



CPED STAFF REPORT

Prepared for the City Planning Commission

CPC Agenda Item #7
 April 7, 2014
 BZZ-6473

LAND USE APPLICATION SUMMARY

Property Location: 1014-1022 Essex Street SE and 506 Huron Boulevard SE
Project Name: Essex Hotel Project
Prepared By: Aaron Hanauer, Senior City Planner, (612) 673-2494
Applicant: Daniel Oberpriller, CPM Development
Project Contact: Scott Nelson, DJR Architecture
Request: To allow for the construction of a five-story, 122-room, hotel
Required Applications:

Rezoning	Petition to rezone the property located at 1014, 1018 and 1022 Essex Street SE, 506 Huron Boulevard SE from the R5/Multiple Family District to the C3A/Community Activity Center District
Conditional Use Permit	Conditional use permit application to increase the maximum permitted height of a commercial building from 4 stories or 56 feet to approximately 5 stories or 63 feet at the tallest point
Variance	Variance to increase the maximum floor area ratio from 2.7 to 3.2
Variance	Variance to reduce the interior setback (south) from 5 feet to 2.1 feet to allow for an uncovered driveway
Variance	Variance to reduce the number of off-street loading spaces from 2 to 0
Variance	Variance to reduce the front yard requirement adjacent to the south lot line to allow a transformer
Site Plan Review	To allow for the construction of a five-story, 122-room, hotel

SITE DATA

Existing Zoning	R5/Multiple Family District UA/University Area Overlay District
Lot Area	19,782 square feet / 0.45 acres
Ward(s)	2
Neighborhood(s)	Prospect Park East River Road
Designated Future Land Use	Public and institutional
Land Use Features	University of Minnesota Growth Center, Stadium Village Transit Station Area
Small Area Plan(s)	Stadium Village University Avenue Station Area Plan (2012)

Date Application Deemed Complete	March 14, 2014	Date Extension Letter Sent	September 30, 2014
End of 60-Day Decision Period	May 13, 2014	End of 120-Day Decision Period	July 12, 2014

BACKGROUND

SITE DESCRIPTION AND PRESENT USE. The subject site is at the southwest corner of Essex Street SE and Huron Boulevard SE and contains three parcels: 1014, 1020 and 1022 Essex Street/506 Huron Boulevard SE. The lots 1014 and 1020 Essex Street SE each have a single family house; the lot at 1022 Essex Street SE/506 Huron Boulevard SE has two single-family houses. There is a large bituminous contiguous parking area at the back of these properties that provides a total of 14 off-street parking spaces.

In and around December 2013, the applicant requested a demolition of historic review letter for the four houses on the project site. A historic review letter provides an applicant a determination on whether a wrecking permit can be administratively approved or will require a heritage preservation commission review via a demolition of historic resource application. The historic review letter determination is based on the historic information known at the time of the review. None of the four houses on the subject site met the definition of a historic resource.

SURROUNDING PROPERTIES AND NEIGHBORHOOD. The subject property is located in the Prospect Park East River Road neighborhood and near the University of Minnesota East Bank campus. The subject site is also located three blocks from the METRO Green Line Stadium Village Light Rail Station.

The surrounding area has a variety of uses. Between Essex Street SE and Washington Avenue SE to the north, there is primarily a mix of medium/high-density residential and commercial properties. Between Essex Street SE and East River Road to the south, it is primarily a mix of low, medium and high-density residential. In the past four years, there have been two high-density residential apartment buildings built within three blocks of the project site; the six-story Solhaus Tower at 515 Huron Boulevard SE and the six-story Solhaus at 2428 Delaware Street SE. In addition, the five-story, 330,000 square foot University of Minnesota Ambulatory Care Center is currently under construction a block to the west and the 11-story, WaHu apartment building at 1016 Washington Avenue SE is anticipated to be under construction in 2014.

PROJECT DESCRIPTION. The applicant is proposing to demolish the four houses on the project site and build a five-story extended-stay hotel with 122 rooms and underground parking for 41 cars. The proposed five-story, u-shaped hotel would have the main pedestrian and off-street parking entrance on Essex Street SE. A covered, vehicle drop-off area is proposed in front of the main entrance. The underground parking is accessed via a one-way driveway from Essex Street SE; those exiting the underground parking area will use a one-way driveway onto Huron Boulevard SE. The building's main exterior materials are cast stone, red brick and stucco. The applicant anticipates that patients of the University of Minnesota Ambulatory Care Center and their families will make up a large percentage of their clientele, as well as visiting professors, researchers and medical professionals working with the University.

COW REVIEW: The applicant attended the January 30, 2014, Planning Commission Committee of the Whole meeting. The applicant has updated their plans to reflect Planning Commission feedback in the following ways:

- Designed a more prominent entrance along Essex Street SE which includes increasing the width of the entry/columns;
- Increased the window and opening sizes (including parking area) along the first floor of Essex Street SE;

- Provided more details of a landscaping/public realm plan along Essex Street SE and Huron Boulevard SE including seasonal grasses, shrubs, trees and benches;
- Provided greater activity at the corner of Huron Boulevard SE and Essex Street SE by changing the interior use from hotel rooms to a dining room intended for hotel guests for morning and afternoon functions. In addition, the applicant is proposing to provide sidewalk seating with direct access to the dining room;
- Proposed eight bike parking spaces in the boulevard along Essex Street SE.

PUBLIC COMMENTS. As of March 31, 2014, no public comments have been received. Any correspondence received prior to the public meeting will be forwarded on to the Planning Commission for consideration.

ANALYSIS

REZONING

The Department of Community Planning and Economic Development has analyzed the application for a petition to rezone the property at_1014-1022 Essex and 506 Huron Boulevard SE, based on the following [findings](#):

I. *Whether the amendment is consistent with the applicable policies of the comprehensive plan.*

The proposed rezoning for the properties at 1014-1022 Essex Street SE and 506 Huron Boulevard SE from R5/Multiple Family District to C3A/Community Activity Center District is consistent with the applicable policies of the comprehensive plan and the recently updated *Stadium Village University Avenue Station Area Plan* that provides more detailed guidance for the immediate area. The R5 Multiple Family District is established to provide an environment of high-density apartments, congregate living arrangements and cluster developments on lots with a minimum lot area of 5,000 square feet. The purpose of the C3A zoning is established to provide for the development of major urban activity and entertainment centers with neighborhood scale retail sales, residential uses and institutional and public uses.

There are three parts of the Minneapolis comprehensive plan's land use chapter that provide guidance for considering a rezoning application: the identified land use features that are part of, adjacent to, or nearby a project site (e.g. University of Minnesota Growth Center), the future land use map and the general land use policy. The proposed rezoning from R5 to C3A is consistent with the three applicable comprehensive plan land use features; the University of Minnesota Growth Center, the Stadium Village Transit Station Area and the neighboring Stadium Village Activity Center (a half block to the north).

The subject site is located within the University of Minnesota Growth Center. A growth center is described by the comprehensive plan as a busy, interesting and attractive place characterized by a concentration of business and employment activity and a wide range of complementary activities taking place throughout the day into the evening. These activities include residential, office, retail, entertainment and recreational uses. The Minneapolis comprehensive plan recognizes the University of Minnesota as being home to one of the largest concentrations of employment in the City. The University of Minnesota is planning a major expansion with the Ambulatory Care Center a ½ block to the west. Allowing for the rezoning from R5 to C3A of the subject properties will allow for more employment opportunities and complementary uses to the institutional ambulatory care center and the multifamily neighboring residential uses. Land Use Policy 1.15 and Implementation Step 1.15.4 support the proposed rezoning based on the project being a growth center.

Land Use Policy 1.15: Support development of growth centers as locations for concentration of jobs and housing and supporting services.

Implementation Step 1.15.4 Promote the integration of major public and private institutional campuses located in growth centers, including health care and educational services, with the function and character of surrounding areas.

The proposed rezoning from R5 to C3A is also consistent with the Stadium Village Transit Station Area and the comprehensive plan policies for transit station areas (TSAs). TSAs are located within an approximate ½ mile radius from transit stations, reflecting an understanding that most walking trips to and from transit stations are ten minutes or less in duration. The subject site is less than a ¼ mile from the actual station (0.246 miles). The comprehensive plan recognizes that potential TSA redevelopment opportunities are generally highest within ¼ mile of the transit station. Allowing for the rezoning from R5 to C3A will allow for positive redevelopment opportunities and complementary uses to the University of Minnesota campus and the neighboring multifamily residential uses. Land Use Policy 1.13 and Implementation Step 1.13.1 support the proposed rezoning based on the project being in a TSA.

Land Use Policy 1.13: Support high-density development near transit stations in ways that encourage transit use and contribute to interesting and vibrant places.

Implementation Step 1.13.1 Encourage pedestrian-oriented services and retail uses as part of higher-density development near transit stations.

The proposed rezoning from R5 to C3A is also consistent with the Stadium Village Activity Center that is located ½ block to the north. Activity centers generally have a mix of uses with citywide and regional draw, including areas of employment and commercial development. The applicant's proposed rezoning from R5 to C3A will allow for complementary uses a ½ block to the south without an adverse impact to the neighboring uses.

The proposed rezoning from R5 to C3A is also consistent with the recently updated future land use. In August 2012, the Minneapolis City Council approved the *Stadium Village University Avenue Station Area Plan*. As part of this planning process, the future land use for the project site and the two blocks to the west was revised from urban neighborhood to public and institutional in large part to reflect the expansion of the University of Minnesota eastward. The Minneapolis comprehensive plan states that the public and institutional future land use is intended to accommodate public and semi-public uses, including museums, hospitals, civic uses and college and university campuses. The comprehensive plan also states that residential is generally not appropriate for the public and institutional future land use areas. The applicant's proposal to rezone the property from R5 to C3A will allow for nonresidential uses (as well as residential uses) that complement neighboring public and institutional uses, while respecting the neighboring multifamily residential uses.

Finally, the proposed rezoning from R5 to C3A is consistent with the recently approved *Stadium Village University Avenue Station Area Plan*. The small area plan breaks the study area into five character areas. The subject site is located within the Huron Boulevard Gateway District. The plan describes the Huron Boulevard Gateway District as a transitional zone which has seen a fair amount of redevelopment in recent decades from industrial to high-density residential. It is anticipated this trend is likely to continue as projects continue to be proposed and built on some of the remaining sites. The subject site and the two blocks to the west are unique part of the Huron Boulevard Gateway District. The future land use for a majority of the blocks in the character area is high-density residential; however, the future land of the subject site and the two blocks to the west is public and institutional. The plan recommendations for the Huron Boulevard Gateway District are

to generally support high-density residential development and to encourage land use along Huron Boulevard SE that supports pedestrian oriented environment, balanced against heavy vehicle traffic flow and acknowledge its role as a gateway to the area. The proposed rezoning from R5 to C3A will support high-density residential, but also support nonresidential uses that the public and institutional future land use category supports.

2. *Whether the amendment is in the public interest and is not solely for the interest of a single property owner.*

A rezoning from R5 to C3A is in the public interest. It will allow for orderly redevelopment in an area that it is encouraged. The rezoning will also support the University of Minnesota Growth Center, the METRO Green Line, the Stadium Village Transit Station Area and the Stadium Village Activity Center, which is located a half block to the north. Furthermore, the proposed rezoning maintains the opportunity for a building in the Huron Boulevard Gateway District to be a gateway landmark to the University of Minnesota, which is encouraged by the *Stadium Village University Avenue Station Area Plan*. Those traveling to the University of Minnesota via Huron Boulevard SE or the METRO Green Line will have a clear view of the subject site.

3. *Whether the existing uses of property and the zoning classification of property within the general area of the property in question are compatible with the proposed zoning classification, where the amendment is to change the zoning classification of particular property.*

The existing uses within the general area of the property are compatible with the proposed zoning classification. A majority of the uses within the area are medium to high-density residential developments. Nonresidential uses include the University of Minnesota Ambulatory Care Center that is currently under construction a half block to the west. The proposed rezoning from R5 to C3A of the subject site will allow for uses on the subject parcels that are compatible with the neighboring residential and nonresidential uses. C3A is supportive of high-density residential and neighborhood scale retail sales and services.

The zoning classification of the surrounding properties is also compatible with the proposed zoning classification. The immediate surroundings of the subject parcels are R5, like the subject properties. However, within a three block area there are a variety of other zoning districts that allow for more intense uses, including C3A. Other more-permissive zoning districts within this three block area include the R6, OR2, C2 and I1.

4. *Whether there are reasonable uses of the property in question permitted under the existing zoning classification, where the amendment is to change the zoning classification of particular property.*

Reasonable uses are allowed under the current zoning, R5 zoning district. However, as the applicant points out, R5 zoning is less compatible compared to C3A zoning with the public and institutional future land use guidance for the site. Residential uses are the primary use in the R5 zoning district and residential uses are not typically encouraged in the public and institutional future land use category. The R5 zoning district only allows five commercial uses. The proposed C3A rezoning would increase the number of commercial uses as permitted or conditional from five to 48. The C3A zoning district also allows 10 residential uses as permitted and conditional.

