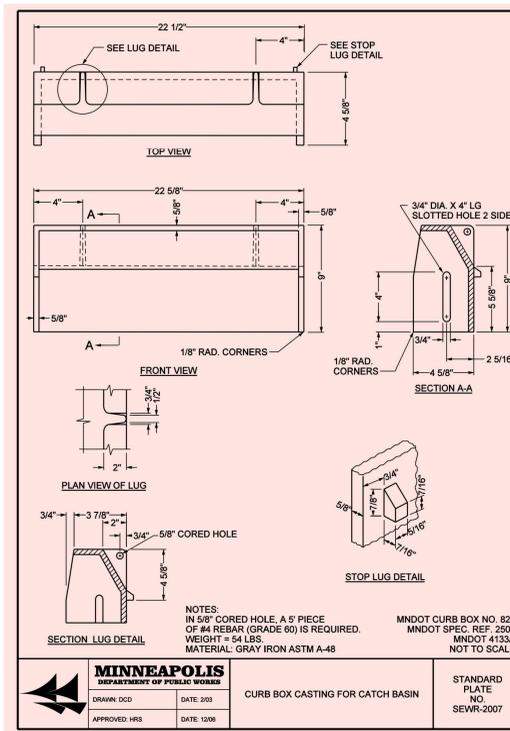
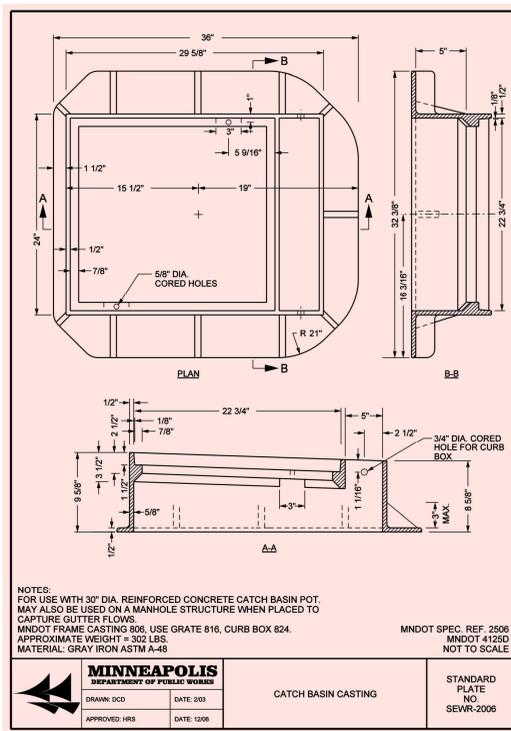
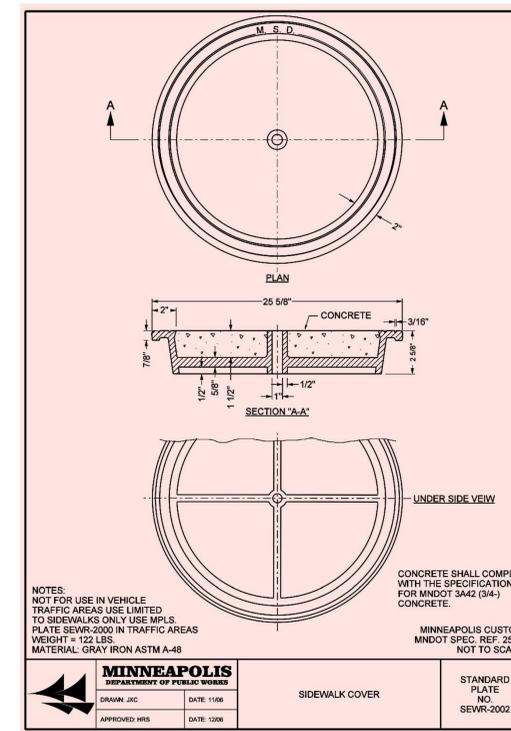
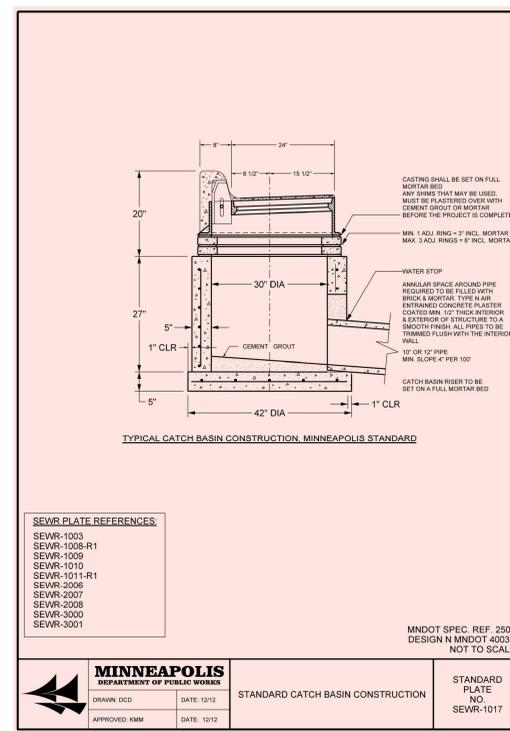
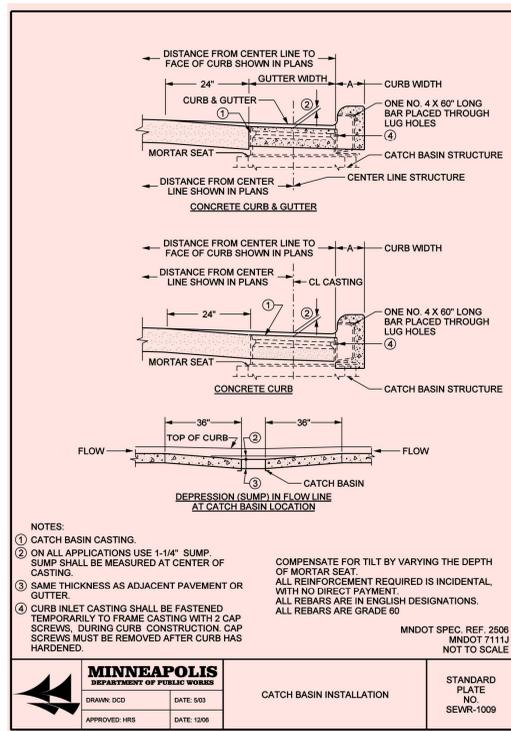
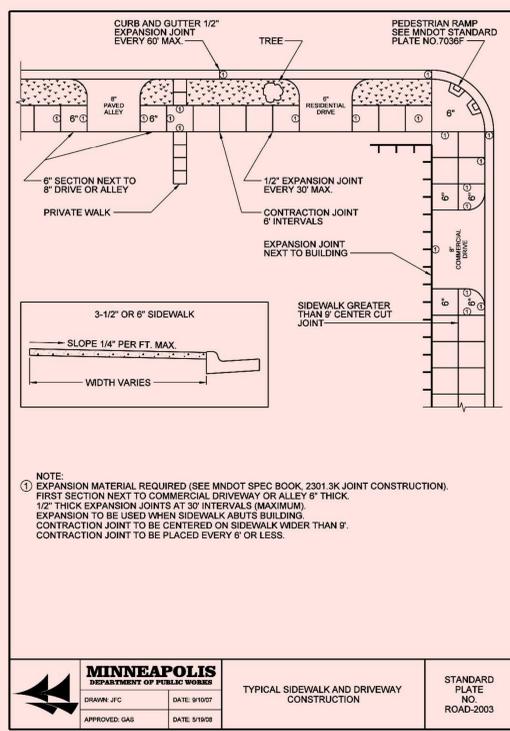
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SPECIFICATION NOTES:
ALL P.V.C. PIPE SHALL MEET OR EXCEED ASTM D 1784 AND ASTM D 2241, AASHTO M 278, CLASS PF 50, P.V.C., SDR-26; ASTM D 3034, TYPE PSM P.V.C. SEWER PIPE, SDR 26 ASTM 758, SMOOTH WALL P.V.C., TYPE PS 46. ALL SANITARY CONNECTIONS LESS THAN 10" DIAMETER SHALL BE SCH 40 AND CLEANED WITH PROPER PRIMER AS DEFINED IN THE RESPECTIVE ASTM SPECIFICATION FOR SOLVENT JOINTS AND ALL JOINTS SHALL BE SOLVENT CEMENTED JOINTS WITH PROPER ASTM APPROVED CEMENTS. ALL SANITARY CONNECTIONS 10" DIAMETER OR GREATER SHALL BE SDR-26 OR GREATER WITH COMPRESSION FITTINGS MEETING ASTM STANDARDS. ALL MAINLINE SANITARY SEWERS SHALL BE VITRIFIED CLAY PIPE (VCP) UNLESS APPROVED BY CITY ENGINEER TO SUBSTITUTE PVC. SANITARY MAINS CONSTRUCTED OF PVC MATERIALS SHALL BE SDR-26 OR GREATER.

STORM DRAIN ONLY
SDR-35 MAY BE SPECIFIED FOR CITY CB RUNS ONLY. AT THE DISCRETION OF THE CITY PROJECT ENGINEER ALL STORM DRAIN MAIN LINES SHALL BE REINFORCED CONCRETE MATERIALS WHERE POSSIBLE. PVC STORM DRAINS SHALL BE PRE APPROVED BY THE SEWER MAINTENANCE ENGINEER.

ALL DIMENSIONS BELOW ARE VARIABLE BY MANUFACTURER
ALL PVC SHALL BE CONSTRUCTED FROM NEW MATERIALS

NOMINAL PIPE SIZE (P.V.C. SDR 26)	AVERAGE I.D.		WALL THICKNESS		WEIGHT		MAX W.P. (PSI)		
	40	80	SDR 26	40	80	SDR 26	40	80	
4"	4.500	3.998	3.786	4.134	0.237	0.337	0.173	2,006	2,762
5"	5.563	5.017	4.767	5.108	0.258	0.375	0.214	2,726	3,667
6"	6.625	6.031	5.709	6.084	0.280	0.432	0.255	3,535	5,313
8"	8.625	7.943	7.565	7.921	0.322	0.500	0.332	5,305	8,058
10"	10.750	9.976	9.492	9.874	0.365	0.593	0.413	7,532	11,956
12"	12.750	11.890	11.284	11.711	0.408	0.687	0.460	9,949	16,437
14"	14.000	13.072	12.410	12.860	0.437	0.750	0.538	11,810	19,790
16"	16.000	14.940	14.214	14.696	0.500	0.843	0.615	15,416	25,430
18"	18.000	16.809	16.014	16.533	0.562	0.937	0.692	20,112	31,830
20"	20.000	18.743	17.814	18.370	0.593	1.031	0.769	23,624	40,091
24"	24.000	22.544	21.418	22.043	0.687	1.218	0.923	32,873	56,882

DIMENSIONS ARE IN INCHES
WEIGHTS ARE IN POUNDS PER FT.
MINIMUM COVER ON PIPE IS 18" LESS THAN 18" USE DUCTILE IRON PIPE CLASS 52 OR EQUIVALENT.
ALTERNATE PIPE MATERIAL FOR MAINLINE CONSTRUCTION.

MINNEAPOLIS DEPARTMENT OF PUBLIC WORKS
DRAWN: DCD DATE: 12/02
APPROVED: HRS DATE: 12/06

PVC PIPE STANDARDS

STANDARD PLATE NO. SEWR-4015

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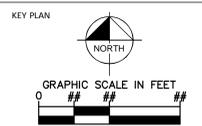
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einess swenson graham architects
 500 WASHINGTON AVENUE SOUTH
 MINNEAPOLIS, MINNESOTA 55415
 P. 612.339.5508
 F. 612.339.5382
 WWW.ESGARCH.COM

Signature _____
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Kimley»Horn
 2550 UNIVERSITY AVE. WEST, SUITE 238N
 ST. PAUL, MINNESOTA 55114
 TEL. NO. (651) 645-4197

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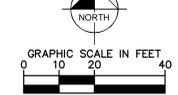
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KEY PLAN



2622 WEST LAKE STREET
 LANDSCAPE PLAN

L1.1

PROJECT PLANT SCHEDULE

TREES	CODE	QTY	BOTANICAL NAME	COMMON NAME	CONT.	CAL.
	SSB	5	BETULA NIGRA 'SHILOH SPLASH'	SHILOH SPLASH CLUMP	B & B	6' HT
	SHL	4	GLEDITSIA TRIACANTHOS 'SKYLINE'	SKYLINE HONEY LOCUST	B & B	2.5' CAL.
	PFC	7	MALUS X 'PRAIRIFIRE'	PRAIRIFIRE CRAB APPLE	B & B	2.5' CAL.
	NHE	10	ULMUS CARPINIFOLIA 'NEW HORIZON'	NEW HORIZON ELM	B & B	2.5' CAL.

SHRUBS	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE
	RGD	28	CORNUS ALBA 'SIBIRICA RED GNOME'	RED GNOME DOGWOOD	#5 CONT.
	HBA	46	HYDRANGEA ARBORESCENS 'ANNABELLE'	ANNABELLE SMOOTH HYDRANGEA	#5 CONT.
	APC	28	RIBES ALPINUM	ALPINE CURRANT	#2 CONT.
	FPR	50	ROSA 'UHLATER'	PAVEMENT FOXI ROSE	#3 CONT.
	NFS	29	SPIRAEA JAPONICA 'NEON FLASH'	NEON FLASH SPIREA	#5 CONT.
	TAY	17	TAXUS X MEDIA 'TAUNTON'	TAUNTON YEW	#5 CONT.

ANNUALS/PERENNIALS	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE
	GMF	25	GERANIUM SANGUINEUM 'MAX FREI'	GERANIUM MAX FREI	4" POT
	GEO	15	GERANIUM X 'ORION'	GERANIUM ORION	4" POT
	GMD	207	HEMEROCALLIS X 'GRAPE MAGIC'	GRAPE MAGIC DAYLILY	#1 CONT.
	RSD	204	HEMEROCALLIS X 'RUBY STELLA'	DAYLILY RUBY STELLA	#1 CONT.
	SSD	184	HEMEROCALLIS X 'STELLA SUPREME'	DAYLILY STELLA SUPREME	#1 CONT.
	CWL	33	NEPETA X FAASSENII 'WALKERS LOW'	WALKERS LOW CATMINT	#1 CONT.
	SDP	36	PETUNIA X HYBRIDA 'STRAWBERRY DADDY'	STRAWBERRY DADDY PETUNIA	4" POT

GRASSES	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE
	KFG	95	CALAMAGROSTIS X ACUTIFLORA 'KARL FOERSTER'	FEATHER REED GRASS	#2 CONT.
	RFG	4	PENNISETUM SETACEUM 'ATROPURPUREA'	RED FOUNTAIN GRASS	#1 CONT.
	LBS	75	SCHIZACHYRIUM SCOPARIUM 'BLUE HEAVEN'	BLUE HEAVEN LITTLE BLUESTEM	#2 CONT.

GROUND COVERS	CODE	QTY	BOTANICAL NAME	COMMON NAME	CONT.
	SOD	7,821 SF	SOD	SOD	SOD
	GPV	41 SF	SOLANUM JASMINOIDES 'GEORIGA LIGHT GREEN'	POTATO VINE GEORIGA LIGHT GREEN	3.5" POT

WEST LAKE STREET BOULEVARD PLANT SCHEDULE

TREES	CODE	QTY	BOTANICAL NAME	COMMON NAME	CONT.	CAL.
	SHL	4	GLEDITSIA TRIACANTHOS VAR. INERMIS 'SKYOLE'	SKYLINE HONEYLOCUST	B & B	2.5' CAL.
	ABM	3	ACER FREEMANII 'AUTUMN BLAZE'	AUTUMN BLAZE MAPLE	B & B	2.5' CAL.