5. *Whether there has been a change in the character or trend of development in the general area of the property in question, which has taken place since such property was placed in its present zoning classification, where the amendment is to change the zoning classification of particular property.*

There has been a change in the character and trend of development in the area of the properties in question since the property was placed in its present zoning classification. The subject parcels have

been zoned R5 since 1963. In the last four years there have been at least four major residential and non-residential developments within a three block area that have been built or approved. This includes the six-story, Solhaus Tower and Solhaus Apartment buildings that have been completed, the University of Minnesota Ambulatory Care Center (under construction) and the 11-story WaHu Student Housing project that is anticipated to break ground in 2014.

As part of the 2012 *Stadium Village University Avenue Station Area Plan*, the study area was divided into five character areas. The subject site is in the Huron Boulevard Corridor Character Area. The 2012 plan describes this area as a “transitional zone which has seen a fair amount of redevelopment in recent decades from industrial to high-density residential.” The *Stadium Village University Avenue Station Area Plan* also anticipated that additional development would take place in this character area as older residential structures became more appealing for redevelopment.

FOR REZONINGS ONLY

ZONING PLATE NUMBER. 22

LEGAL DESCRIPTION. Parcel 1: The West 50 feet of Lots 6 and 7, Block 31, Regents Addition, Hennepin County, Minnesota. (Abstract Property).

Parcel 2: The West 50 feet of the East 100 feet of Lots 6 and 7, Block 31, Regents Addition, Hennepin County, Minnesota (Abstract Property).

Parcel 3: The East 50 feet of Lots 6 and 7, Block 31, Regents Addition, Hennepin County, Minnesota.

CONDITIONAL USE PERMIT

The Department of Community Planning and Economic Development has analyzed the application to allow a conditional use permit application to increase the maximum permitted height of a commercial building from 4 stories or 56 feet to approximately 5 stories or 63 feet at the tallest point based on the following [findings](#):

1. *The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

The establishment, maintenance or operation of the conditional use for a five-story, 63 foot tall structure (at its tallest point) will not be detrimental to or endanger the public health, safety, comfort or general welfare. The existing R5 and the proposed C3A zoning districts allow buildings by right to be 56 feet tall when they are maximum of four stories. With the exception of the prominent roof line canopy/stairwell along Huron Boulevard SE, the building will be under 56 feet tall. The canopy/stairwell along Huron Boulevard SE comprises only three percent of the overall building footprint (376 square feet of 12,783 square feet) and is approximately 60 feet away from the nearest nonresidential structure at 501 Huron Boulevard SE and 80 feet away from the nearest residential structure at 1015 Essex Street SE. Allowing the building to be five stories rather than four stories will also not be detrimental or endanger the public health, safety, comfort or general welfare.

2. *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

The additional height is not expected to be injurious to the use and enjoyment of other property in the vicinity. The proposed development is in compliance with the existing and proposed zoning district height requirement of 56 feet for 97 percent of the building footprint. The portion of the building that is seven feet taller than the zoning district height requirement is setback more than 60 feet from neighboring nonresidential and residential uses. In addition, the shorter buildings closest to the taller part of the proposed hotel are located a similar distance to taller buildings at 515 Huron Boulevard SE (6 stories) and the Stadium View Apartments (10 stories) at 2508 Delaware Street SE. The additional height will also not impede the normal or orderly development and improvement of surrounding property. The Huron Boulevard Gateway District, which the property is located in, is recognized by the Stadium Village University Avenue Transit Station Area Plan as a prime area for redevelopment into high-density residential that will typically be built five stories or taller.

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

Adequate utilities, access roads and drainage will be provided as part of the project. The applicant received initial City of Minneapolis department comments (including Public Works) at a meeting on March 19, 2014. None of the comments received by City of Minneapolis departments will require major revisions to the siting, access and design of the building. The applicant will be required to continue to work closely with the Public Works Department, the Plan Review Section of the Inspections Department and the various utility companies during the duration of the development should the applications be approved. This would be required to ensure that all procedures are followed and that the development complies with all City and other applicable requirements.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

The applicant has proposed adequate measures to minimize traffic congestion in the public streets. The applicant is proposing to meet their minimum off-street parking requirement (41 off-street parking spaces) and provide eight bicycle parking spaces near the front entrance; a hotel use is not required to provide bike parking. The project will also have a well-designed off-street loading/unloading near the front entrance for guests. Furthermore, the applicant's proposal to have all underground parking exit onto Huron Boulevard SE will provide a direct route to Interstate 94. Finally, being located within three blocks of the METRO Green Line-Stadium Village Transit station will increase the percentage of people traveling to the hotel by mass transit.

The applicant submitted a draft travel demand management plan (TDMP) for the proposed hotel on March 20, 2014, that further shows their commitment to minimize traffic congestion in the public streets. The developer has identified a mode split of 30 percent automobile, 20 percent shuttle/taxis/private transportation services, 30 percent transit and 20 percent bike/walk. The TDMP also states that the hotel management will be committed to meeting the mode split by developing and implementing an incentive program to encourage guests and employees to use alternative modes of transportation, designating a TDM liaison to coordinate the various TDM strategies and monitoring the implementation of the proposed TDMP actions and progress made toward achieving the TDMP mode split goals.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

Approving the conditional permit for the proposed height increase would be consistent with the following land use, transportation and urban design policies of the comprehensive plan.

Land Use Policy 1.2: Ensure appropriate transitions between uses with different size, scale and intensity.

I.2.1 Promote quality design in new development, as well as building orientation, scale, massing, buffering and setbacks that are appropriate with the context of the surrounding area.

Land Use Policy I.13: Support high-density development near transit stations in ways that encourage transit use and contribute to interesting and vibrant places.

I.13.1 Encourage pedestrian-oriented services and retail uses as part of higher-density development near transit stations.

I.13.2 Pursue opportunities to integrate existing and new development with transit stations through joint development.

Policy I.15: Support development of growth centers as locations for concentration of jobs and housing and supporting services.

I.15.1 Support development of growth centers through planning efforts to guide decisions and prioritize investments in these areas.

I.15.2 Support the intensification of jobs in growth centers through employment generating development.

Transportation Policy 2.4: Make transit a more attractive option for both new and existing riders.

2.4.3 Encourage higher intensity and transit-oriented development to locate in areas well served by transit.

Urban Design Policy 10.9: Support urban design standards that emphasize traditional urban form with pedestrian scale design features at the street level in mixed-use and transit-oriented development.

10.9.1 Encourage both mixed-use buildings and a mix of uses in separate buildings where appropriate.

Urban Design Policy 10.10: Support urban design standards that emphasize a traditional urban form in commercial areas.

10.10.1 Enhance the City's commercial districts by encouraging appropriate building forms and designs, historic preservation objectives, site plans that enhance the pedestrian environment and by maintaining high quality four season public spaces and infrastructure.

10.10.4 Orient new buildings to the street to foster safe and successful commercial nodes and corridors.

10.10.6 Require storefront window transparency to assure both natural surveillance and an inviting pedestrian experience.

Urban Design Policy 10.18: Reduce the visual impact of automobile parking facilities.

6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

With the approval of the rezoning, conditional use permit, variances and site plan review applications, this development would be in conformance with the applicable regulations of the zoning code.

ADDITIONAL STANDARDS TO INCREASE MAXIMUM HEIGHT

In addition to the conditional use standards, the City Planning Commission shall consider, but not be limited to, the following factors when determining the maximum height:

1. Access to light and air of surrounding properties.

The proposed building will have no effect on the access to light and air of surrounding properties of surrounding buildings. The building is meeting the zoning district setbacks and approximately 97 percent of the building will meet the existing and proposed zoning district height requirement of 56 feet. The applicant is seeking a conditional use permit to allow a 376 square foot portion of the building to extend to 63 feet in height along Huron Boulevard SE. This portion of the building will be at least 60 feet from the nearest building, which provides ample distance to not have a shadowing impact.

2. Shadowing of residential properties or significant public spaces.

The portion of the project that extends beyond the zoning district height requirement would not create a shadowing effect on residential properties or significance public spaces. The portion of the building that exceeds the zoning district height requirement is located at least 80 feet away from the nearest residential structure (1015 Essex Street SE). As shown by the applicant’s shadowing study, this separation distance will eliminate any shadowing impact of the small portion of the building that will be seven feet higher than allowed. The additional height request will also not have an impact on significance public spaces. The nearest significant public spaces are Luxton Park three blocks to the SE and TCF Stadium Plaza, three blocks to the north.

3. The scale and character of surrounding uses.

The proposed development will fit in with the scale of the surrounding uses. Although the proposed six-story building is taller than the older, nearby 1.5 to three -story residential structures, it is shorter than the newer apartment building development within the immediate area. This includes the six-story Solhaus Tower directly across Huron Boulevard SE (515 Huron Boulevard SE), the six-story Solhaus located one block to the northeast (2428 Delaware Street SE) and the 11-story WaHu Student Housing project, 2.5 blocks to the northeast (2408 University Avenue SE).

4. Preservation of views of landmark buildings, significant open spaces or water bodies.

The proposed project will not have an impact of views of landmark buildings, significant open spaces or water bodies. The project will be located five blocks from the nearest local historic landmark (Fire Station #19), more than three blocks from the nearest significant open space and more than five blocks from the nearest water body.

VARIANCE: FAR

The Department of Community Planning and Economic Development has analyzed the application for a variance to increase the maximum floor area ratio from 2.7 to 3.2 based on the following [findings](#):

- 1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

The applicant is seeking a variance to increase the maximum floor area ratio from 2.7 to 3.2. Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The subject site is located within the University of Minnesota Growth Center and within a ¼ mile of the Stadium Village Transit Station Area. The comprehensive plan generally supports greater development within growth centers and transit station areas greater than 2.7. Recent nearby

examples of greater F.A.R. ratios include the Solhaus Tower at 515 Huron Boulevard SE (3.6 F.A.R.), the Solhaus at 2428 Delaware Street SE (3.6 F.A.R) and the WaHu Student Housing project (5.1 F.A.R.).

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The request is reasonable and is in keeping with the spirit and intent of the ordinance and the comprehensive plan. Even though the building exceeds floor area ratio requirement, the applicant has taken measures to reduce the building's impact by having all of the off-street parking underground and complying with the height requirement for 97 percent of the building.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property. If granted, the new development will have a lower floor area ratio compared to the new development within a three-block area. This includes the Solhaus Tower at 515 Huron Boulevard SE (3.6 F.A.R.), the Solhaus at 2428 Delaware Street SE (3.6 F.A.R) and the WaHu Student Housing project (5.1 F.A.R.).

The proposed variance will also not be injurious to the use or enjoyment of other property in the vicinity nor be detrimental to the health, safety, or welfare of the general public. The applicant is proposing to meet or exceed the building setbacks on all side of the building and will meet the height requirement for the existing and proposed zoning district of 56 feet for 97 percent of the building.

VARIANCE: Setback driveway

The Department of Community Planning and Economic Development has analyzed the application to allow for a variance to reduce the interior setback (south) from 5 feet to 2.1 feet to allow for an uncovered driveway based on the following [findings](#):

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

The applicant is proposing to reduce the setback from 5 feet to 2.1 feet along the south interior property line to allow for an uncovered driveway. Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The design incorporates separate ingress and egress ramps based on consultation with Minneapolis Public Works to mitigate congestion and improve traffic flow on Essex Street SE and Huron Boulevard SE. As the applicant points out, the drive ramps have to be located at the edges of the property in order to allow for required circulation and parking stalls.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The property owner proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the compressive plan. The purpose of the setback requirement for driveways of nonresidential uses next to residential uses is to minimize the conflict

among the land uses by providing adequate open space and separation. The applicant has made efforts to develop a compatible use that minimizes any conflict between the hotel use and neighboring residential use. The applicant is proposing a 12-foot wide driveway along the southern property line for exiting traffic, the narrowest driveway allowed in the C3A zoning district. In addition, the applicant is proposing to screen the driveway with a three-foot high, metal, decorative fence and arborvitae. Furthermore, the impact of the driveway being 2.1 feet from the property line will be mitigated by the fact the driveway is lower than the adjacent property for the entire property and the neighboring apartment building is nine feet from the shared property line.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Granting of the proposed variance will not alter the essential character of the locality or be injurious to the use and enjoyment of other property in the vicinity. The applicant has taken measures to reduce the impact of having a driveway at 2.1 feet from the property line including planting of arborvitae along the property line and by having the driveway lower than the adjacent property.

If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties. The driveway will be nine feet away from the neighboring building and the applicant is proposing a 3.5 high railing the first 80 feet of the driveway. For the remaining 14.25 feet, the applicant is proposing to install a three foot high, metal decorative fence.

VARIANCE: Loading

The Department of Community Planning and Economic Development has analyzed the application to reduce the number of off-street loading spaces from 2 to 0 based on the following [findings](#):

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

Per Table 541-8, the proposed hotel project requires two large loading spaces. Per 541.560, the loading spaces are required to be at least 12 feet in width by at least 50 feet in length, exclusive of aisle and maneuvering space. In addition, a vertical clearance of at least 14 feet is required. Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The subject site's water table is at or near 13 feet below grade. The depth of the water table and City of Minneapolis strong encouragement of having subgrade structures built to prevent the infiltration of groundwater without the need for a permanent dewatering system will not allow for the construction of the basement to go below 13 feet. This is not sufficient in meeting the 14-foot vertical clearance for off-street loading spaces. In addition, there is limited space above ground to meet all zoning code and use needs. The applicant is proposing a drop-off area in the front of the Essex Street SE entrance. However, this area will not count towards providing a loading space because the vertical clearance for the new construction first floor is less than 14 feet.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The property owner proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and comprehensive plan. The intent of the zoning code in requiring off-street loading spaces is to reduce traffic congestion in the public streets. The applicant

states that deliveries will be made by small trucks that can use the on-site drop-off zone and the main entry. The applicant also states that large trucks will not be servicing the hotel. CPED recognizes that the number of deliveries will be substantially less for the proposed hotel given that there will not be a restaurant, meeting hall, or event facility. With the drop-off area in front of the building, the applicant will be able to accommodate the loading and unloading of at least two vehicles at a time.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Granting of the proposed variance will not alter the essential character of the locality, given that the proposed development has an on-site drop-off area that will accommodate smaller trucks that will provide deliveries to the hotel.

In addition, granting of the variance will also not be injurious to the use or enjoyment of other property in the vicinity, not be detrimental to the health, safety, or welfare of the general public or those utilizing the property or nearby property given that most of the deliveries will take place onsite.

VARIANCE: Setback transformer

The Department of Community Planning and Economic Development has analyzed the application for a variance to reduce the front yard requirement adjacent to the south lot line to allow a transformer based on the following [findings](#):

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

The applicant is proposing a transformer that would be located 16.4 feet from the south interior side property line and 9 feet from the front yard setback along Huron Boulevard, SE. The proposed transformer would be approximately five feet tall and 14 square feet (four feet by 3.2 feet). The concrete pad underneath the electrical transformer is approximately 90 square feet. Per 548.140 (b) (1), the established front yard setback is 14.25 feet for the first 25 feet from the adjacent residential property to the south.

Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone. The applicant has indicated that the electrical transformer needs to be readily accessible by the power company for maintenance and repair purposes. In addition, the electrical transformer has to maintain a ten foot setback from any operable openings in the building. This requirement limits where the electrical transformer can be placed on the site. To help minimize the transformer visibility, CPED is recommending that the applicant work with staff to provide landscaping to the east and south of the transformer to minimize its visibility from Huron Boulevard SE and the neighboring property to the south.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The property owner proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and comprehensive plan. The intent of the zoning code in not allowing transformers in required yards is to provide for the orderly development and use of land and to minimize conflicts between adjacent land uses by regulating the dimension and use of yards in order to provide adequate light, air, open space and separation of uses. The applicant has proposed to place the transformer 16.4 feet from the south property line which is 25 feet away from the neighboring building to the south. This distance, combined with the applicant's proposed screening plan and CPED's recommended condition of approval of having additional landscaping to the south and east of the transformer will assist in minimizing the conflict between the subject site and the residential use to the south.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Granting of the proposed variance will not alter the essential character of the locality or be injurious to the use and enjoyment of other property in the vicinity. The applicant has taken measures to reduce the impact of having the transformer in the front yard setback. The applicant has located the transformer 16 feet from the shared southern property line and 25 feet from the neighboring building to the south. In addition, the applicant is proposing to plant arborvitae and two trees along the south property line and install a decorative metal fence to help screen the transformer from the property to the south. The proposed variance will not be detrimental to the health, safety or welfare of the general public or of those utilizing the property or nearby properties based on the applicant's proposed transformer location, the screening plan and CPED's condition of approval for additional screening.

SITE PLAN REVIEW

The Department of Community Planning and Economic Development has analyzed the application based on the required [findings](#) and [applicable standards](#) in the site plan review chapter:

I. Conformance to all applicable standards of Chapter 530, Site Plan Review.

Building Placement and Design – Requires alternative compliance

- The proposed building will have a street presence at or near the property lines along Essex Street SE and Huron Boulevard SE, while respecting the required setbacks. This will help the building reinforce the street wall along both streets. The placement of the building and the proposed fenestration that exceeds minimum requirements will maximize natural surveillance and visibility of the area. The building entrances and exits near the public sidewalks will facilitate pedestrian access and circulation.
- As previously stated, the proposed building will have a street presence at or near the property lines along Essex Street SE and Huron Boulevard SE. However, there are two portions of the building that do not meet the standard of having the building less than eight feet from the front property line. The western 80 feet of the building's first floor along Essex Street will only have pillars at the property line for a covered drop-off area and recessed entrance. The active space and main building entrance is setback 19 feet at its furthest point from Essex Street SE. Alternative compliance is requested. In addition, an 11-foot section of the building along Huron Boulevard SE near the south property line is setback 18 feet. However, the applicant is proposing this to comply with the zoning code's established front yard setback when bordering a residential property. Given the location of the adjacent residential property to the south, the

subject property's front yard setback is 14.25 feet for the first 25 feet of the property measured from the southern property line. Alternative compliance is not required for this portion of the building.

- The area between the building and the lot lines include amenities. The applicant is proposing enhanced landscaping and seating areas in these areas along both Essex Street SE and Huron Boulevard SE.
- The subject property is a corner lot. The applicant has oriented the building to provide the principal entrance facing the front lot line along Essex Street SE.
- All proposed on-site accessory parking will be entirely below grade.
- The applicant has proposed windows that are in compliance with Chapter 530. The applicant has also proposed architectural detail and windows that create visual interest. This includes designing the building in a classic u-shape hotel design, having windows wrap the corners of the building on each floor and having the portion of the building closest to Huron Boulevard SE clad nearly all in glass along with a projecting roof canopy. Furthermore, the applicant's proposed evenly distributed fenestration and glazing percentages that exceed the zoning code's requirements will increase the security of adjacent outdoor spaces by maximizing natural surveillance and visibility.
- The proposed building has a u-shape design, a prominent roof canopy along Essex Street SE and a portion of the building that is stepped closest to Huron Boulevard SE that is clad nearly entirely in glass. These elements provide recesses and projections that effectively divide the building into smaller identifiable sections.
- The proposed building does not have any blank, uninterrupted walls that exceed 25 feet in length. The distance between windows, entries, recesses or projections does at its furthest point is 21 feet.
- The four elevations will be comprised of brick, stucco, glass and cast concrete (replicating stone on the first floor). All of these materials are considered durable materials.
- The exterior materials and appearance of the west elevations is similar to and compatible with the front of the building. The applicant has continued the brick cladding for the first 80 feet of the building. The exterior materials and appearance of the south elevation, however, is not compatible with the front of the building. With the exception of one band of brick, the upper floors are clad in stucco. Alternative compliance is required.
- The applicant is not proposing to use plain face concrete block as an exterior material.
- The applicant has designed the principal entrance for the nonresidential use on Essex Street SE to be clearly defined and emphasized with a prominent canopy and pillars that will be proud of the building wall.
- The applicant is required to have 30 percent of the walls on the first floor and 10 percent of the walls on the upper floors along Essex Street SE and Huron Boulevard SE be windows. The applicant's proposal will exceed these requirements (see Table I below). The windows included in this calculation will be vertical in proportion, distributed in a more or less even manner and will not have a bottom of the window higher than four feet above grade. As a condition of approval, the first floor windows shall have clear or lightly tinted glass with a visible light transmittance ratio of 0.6 or higher and the ground floor will have windows that allow views into and out of the building at eye level without shelving, mechanical equipment or other similar fixtures that block views into and out of the building in the area between four and seven feet above the adjacent grade for the required window calculations.
- The first floor of the Essex Street SE and the Huron Boulevard SE are designed to accommodate active functions. None of the first floor of either street elevation is designed for parking, loading, storage, or mechanical equipment rooms.

- The applicant is proposing a flat roof, which is similar to the neighboring buildings that primarily have flat roofs.
- The applicant is proposing an underground parking garage that will not be visible from the public street or from properties to the west and south.

Table I. Percentage of Windows Required for Elevations Facing a Public Street, Sidewalk, Pathway, or On-Site Parking for a Nonresidential Use

	Code Requirement		Proposed	
Essex Street SE				
1 st Floor	30% minimum	305 sq. ft.	60%	610 sq. ft.
2 nd Floor and Above	10% minimum	102 sq. ft.	26%	266 sq. ft.
Huron Boulevard SE				
1 st Floor	30% minimum	281 sq. ft.	41%	387 sq. ft.
2 nd Floor and Above	10% minimum	94 sq. ft.	25%	235 sq. ft.

Access and Circulation – Requires alternative compliance

- The main entrance along Essex Street SE has a drop-off area that will double as the pedestrian connection to the Essex Street SE sidewalk. This area will be well-lighted. As a condition of approval to help delineate a clear walkway for pedestrians, CPED is recommending that the applicant install a differentiated pavement that leads from the vestibule to the Essex Street SE sidewalk on both sides of the drive apron.
- The applicant is not proposing a transit shelter for the proposed hotel project.
- Vehicular access/circulation is designed to minimize conflicts with pedestrians and surrounding residential uses. Essex Street SE is a one-way street with a bike lane. The applicant is proposing a drop-off area on the property that will reduce the amount of parking of guests in the bike lane. To reduce the conflict with pedestrians, the applicant has proposed to narrow the curb cuts along Essex Street SE (14 feet and 16 feet) and Huron Boulevard SE (12 feet). However, as stated above, alternative compliance is required for not having a clear and well-lighted walkway of at least four feet in width connecting building entrances to the adjacent public sidewalk.
- The proposed design is intended to minimize the impact of traffic on residential uses. The applicant is proposing that vehicles access the site along Essex Street SE and exit the site via Huron Boulevard SE. The applicant’s proposal to direct exiting traffic to Huron Boulevard SE will reduce the traffic buildup that takes place at the Essex Street SE and Huron Boulevard SE traffic light. The proposed hotel will have the same number of curb cuts as existing conditions in the same locations, two along Essex Street SE and one along Huron Boulevard SE. The subject properties are not adjacent to an alley.
- The proposed design is intended to minimize the use of impervious surfaces. The area of the site that is not building or driveway will be landscaped. In addition, the applicant has reduced the width of driveways in order to increase the square footage dedicated to landscaping/pervious surfaces.

Landscaping and Screening – Requires alternative compliance

- The applicant is proposing extensive landscaping between the building and property lines along Essex Street SE and Huron Boulevard SE that complements the scale of development and surroundings. The applicant’s proposal will improve the landscaping appearance compared to existing conditions. The proposed landscaping creates will also create a contiguous planting area.
- The applicant’s landscaping proposal exceeds the minimum requirements outlined in Section 530.160 of the zoning code (see Table 2 below). The applicant is required to have 20 percent of

the site not occupied by the building be landscaped, one canopy tree for every 500 square feet of required landscaping and one shrub for every 100 square feet of required landscaping. The applicant is proposing to have 24 percent of the site not occupied by the building be landscaped, 11 canopy trees and 57 shrubs.

- The proposed drop-off area along Essex Street SE is proposed to be 1.5 feet from the front property line.
- Per Section 530.170 of the Minneapolis zoning code, loading facilities fronting a public street, require a landscaped yard of at least seven feet wide between the loading area and public street. In addition, the landscaped area shall include screening consisting of either a masonry wall, fence, berm, or hedge or combination thereof that forms a screen three feet in height and not less than 60 percent opaque and not less than one tree shall be provided for every 25 feet of the loading area lot frontage. The applicant’s proposal does not include landscaping and screening that meet these requirements. Alternative compliance is required for the landscaped yard, screening and tree requirement.
- The applicant is not proposing a surface parking lot for the hotel.
- All areas not covered by the building, driveways and walkways are proposed to be covered by landscaping.
- As a condition of approval, the installation and maintenance of all landscape materials shall comply with 530.210.

Table 2. Landscaping and Screening Requirements

	Code Requirement	Proposed
Lot Area	19,782	N/A
Building footprint	12,783	N/A
Remaining Lot Area	6,999	N/A
Landscaping Required	1,400 sq. ft.	1,646 sq. ft.
Canopy Trees (1: 500 sq. ft.)	3 trees	7 trees
Shrubs (1: 100 sq. ft.)	14 shrubs	57 shrubs

Additional Standards – Meets requirements

- The proposed project does not have a parking lot. The driveway is designed with continuous curbing. This does not provide on-site retention and filtration of stormwater. However, the on-site retention and filtration of the small project site is not practical.
- The proposed project will not obstruct views of important elements of the City such as parks and greenways, significant buildings and water bodies.
- The proposed building is located and arranged to minimize shadowing on adjacent properties. The proposed building is meeting the building setbacks on the four sides (see Table 7 below). In addition, the proposed building will meet the zoning district height requirement of 56 feet for 97 percent of the building’s footprint. The only portion of the building that extends beyond the zoning district’s height allowance is the roof canopy/stairwell along Huron Boulevard SE. This portion of the building is located 60 feet away from the nearest nonresidential structure at 501 Huron Boulevard SE and 80 feet away from the nearest residential structure at 1015 Essex Street SE.
- The proposed building will not have a major impact on wind currents at ground level.
- Crime prevention through environmental design elements have been included in the proposed plan. This includes an evenly distributed fenestration proposal that exceeds zoning code requirements and will allow for natural observation and opportunities for people to observe adjacent spaces and public sidewalks on all sides of the building. The applicant has incorporated

territorial reinforcement and space delineation with the proposed placement of the building, landscaping and fencing. As a condition of approval, CPED is recommending that the fence be an open and decorative fence made of metal. In addition, CPED is recommending the applicant continue the open and decorative fence on the east and south side of the transformer. Furthermore, the proposed project provides adequate natural access control to guide people coming to and going from the site. The Essex Street SE elevation and entrance is designed to attract people to enter and exit that part of the building. As conditioned, the applicant's proposed lighting levels will maintain an acceptable level of security at all building entrances within the drop-off area that maintains a minimum acceptable level of security while not creating glare or excessive lighting of the site.