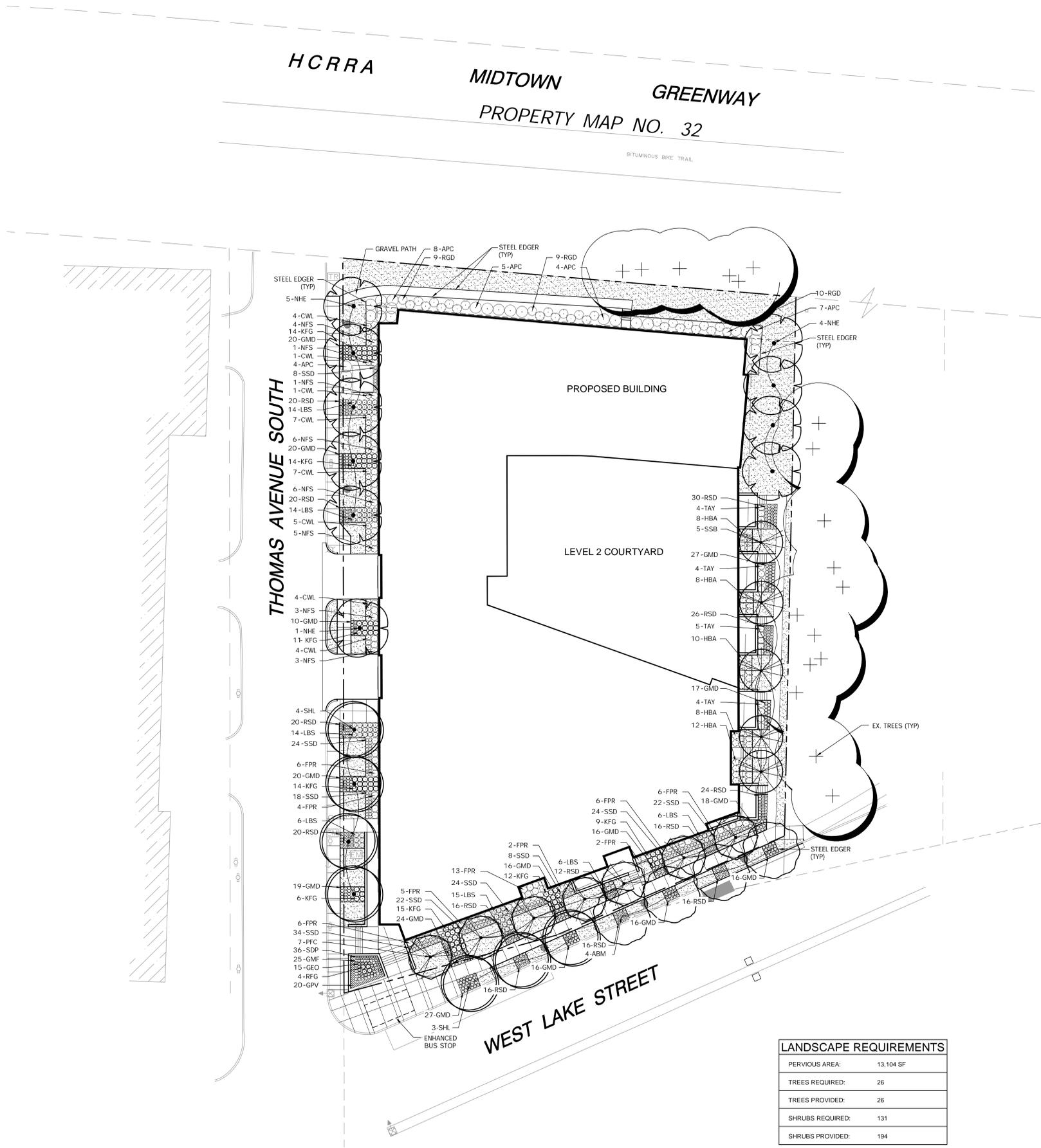
ANNUALS/PERENNIALS	CODE	QTY	BOTANICAL NAME	COMMON NAME	SIZE
	GMD	75	HEMEROCALLIS X 'GRAPE MAGIC'	GRAPE MAGIC DAYLILY	#1 CONT.
	RSD	48	HEMEROCALLIS X 'RUBY STELLA'	DAYLILY RUBY STELLA	#1 CONT.

NOTE TO CONTRACTOR: IF GRAPHIC REPRESENTATION OF PLANTINGS ON PLANS DOES NOT MATCH QUANTITIES IN PLANT LIST, GRAPHIC REPRESENTATION OF PLANTINGS ON PLANS WILL GOVERN.

LANDSCAPE NOTES

- ALL TURF AREAS TO RECEIVE A MINIMUM OF 4" OF TOPSOIL.
- ALL PLANT MATERIAL SHALL BE HEALTHY, VIGOROUS, AND FREE OF PESTS AND DISEASE.
- ALL TREES SHALL HAVE A STRAIGHT TRUNK AND FULL HEAD AND MEET ALL REQUIREMENTS SPECIFIED.
- ALL MATERIALS ARE SUBJECT TO THE APPROVAL OF THE LANDSCAPE ARCHITECT BEFORE, DURING, AND AFTER INSTALLATION.
- CONTRACTOR SHALL LOCATE ALL EXISTING UNDERGROUND UTILITIES AND NOTIFY LANDSCAPE ARCHITECT OF ANY CONFLICTS. CONTRACTOR SHALL EXERCISE CAUTION WHEN WORKING IN THE VICINITY OF UNDERGROUND UTILITIES.
- CONTRACTOR SHALL BE RESPONSIBLE FOR DELIVERY, SCHEDULE AND PROTECTION BETWEEN DELIVERY AND PLANTING TO MAINTAIN HEALTHY PLANT CONDITIONS.
- ANY PLANT MATERIAL WHICH IS DISEASED, DISTRESSED, DEAD, OR REJECTED SHALL BE PROMPTLY REMOVED FROM THE SITE AND REPLACED WITH MATERIAL OF THE SAME SPECIES, QUANTITY, AND SIZE AND MEETING ALL PLANT LIST SPECIFICATIONS.
- STANDARDS SET FORTH IN 'AMERICAN STANDARD FOR NURSERY STOCK' REPRESENT GUIDELINE SPECIFICATIONS ONLY AND SHALL CONSTITUTE MINIMUM QUALITY REQUIREMENTS FOR PLANT MATERIAL.
- INSTALL LAWN SOD IN AREAS SHOWN ON PLAN.
- ALL PLANTING AREAS TO HAVE IRRIGATION.
- COORDINATE ALL PLANTINGS WITH IRRIGATION CONTRACTOR. PROTECT ALL IRRIGATION SYSTEM COMPONENTS DURING PLANTING.
- INSTALL STEEL EDGER (BLACK) ANYWHERE SOD ADJUTS SHRUB/PERENNIAL BEDS AND AT EDGES OF GRAVEL PATH.

1 PLANTING PLAN



LANDSCAPE REQUIREMENTS	
PERVIOUS AREA:	13,104 SF
TREES REQUIRED:	26
TREES PROVIDED:	26
SHRUBS REQUIRED:	131
SHRUBS PROVIDED:	194



einess swenson graham architects
 500 washington avenue south
 minneapolis minnesota 55415
 p. 612.339.5508
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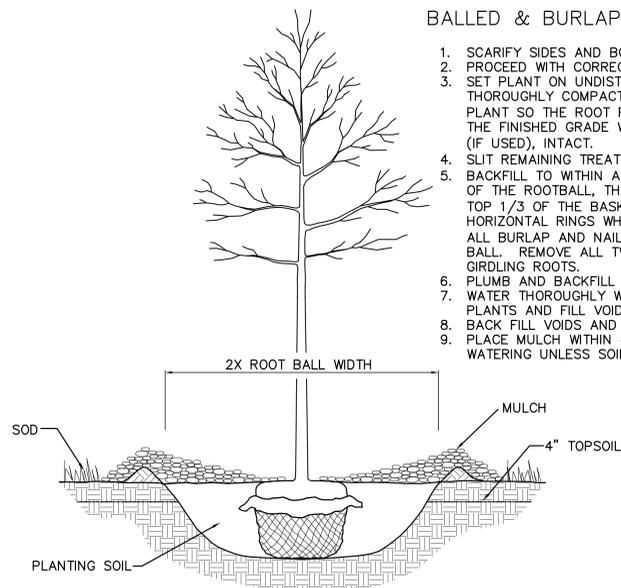
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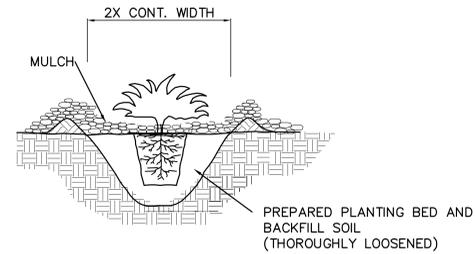
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BALLED & BURLAPPED STOCK



1. SCARIFY SIDES AND BOTTOM OF HOLE.
2. PROCEED WITH CORRECTIVE PRUNING.
3. SET PLANT ON UNDISTURBED NATIVE SOIL OR THOROUGHLY COMPACTED PLANTING SOIL. INSTALL PLANT SO THE ROOT FLARE IS AT OR UP TO 2" ABOVE THE FINISHED GRADE WITH BURLAP AND WIRE BASKET, (IF USED), INTACT.
4. SLIT REMAINING TREATED BURLAP AT 6" INTERVALS.
5. BACKFILL TO WITHIN APPROXIMATELY 12" OF THE TOP OF THE ROOTBALL, THEN WATER PLANT. REMOVE THE TOP 1/3 OF THE BASKET OR THE TOP TWO HORIZONTAL RINGS WHICHEVER IS GREATER. REMOVE ALL BURLAP AND NAILS FROM THE TOP 1/3 OF THE BALL. REMOVE ALL TWINE. REMOVE OR CORRECT STEM GIRDLING ROOTS.
6. PLUMB AND BACKFILL WITH PLANTING SOIL.
7. WATER THOROUGHLY WITHIN 2 HOURS TO SETTLE PLANTS AND FILL VOIDS.
8. BACK FILL VOIDS AND WATER SECOND TIME.
9. PLACE MULCH WITHIN 48 HOURS OF THE SECOND WATERING UNLESS SOIL MOISTURE IS EXCESSIVE.

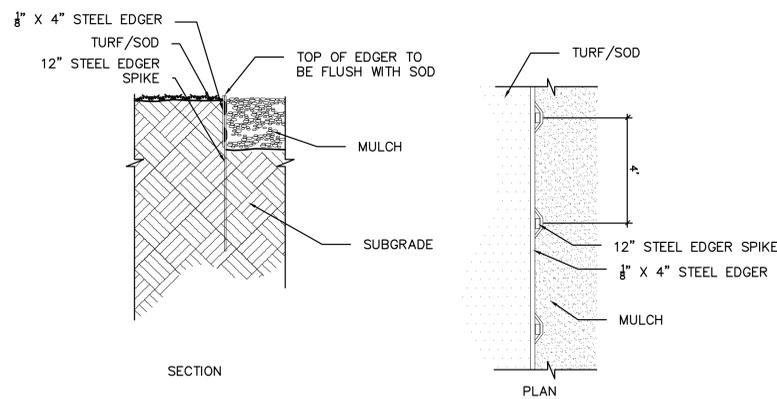
1 TREE PLANTING DETAIL
 L1.2



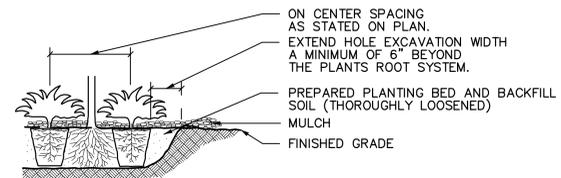
CONTAINER STOCK

1. SCARIFY SIDES AND BOTTOM OF HOLE.
2. PROCEED WITH CORRECTIVE PRUNING OF TOP AND ROOT.
3. REMOVE CONTAINER AND SCORE OUTSIDE OF SOIL MASS TO REDIRECT AND PREVENT CIRCLING FIBROUS ROOTS. REMOVE OR CORRECT STEM GIRDLING ROOTS.
4. SET PLANT ON UNDISTURBED NATIVE SOIL OR THOROUGHLY COMPACTED PLANTING SOIL. INSTALL PLANT SO THE TOP OF THE ROOT FLARE IS AT OR UP TO 2" ABOVE THE FINISHED GRADE.
5. PLUMB AND BACKFILL WITH PLANTING SOIL.
6. WATER THOROUGHLY WITHIN 2 HOURS TO SETTLE PLANTS AND FILL VOIDS.
7. BACK FILL VOIDS AND WATER SECOND TIME.
8. PLACE MULCH WITHIN 48 HOURS OF THE SECOND WATERING UNLESS SOIL MOISTURE IS EXCESSIVE.

2 SHRUB PLANTING DETAIL
 L1.2



3 LANDSCAPE EDGER DETAIL
 L1.2



4 PLANTING DETAIL FOR MASS PLANTING BEDS
 L1.2

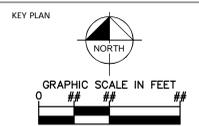
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eines swenson graham architects
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 minneapolis minnesota 55415
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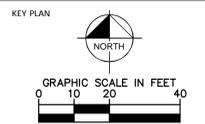
LAND USE APPLICATION
 5/16/2014

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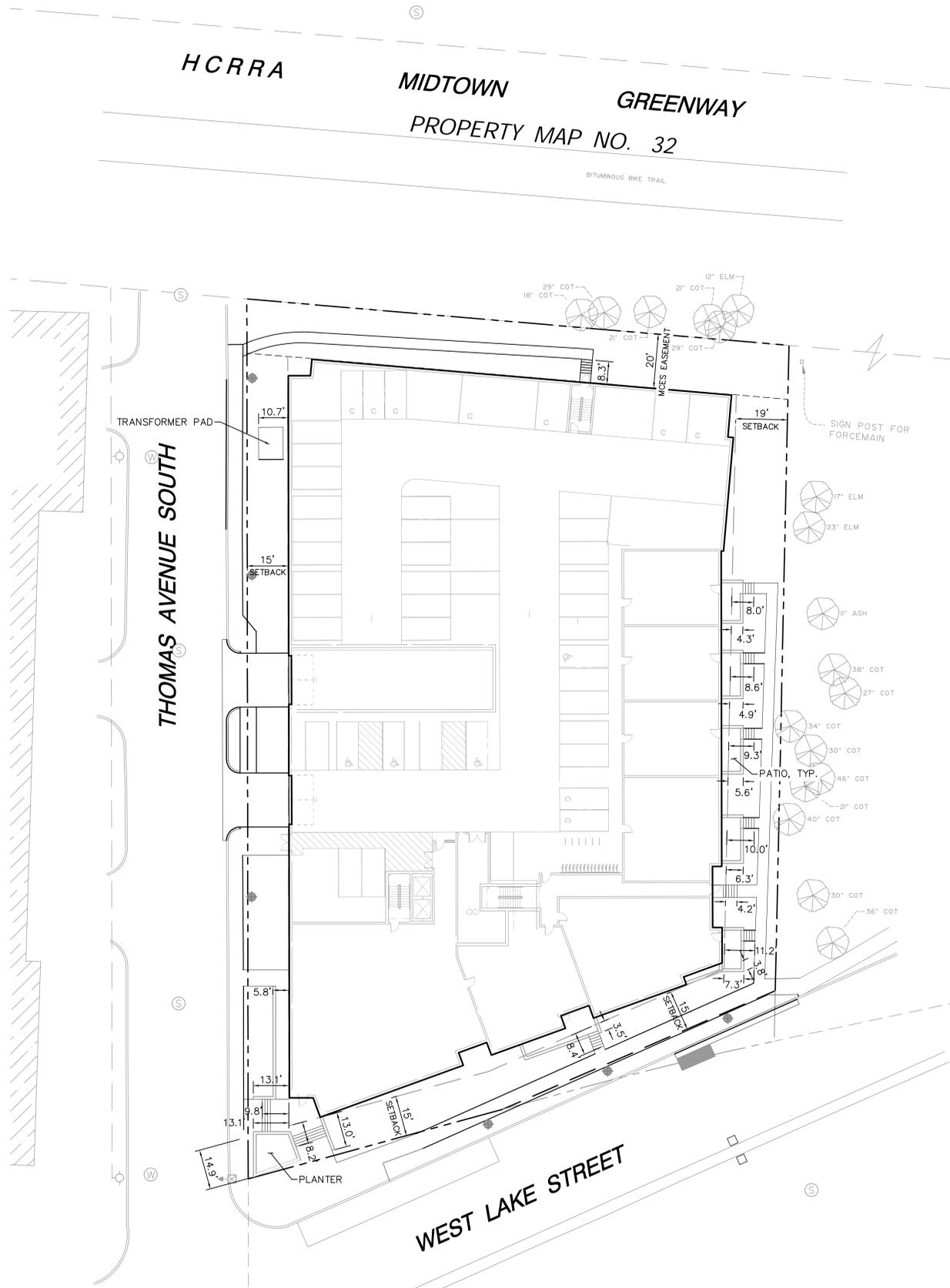
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160739000
 PROJECT NUMBER