- The project site did not include locally designated historic structures or structures that are considered eligible for designation. As part of the project the applicant will demolish these four houses.

2. Conformance with all applicable regulations of the zoning ordinance.

The properties at 1014-1022 Essex Street SE and 506 Huron Boulevard SE are zoned R5 and are located within the University Area Overlay District. The applicant has requested a rezoning of the subject parcels from R5 to C3A to allow for a hotel. A hotel is a permitted use in the C3A zoning district. If the rezoning application, conditional use permit, variances and site plan are approved the project will comply with all applicable regulations of the zoning ordinance.

Off-street Parking and Loading – Requires a variance (off-street loading spaces)

- The applicant's proposed off-street parking plan is in compliance with the off-street parking minimum and maximum requirement for vehicles (see Table 3 below). The minimum vehicle parking requirement for a hotel is one space per three guest rooms plus parking equal to 10 percent of the capacity of persons for affiliated uses such as dining or meeting rooms. The maximum vehicle parking is one space per guest room plus parking equal to 30 percent of the capacity of persons for affiliated uses such as dining and meeting rooms. As part of the proposed project, there will be no dining or meeting rooms for non-overnight guests. Therefore, the off-street parking requirement for the project is based on the number of guest rooms only. The proposed hotel project would have 122-guest rooms which would require a minimum of 41 off-street parking spaces. The maximum number of off-street parking spaces allowed for a 122-guest room hotel is 122 off-street parking spaces. In order to meet Americans with Disabilities Act (ADA) accessibility guidelines two of the 41 off-street parking spaces will be designated as handicap spaces. The applicant is proposing that 24 percent of the off-street parking spaces be compact spaces (nine of the 41 spaces). Per Section 541.330 of the Minneapolis zoning code, 25 percent of the off-street parking spaces can be compact spaces.
- Per Table 541-3, a hotel use does not have a bicycle parking requirement. However, the applicant is proposing to provide bike parking for eight bikes in the public right-of-way, approximately 20 feet from the principal hotel entrance.
- The applicant's proposal does not meet the Minneapolis zoning code's loading requirements. The applicant is seeking a variance. Their proposal is evaluated in the variance section above.

Table 3. Vehicle Parking Requirements Per Use ([Chapter 541](#))

	Minimum Parking Requirement	Applicable Reductions	Total Minimum Requirement	Maximum Parking Allowed	Proposed
Hotel/Total	41	N/A	41	122	41

Table 4. Bicycle Parking and Loading Requirements (Chapter 54I)

	Minimum Bicycle Parking	Minimum Short-Term	Minimum Long-Term	Proposed	Loading Requirement	Proposed
Hotel/Total	N/A	N/A	N/A	8	Medium/2 large loading spaces	0

Building Bulk and Height – Requires a variance (F.A.R.) and a conditional use permit (height)

- There is not a minimum lot area requirement for a hotel in the C3A Zoning District.
- The applicant is requesting a variance to increase the floor area ratio from 2.7 to 3.2.
- The applicant is requesting a conditional use permit to allow for an increase in height from 4 stories/56 feet to 5 stories/63 feet.

Table 5. Building Bulk and Height Requirements

	Code Requirement	Proposed
Minimum Lot Area	N/A per 548.340	19,782 sq. ft. / 0.45 acres
Gross Floor Area (GFA)	N/A	62,787 sq. ft
Maximum Floor Area Ratio (GFA/Lot Area)	2.7 per 548.350	3.2
Maximum Building Height	4 stories/56 feet, whichever is less per 548.350	5 stories/63 feet

Residential Lot Requirements –/Not applicable

- The proposed development is a nonresidential use, therefore, does not have residential lot requirements.

Yard Requirements – Requires a variance (driveway and transformer)

- The proposed building will meet all yard requirements. The applicant, however, is requesting a variance to reduce the interior setback (south) from 5 feet to 2.1 feet to allow for an uncovered driveway and a variance to reduce the front yard requirement adjacent to the south lot line to allow for a transformer (see variance section above for evaluation).

Table 7. Minimum Yard Requirements for a Hotel

	Zoning District	Overriding Regulations	Total Requirement	Proposed
Front (Essex)	0 ft.	Established front yard setback for the first 25 feet from the residence district boundary to the west (548.140 (b))	16.25 ft.	16.25 ft. for the first 25 feet from the residence district boundary to the west
Front (Huron)	0 ft.	Established front yard setback for the first 25 feet from the residence district boundary to the south (548.140 (b))	14.3 ft.	14.3 ft. for the first 25 feet from the residence district boundary to the south
Interior (West)	5+2X provided that the minimum interior side yard is not greater than fifteen (15) feet. ft.*	N/A	13 ft.	18 ft.
Interior (South)	5+2X provided that the minimum interior side yard is not greater than fifteen (15) feet. ft.*	N/A	13 ft.	15.2 ft.

* X = Number of stories above the first floor

Signs – Not applicable

- All new signs are required to meet the requirements of Chapter 543 of the zoning code. Separate permits are required from the Zoning Office for any proposed signage. The applicant states the signage for the proposed hotel is to be determined.

Dumpster Screening – Meets requirements

- The applicant’s proposal for screening of refuse and recycling containers meets requirements. Refuse storage containers are required to be screened from the street, adjacent residential uses located in a residence or office residence district and adjacent permitted or conditional residential uses. The applicant is proposing to have the refuse and recycling containers within the underground garage.

Screening of Mechanical Equipment – As conditioned, meets requirements

- The applicant is proposing two roof top mechanical units that are setback from the street. These units are shown to be screened on the proposed roof plan. As conditioned, the applicant’s rooftop mechanical equipment will be screened to be in compliance with Section 535.70.

Fencing- Meets requirements

- The applicant’s proposal for a fence and railing is in compliance with the fence requirements in Chapter 535 of the Minneapolis zoning code. Along the western property line, the applicant is proposing a 3-foot high fence for the first 16 feet from Essex Street SE and then a 3.5 foot high

railing along the outer edge of ramp to the parking garage. Along the southern property line, the applicant is proposing to install a 3-foot high fence for the first 18 feet from Huron Boulevard SE, and then install a 3.5 foot high railing along the outer edge of the parking garage. The applicant states that the fence design will be in the traditional wrought iron picket style and constructed of aluminum.

Lighting – As conditioned, meets requirements

- The applicant did not submit a lighting plan, however, they provided details of their lighting plan. They are committed to submitting a photometric plan that is in compliance with the zoning code. The applicant is proposing to add wall mounted sconces on the building elevations on Huron Boulevard SE and Essex Street SE and wall sconces and ceiling mounted lights in the recessed drop-off area. In addition, wall mounted lights will also be provided on the building's south and west faces for security but will be cutoff fixtures so as not cast light onto the adjacent residential areas. As conditioned, the applicant's proposal will meet the lighting requirements outlined in Section 535.590 and 541.57 of the Minneapolis zoning code.

Impervious Surface Area – Not applicable

- The C3A Zoning District does not have an impervious surface requirement. The project will meet its landscaping and screening requirement as outlined above.

Specific Development Standards – Meets requirements

- The applicant's proposal meets the specific development standards for hotels in Chapter 536. Chapter 536 requires that hotels outside of downtown districts provide a minimum of 50 guest sleeping rooms. The applicant is proposing 122 guest-sleeping rooms.

University Area Overlay District Standards – Not applicable

- The University Area (UA) Overlay District standards are not applicable for the proposed commercial use. The UA overlay requirements pertain to residential development.

3. Conformance with the applicable policies of *The Minneapolis Plan for Sustainable Growth*.

The Minneapolis Plan for Sustainable Growth identifies the site as public and institutional on the future land use map. The proposed development is consistent with the public and institutional land use category guidance. The proposed development is consistent with the land use, transportation and urban design policies and implementation steps listed in Finding One of the rezoning application and Finding Five of the conditional use permit application. In addition, the project is in compliance with the following economic development, environmental and urban design policies and implementation steps.

Economic Development Policy 4.1: Support private sector growth to maintain a healthy, diverse economy.

Economic Development Policy 4.9: Focus economic development efforts in strategic locations for continued growth and sustained vitality.

- 4.9.1 Prioritize economic development efforts around designated neighborhood commercial nodes, commercial corridors, activity centers and growth centers.

Environmental Policy 6.8: Encourage a healthy thriving urban tree canopy and other desirable forms of vegetation.

6.8.2 Achieve, at a minimum, no net loss of the urban tree canopy by maintaining and preserving existing trees and planting new trees on public and private property.

Urban Design Policy 10.11: Seek new commercial development that is attractive, functional and adds value to the physical environment.

10.11.1 Require the location of new commercial development (office, research and development and related light manufacturing) to take advantage of locational amenities and coexist with neighbors in mixed-use environments.

10.11.4 Maximize the year round potential for public transit, biking and walking in new developments.

Urban Design Policy 10.13: Work with institutional and public partners to assure that the scale and form of new development or expansion will occur in a manner most compatible with the surrounding area.

10.13.3 Encourage institutional uses and public buildings and facilities to incorporate architectural and site design that is reflective of their civic importance and that identifies their role as focal points for the community.

Urban Design Policy 10.17: Provide sufficient lighting to reflect community character, provide a comfortable environment in a northern city and promote environmentally friendly lighting systems.

10.17.1 Provide high-quality lighting fixture designs that are appropriate to street types and land use and that provide pedestrian friendly illumination, but minimize glare and dark sky conditions and other unnecessary light pollution.

Urban Design Policy 10.18: Reduce the visual impact of automobile parking facilities.

Urban Design Policy 10.19: Landscaping is encouraged in order to complement the scale of the site and its surroundings, enhance the built environment, create and define public and private spaces, buffer and screen, incorporate crime prevention principles and provide shade, aesthetic appeal and environmental benefits.

10.19.4 Landscaped areas should be maintained in accordance with Crime Prevention Through Environmental Design (CPTED) principles, to allow views into and out of the site, to preserve view corridors and to maintain sight lines at vehicular and pedestrian intersections.

10.19.7 Boulevard landscaping and improvements, in accordance with applicable City policies, are encouraged.

4. Conformance with applicable development plans or objectives adopted by the City Council.

The proposed project is in compliance with the following land use and public realm recommendations of the Stadium Village University Avenue Small Area Plan.

- Land Use: Generally speaking support high-density residential development in the Huron Boulevard Corridor Character Area.
- Land Use: Encourage land uses along Huron to support a pedestrian oriented environment, balanced against heavy vehicle traffic flow and acknowledging its role as a gateway to the area.

- **Urban Design and Public Realm:** Where existing sidewalks are less than 10 feet wide, encourage new buildings to be set back a minimum of 5-6 feet (within the frontage zone) when possible, to create wider sidewalks for outdoor seating and streetscape amenities.

Even though the applicant is proposing a nonresidential use, the proposed rezoning from R5 to C3A will continue to support the high-density residential development that the small area plan calls for by having a zoning district that is complementary to adjacent medium and high-density residential areas. It should also be noted project site is one of only four blocks within the Huron Boulevard Corridor Character Area that has a public and institutional future land use designation rather than high-density residential for the other blocks and parcels in the character area.

As highlighted in the site plan review section of the staff report, the applicant has proposed improvements along Huron Boulevard SE to improve the pedestrian experience with enhanced landscaping and benches. In addition, the applicant has made efforts to make the building a gateway along Huron Boulevard SE with the projecting roof canopy and large vertically oriented glazing area on the portion of the building that is set forward from the rest of the east elevation.

5. Alternative compliance.

The Planning Commission or zoning administrator may approve alternatives to any site plan review requirement upon finding that project meets one of three criteria required for [alternative compliance](#). Alternative compliance is requested for the following requirements:

- **First floor of the building shall be located not more than eight feet from the front lot line.** The placement of the building on Essex Street SE will not reinforce the street wall or be within eight feet of the adjacent street for the western 87 feet of the north elevation. This portion of the building is setback 19 feet from the front property line and is designed to be a drop-off area and recessed entrance. Even though the first floor is setback 19 feet on the west side, the applicant has designed the outer portion of the covered drop-off area and entrance to match the eastern side of the Essex Street SE elevation that is at property line by continuing the cast stone detail to the property line. The openings in the drop-off area are close in size to the window openings on the west side of the elevation and have a base to help with a building presence at the property line. Behind the drop-off area are large windows for a library and lobby area. The windows and active functions will help provide street activity. In addition, the upper floors of the building extend to the property line along Essex Street SE. For these reasons, staff recommends that the Planning Commission grant alternative compliance.
- **The exterior materials and appearance of south side wall shall be similar to and compatible with the front of the building.** The exterior materials and appearance of the south elevation is not consistent with the front elevations of the building. With the exception of one band of brick on the south elevation, the upper floors are clad in stucco. This creates a flat-surface for nearly the entire elevation. Staff is recommending that the Planning Commission require the applicant to replace the stucco with brick on floors two through four for the first two bays nearest Huron Boulevard SE. This design detail would be similar to the north elevation and will be clearly visible for the thousands of people that travel on Huron Boulevard SE on a daily basis.
- **Clear and well-lighted walkways connect building entrance to public sidewalk.** A clear and well-lighted walkway of at least four feet in width is required to connect the building entrance to the adjacent public sidewalk. The main entrance on Essex Street SE is also the exiting point for vehicles using the drop-off area. Vehicular traffic will not be traveling at fast speeds within the drop-off area; however, to help improve the safety for pedestrians accessing the main entrance from Huron Boulevard SE or Erie Street SE, staff is recommending that the

Planning Commission require the applicant to install a differentiated pavement that leads from the vestibule to the Essex Street SE sidewalk on both sides of the drive apron.

- **For loading facilities fronting a public street, a landscaped yard of at least seven feet wide shall be provided between the loading area and public street.** The applicant is not proposing to install a landscaped yard between the loading area and the property line. The proposed covered drop-off area is located 1.5 feet from the property line. However, the applicant is proposing to landscape the eight-foot-wide boulevard that is located between the building and the Essex Street SE sidewalk. In addition, the design of the covered drop-off area and the location at the property line helps the building have a street presence. For these reasons, staff recommends that the Planning Commission grant alternative compliance.
- **For loading areas fronting a public street, screening consisting of either a masonry wall, fence, berm or hedge or combination thereof that forms a screen three feet in height and not less than 60 percent opaque shall be provided.** The applicant is not proposing to install screening with an opacity of 60 percent between the loading area and the property line. The applicant has designed the outer portion of the covered drop-off area to match the eastern side of the Essex Street SE elevation that is at property line by continuing the cast stone detail to the property line. This includes having a 1.5 foot tall base for the entire loading area. In addition, the applicant is proposing to install large windows for a library and lobby area immediately behind the drop-off area. The windows and the building's active functions will provide street eyes on the street. For these reasons, staff recommends that the Planning Commission grant alternative compliance in not requiring additional screening for the loading area.
- **Not less than one (1) tree shall be provided for each twenty-five (25) linear feet or fraction thereof of parking or loading area lot frontage.** The applicant is not proposing to install a tree every 25 feet on the property between the loading area and property line. However, with Park Board approval, the applicant is proposing to install two canopy trees in the public-right-of-way that will help screen the 50-foot loading area. In addition, the applicant is proposing to install large windows for a library and lobby area immediately behind the drop-off area. The windows and active functions will help provide street activity. For these reasons, staff recommends that the Planning Commission grant alternative compliance in not requiring additional screening for the loading area.

RECOMMENDATIONS

Recommendation of the Department of Community Planning and Economic Development for the rezoning application:

The Department of Community Planning and Economic Development recommends that the City Planning Commission and City Council adopt the findings and **approve** the rezoning of the properties located at 1014-1022 Essex Street SE and 506 Huron Boulevard SE from the R5/Multiple Family District to the C3A/Community Activity Center District.

Recommendation of the Department of Community Planning and Economic Development for the conditional use permit:

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the application for a conditional use permit to allow an increase in the maximum permitted height of a commercial building from 4 stories or 56 feet

to approximately 5 stories or 63 feet at the tallest point for the properties located at 1014-1022 Essex Street SE and 506 Huron Boulevard SE, subject to the following conditions:

- I. The conditional use permit shall be recorded with Hennepin County as required by Minn. Stat. 462.3595, subd. 4 before building permits may be issued or before the use or activity requiring a conditional use permit may commence. Unless extended by the zoning administrator, the conditional use permit shall expire if it is not recorded within two years of approval.

Recommendation of the Department of Community Planning and Economic Development for the variance:

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the application for a variance to allow for an increase in the maximum floor area ratio from 2.7 to 3.2 for the commercial building located at 1014-1022 Essex Street SE and 506 Huron Boulevard SE.

Recommendation of the Department of Community Planning and Economic Development for the variance:

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the application for a variance to reduce the interior setback (south) from 5 feet to 2.1 feet to allow for an uncovered driveway for the proposed project located at 1014-1022 Essex Street SE and 506 Huron Boulevard SE.

Recommendation of the Department of Community Planning and Economic Development for the variance:

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the application for a variance to reduce the number of off-street loading spaces from 2 to 0 for the proposed project located at 1014-1022 Essex Street SE and 506 Huron Boulevard SE.

Recommendation of the Department of Community Planning and Economic Development for the variance:

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the application for a variance to reduce the front yard requirement adjacent to the south lot line to allow a transformer for the proposed project located at 1014-1022 Essex Street SE and 506 Huron Boulevard SE.

Recommendation of the Department of Community Planning and Economic Development for the Site Plan Review:

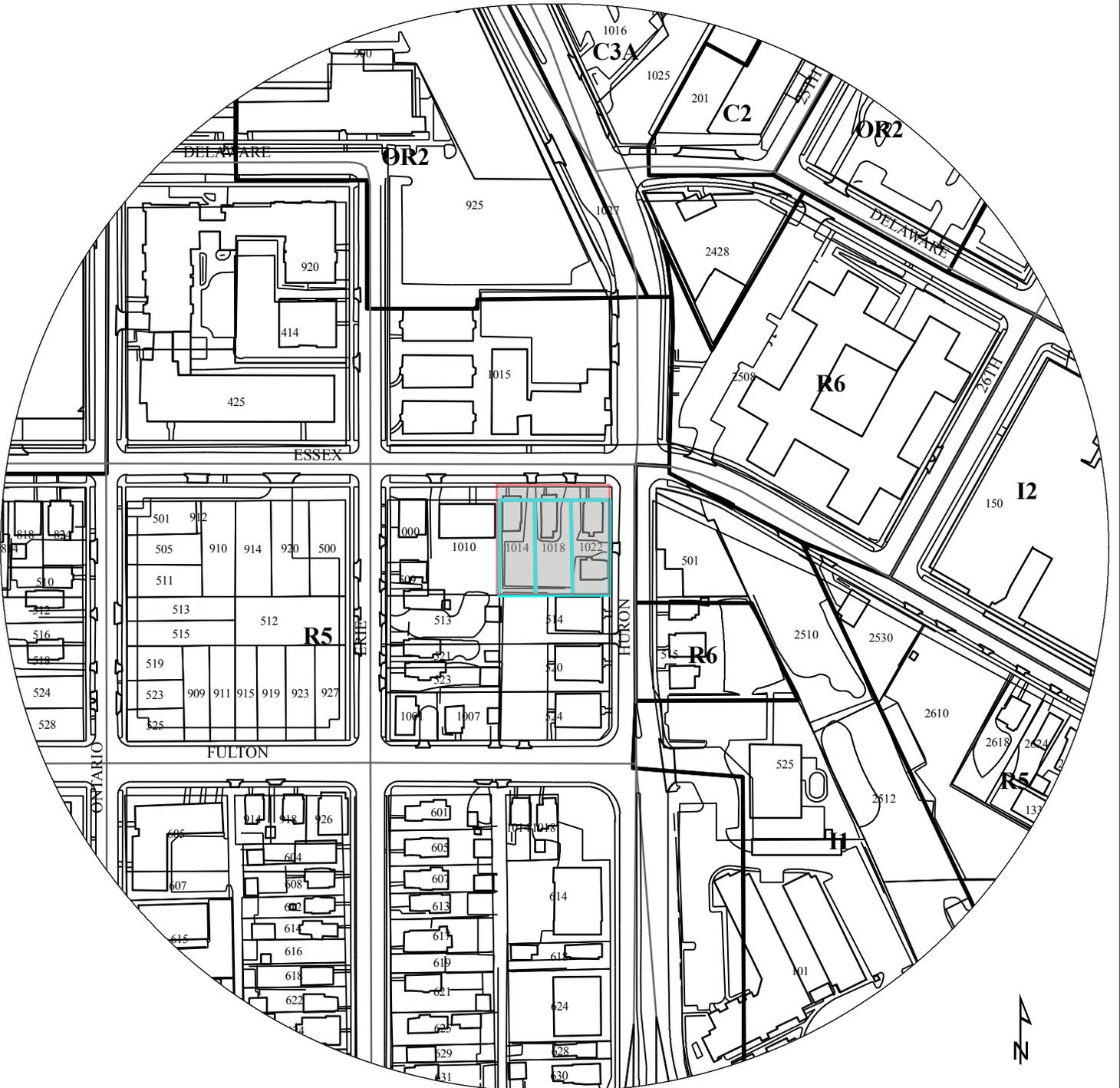
The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the site plan review application to allow for the construction of a five-story, 122-room hotel for the property at 1014-1022 Essex Street SE and 506 Huron Boulevard SE, subject to the following conditions:

1. To ensure compatibility on all sides of the building, the south elevation shall have the stucco replaced with brick on floors two through four for at least two additional bays nearest Huron Boulevard SE.
2. The first floor windows shall have clear or lightly tinted glass with a visible light transmittance ratio of 0.6 or higher and the ground floor will have windows that allow views into and out of the building at eye level without shelving, mechanical equipment or other similar fixtures that block views into and out of the building in the area between four

- (4) and seven (7) feet above the adjacent grade for the required window calculations.
3. The applicant shall work with staff to provide landscaping to the east and south of the transformer to minimize its visibility from Huron Boulevard SE and the neighboring property to the south.
 4. The installation and maintenance of all landscape materials shall comply with 530.210. The rooftop mechanical equipment shall be screened to be in compliance with Section 535.70.
 5. A lighting plan shall be submitted that is in compliance with Section 535.590 and 541.570 of the Minneapolis zoning code.
 6. Within the drop-off area, the applicant shall install a differentiated pavement that leads from the vestibule to the Essex Street SE sidewalk on both sides of the drive apron.
 7. The applicant shall install a fence on the east and south side of the transformer. The fence around the transformer and along the south and west property lines shall be an open and decorative metal fence.
 8. Department of Community Planning and Economic Development staff review and approval of the final building elevations, floor, site, lighting and landscape plans.
 9. Site improvements required by Chapter 530 or by the City Planning Commission shall be completed by April 7, 2016, unless extended by the Zoning Administrator, or the permit may be revoked for non-compliance.

ATTACHMENTS

1. Zoning, future land use map and aerial
2. Council member and neighborhood notification
3. Statement of purpose, description of project and statement to findings
4. Site context and images
5. University district development principles
6. Architectural plan set: rendering, survey, site plan, floor plans, elevations and perspectives
7. Civil plan set
8. Shadow study and PDR report