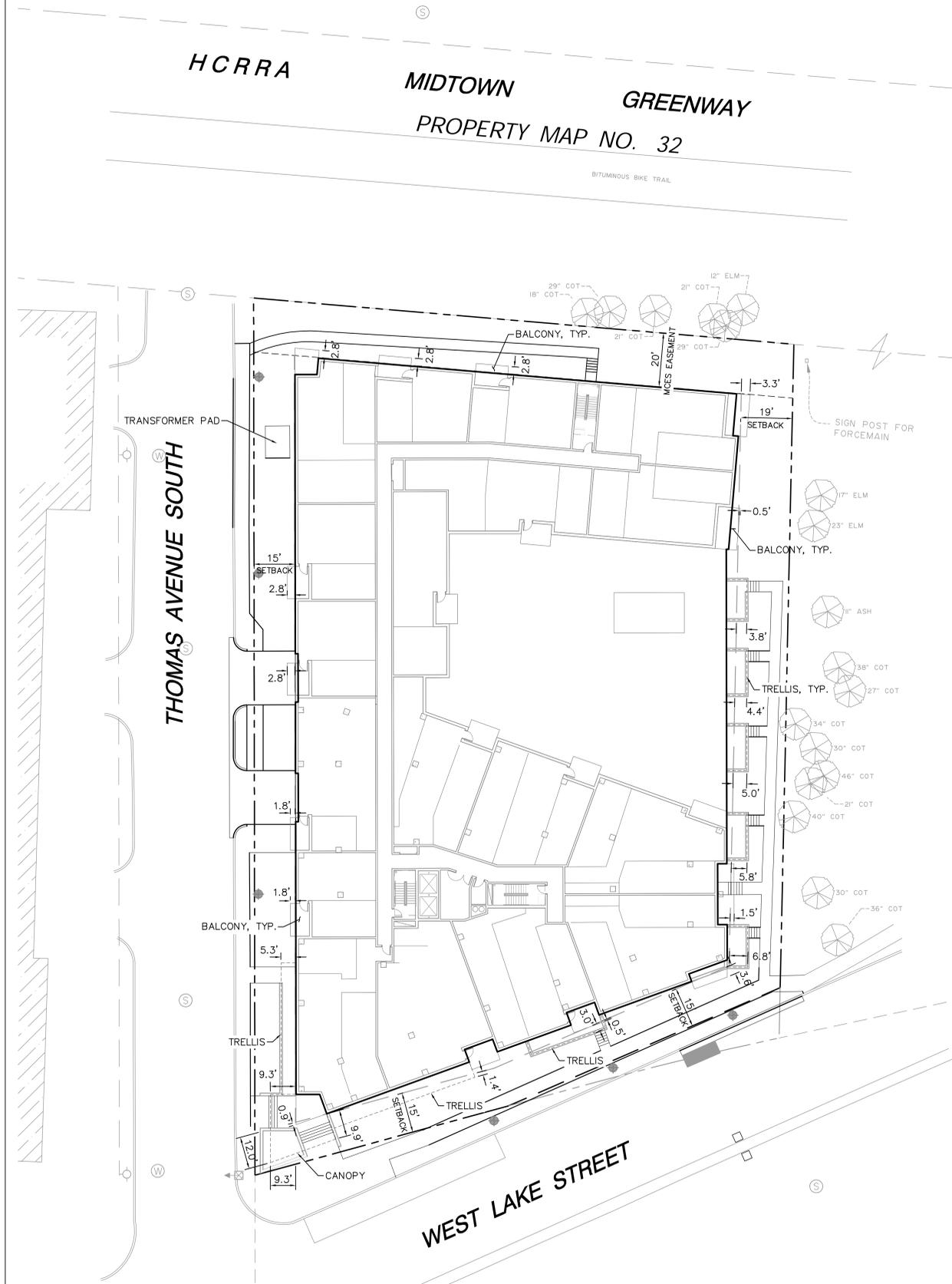


2622 WEST LAKE STREET
 ENCROACHMENTS IN
 SETBACK PLAN

EX-A



GROUND LEVEL PLAN



UPPER LEVEL PLAN



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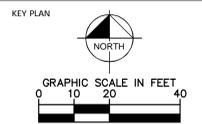
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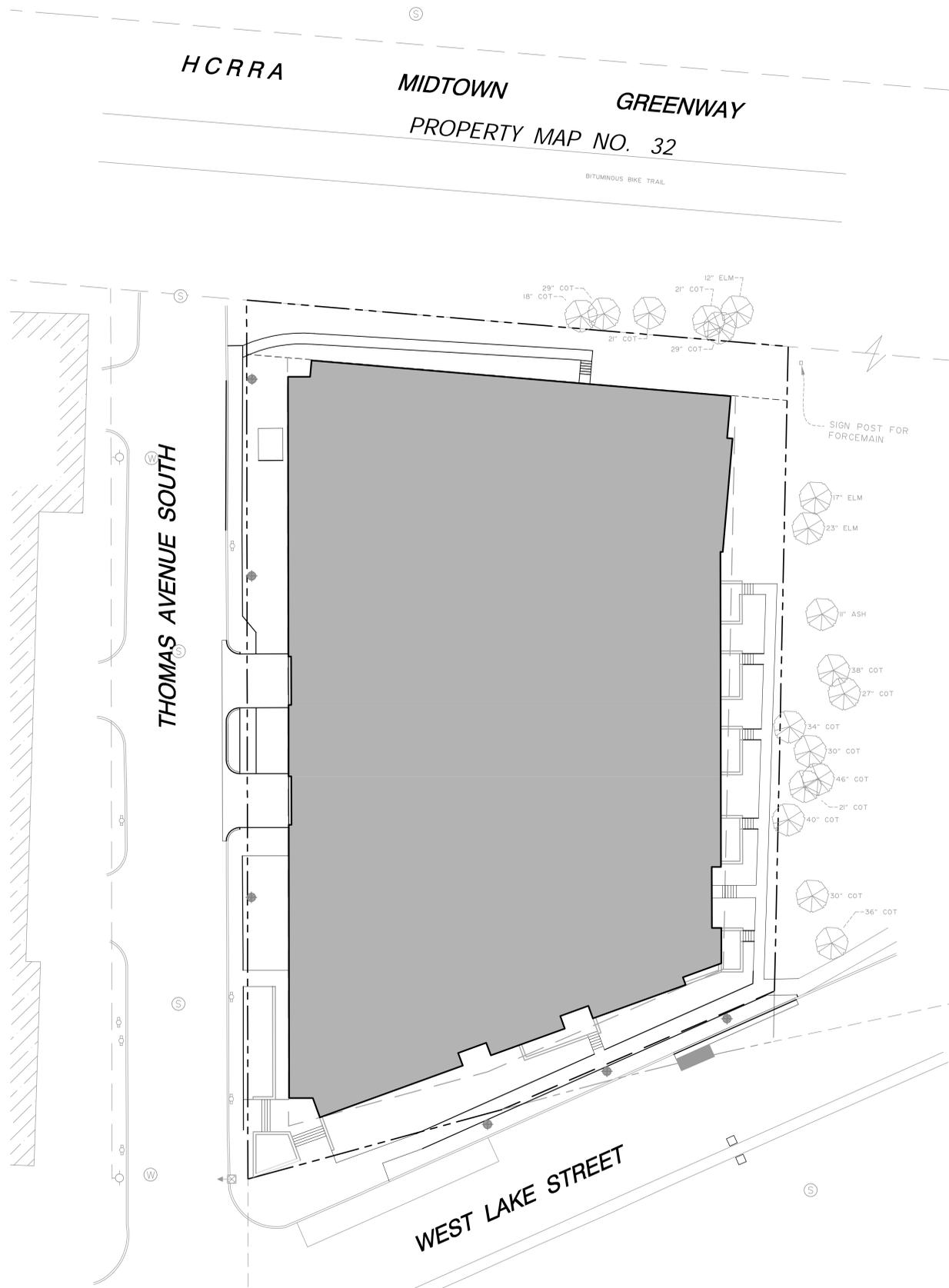
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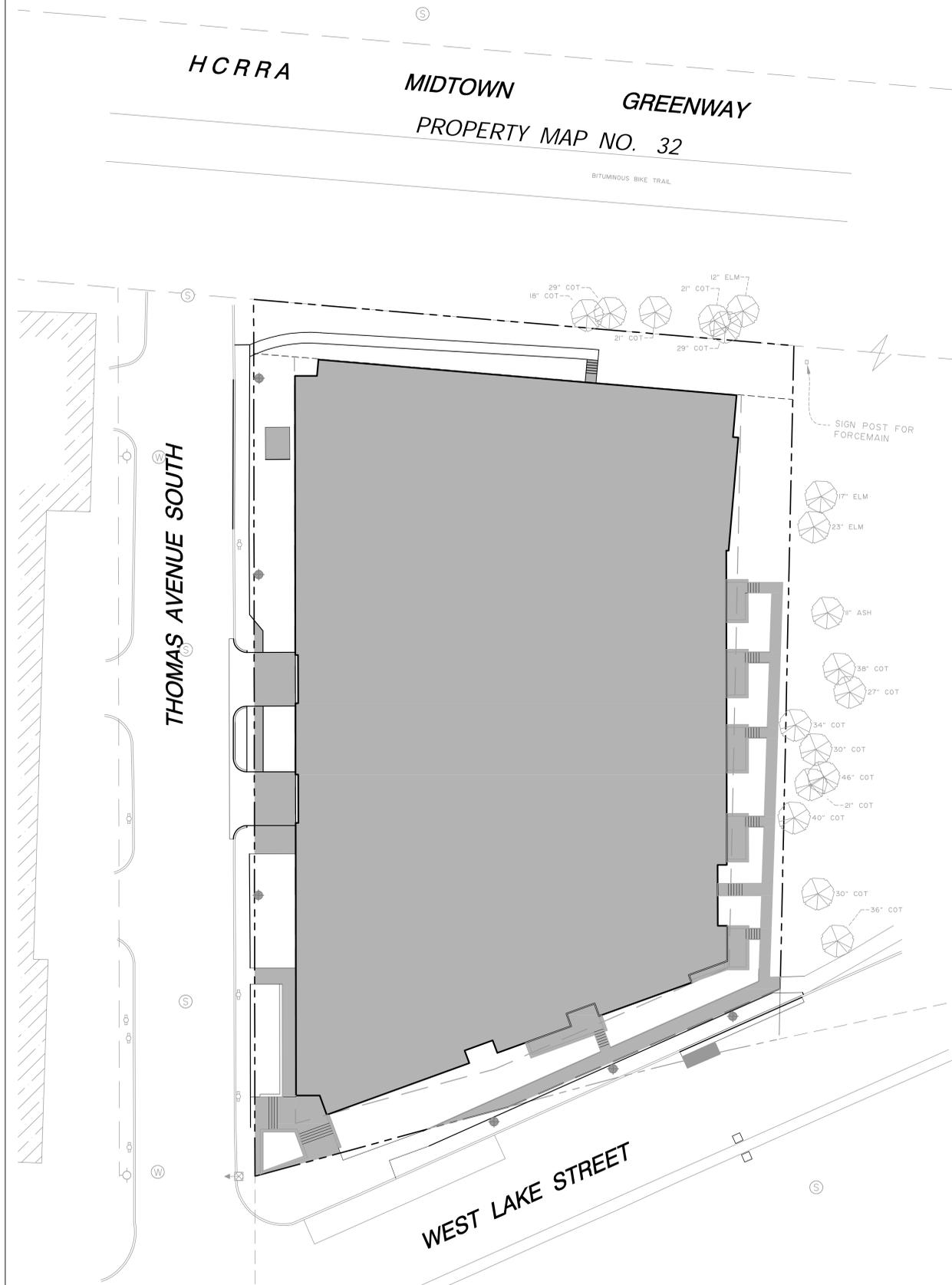
2622 WEST LAKE STREET
 LOT AND IMPERVIOUS SURFACE COVERAGE

EX-B



LOT COVERAGE

Maximum Allowable Lot Coverage: $0.70 \times 56,315 = 39,420.50$ square feet
 Proposed Building Footprint = 39,369 square feet
 Proposed < Maximum Allowable



IMPERVIOUS SURFACE COVERAGE

Maximum Allowable Impervious Surface Coverage: $0.85 \times 56,315 = 47,867$ square feet
 Proposed Impervious Surface = $44,086 / 56,315 \times 100 = 78.8\%$



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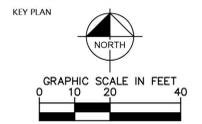
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2622 WEST LAKE STREET
 RENDERED LANDSCAPE PLAN