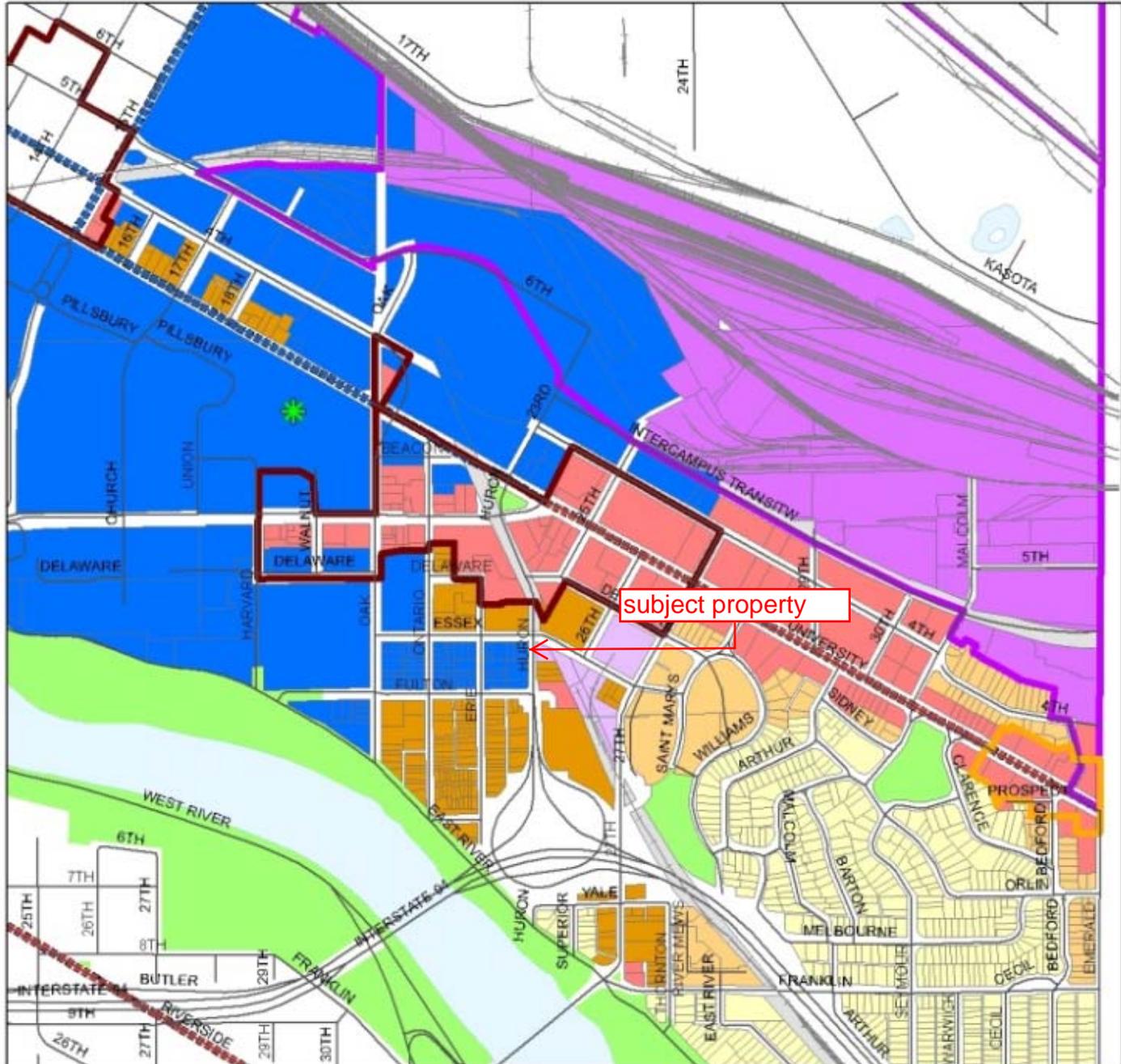
PROPERTY ADDRESS

1014, 1018, and 1022 Essex Avenue SE and 506 Huron Blvd SE

FILE NUMBER

BZZ-6384

Map 6.1 Stadium Village Future Land Use



Legend

- Future Land Use**
- Low density residential
 - Medium density residential
 - High density residential
 - Mixed use
 - Public and institutional
 - Transitional industrial
 - Industrial/office
 - Park/open space
 - Transportation/connector
- Transit Station
 - Growth Center
 - Activity Center
 - Neighborhood Commercial Node
 - Industrial Employment District
 - Community Corridor
 - Commercial Corridor
 - Centerline



CPED Department
Planning Division
April 23, 2012





University of Minnesota-Twin Cities

SE Ontario St

SE Erie St

SE Delaware

SE Essex St

Huron Blvd SE

SE Ontario St

SE Erie St



February 26, 2014

PPERR Neighborhood
c/o PPERRIA
Attn: Dick Brownlee
66 SE Malcolm Avenue
Minneapolis, MN 55414

PROPOSED HOTEL ESSEX & HURON

PROJECT NARRATIVE

PROJECT DESCRIPTION:

The proposed 122 room hotel project is a redevelopment of the southwest corner of Essex Street SE and Huron Boulevard SE. The site currently contains 4 single family homes. The proposed Hotel would be 5 stories tall with underground parking for 41 cars. The site is a block east of the new U of M Ambulatory Care Center currently under construction. It is anticipated that many patients and their families will be guests at this Hotel.

ZONING / VARIANCES / CONDITIONAL USE PERMIT (CUP):

The site is currently zoned R-5 Multiple Family. We are proposing to re-zone to C3A Community Activity Center District to allow construction of the Hotel. A variance will be required to increase FAR from 53,411 SF allowable (2.7) to 62,684 proposed (3.17). Variances are also required for setbacks on the west and south sides for driveways (ramps) to below grade parking garage. A variance is also required for no designated loading space. Required building setbacks are maintained to residential properties at the south and west. A conditional use permit is required to increase the height from 4 stories to 5.

SITE DESIGN:

The site design provides for entries on both Huron and Essex with the main entry being located on Essex, recessed below the upper levels in order to provide a safe drop off area with stacking space off the street and right-of-way. The lower level parking garage is accessed via a ramp from the east side on Essex and exits to a right turn only onto Huron. This was done to meet requirements of the City of Minneapolis Traffic Department.

The first floor building plan places active uses along the Essex entry side and at the corner with the main breakfast room and lounge located there. There will also be outdoor seating off this room adjacent to the sidewalk. Extensive landscaping consisting of perennial flowers, shrubs and trees will be provided along both the Essex and Huron sidewalks.

BUILDING DESIGN & FEATURES:

The building materials will be a blend of traditional and contemporary with a cast stone, red brick and stucco with cornice bands and large window areas at both Essex and Huron entries. The upper level plan has larger rooms at the corners with corner windows at each location. The rooms are designed for use as extended stay with kitchenettes provided in rooms, and many connecting rooms which will be desirable for the guests and families that are patients of the Ambulatory Care Center.

Sincerely,

DJR ARCHITECTURE, INC.

A handwritten signature in black ink, appearing to read "Scott Nelson". The signature is fluid and cursive, with the first name "Scott" and last name "Nelson" clearly distinguishable.

Scott Nelson
Principal

February 26, 2014

Mr. Cam Gordon
City of Minneapolis – Ward 2
350 South 5th Street, Room 307
Minneapolis, MN 55415

PROPOSED HOTEL ESSEX & HURON

PROJECT NARRATIVE

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DJR ARCHITECTURE, INC.

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Scott Nelson
Principal

ESSEX AND HURON HOTEL PROJECT

STATEMENT OF PURPOSE AND DESCRIPTION OF PROJECT

REVISED MARCH 24, 2014

Project Purpose:

The proposed 122-room hotel project is a redevelopment of the southwest corner of Essex Street SE and Huron Boulevard SE. The site currently contains 4 aged and run down student-rental houses and large, bituminous parking areas. The site is a block east of the University of Minnesota Ambulatory Care Center currently under construction. It is anticipated that many patients and their families will be guests at the hotel, as well as visiting professors, researchers and medical professionals working with the University.

Project Description:

The hotel will be 5 stories tall with underground parking for 41 cars. The building materials will be a blend of traditional and contemporary materials of cast stone, red brick and stucco with cornice bands and large window areas at entries on both Essex and Huron. The main entry is located on Essex, recessed below the upper levels in order to provide a safe drop off area with stacking space off the street and right-of-way. The lower level parking garage is accessed via a ramp from the east side on Essex and exits to a right turn only onto Huron.

The first floor plan places active uses along the Essex entry side and places the main breakfast room and lounge at the Essex Huron corner. There will be outdoor seating off this room adjacent to the sidewalk. The upper level plan has larger rooms at the corners with corner windows. The rooms are designed for use as extended stay with kitchenettes provided in rooms and many connecting rooms which will be desirable for the guests and families that are patients of the Ambulatory Care Center or working temporary assignments at the University. Extensive landscaping consisting of perennial flowers, shrubs and trees will be provided along both the Essex and Huron sidewalks.

Required Zoning Applications:

1. Rezoning from R5, Multiple-family District to C3A, Community Activity Center District to allow construction of a hotel.
2. Conditional Use Permit (CUP) to increase allowed height from 4 stories/56 feet to 5 stories/62 feet.
3. Floor area ratio (FAR) variance from 2.7, which would allow 53,411 SF of gross floor area, to 3.17 to allow the proposed 62,684 SF of gross floor area.
4. Yard variances from 5 feet to 3 feet in south side yard to allow uncovered driveway ramps and to allow a transformer in the east front yard.
5. Loading variance from two large loading spaces to no designated loading space.
6. Site plan review.

REZONING REQUIRED FINDINGS

The site is currently zoned R5, Multiple-family District. This application seeks approval of rezoning to the C3A, Community Activity Center District. Rezoning to C3A is requested to allow for the proposed hotel use. C3A is the least intensive district in which a hotel is allowed. The proposed rezoning is consistent with the required findings under § 525.280 of the Zoning Code.

1) *Whether the amendment is consistent with the applicable policies of the comprehensive plan.*

As reflected in the *Stadium Village University Avenue Station Area Plan* (the SV Plan) adopted by the City Council in August 2012, the future land use guidance for the project site is Public and Institutional because it is within an area identified for potential expansion of the University and University-related uses. The site is also associated with the University of Minnesota Growth Center and is within a half block of the Activity Center. It is three blocks from a Green Line LRT station and within a Transit Station Area. Although Huron Boulevard is not a designated Commercial or Community Corridor, it is a major route for traffic into and out of the University area via the Huron – I94 ramp just south of the project site. Huron is also a major bus transit route.

Per the guidance in the *Minneapolis Plan for Sustainable Growth*, (the MPSG), Public and Institutional (PI) accommodates public and semi-public uses, including museums, hospitals, civic uses, stadiums, airport related uses, and college and university campuses. The MPSG states that residential use “is generally not appropriate for these areas.” Thus, rezoning to a non-residential district is consistent with the recently adopted future land use goals of the SV Plan. Further, an extended stay hotel is an important accessory use and support service to the nearby medical and University campus uses in the PI area. Therefore, rezoning to C3A, the least intensive district in which hotels are allowed, is consistent with the purpose of PI-designated areas.

Similarly, the proposed C3A zoning to allow a hotel use is consistent with the following MPSG policy for Growth Centers:

Land Use Policy 1.15: Support development of Growth Centers as locations for concentration of jobs and housing, and supporting services.

1.15.4 Promote the integration of major public and private institutional campuses located in Growth Centers, including health care and educational services, with the function and character of surrounding areas.

The hotel will provide a much-needed support service to the nearby medical and educational facilities. The hotel use and new development will serve as a transition between those facilities and the surrounding area and will be compatible in function and character with the trend of redevelopment along Huron and in the Stadium Village Activity Center.

The MPSG describes Activity Centers as “the places that shape Minneapolis’ urban identity. They attract residents, workers, and visitors from throughout the city and region. Activity Centers support a wide range of commercial, office, and residential uses.” There is a great need for additional hotel amenities that the C3A zoning district allows for visitors to both the University and the Stadium Village Activity Center. C3A zoning is traditionally associated with

Activity Centers, but is not strictly limited to sites within those boundaries. (Although outside of the Stadium Village Activity Center, 2725 University Avenue was rezoned to C3A in 2005.) Rezoning the project site to C3A will be compatible with and support the uses within and around the Activity Center boundary which is just a half block away.

Rezoning of this site is also consistent with the Land Use Policy 1.13 related to Transit Station Areas:

Land Use Policy 1.13: Support high density development near transit stations in ways that encourage transit use and contribute to interesting and vibrant places.

Rezoning to C3A will allow a hotel near a transit station that can be utilized by visitors and guests who do not otherwise require personal vehicles.

The site is also located within the Huron Boulevard Corridor Character Area of the SV Plan, which is described as “a transitional zone which has seen a fair amount of redevelopment in recent decades from industrial to high density residential.” The SV Plan includes the following land use recommendations for this area:

- Generally speaking support high density residential development in this area.
- Encourage land uses along Huron to support a pedestrian oriented environment, balanced against heavy vehicle traffic flow, and acknowledging its role as a gateway to the area.

Consistent with this guidance, C3A zoning allows for high density residential development. C3A zoning also responds to the more specific land use guidance in the SV Plan for Public and Institutional (as opposed to residential) land use on this block. The proposed rezoning will promote redevelopment along Huron that will significantly improve a very challenged pedestrian environment. The hotel plan provides for active uses at the corner of Essex and Huron, outdoor seating and benches, landscaping and trash and recycling receptacles along the street. The streetscape will be clean, safe and well-lit. The project should catalyze further redevelopment along the west side of Huron and reinforce the “gateway” character of Huron near the I 94 intersection.

2) Whether the amendment is in the public interest and is not solely for the interest of a single property owner.

The proposed rezoning will enhance the redevelopment potential of the site, but it is not solely for the interest of the property owner. Rezoning to C3A is in the public interest because it will allow for a level of density and the type of commercial use that will sustain and enhance the University Growth Center, the SV Activity Center and the Transit Station Area. A hotel will serve an unmet demand for lodging in the area, in particular extended stay lodging suitable for guests who are patients at area medical facilities or visiting University employees.

3) Whether the existing uses of property and the zoning classification of property within the general area of the property in question are compatible with the proposed zoning classification, where the amendment is to change the zoning classification of a particular property.

The existing zoning on this block is R5 and it is occupied primarily by 2-story rental houses and small apartment buildings. More 2- and 3-story apartment buildings zoned R5 are located on the block to the north across Essex. The blocks to the south and southwest are also zoned R5 and contain a variety of rental housing. Although also zoned R5, the block to the east is being redeveloped by the University as the Ambulatory Care Center (University development is generally exempt from compliance with City zoning regulations). Over the last couple decades, properties along the east side of Huron have been redeveloped with high density, 4, 6 and 10 story apartments targeted for students on land zoned R6, C3A and I1/ILOD. Starting a half block to the north, the zoning becomes OR2 , C2 and C3A and there are a variety of high density residential, commercial and mixed use developments. C3A zoning and the proposed hotel use are compatible with the variety of zoning classifications and uses within the general area.

4) Whether there are reasonable uses of the property in question permitted under the existing zoning classification, where the amendment is to change the zoning classification of particular property.

There are reasonable uses of the property permitted under the R5 District, however, R5 zoning is less compatible than is C3A with the Public and Institutional guidance for the site and will not allow for a hotel, which is a particularly desirable support service and amenity to complement the surrounding Growth Center, Activity Center and Transit Station Area land use features.

5) Whether there has been a change in the character or trend of development in the general area of the property in question, which has taken place since such property was placed in its present zoning classification, where the amendment is to change the zoning classification of particular property.

The SV Plan identifies the Huron Boulevard Corridor as a transitional zone that has seen a fair amount of redevelopment, which is expected to continue. Most redevelopment along Huron and in the SV Activity Center to date has been high density residential and mixed-use development. The University's development of an Ambulatory Care Center a block west of the site further diversifies the type of redevelopment in the area. The character of redevelopment in the area supports the proposed C3A zoning and the need for the complementary hotel use it will allow.

**CONDITIONAL USE PERMIT FOR ADDITIONAL HEIGHT
REQUIRED FINDINGS**

A conditional use permit is being applied for to allow a building height of 5 stories/ 62 feet, which exceeds the 4-story, 56-foot limit of the C3A District. The request for increased height for the project meets the required findings for the issuance of a conditional use permit under § 525.340 and the additional considerations for increased height in § 548.110 of the Zoning Code.

1) That the establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.

A new, 5-story building will not be detrimental to or endanger the public health, safety, comfort or general welfare. The majority of the building is shorter than the 56-foot limit of the C3A (and the R5) District. The height to the top of the parapet is 55 feet. Only the roof line above the Huron entry is raised to 62 feet as an architectural accent to emphasize the entry. The development will comply with all applicable building codes, life safety ordinances and Public Works requirements.

2) The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district or substantially diminish property value.

The proposed 5-story building will not be injurious to the use and enjoyment of other property. As noted above, the majority of the building will comply with the general 56-foot limit. It will not impede the normal and orderly development of the area. The height of the building will be compatible with the recent redevelopment along the east side of Huron with buildings that are 6 and 10 stories.

3) Adequate utilities, access roads, drainage, necessary facilities and other measures have been or will be provided.

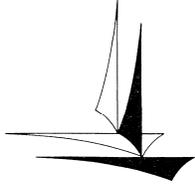
Adequate utilities, access, drainage, and other necessary facilities will be provided for the project and the development team will continue to work closely with Public Works, Plan Review and Planning staff to comply with City and other applicable requirements.

4) Adequate measures have been or will be taken to minimize traffic congestion in the public streets.

The additional height of the building will have no impact on traffic congestion in the public streets. The project will meet the parking requirements of the Zoning Code. The project team has worked closely with Public Works and Planning to design the proposed access and egress for the underground parking and an on-site drop off/pick up drive at the Essex entry to prevent on-street vehicle loading.

5) The conditional use is consistent with the applicable policies of the comprehensive plan.

As discussed above, the proposed rezoning to C3A to allow for a hotel is consistent with the applicable land use policies of the City's comprehensive plan and the Stadium Village small area plan. The proposed height is also consistent with the urban design policies in Chapter 10 of the



Minneapolis Development Review
250 South 4th Street
Room 300
Minneapolis, MN 55415

Preliminary Development Review Report

Development Coordinator Assigned: **DONALD ZART**
(612) 673-2726
don.zart@minneapolismn.gov

Status *
RESUBMISSION REQUIRED

Tracking Number:	PDR 1001179
Applicant:	CPM DEVELOPMENT 2919 KNOX AVE S MINNEAPOLIS, MN 55408
Site Address:	506 HURON BLVD SE 1014 ESSEX ST SE 1018 ESSEX ST SE 1022 ESSEX ST SE
Date Submitted:	11-MAR-2014
Date Reviewed:	20-MAR-2014

Purpose

The purpose of the Preliminary Development Review (PDR) is to provide Customers with comments about their proposed development. City personnel, who specialize in various disciplines, review site plans to identify issues and provide feedback to the Customers to assist them in developing their final site plans.

The City of Minneapolis encourages the use of green building techniques. For additional information please check out our green building web page at: http://www.ci.minneapolis.mn.us/mdr/GreenBuildingOptions_home.asp.

DISCLAIMER: *The information in this review is based solely on the preliminary site plan submitted. The comments contained in this report are preliminary ONLY and are subject to modification.*

Project Scope

122 room , five story hotel

Review Findings (by Discipline)

Fire Safety

- Provide required fire suppression system throughout building
- Fire department connection must be located on the address side of building and within 150 feet of a fire hydrant
- Provide required fire alarm system throughout building
- Maintain fire department access at all times

*Approved: You may continue to the next phase of developing your project.

*Resubmission Required: You cannot move forward or obtain permits until your plans have been resubmitted and approved.

□ Zoning - Planning

- New hotel project requires the following land use applications that will be decided by the Planning Commission. The Planning Commission is scheduled to review the project on April 7, 2014
 - Petition to rezone the properties from the R5/Multiple Family District to the C3A/Community Activity Center District.
 - Conditional use permit application to increase the maximum permitted height of a commercial building from 4 stories or 56 feet to approximately 5 stories or 63 feet at the tallest point.
 - Variance to increase the maximum floor area ratio from 2.7 to 3.2.
 - Variance to reduce the interior setback (south) from 5 feet to 1.9 feet to allow for an uncovered driveway.
 - Variance to reduce the front yard requirement adjacent to the south lot line to allow a transformer.
 - Variance to reduce the number of off-street loading spaces from 2 to 0.
 - Site plan review.

□ Addressing

- The proposed address will be 1020 Essex St SE.
- This building is also considered to have a parking ramp per MCO Chapter 108. As such, within 5 years of the date of the certificate of occupancy being issued, the parking ramp will be required to have annual inspections and apply for a Ramp Operating Certificate.
- When assigning suite sequences the following guidelines are as follows:
 - The first one to two digits of the suite sequence number will designate the floor number of the site.
 - The last two digits of the suite sequence number will designate the unique ID for the unit (condo, suite, unit, or apartment).
 - Suite sequence digit numbers will be assigned to dwelling, commercial and retail units, not common areas. For example, laundry rooms, saunas, workout rooms, etc., would not be assigned numbers.
 - Please provide each condo, suite, unit or apartment number.

□ Parks - Forestry

- Contact Paul Martinson (612-499-9209) regarding removal or protection of trees during construction in the city right of way.
- Effective January 1, 2014, the City of Minneapolis and the Minneapolis Park and Recreation Board adopted an update to the existing Parkland Dedication Ordinance.
- The adopted City of Minneapolis Parkland Dedication ordinance is located in Section 598.340 of the City's Land Subdivision ordinance:
- <http://library.municode.com/index.aspx?clientId=11490>
- As adopted, the fee in lieu of dedication for new residential units is \$1,500 per unit (affordable units excluded per ordinance) and for commercial and industrial development it is \$200 per development employee (as defined in ordinance). Any dedication fee (if required) must be paid at the time of building permit issuance. There is also an administration fee that is 5% of the calculated park dedication fee.
- As proposed, for your project the hotel on Essex & Huron, the calculated dedication fee is as follows:

• Fee based on employee per sq ft	=	\$12,600
• 5% of \$12,600 (Administration Fee)	=	\$ 630
• Total	=	\$13,230
- This is a preliminary calculation based on your current proposal; a final calculation will be made at the time of building permit submittal.
- For further information, please contact Don Zart at 612-673-2726.

❑ Right of Way

- An encroachment permit shall be required for all streetscape elements in the Public right-of-way such as: plants & shrubs, planters, tree grates and other landscaping elements, sidewalk furniture (including bike racks and bollards), and sidewalk elements other than standard concrete walkways such as pavers, stairs, raised landings, retaining walls, access ramps, and railings (NOTE: railings may not extend into the sidewalk pedestrian area). Please contact Bob Boblett at (612) 673-2428 for further information.
- Note to the Applicant: Any elements of an earth retention system and related operations (such as construction crane boom swings) that fall within the Public right-of-way will require an encroachment permit application. If there are to be any earth retention systems which will extend outside the property line of the development then a plan must be submitted showing details of the system. All such elements shall be removed from the Public right-of-way following construction with the exception of tie-backs which may remain but must be uncoupled and de-tensioned. Please contact Bob Boblett at (612) 673-2428 for further information.
- In addition, any elements of an earth retention system and related excavations that fall within the Public right-of-way will require a "Right-of-Way Excavation Permit". This permit is typically issued to the General Contractor just prior to the start of construction. However, it is the Applicant's responsibility to insure that all required permits have been acquired by its consultants, contractors, sub-contractor's prior to the start of work.
- Contact Paul Cao at (612) 673-2943 for position and alignment of the bike racks proposed in the Public right-of-way. If the racks are privately owned, they will require an encroachment permit.

❑ Street Design

- The plan as submitted meets the requirements of the Public Works Street Design Division.

❑ Sidewalk

- ADA compliant pedestrian ramps are required at each crosswalk at the intersection of Essex St. S.E. and Huron Boulevard S.E. Show the pedestrian ramps on all related plan sheets. Construct two (2) ADA compliant pedestrian ramps at this location

❑ Traffic and Parking

- The Applicant shall insure that a 20'x20' clear unobstructed sight triangle is maintained at the exit points. All vehicle access points to the underground ramp shall not have more than a 4% slope for the first 20' from the property line (show ramp slope on your plans). In the event that it is not possible to achieve these requirements the Applicant shall provide sufficient pedestrian safety mitigation measures such as pedestrian warning signage, flashers, and audible warning devices.
- The ramp exit shall be signed as a "right-out" only.
- Previous discussions regarding direction of traffic flow within the "drop-off" area do not match the proposed site plan. Redirect the traffic flow in the drop off area so vehicles have direct access to the ramp.
- Contact Allan Klugman(612) 673-5750 prior to construction for the temporary removal/temporary relocation of any City of Minneapolis signal system that maybe near the construction limits as the construction project will assume all costs.
- Please contact Bill Prince at (612) 673-3901 regarding existing and proposed street lighting. All street lighting (existing and proposed) shall be shown clearly on the site plan.

❑ Water

- The plan as submitted meets the requirements of the Public Works Water Maintenance & Distribution Division.
- Note to the Applicant: It is recommended that the size of the 6 x 6" domestic line could be reduced to a 6 x 4" service line. Please contact Rock Rogers at (612) 673-2286, to review domestic and fire service design, connections, and sizes.

❑ Business Licensing

- If a food related business will be occupying a portion of this building please work with Becky Anger (612)673-2690 for the Health Department review.

❑ Environmental Health

- Properties in the area have been identified with soil contamination. The soil contamination may have extended offsite and affected adjacent properties. It is recommended the developer/owner contact the Minnesota Pollution Control Agency Voluntary Investigation and Clean-up Program to review the files. If the contamination is present a letter of no association for the existing site contamination should be requested. As part of this program the developer/owner typically will only be required to address impacted soil disturbed during site activities. If contaminated soil is encountered it must be reported to the Minnesota Duty Officer at 651-649-5451.
- Several projects in the area have had to dewater installation of footings and foundation. If dewatering is required during site construction see below for city permit requirements. Subgrade structures should be designed to prevent infiltration of groundwater without the need for a permanent dewatering system being installed. If a continuously operating permanent dewatering system is needed it must be approved as part of the sanitary sewer and storm drain site plan approval prior to construction beginning.
- No construction, demolition or commercial power maintenance equipment shall be operated within the city between the hours of 6:00 p.m. and 7:00 a.m. on weekdays or during any hours on Saturdays, Sundays and state and federal holidays, except under permit. Contact Environmental Services at 612-673-3867 for permit information.
- Permits and approval are required from Environmental Services for the following activities: Temporary storage of impacted soils on site prior to disposal or reuse; Reuse of impacted soils on site; Dewatering and discharge of accumulated storm water or ground water, underground or aboveground tank installation or removal, well construction or sealing. Contact Tom Frame at 612-673-5807 for permit applications and approvals.
- A review of the project, permits issued and an inspection from Environmental Service for identification of equipment and site operations that require annual registration with the City of Minneapolis will occur for this project.

❑ Sewer Design

- Groundwater: Please provide a copy of any geotechnical reports for the site. It must be adequately demonstrated that pumping of groundwater will not be necessary in order to keep the below grade areas dry. Please note that typical soil borings, completed for the purpose of designing building pads and foundations, may not give an accurate determination of the seasonal high groundwater elevation on a site. This should be thoroughly evaluated.
- Stormwater Management: Please identify the location of any proposed foundation or drain tile connections or discharges.
- Utility Connections: Please note the existing sanitary sewer service connections are the responsibility of the property owner, including within the right-of-way to the main. If any damage occurs due to failure of any service the property owner is liable for the damage. If the services are removed to the main, they would need to be completely removed from the right-of-way and bulkheaded at the main.
- Please note on the plan that the proposed storm sewer connection to the main must be made with a saddle tee fitting, per City of Minneapolis Standard Supplemental Specifications.
- Utility: Records indicate an existing City storm sewer main in Huron Blvd SE, west of the tunnel. Please show this pipe on the plans with the corresponding pipe size, type and invert elevations. For City sanitary and storm sewer infrastructure records contact (612) 673-2405. If feasible, the proposed storm sewer connection should be made to this pipe instead of the tunnel, as proposed.
- For comments or questions on Public Works Surface Water & Sewers Division related requirements please contact Jeremy Strehlo, (Professional Engineer) at (612) 673-3973, or jeremy.strehlo@minneapolismn.gov

❑ Historical Preservation Committee

- Any time there is a demolition proposed for an existing structure a review by city staff will perform a review for Historic Preservation. Please have a licensed wrecking contractor submit for their wrecking permits for the existing structures.