EX-C

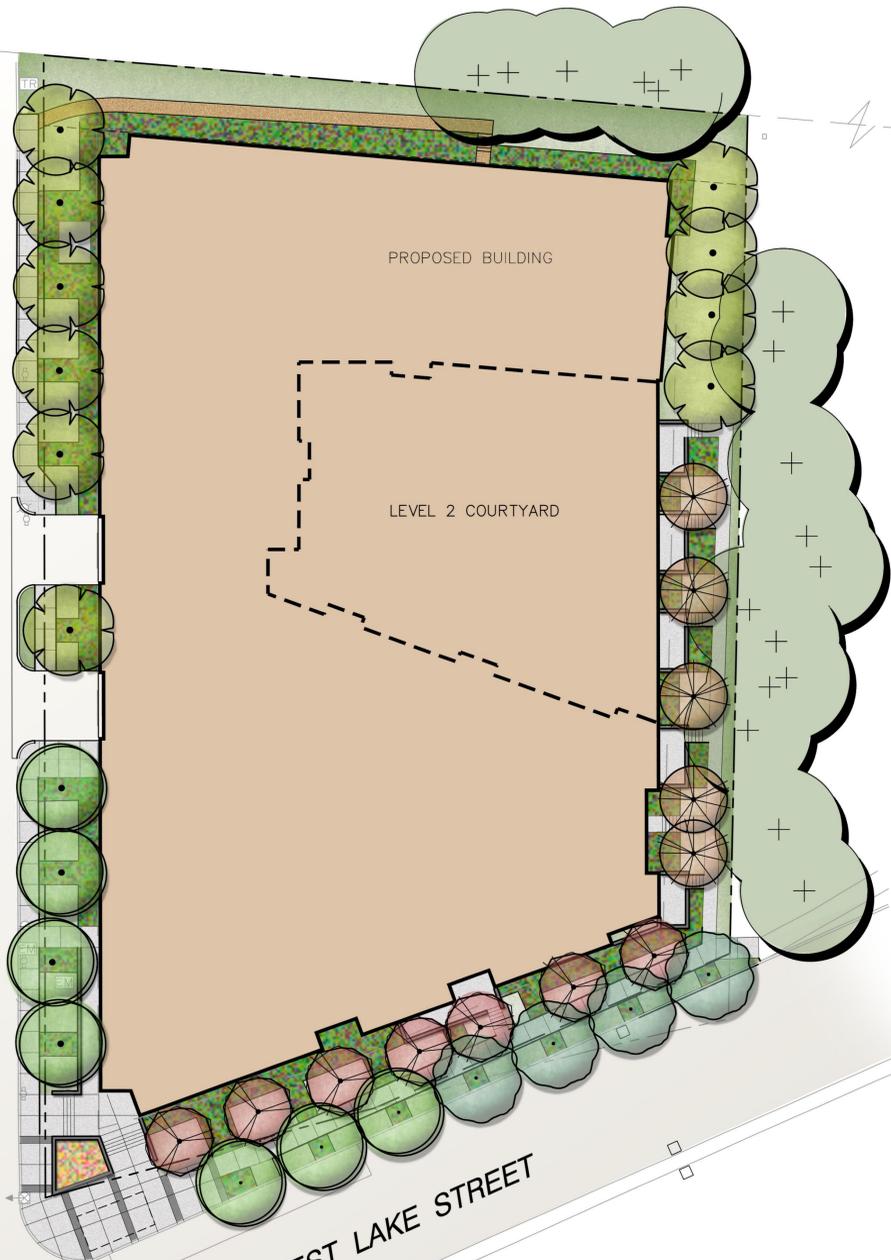
HCRRR
 MIDTOWN GREENWAY
 PROPERTY MAP NO. 32
 BITUMINOUS BIKE TRAIL

THOMAS AVENUE SOUTH

WEST LAKE STREET

LEGEND

TREES	BOTANICAL NAME	COMMON NAME
	ACER FREEMANII 'AUTUMN BLAZE'	AUTUMN BLAZE MAPLE
	BETULA NIGRA 'SHILOH SPLASH'	SHILOH SPLASH CLUMP
	GLEDITSIA TRIACANTHOS VAR. INERMIS 'SKYOLE'	SKYLINE HONEYLOCUST
	MALUS X 'PRAIRIFIRE'	PRAIRIFIRE CRAB APPLE
	ULMUS CARPINIFOLIA 'NEW HORIZON'	NEW HORIZON ELM
SHRUBS/PERENNIALS	BOTANICAL NAME	COMMON NAME
	CALAMAGROSTIS X ACUTIFLORA 'KARL FOERSTER'	FEATHER REED GRASS
	CORNUS ALBA 'SIBIRICA RED GNOME'	RED GNOME DOGWOOD
	HEMEROCALLIS X 'GRAPE MAGIC'	GRAPE MAGIC DAYLILY
	HEMEROCALLIS X 'RUBY STELLA'	DAYLILY RUBY STELLA
	HEMEROCALLIS X 'STELLA SUPREME'	DAYLILY STELLA SUPREME
	HYDRANGEA ARBORESCENS 'ANNABELLE'	ANNABELLE SMOOTH HYDRANGEA
	NEPETA X FAASSENII 'WALKERS LOW'	WALKERS LOW CATMINT
	RIBES ALPINUM	ALPINE CURRANT
	ROSA 'UHLATER'	PAVEMENT FOXI ROSE
	SCHIZACHYRIUM SCOPARIUM 'BLUE HEAVEN'	BLUE HEAVEN LITTLE BLUESTEM
	SPIRAEA JAPONICA 'NEON FLASH'	NEON FLASH SPIREA
	TAXUS X MEDIA 'TAUNTONI'	TAUNTON YEW
ANNUALS	BOTANICAL NAME	COMMON NAME
	GERANIUM SANGUINEUM 'MAX FREI'	GERANIUM MAX FREI
	GERANIUM X 'ORION'	GERANIUM ORION
	PENNISSETUM SETACEUM 'ATROPURPUREA'	RED FOUNTAIN GRASS
	PETUNIA X HYBRIDA 'STRAWBERRY DADDY'	STRAWBERRY DADDY PETUNIA
	SOLANUM JASMINOIDES 'GEORIGA LIGHT GREEN'	POTATO VINE
GRASS	BOTANICAL NAME	COMMON NAME
	TURF GRASS	TURF GRASS



LANDSCAPE REQUIREMENTS	
PERVIOUS AREA:	13,104 SF
TREES REQUIRED:	26
TREES PROVIDED:	26
SHRUBS REQUIRED:	131
SHRUBS PROVIDED:	194



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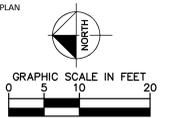
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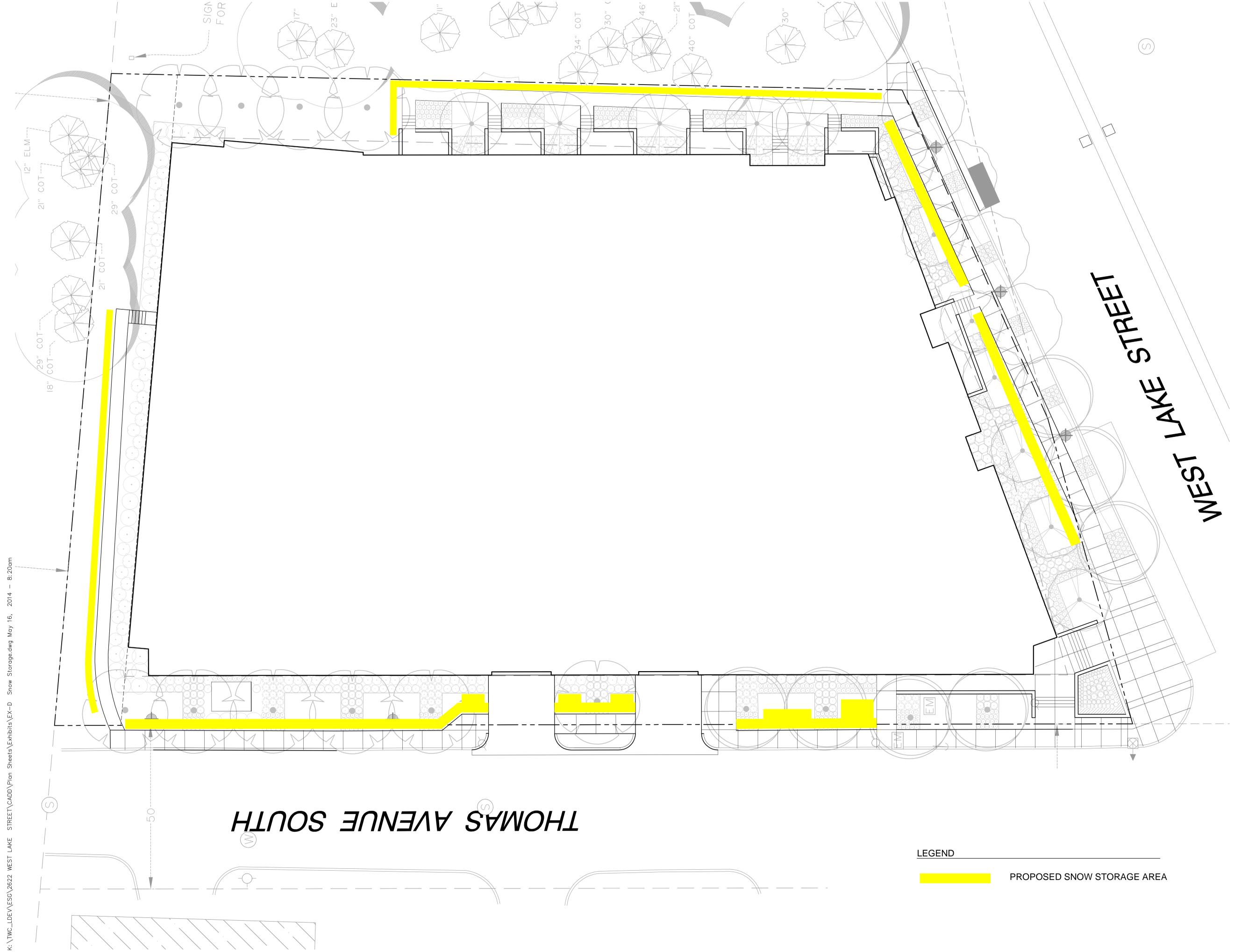
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KEY PLAN



2622 WEST LAKE STREET
SNOW STORAGE PLAN

EX-D



LEGEND

 PROPOSED SNOW STORAGE AREA

K:\TWC_LDEV\ESG\2622 WEST LAKE STREET\CADD\Plan Sheets\Exhibits\EX-D Snow Storage.dwg May 16, 2014 - 8:20am



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5/16/2014

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Author	Checker
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KEY PLAN

2622 W Lake St

EXISTING SURROUNDING CONTEXT

A0.0

EXISTING SURROUNDING CONTEXT



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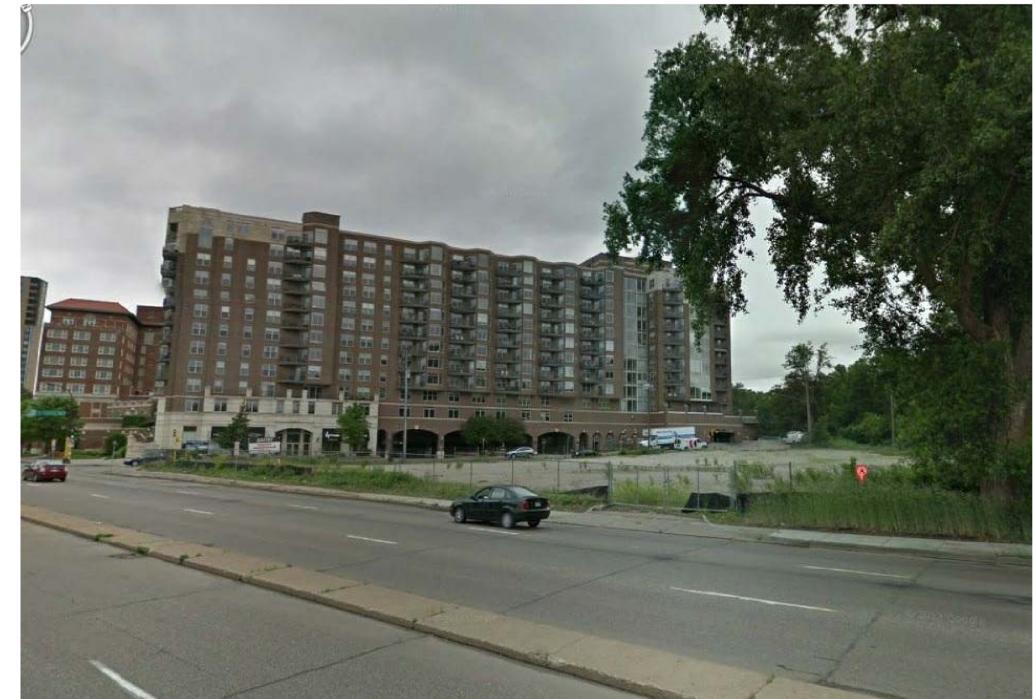
VIEW FROM NORTHWEST AT THOMAS AVE



VIEW FROM EAST INCLUDING EXISTING PARK-LAND TREES



VIEW FROM SOUTHWEST AT CORNER OF THOMAS AVE



VIEW FROM EAST AT SOUTHEAST CORNER OF SITE

LAND USE
APPLICATION
5/16/2014

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KEY PLAN

2622 W Lake St

EXISTING SITE CONTEXT
IMAGES

A0.1



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KEY PLAN

2622 W Lake St

VIEWS FROM SOUTH
A0.2

BUILDING AS LOCATED IN CONTEXT WHEN VIEWED FROM LAKE CALHOUN



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KEY PLAN

2622 W Lake St

VIEW FROM WEST

A0.3

BUILDING IN CONTEXT ALONG WEST LAKE STREET

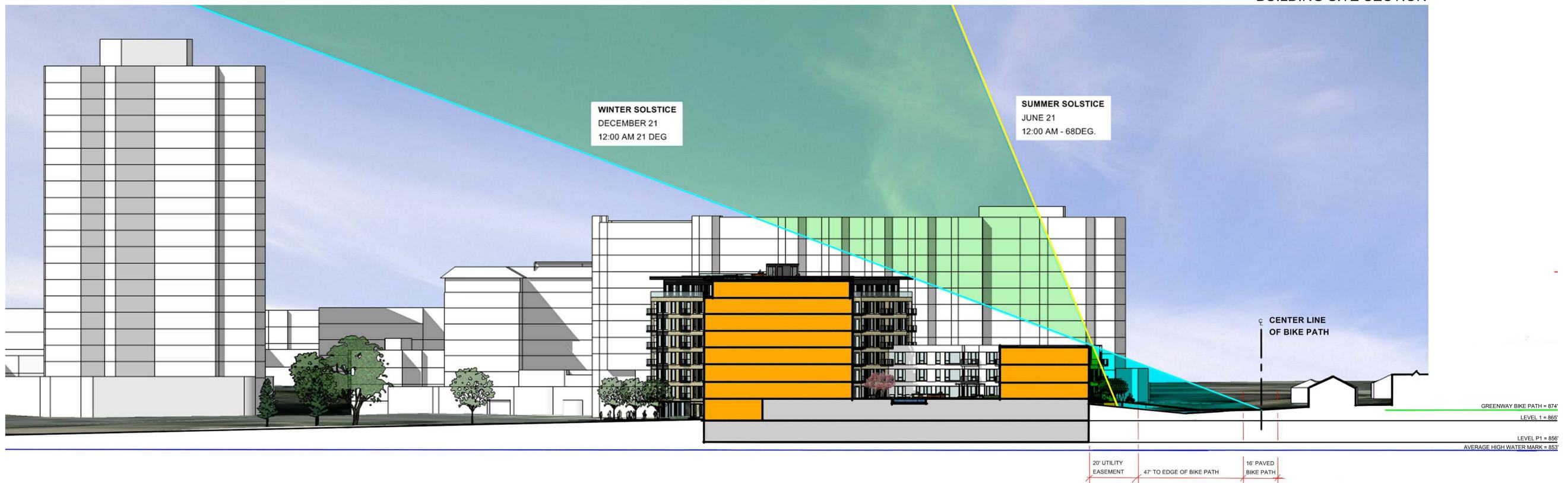


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KEY PLAN

2622 W Lake St

SITE CONTEXT SECTION

A0.4



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KEY PLAN

2622 W Lake St

BUILDING PERSPECTIVES

A0.5a

VISUAL MASSING STUDY FROM EAST ILLUSTRATING STEP DOWN



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KEY PLAN

2622 W Lake St

BUILDING PERSPECTIVES

A0.5b

VISUAL MASSING DIAGRAM FROM THE WEST ILLUSTRATING STEP DOWN



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KEY PLAN

2622 W Lake St

BUILDING PERSPECTIVES

A0.5c

SKETCH UP STUDY VIEW AT SOUTHWEST CORNER LOBBY ENTRANCE



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KEY PLAN

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BUILDING PERSPECTIVES
A0.5d

SKETCH UP STUDY VIEW FROM LAKE STREET AT SOUTHEAST CORNER



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BUILDING PERSPECTIVES

A0.5e

SKETCH UP STUDY VIEW OF WALK-OUT TOWNHOME FLATS ALONG EAST SIDE



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2622 W Lake St

BUILDING PERSPECTIVES

A0.5f

SKETCH UP STUDY VIEW FROM GREENWAY AT THOMAS AVENUE SOUTH



BIRDSEYE AERIAL FROM SOUTH EAST



BIRDSEYE AERIAL FROM NORTH EAST

2622 W Lake St

2622 W Lake Street
Minneapolis, MN 55416



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BUILDING PERSPECTIVES
A0.5g



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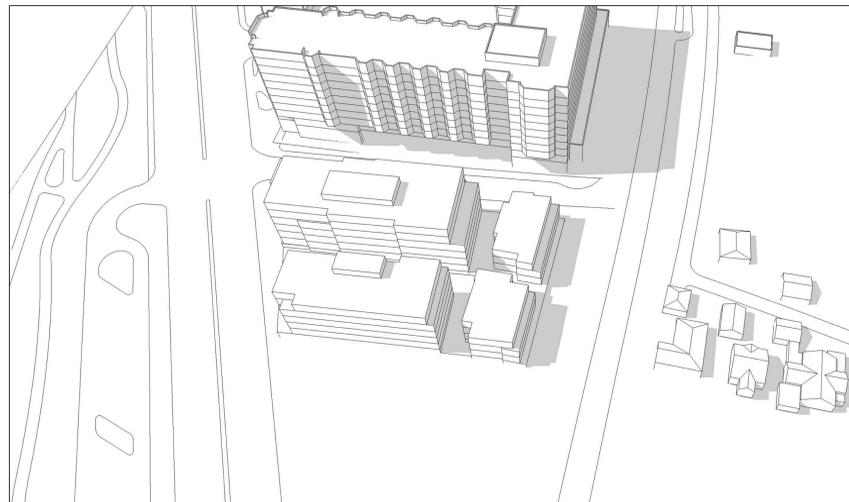
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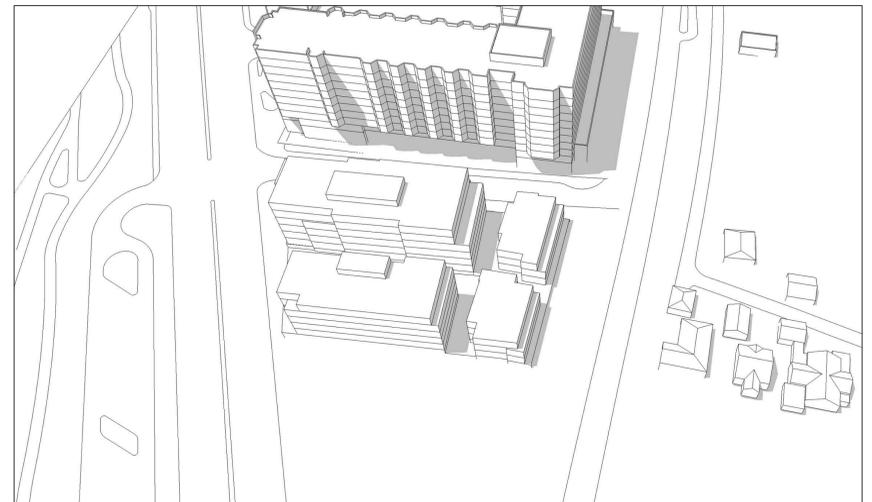
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2006 LANDER PLAN - DECEMBER 21 @ NOON



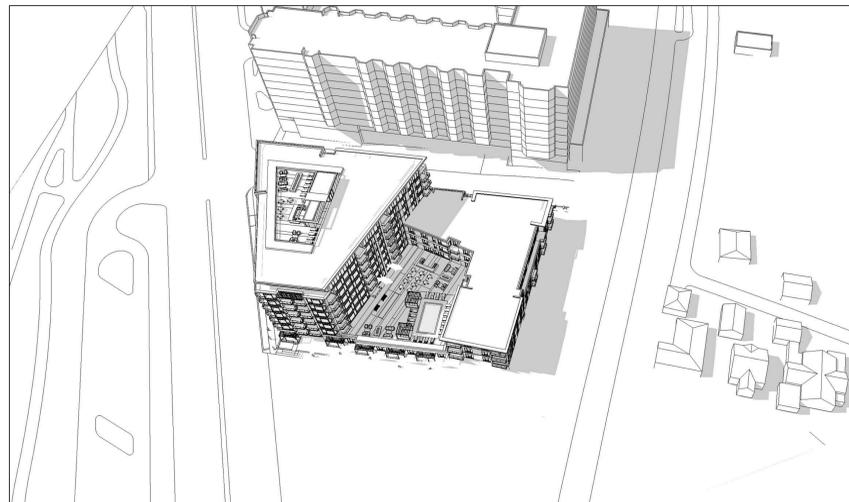
2006 LANDER PLAN - MAR/SEPT 21 @ NOON



2006 LANDER PLAN - JUNE 21 @ NOON



CURRENT PROPOSED PLAN - DECEMBER 21 @ NOON



CURRENT PROPOSED PLAN - MAR/SEPT 21 @ NOON



CURRENT PROPOSED PLAN - JUNE 21 @ NOON

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5/16/2014

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KEY PLAN

2622 W Lake St

SHADOW STUDIES
A0.6

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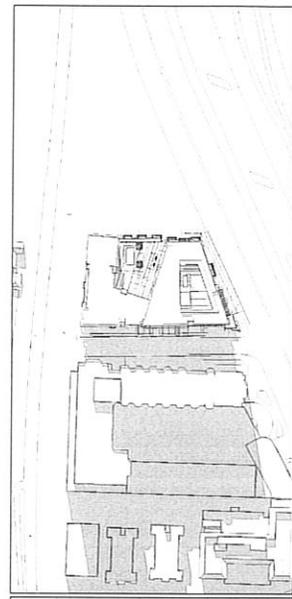
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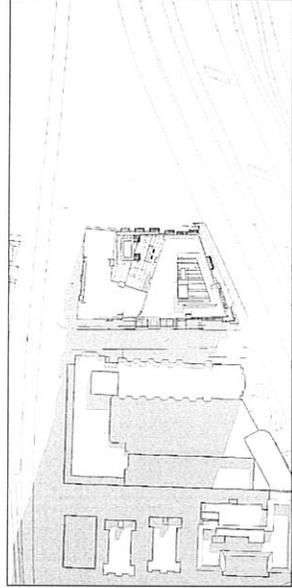
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 PROJECT: 2622 W LAKE ST
 SHEET: A0.7

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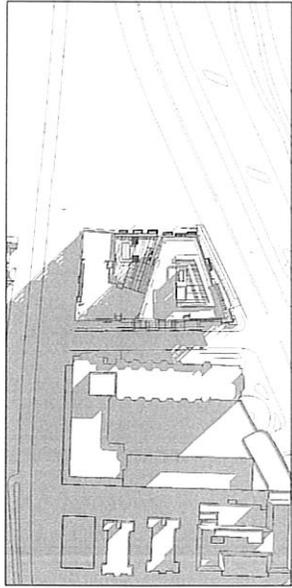
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ORIGINAL DATE: 5/16/2014	DATE:
REVISIONS:	No. Description Date
2.13.2014	PROJECT NUMBER: ESG
ESG	PROJECT: 2622 W LAKE ST
ESG	SHEET: A0.7
ESG	DATE: 5/16/2014
2622 W LAKE ST	
SHADOW STUDIES A0.7	



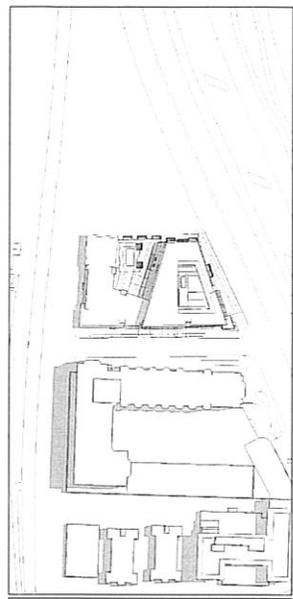
JUNE MORNING - 7 AM



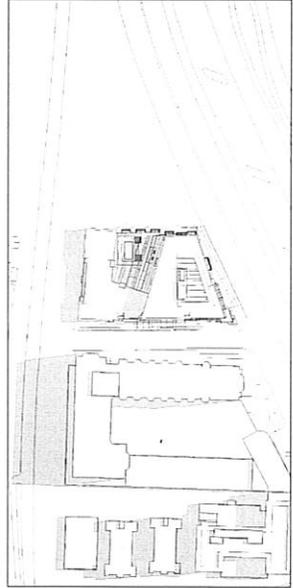
MARCH/SEPTEMBER MORNING - 8 AM



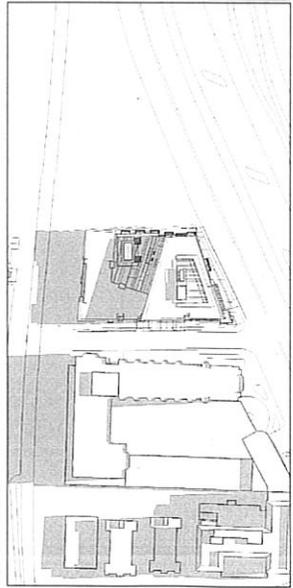
DECEMBER MORNING - 9 AM



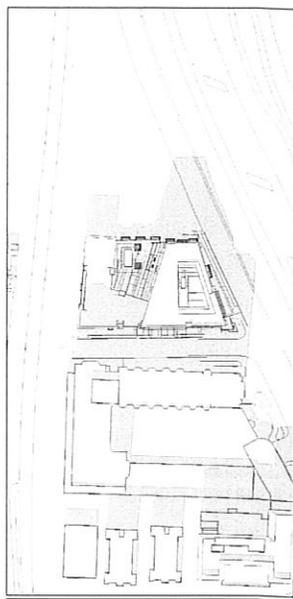
JUNE NOON



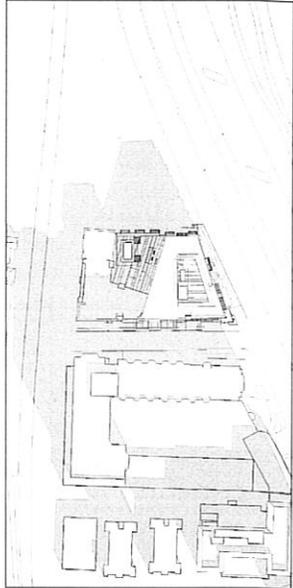
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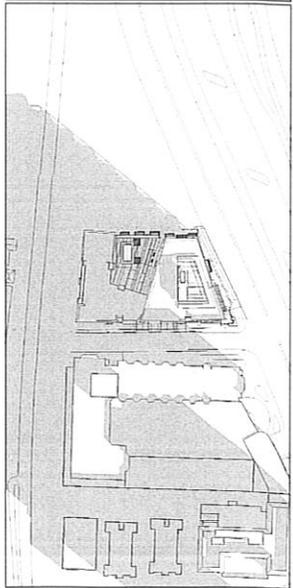
DECEMBER NOON



JUNE EVENING - 5 PM



MARCH/SEPTEMBER EVENING - 4 PM



DECEMBER EVENING - 3 PM



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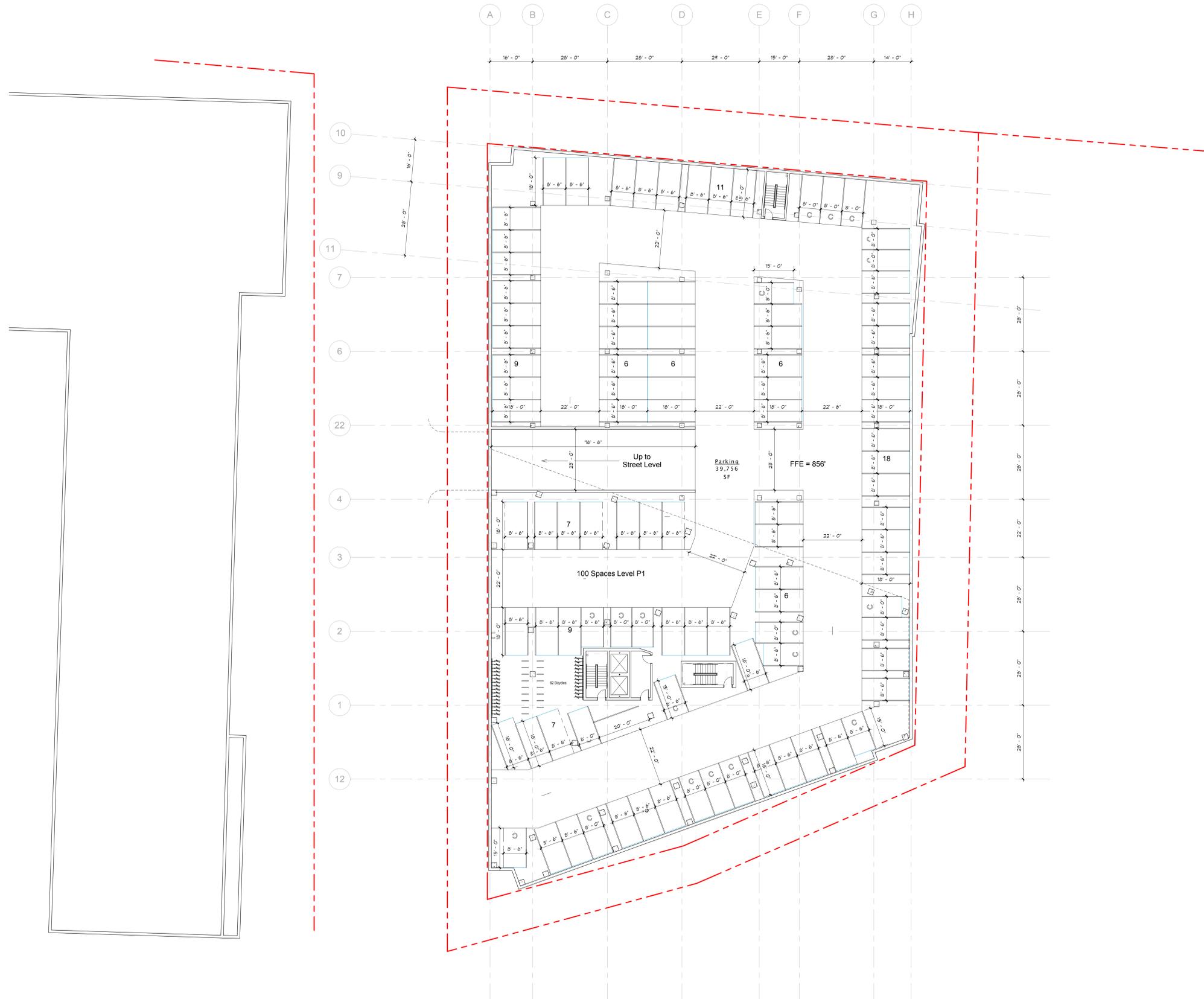
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KEY PLAN

2622 W Lake St

P1 PARKING PLAN
A1.0

1 PARKING LEVEL PLAN P1
A1.0 1/16" = 1'-0"



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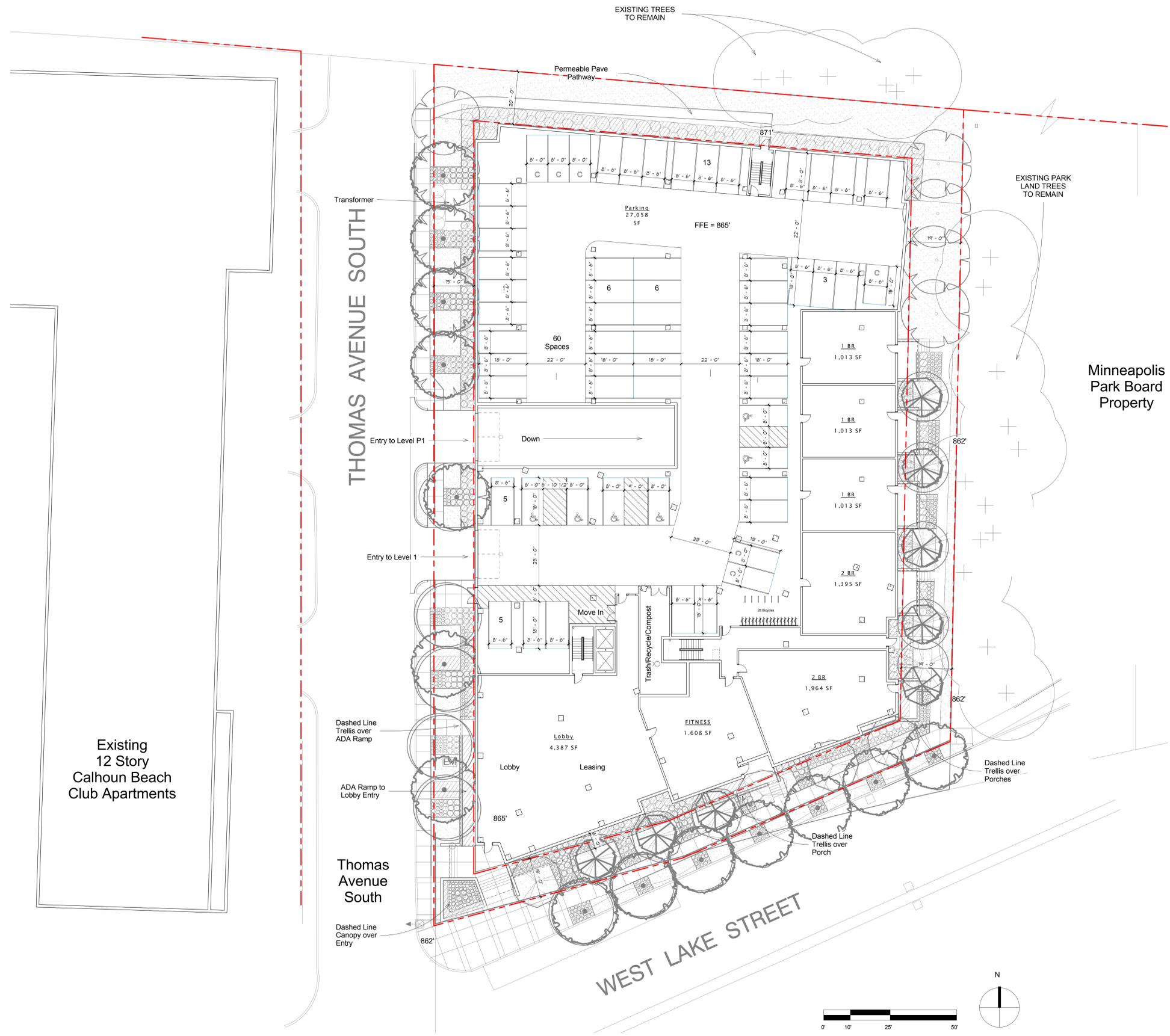
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Minneapolis Park Board Property

Existing 12 Story Calhoun Beach Club Apartments

LAND USE APPLICATION
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KEY PLAN

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LEVEL ONE PLAN
A1.1



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Typed or Printed Name _____

License # _____ Date _____

NOT FOR CONSTRUCTION

LAND USE APPLICATION
5/16/2014

ORIGINAL ISSUE: 5/16/2014

REVISIONS

No. Description Date

213538

PROJECT NUMBER

ESG

DRAWN BY

ESG

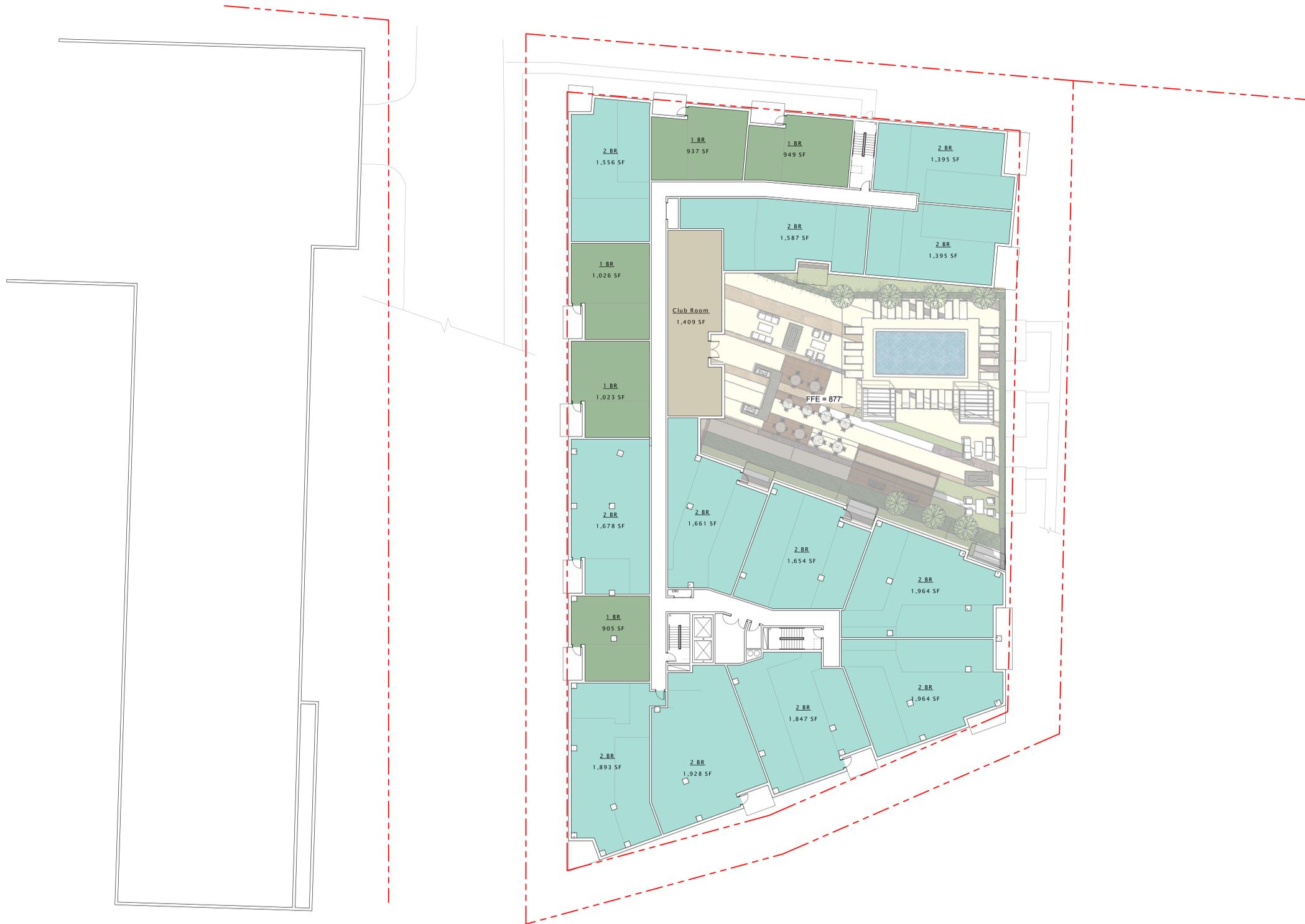
CHECKED BY

KEY PLAN

2622 W Lake St

LEVEL TWO PLAN

A1.2



1 LEVEL 2 PLAN
A1.2 1/16" = 1'-0"



elness swenson graham architects
500 WASHINGTON AVENUE SOUTH
MINNEAPOLIS, MINNESOTA 55415
P. 612.339.5508
F. 612.339.5382
WWW.ESGARCH.COM

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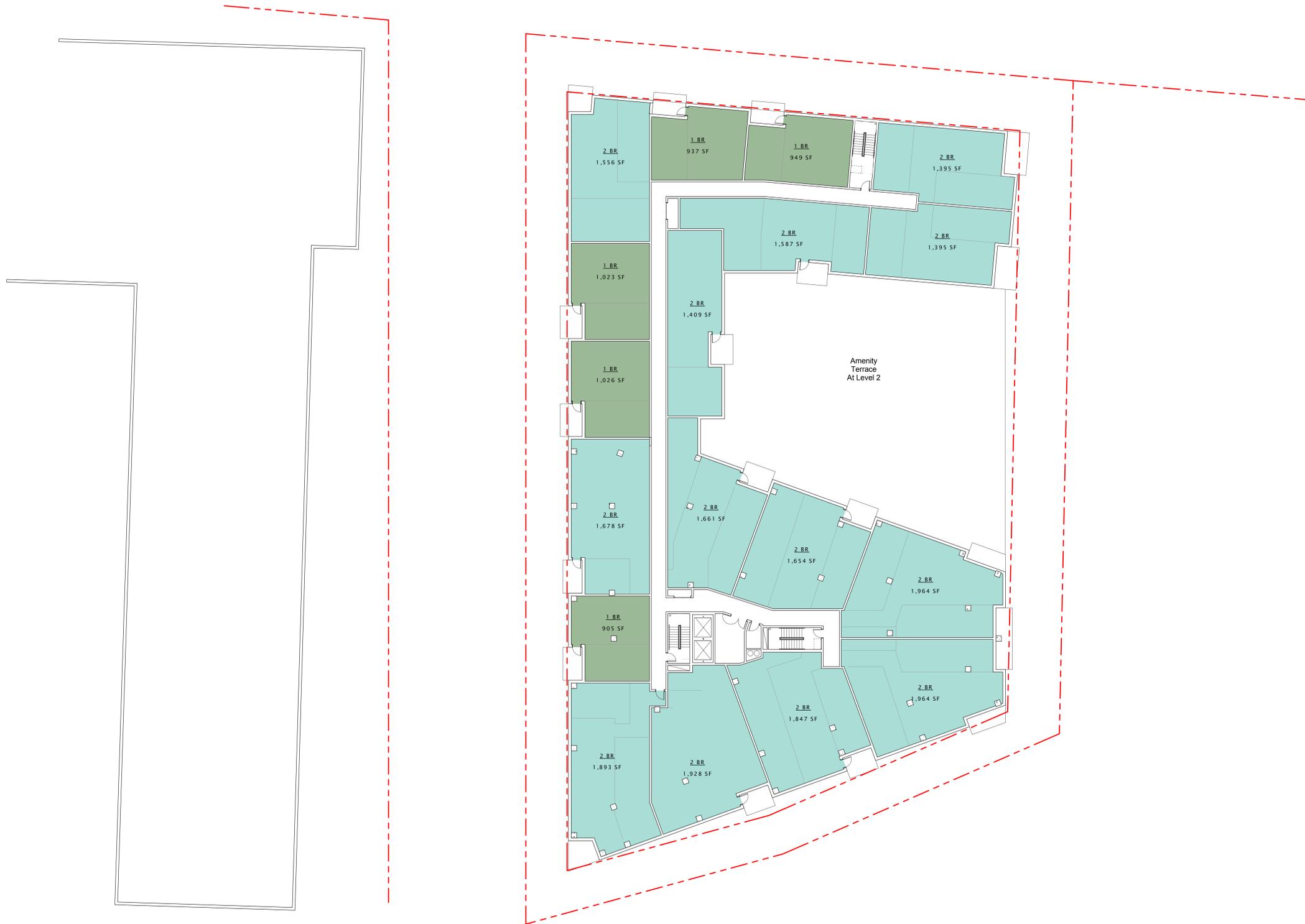
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KEY PLAN

2622 W Lake St

LEVEL THREE PLAN

A1.3



1 LEVEL 3 PLAN
A1.3 1/16" = 1'-0"



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PROJECT NUMBER

Author ESG
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KEY PLAN

2622 W Lake St

LEVEL FOUR PLAN
A1.4



1 LEVEL 4 PLAN
A1.4 1/16" = 1'-0"



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minneapolis minnesota 55415
p. 6 1 2 . 3 3 9 . 5 5 0 8
f. 6 1 2 . 3 3 9 . 5 3 8 2
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5/16/2014

ORIGINAL ISSUE: 05/05/14

REVISIONS

No. Description Date

213538
PROJECT NUMBER

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KEY PLAN

2622 W Lake St

LEVEL 5-7 PLAN
A1.5

1 LEVEL 5-7 PLAN
A1.5 1/16" = 1'-0"



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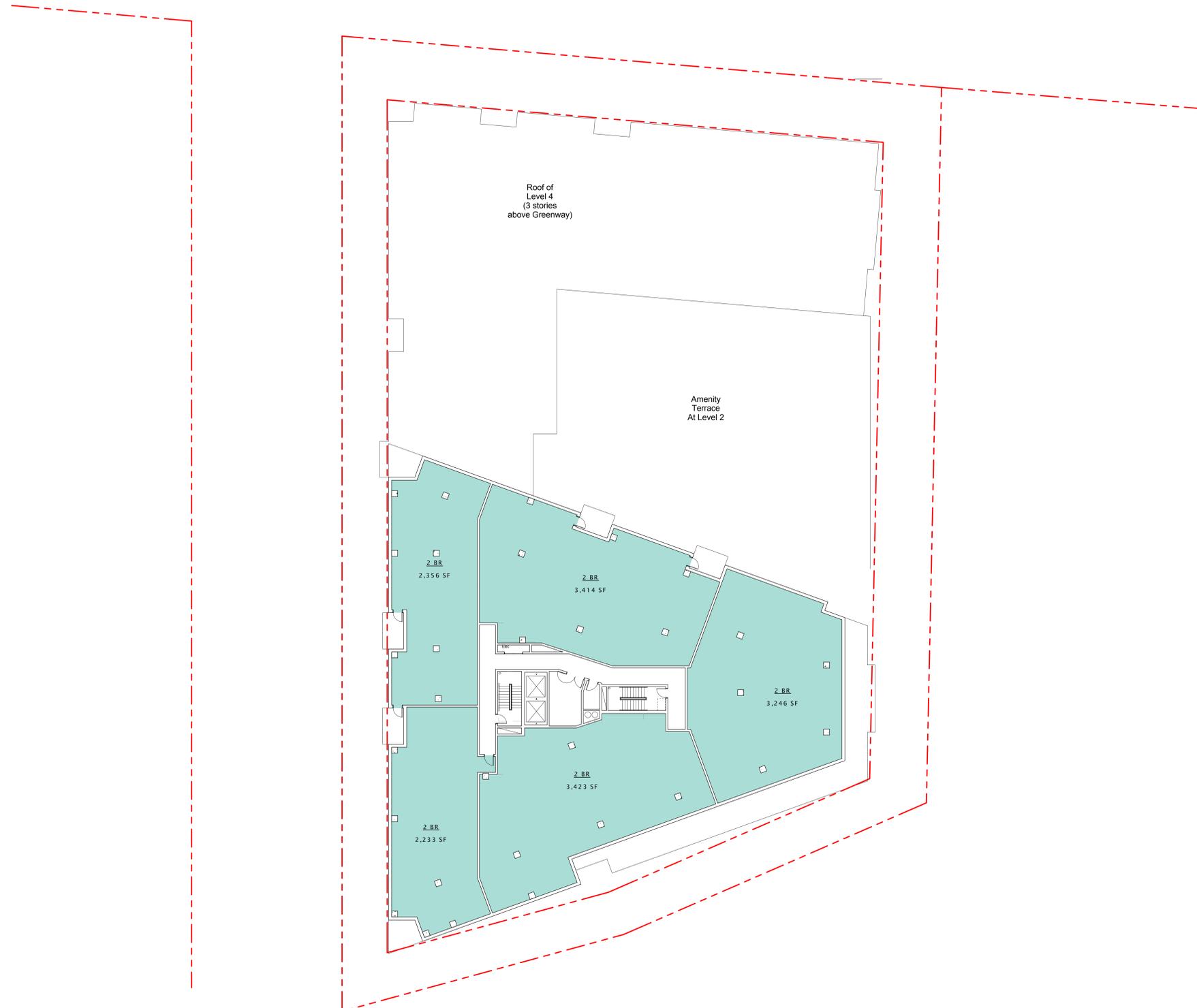
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LAND USE APPLICATION
5/16/2014

ORIGINAL ISSUE: 05/05/14

REVISIONS

No. Description Date

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KEY PLAN

2622 W Lake St

LEVEL 8 PLAN
A1.6



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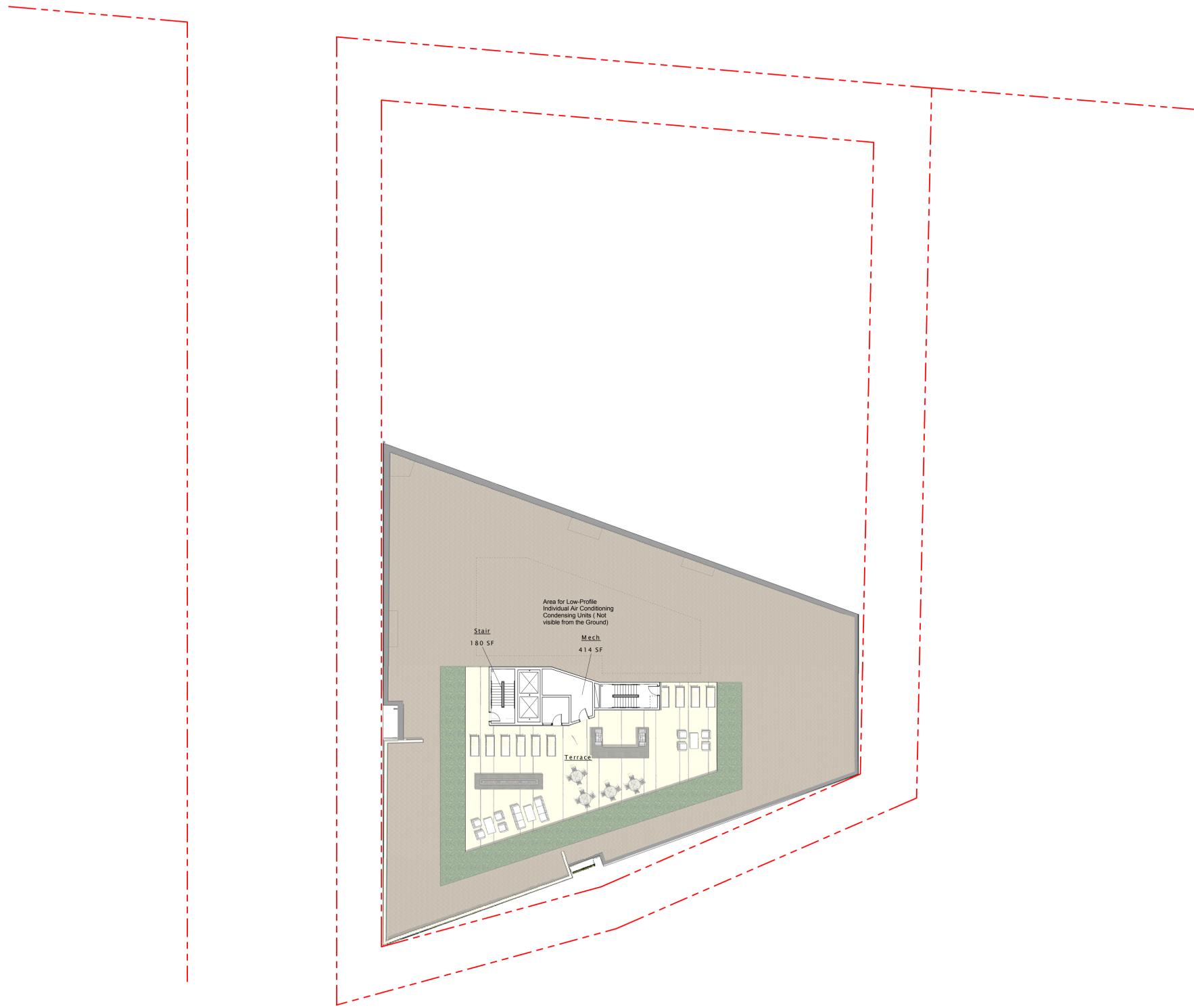
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LAND USE APPLICATION
5/16/2014

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No. Description Date

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PROJECT NUMBER

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KEY PLAN

2622 W Lake St

ROOF LEVEL PLAN
A1.7

1 ROOF LEVEL PLAN
A1.7 1/16" = 1'-0"



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Signature _____

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License # _____ Date _____

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1 SOUTH ELEVATION
A3.1 3/32" = 1'-0"



2 WEST ELEVATION
A3.1 3/32" = 1'-0"

EXTERIOR MATERIAL KEYNOTES

1A	BRICK VENEER COLOR #1
1B	BRICK VENEER COLOR #2
2	STONE VENEER
5A	PREFINISHED COMPOSITE METAL PANEL COLOR #1
5B	PREFINISHED COMPOSITE METAL PANEL COLOR #2
6A	STUCCO COLOR #1
6B	STUCCO COLOR #2
6C	IFE
8A	PREFINISHED ALUMINUM WINDOW/DOOR COLOR #1
8B	PREFINISHED ALUMINUM WINDOW/DOOR COLOR #2
8C	FIBERGLASS WINDOW/DOOR COLOR #1
8D	FIBERGLASS WINDOW/DOOR COLOR #2
9	PREFINISHED ALUMINUM GARAGE DOOR - COLOR TO MATCH BRICK
10	PREFINISHED ALUMINUM RAILING
11	CUSTOM MAIN ENTRY DOOR
12	PREFINISHED METAL FASCIA
13	SIGNAGE

LAND USE APPLICATION
5/16/2014

ORIGINAL ISSUE: 5/16/2014

REVISIONS

No.	Description	Date

213538
PROJECT NUMBER

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KEY PLAN

2622 W Lake St

EXTERIOR ELEVATIONS
A3.1



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f. 6 1 2 . 3 3 9 . 5 3 8 2
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License # _____ Date _____

EXTERIOR MATERIAL KEYNOTES

- 1A BRICK VENEER COLOR #1
- 1B BRICK VENEER COLOR #2
- 2 STONE VENEER
- 5A PREFINISHED COMPOSITE METAL PANEL COLOR #1
- 5B PREFINISHED COMPOSITE METAL PANEL COLOR #2
- 6A STUCCO COLOR #1
- 6B STUCCO COLOR #2
- 8C IFE
- 8A PREFINISHED ALUMINUM WINDOW/DOOR COLOR #1
- 8B PREFINISHED ALUMINUM WINDOW/DOOR COLOR #2
- 8C FIBERGLASS WINDOW/DOOR COLOR #1
- 8D FIBERGLASS WINDOW/DOOR COLOR #2
- 9 PREFINISHED ALUMINUM GARAGE DOOR - COLOR TO MATCH BRICK
- 10 PREFINISHED ALUMINUM RAILING
- 11 CUSTOM MAIN ENTRY DOOR
- 12 PREFINISHED METAL FASCIA
- 13 SIGNAGE



1 NORTH ELEVATION
A3.2 3/32" = 1'-0"

NOT FOR CONSTRUCTION



2 EAST ELEVATION
A3.2 3/32" = 1'-0"

LAND USE APPLICATION
5/16/2014

ORIGINAL ISSUE: 5/16/2014

REVISIONS

No.	Description	Date

213538 PROJECT NUMBER

ESG DRAWN BY ESG CHECKED BY

KEY PLAN

2622 W Lake St

EXTERIOR ELEVATIONS
A3.2



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Signature _____

Typed or Printed Name _____

License # _____ Date _____

EXTERIOR MATERIAL KEYNOTES

- 1A BRICK VENEER COLOR #1
- 1B BRICK VENEER COLOR #2
- 2 STONE VENEER
- 5A PREFINISHED COMPOSITE METAL PANEL COLOR #1
- 5B PREFINISHED COMPOSITE METAL PANEL COLOR #2
- 6A STUCCO COLOR #1
- 6B STUCCO COLOR #2
- 8C IFE
- 8A PREFINISHED ALUMINUM WINDOW/DOOR COLOR #1
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- 8C FIBERGLASS WINDOW/DOOR COLOR #1
- 8D FIBERGLASS WINDOW/DOOR COLOR #2
- 9 PREFINISHED ALUMINUM GARAGE DOOR - COLOR TO MATCH BRICK
- 10 PREFINISHED ALUMINUM RAILING
- 11 CUSTOM MAIN ENTRY DOOR
- 12 PREFINISHED METAL FASCIA
- 13 SIGNAGE



1 COURTYARD NORTH ELEVATION
A3.3 3/32" = 1'-0"

NOT FOR CONSTRUCTION



2 COURTYARD EAST ELEVATION
A3.3 3/32" = 1'-0"



3 COURTYARD SOUTH ELEVATION
A3.3 3/32" = 1'-0"

LAND USE APPLICATION
5/16/2014

ORIGINAL ISSUE: 5/16/2014

REVISIONS

No.	Description	Date

213538

PROJECT NUMBER

ESG

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ESG

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KEY PLAN

2622 W Lake St

EXTERIOR ELEVATIONS

A3.3

LIGHTING NOTES:

- FOR WORK WITHIN THE RIGHT-OF-WAY OF WEST LAKE STREET, A HENNEPIN COUNTY RIGHT-OF-WAY PERMIT WILL BE REQUIRED.



einess swenson graham architects
 500 washington avenue south
 minneapolis minnesota 55415
 p. 612.339.5508
 f. 612.339.5382
 www.esgarch.com

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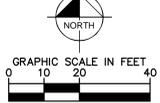
LAND USE APPLICATION
 5/16/2014

ORIGINAL ISSUE: 5/16/2014

REVISIONS		
No.	Description	Date
1	CITY COMMENTS	5/22/14

160739000
 PROJECT NUMBER

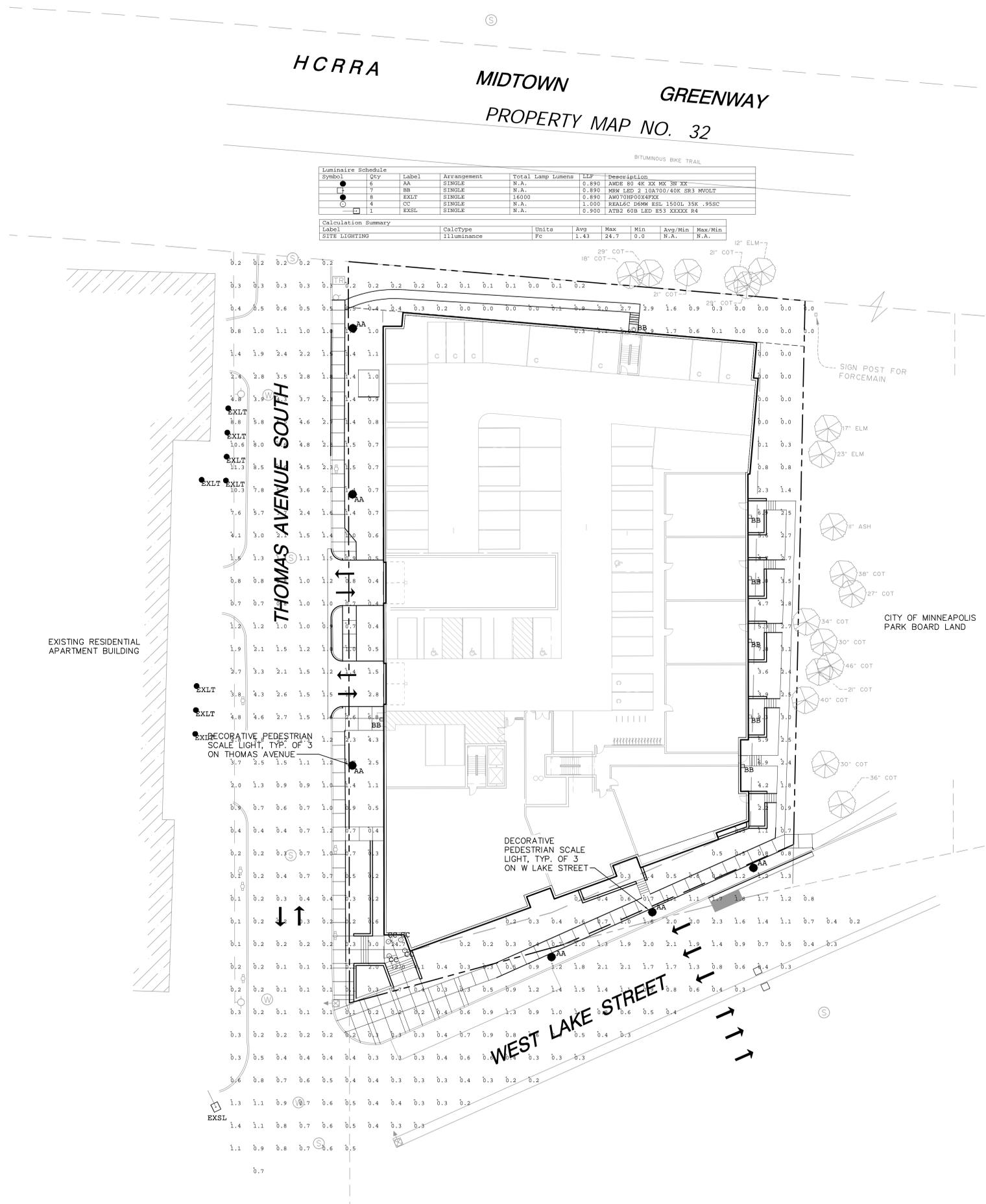
KEY PLAN



2622 WEST LAKE STREET
 LIGHTING PLAN

E1.0

K:\TWC_LDEV\ESG\2622 WEST LAKE STREET\FROM OTHERS\PARSONS\2014-05-22 Lighting Plan.dwg May 22, 2014 - 1:39pm



Symbol	Qty	Label	Arrangement	Total Lamp Lumens	LF	Description
●	6	AA	SINGLE	N.A.	0.890	AWD8 80 4K XX MX 3N XX
○	7	BB	SINGLE	N.A.	0.890	MR8 LED 2 10A700/40K SR3 MVO/IT
●	8	EXLT	SINGLE	16000	0.890	AWD7000D04KXX
○	4	CC	SINGLE	N.A.	1.000	REAL6C D6MW ESL 1500L 35K .958C
□	1	EXSL	SINGLE	N.A.	0.900	ATB2 60B LED E53 XXXXX R4

Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
SITE LIGHTING	illuminance	Fc	1.43	24.7	0.0	N.A.	N.A.

7. Please provide a lighting plan showing foot-candle levels and fixture details.

A Lighting and Photometric Plan has been prepared and is attached. This plan proposes street lighting on Thomas Avenue and West Lake Street consisting of three “lantern” style fixtures on each street. The fixture selected is the Holophane Washington “lantern” fixture with 63 LEDs, full cut-off. This fixture is specified in the Standard Supplemental Specifications for the Construction of Public Infrastructure in the City of Minneapolis and was recently used on Cedar Avenue. Additional details will be provided on the construction drawings, including the City of Minneapolis standard plates for foundation, pole, and electrical. The performance criteria for “Pedestrian Areas” have been taken into account for this layout per the City of Minneapolis Street Lighting Policy, and the existing street light at the intersection of Thomas Avenue and West Lake Street has been factored into the calculations. Exterior building lighting has been assumed at each porch and four recessed can lights have been assumed for the entryway.



“Lantern” Style Fixture

May 16, 2014

Council Member Lisa Goodman
350 S. 5th St., Room 307
Minneapolis, MN 55415

Re: 2622 W Lake St – Land Use Application

Dear Council Member Goodman:

On behalf of Greystar, we are writing to let you know we will be submitting a Land Use Application and Preliminary Development Review for 2622 West Lake Street. We thank you for your input and design suggestions to soften and step the West Lake Street massing and façade which we have incorporated into the design.

Project Purpose and Vision

The purpose and vision for 2622 West Lake Street is the creation of a landmark quality residential community that is designed around key design principles developed through collaboration with the CIDNA Land Use and Development Committee. The Design is also in response to input from Council Member Goodman's office resulting in a softening and stepping of the upper floor from West to East. The redevelopment will replace an empty parcel which has been fenced off and vacant since a former office structure was demolished in 2006.

- Provide quality residential consistent with the City classification of West Lake as a High-Density Corridor.
- Comply with CIDNA parameters, items in the CIDNA Resolution and the Minneapolis Plan for Sustainable Growth.
- Preserve access to light and air and minimize shadowing on adjacent parks and the Midtown Greenway.
- Fit scale and character of the surrounding context including the 12 story Calhoun Beach Club Apartments, old Calhoun Beach Club, the Midtown Greenway and the single-family residences.
- Minimally impact the view shed from the shorelines, vehicular ways, walkways, and bikeways of Lake Calhoun, Lake of the Isles and Cedar Lake.
- Create a step down transitional "book end" from the massive 12 story Calhoun Beach Club Apartments.

- Provide a lower height geometric matched balance to the old Calhoun Beach Club.
- Provide a wide landscaped boulevard setback along West Lake shaped by the new building façade, new street trees and public realm streetscape amenities.
- Overall height to match urban mature tree line.
- Collaborate with CIDNA, the Park Board, the Greenway Coalition and the HCRRRA to create new and safer pedestrian and bike pathways, both near term and long range.
- Create landmark quality architecture through the use of high quality materials and Type 1 construction.

Land Use Applications

The land is zoned OR-2. We anticipate a few land use applications will be required:

- Conditional use permit: To increase the height of the building located in the SH (Shoreland) Overlay District from 2.5 stories/35 feet to 89 feet high (8 stories) from West Lake Street (plus an approximately 2-foot high parapet and two appurtenant elevator and stairway penthouses).
- Variance of the 15-foot front yard setback along West Lake Street for balconies, patios, exterior stairs, an entry trellis and similar features.
- Variance of the 15-foot corner side yard setback along Thomas Avenue South for balconies, patios, entry trellis and similar features.
- Variance of the 19-foot east interior side yard setback for balconies, patios, entry trellis and similar features.
- Variance of the 19-foot north rear yard setback for balconies, patios, entry trellis and similar features.
- Site Plan Review.

Architectural Description

The architectural design and massing of 2622 West Lake Street is the result of a close collaboration with CIDNA based on key urban design and context parameters and the Minneapolis Plan. The design is also responsive to input from Council Member Goodman's office resulting in a softening and stepping of the massing from west to east. The shape and geometry of the design is an organic response to the myriad of environmental and contextual forces. The design will feature the highest quality materials including stone, large window walls, natural metal and brick; all class A materials. The structure will be concrete and steel to achieve

the highest quality, “100 year” performance. The design aesthetic is contemporary and timeless with the use of warm, natural materials and glass.

Public Realm and Streetscape: Key Overall Features: Initial and Long Term

The overall design of 2622 is driven by the goal of enhancing the public realm and streetscapes surrounding the proposed residential building.

- The massing of the design directly corresponds to allowing solar penetration to the public Midtown Greenway throughout the year including the December 21 winter solstice. The massing also responds to the request for the building to step back and down from west to east. The design proposes a high quality three story architectural definition to the Greenway public realm. The grading improvements between the Midtown Greenway Bike Trail and the new building facade will allow a future potential bike and pedestrian pathway.
- As requested by CIDNAs long range planning goals, the design will allow for the creation of a future bike and pedestrian path to be located between the new building and the existing Greenway bike track. This new potential bike and pedestrian path would connect across the Midtown Greenway and allow for a connection to south of Lake of the Isles and the large dog park.
- The proposed design will convert Thomas Ave South into a pedestrian and bike friendly streetscape. This improved streetscape public realm will connect to the controlled intersection at Thomas Ave South and West Lake Street. It is desired to get the County to allow a longer pedestrian crossing signal at this key controlled intersection to provide a more pleasant and safe pedestrian and bike crossing experience and also to provide for overall traffic calming at the currently high speed West Lake corridor.
- The proposed design will provide major streetscape improvements within the wide setback along West Lake including a green boulevard between the curb and the new sidewalk featuring permeable ground cover and hearty street trees, a widened pavement area for pedestrians and a permeable landscape zone featuring trees and lower plants between the pedestrian walkway and the building façade.
- The proposed design will feature park side town home flats that will shape the west edge of the public Park Board triangular green space to the east. These town home flats will be set back a full 19 feet from the property line and feature individual door fronts and a rich landscaped layer.

Green and Sustainable Features

A key sustainability strategy for 2622 West Lake Street is the location along the Midtown Greenway which will allow residences to use multiple modes of transportation including bike commuting along the Greenway. With the future potential addition of LRT and long range plans for a Streetcar, the project has the potential of becoming a truly pedestrian and transit oriented community that allows residents to live, work and play without dependence on daily automobile usage. The development team is committed to the sustainable design principles reflected in the City's comprehensive plan. Our sustainable design mission is to promote livable communities through the use of energy efficient systems, green building practices, reduced dependency on automobiles, creative density, high quality pedestrian and bicycle public realm and preservation of natural resources. The development will feature a series of green elements including green construction practices and materials specification, thermal high-efficiency window and exterior envelope systems, the pursuit of LEED equivalent performance and participation in the Xcel Energy Design Assistance Program.

Sincerely,

ELNESS SWENSON GRAHAM ARCHITECTS, INC.

A handwritten signature in black ink, appearing to read "David L. Graham". The signature is fluid and cursive, with a long horizontal stroke at the end.

David L. Graham, FAIA, LEED AP
Principal

cc: David Reid

May 16, 2014

Robert Corrick
Chair
Cedar Isles Dean Neighborhood Association
Land Use and Development Committee
PO Box 16270
Elmwood Branch
Minneapolis, MN 55416

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ELNESS SWENSON GRAHAM ARCHITECTS, INC.

A handwritten signature in black ink, appearing to read "David L. Graham". The signature is fluid and cursive, with a long horizontal stroke at the end.

David L. Graham, FAIA, LEED AP
Principal

cc: David Reid

CEDAR-ISLES-DEAN NEIGHBORHOOD ASSOCIATION (CIDNA)
**RESOLUTION NOT TO OPPOSE THE GREYSTAR APARTMENTS
PROPOSED AT 2622 WEST LAKE STREET**

APRIL 9, 2014

1. WHEREAS Greystar GP II, LLC (the “Developer”) has proposed an apartment project at 2622 West Lake Street (the “Project”) as described in the attached “GREYSTAR, A Residential Proposal for 2622 West Lake Street ” dated April 7, 2014 by ESG Architects (the “Exhibit”). The Exhibit is included as part of the Project description in this resolution. The Project will include the following characteristics:
 - a. Floor Area: The Project will have approximately 239,539 gross square feet. Excluding one level of parking below ground (approximately 40,313 square feet) and parking on the first level above ground (approximately 30,281 square feet), the Project will have approximately 168,945 square feet of residential floor area. The Floor Area Ratio (“FAR”) would be 3.0, the maximum allowed by the zoning code, including a 20% credit for the underground parking garage. (The FAR is based on total site area of 57,190 square feet less 875 square feet for the West Lake Street right of way.)
 - b. Units: Approximately 90 units
 - c. Height: The building will be 89 feet high (8 stories) from West Lake Street, which will exclude an approximately 2-foot high parapet. Two appurtenant elevator and stairway penthouses will extend approximately 12 to 15 feet above the parapet near the center of the roof, with openings on the roof level. The building will step down to three stories on the Midtown Greenway and 3-4 stories on Thomas Avenue South.
 - d. Greenway Shadowing: The Project will shade about one half of the width of the Midtown Greenway bicycle trail at noon on winter solstice (see shadow studies in the Exhibit).
 - e. Parking: Approximately 155 stalls including guest parking stalls resulting in a parking ratio of 1.72.
 - f. Courtyard: There will be an interior courtyard with swimming pool facing the park to the east.
 - g. Rooftop Patio: An approximately 2,200 square foot rooftop patio with furniture to accommodate up to 20 people will be located on the southern portion of the roof along West Lake Street, setback from the parapets as shown on the Exhibit.

(Building areas and dimensions, and unit and parking counts are approximate, but will not change significantly. The Project is subject to refinement in design development and construction document phases. Capacity of courtyard and rooftop patio will be determined by the building code. Developer will report final areas, unit count and parking count on completion of construction documents.)

2. WHEREAS the Developer is seeking a conditional use permit to exceed the 56-foot height limit of the OR2 zoning classification and the 35-foot height limit of the Shoreland Overlay District. The Developer is not requesting zoning variances except

for small front-and side-yard setback variances for balconies, patios, entry trellis and similar features.

3. WHEREAS the Project strikes a reasonable balance between respect for the shoreland, parks, trails, and single-family residences while considering the context of the 12-story Calhoun Beach Club Apartments and the City's classification of West Lake Street as a high-density thoroughfare. With respect to the Shoreland Overlay District, the Project design offers the following features in support of a conditional use permit:
 - a. The Project will preserve access to light and air and minimize shadowing on adjacent parks, the Midtown Greenway, and single-family residences.
 - b. The Project will fit the scale and character of the surrounding neighborhood including the 12-story Calhoun Beach Club Apartments, old Calhoun Beach Club building, the Midtown Greenway, and single-family residences.
 - c. The view-shed from the shorelines, vehicular ways, walkways, and bikeways of Lake Calhoun, Lake of the Isles, and Cedar Lake will be minimally impacted by the Project.
 - d. On the north Calhoun skyline, the Project will present a graceful stepped-down interface with the 12-story Calhoun Beach Club Apartments.
 - e. The Project will present a geometric match to the original Calhoun Beach Club on the opposite side of CBC Apartments.
 - f. The massive eastern wall of CBC Apartments will be addressed with an artful triangular bookend building converging to east.
 - g. The 30-foot setback from Lake Street will match the setback of the CBC Apartments, creating a clear and aesthetic definition of the Lake Street corridor. This type of setback is supported by modern urban planning principles and city planning practices.
 - h. Project height approximately will match the tree line.
4. WHEREAS the lower parking level will be above the water table, and no permanent dewatering or pumping of ground water will be required; the stormwater management system will direct surface water to below-grade stormwater rate and filtration chambers within the building setback before being released at a controlled rate into the municipal storm sewer system, in accordance with plans approved by the City Engineer.
5. WHEREAS the emergency generator exhaust will be muffled to city zoning standards and located on Thomas Avenue South near the parking entrance.
6. WHEREAS there will be no central compressor for air conditioning. Air conditioning and heating will be accomplished with individual room units with rooftop condensers. Noise from these units is not expected to be noticeable.
7. WHEREAS there is no tall landscaping proposed on the Greenway side of the Project. The Developer will use its best efforts to add attractive and tall landscaping in this area and consider design improvements that will break up the wall of the building in this area.

8. WHEREAS Thomas Avenue South will be beautified with trees and other improvements including landscaping at the north of the dead end street. CIDNA will collaborate between the Developer, Calhoun Beach Club Apartments and the City.
9. WHEREAS delivery trucks will not stop on West Lake Street. Trucks will enter Thomas Avenue South from West Lake Street and return using a three-point turn into the building.
10. WHEREAS the CIDNA Committee will be informed periodically about major Project developments including updates at the end of the schematic design, design development, and construction document phases of the project. The Developer will present a regularly updated construction schedule to the Committee, and the first schedule would be presented early in the planning process.
11. WHEREAS the Developer will use its best efforts to minimize the impact of construction on West Lake Street traffic. A traffic plan will be shared with the CIDNA Committee, which will include a plan for delivery of materials to and from the site outside of rush hour. Equipment and delivery trucks will not stop on West Lake Street unless absolutely necessary. Developer will stage construction to avoid or minimize use of a traffic lane on West Lake Street for construction.
12. WHEREAS significant pile driving is expected for the Project. The Developer will use its best efforts to minimize noise and vibrations caused by pile driving including use of monitoring equipment. The Developer will inform the Committee of the pile-driving schedule very early in the construction planning process.
13. WHEREAS CIDNA is concerned for safe accessibility in arriving and boarding at the Metro Transit Bus Stop at West Lake Street and Thomas Avenue South located at the southwest corner of the Project, and how this condition becomes significant for pedestrian and bus rider safety during Winter months
14. WHEREAS the Project will be built in accordance with all applicable laws.
15. WHEREAS the Project will be Type 1 construction throughout including the low3-story portions on the Greenway and Thomas Avenue South.
16. WHEREAS the Developer will use its best efforts to save the cottonwood trees on park land directly east of the Project.
17. WHEREAS CIDNA is concerned about safety and walkability of pedestrian crossings on West Lake Street and Excelsior Boulevard.

18. WHEREAS CIDNA is concerned about the very high level of traffic in the neighborhood, which has some of the highest traffic counts in the city and the state, resulting in major traffic congestion at rush hour.
19. THEREFORE BE IT RESOLVED THAT CIDNA supports a community effort to explore ways to relieve traffic congestion, traffic flow, pedestrian safety, and livability in the neighborhood. Part of this effort could consider “boulevardization” of West Lake Street between Dean Parkway and East Calhoun Parkway including narrowing of traffic lanes, a new center median with trees, an allée of trees on the north and south sides of the street, and ornamental lighting. This effort would require cooperation between the County, City and Park Board.
20. THEREFOR BE IT FURTHER RESOLVED THAT CIDNA supports a community effort to build aesthetic and inviting transit stops at Thomas Avenue South and Dean Parkway on West Lake Street.
21. BE IT FURTHER RESOLVED THAT CIDNA supports a community effort to consider a pedestrian and bike trail from Thomas Avenue South, across the north perimeter of the site and park land to the east, north under the Midtown Greenway to Lake of the Isles between the Dog Park and the Soccer Field. The trail would connect with the Midtown Greenway on the north side. As part of this effort, CIDNA may seek designation of Thomas Avenue South as a bike boulevard. This effort would require collaboration between the Developer, the Calhoun Beach Club Apartments, the Park Board, the Hennepin County Regional Railroad Authority, and the City of Minneapolis. CIDNA supports other trails in the neighborhood such as a walkway south of the Midtown Greenway between Thomas Avenue South and Dean Parkway. This resolution is not intended to conflict with the land bridge concept considered by the Park Board in its 2012 charrette. The trail that CIDNA has proposed could be part of the land bridge proposal, or an alternative crossing of Lake Street could replace the trail proposed in this resolution through Thomas Avenue.
22. BE IT FURTHER RESOLVED THAT CIDNA commends members of ELECT, including Arlene Fried, Harriet Horwitz, Sally Anson, and Steve Woldum, who spoke before CIDNA concerning the Project; the efforts of ELECT have helped to limit the height of buildings on our lakes and inform the dialogue.
23. BE IT FURTHER RESOLVED THAT CIDNA does not oppose the Project. CIDNA commends Greystar for addressing many of the neighborhood’s concerns including shoreland compatibility, pedestrian safety and connectivity, interface with parks and the Midtown Greenway, and location of Project mass on West Lake Street. For this resolution to be effective, the following conditions must be satisfied:
 - a. This resolution is acknowledged and agreed by the Developer as set forth below.
 - b. There are no significant changes to the Project, and there is no change to the Project setback from West Lake Street.

- c. The CIDNA Land Use and Development Committee approves the Developer's forthcoming design revisions of the façade.

The Board of Directors of the Cedar-Isles-Dean Neighborhood Association has approved this resolution on April 9, 2014.

_____/s/_____
Craig Westgate
President

_____/s/_____
Rosanne Holloran
Secretary

_____/s/_____
Robert Corrick
Chair, Land Use and Development Committee

The Developer agrees that this resolution and Exhibit accurately describes the Project. The Developer will seek approval from the CIDNA Land Use and Development Committee for significant changes to the Project, except that changes requested by the Minneapolis Planning Commission or its Committees, if any, would be communicated to CIDNA as soon as practicable thereafter. If the Project is changed CIDNA reserves the right to modify or withdraw its support of the Project.

_____/s/_____
David Reid
Senior Managing Director of Development
Greystar GP II LLC

_____/s/_____
Gary Wallace
Managing Director
Greystar GP II, LLC

2834 10th Avenue South
Greenway Level, Suite 2
Minneapolis, MN 55407
Phone: 612-879-0103
Fax: 612-879-0104



April 17, 2014

Hilary Dvorak, Principal City Planner
City of Minneapolis CPED
250 South 4th Street - Room 300
Minneapolis, MN 55415

Dear Ms. Dvorak,

I am writing to express our organizational support for the project at 2622 West Lake Street being developed by Greystar and ESG Architects on the former Weisman Enterprises site.

We have reviewed the project plans as presented to the CIDNA Board of Directors on 4/9/14 and developed in close consultation with the CIDNA Land Use and Development Committee.

We believe this development represents best practices for building along the Greenway (especially on the south side), which is why we are pleased to give it our support. The building steps back from the Greenway, allowing sunlight to reach the trail even in the middle of the winter. We are also excited about the potential pathway to the Lake of the Isles and a possible new connection to the Greenway.

Our support is conditioned on the 4/7/2014 design plans as presented to the CIDNA Board of Directors on 4/9/2014. If during the approval process the design changes substantially and/or in a way that could affect the Greenway, we respectfully request to be notified so that we may review the changes.

We commend Greystar and ESG for their openness to working with CIDNA neighborhood residents and Greenway supporters on the building design. We believe this new building will be great for the Greenway and great for Minneapolis, and we look forward to its completion.

Sincerely,

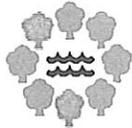
A handwritten signature in black ink, appearing to read "SJA", is written over a light-colored background.

Soren Jensen
Executive Director

cc: David L. Graham, Elness Swenson Graham Architects; Gary Wallace, Greystar Development, LLC; Robert Corrick, CIDNA; Lisa Goodman, Minneapolis City Council.

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LOVE OUR PATHWAYS



**Minneapolis
Park & Recreation Board**

Administrative Offices
2117 West River Road
Minneapolis, MN 55411-2227

Operations Center
3800 Bryant Avenue South
Minneapolis, MN 55409-1000

Phone
612-230-6400

Fax
612-230-6500

www.minneapolisparke.org

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Jayne Miller

Secretary to the Board
Pamela French



June 11, 2014

Hilary Dvorak
Principal Planner
City of Minneapolis
250 South 4th Street, Room 300
Minneapolis, MN 55415

**Re: Proposed Conditional Use Permit at 2622 W. Lake Street
Your File BZZ-6576**

Dear Ms. Dvorak:

I am writing to express the concerns of the MPRB regarding the proposed height increase from 2.5 stories/35 feet to 8 stories for this project. The MPRB has and continues to support the current height restrictions in the Shoreland Overlay District, which we feel are important to the continuity of Chain of Lakes area development and park amenities.

Considering the transit plans for the area, this portion of Lake Street being one of the most traveled roadways in Hennepin County, the Chain of Lakes being one of the most visited areas in Minnesota, and this proposed development resulting in a building disproportionate to other buildings in the area, we believe the proposal will create an adverse impact on the park experience for our residents and visitors, and will diminish the natural amenity of our Chain of Lakes. If such an extreme departure from the height restrictions is to be considered, a traffic study should be conducted to determine the extent of adverse impacts to the surrounding neighborhood and park usage.

We are concerned that the Shoreland Overlay District seems to be routinely ignored these days in these most cherished and fragile natural areas of our city. This proposed conditional use permit to increase the height of the building would continue the negative precedent being set for future proposals, so we do not support it.

Sincerely,

Renay Leone
Real Estate Planner