❑ Construction Code Services

- Dining Lounge appears to have an occupant load that will require 2 means of egress.
- If the trash chute access rooms are accessible to guests then a turn around space is required inside the room.
- If looking to schedule a Preliminary Plan Review appointment, please call 612-673-5839 to set this meeting up.
- Ensure that the laundry termination room meets the requirements of 707.13.4
- West entry / exit forms an exit court less than 10' wide. 1-hr protection is required. 1024.5.2.
- Contact the Met Council regarding receiving a SAC determination for increased water usage. See this link for more information.
http://www.minneapolismn.gov/www/groups/public/@regservices/documents/webcontent/convert_281675.pdf

END OF REPORT

MPSG that generally conclude that medium-scale development such as the proposed project is appropriate in locations like the project site that are located in Activity Centers and Growth Centers. See Urban Design Policy 10.5. The project design is also consistent with the guidance of Urban Design Policy 10.9 regarding development in mixed-use areas to “[s]upport urban design standards that emphasize traditional urban form with pedestrian scale design features at the street level in mixed-use and transit-oriented development” and to “[c]oordinate site designs and public right-of-way improvements to provide adequate sidewalk space for pedestrian movement, street trees, landscaping, street furniture, sidewalk cafes and other elements of active pedestrian areas.” The site plan for the project will add vitality and safety to the pedestrian realm with active ground floor uses, sidewalk seating, landscape buffers, and improved lighting. The vehicle access areas have been carefully designed to minimize vehicle and pedestrian conflicts.

6) *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

With approval of the other zoning applications for the project, it will conform to the applicable regulations of the C3A zoning district.

Additional factors to be considered when determining an increase in height per §548.110.

(1) *Access to light and air of surrounding properties.*

Public right-of-way separates the proposed development from other properties to the east and north. The building will exceed with the 13-foot yard requirements for hotel uses in the commercial districts and adjacent to residential districts. The proposed 5-story building will not impede access to light and air for these surrounding properties.

(2) *Shadowing of residential properties, significant public spaces, or existing solar energy systems.*

The building will not shadow significant public spaces or known solar energy systems. It will cast shadows on residential properties typically to be expected from buildings with heights allowed in the surrounding R5 district. Shadowing on the 2- and 3-story apartments to the north and the 6-story building to the east will be mitigated by the width of the streets between the developments.

(3) *The scale and character of surrounding uses.*

The majority of the structures on the block are 2-story dwellings and small apartment buildings. The 5-story hotel will be taller than those older structures, but consistent with the building scale allowed in the R5 District in which those properties are located. The building stock on the block is not of high quality and redevelopment with density and height similar to the project is desirable. The same is true for the 2- and 3-story apartments across Essex to the north. The height of the proposed project is less than recent 6-story apartment developments across Huron to the east.

(4) *Preservation of views of landmark buildings, significant open spaces or water bodies.*

The Project will not block views of landmark buildings, significant open spaces or water bodies.

**VARIANCE FOR FLOOR AREA RATIO
REQUIRED FINDINGS**

A variance is being requested to increase the allowed FAR from 2.7, which would allow 53,411 SF of gross floor area, to 3.17 to allow the proposed 62,684 SF of gross floor area. The Project meets the required findings for a variance under § 525.500 of the Zoning Code.

1) Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.

Practical difficulties exist because the land use guidance applicable to the site (described in the rezoning and CUP findings) calls for higher density development than is allowed for a hotel use without an FAR variance. Unlike residential-only projects or mixed-use with residential projects, hotels do not receive an automatic 20% density bonus for enclosed parking. If they did, the project would be allowed a FAR of 3.24 without need for a variance. This seems to be an unintended consequence of the zoning regulations, as there is no policy reason to limit the scale of hotel development compared to residential and mixed use projects located in the same zoning districts. These are unique circumstances, not created by the applicant.

2) The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.

The proposed design and FAR of the project are reasonable and consistent with the spirit and intent of the ordinances, the comprehensive plan policies applicable to the site, and the purpose of the C3A District. The increase in FAR allows for a 5th story, but the building height will generally comply with the 56-foot height limit and the building setbacks will meet code requirements. The increase density, therefore, will not be discernable compared to what would be allowed without a variance.

3) The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

The granting of the variance will not alter the essential character of the area, be injurious to the use or enjoyment of other properties, or be detrimental to the public welfare. The proposed number of stories and FAR are in character with recent 6-story developments across Huron at 515 Huron (6 stories, 3.03 FAR) and 2428 Delaware (6 stories, 3.59 FAR). As noted above, the increased density will not be discernable compared to a 4-story building that could be built on the site without a variance. The building will comply with all building and life safety codes.

**VARIANCE FOR YARD SETBACKS
REQUIRED FINDINGS**

Variances are being requested to allow a reduction of the required yard along south interior lot lines for the uncovered driveway ramp and to allow placement of a transformer in the east front yard. Uses in the Commercial zoning districts are not generally subject to yard requirements; however, this site is subject to 15-foot front yard setbacks along Essex and Huron for the first 25 feet from the adjacent properties because those properties have residential (R5) zoning. The property is also subject to a 13-foot interior side and rear yard setback requirement due to the adjacent residential zoning. The building itself will meet the required front, side and rear setbacks. The variances will allow the uncovered portion of the egress ramp to Huron to be within 3 feet of the south lot line (the uncovered drive surface is 3 feet from the lot line; a permitted retaining wall is within 2 feet of the lot line). Variance of the required front yard along Huron will allow the transformer to be placed approximately 9 feet from the east front lot line. The Project meets the required findings for a variance under § 525.500 of the Zoning Code.

1) Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.

Practical difficulties in meeting the setback requirements result from the small size of the site. The drive ramps have to be located at the edges of the property and underground garage in order to allow for required circulation and parking stalls. The design incorporates separate ingress and egress ramps based on consultation with Public Works to mitigate congestion and improve traffic flow on Essex and Huron. The necessary location of the open ramps limits accessible locations for a transformer to near the street frontages so that the transformer is accessible to Xcel. The proposed transformer location is the least obtrusive of these limited locations. These are unique circumstances not created by the applicant.

2) The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.

The proposed locations of the driveway ramp and transformer are reasonable and consistent with the intent of the ordinances, the comprehensive plan policies applicable to the site, and the purpose of the C3A District. The proposed driveways would be permitted obstructions in the required yards if the project were a residential use instead of a hotel. The intent of the ordinance will be met by providing fencing and landscaped screening between the drive ramps and the property lines. The impact of the egress driveway to Huron will be mitigated because it will ramp from below grade. The transformer will be separated from the residence district to the south by approximately 16 feet and will be 9 feet back from the east front property line and the public sidewalk. The proposed location is less visible from the public realm and less impactful on adjacent residential uses than other potential locations where it could be placed along Essex and Huron that are not subject to a front yard setback.

3) The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will

not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

The granting of the variances will not alter the essential character of the area, be injurious to the use or enjoyment of other properties, or be detrimental to the public welfare. As noted above, the same drive ramp would be a permitted obstruction for a residential project that could be built in the adjacent R5 district. The impacts of the driveway will be mitigated by screening. The transformer will be at least 16 feet from the residence to the south and separated from it by a driveway. The variances will not be detrimental because the design will comply with building and life safety codes.

**VARIANCE FOR LOADING
REQUIRED FINDINGS**

A variance is being requested from the requirement that two large loading spaces for the hotel use be provided within the project. Deliveries for the hotel will be made by small trucks that can use the on-site drop off zone and the main entry. Trash pickup will occur in the underground garage, but does not require a dedicated loading zone. The Project meets the required findings for a variance under § 525.500 of the Zoning Code.

1) Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.

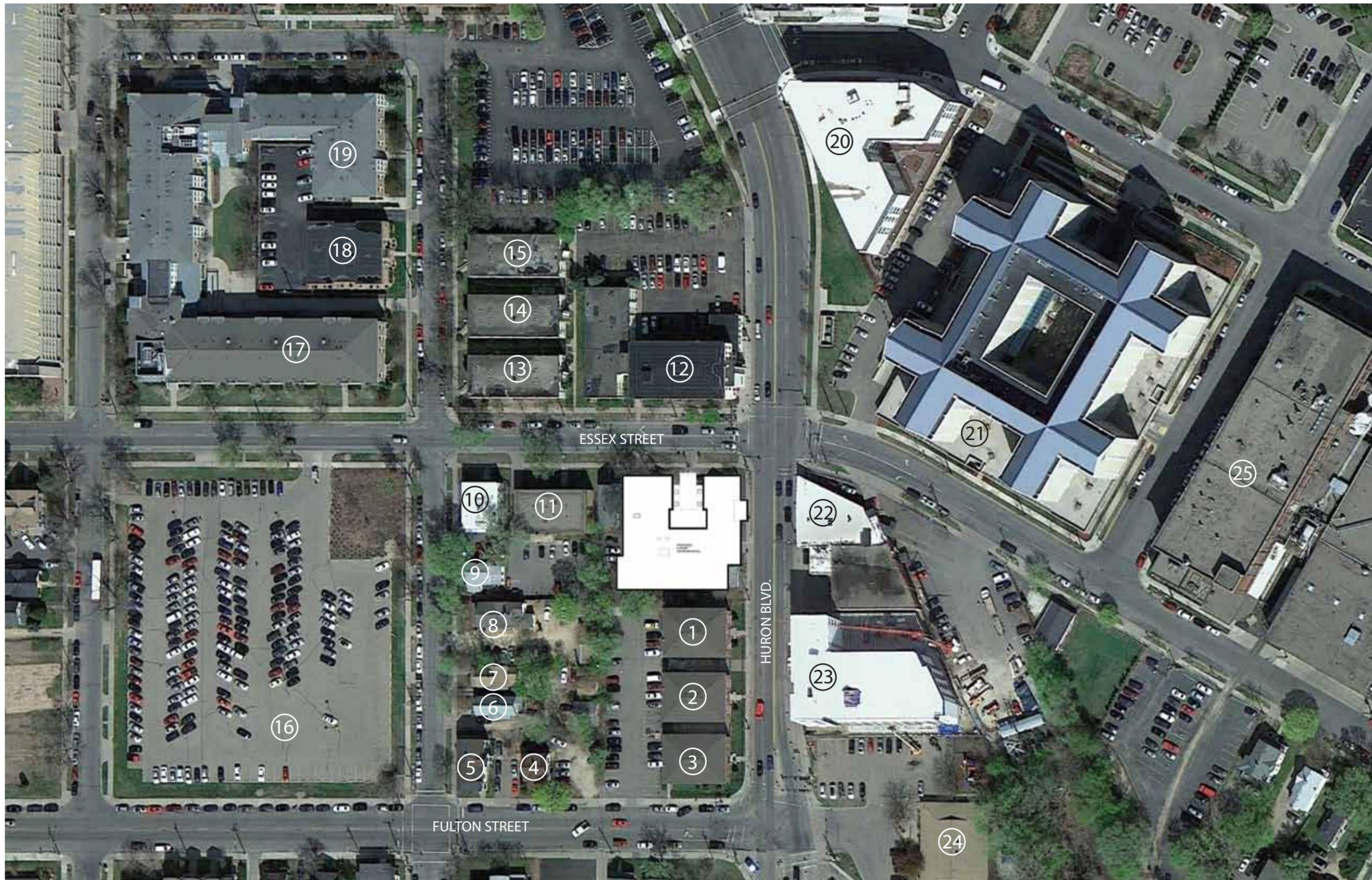
Practical difficulties exist in complying with the loading requirement because of the small size of the site. There is not room for on-site maneuvering and parking for large trucks, either at or below grade. The loading requirement of the Zoning Code is excessive for an extended stay hotel without restaurant or meeting hall components. Large truck deliveries will not be necessary to serve the hotel. These circumstances are unique to the property and have not been created by the applicant.

2) The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.

The proposed use of the on-site drop off zone for deliveries is reasonable for this type of hotel. It is in keeping with the spirit and intent of the City's ordinances and comprehensive plan policies to not require dedicated loading areas when the actual demand for an off-street loading space can be met in a way that makes more efficient use of the site.

3) The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

The requested loading variance will not alter the character of the locality or be detrimental to the public or surrounding properties because loading activity will, nevertheless, occur on site.



- 1. 3-Story Apartment
- 2. 3-Story Apartment
- 3. 3-Story Apartment
- 4. 2-Story House
- 5. 2-Story House
- 6. 2-Story House
- 7. 2-Story House
- 8. 2-Story House
- 9. 2-Story House
- 10. 2-Story Apartment
- 11. 3-Story Apartment
- 12. 3-Story Apartment
- 13. 3-Story Apartment
- 14. 3-Story Apartment
- 15. 3-Story Apartment
- 16. Construction Site
- 17. 3-Story Apartment
- 18. 3-Story Apartment
- 19. 3-Story Apartment
- 20. 6-Story Apartment
- 21. 10-Story Apartment
- 22. 2-Story Commercial
- 23. 6-Story Apartment
- 24. 1-Story Commercial
- 25. 4-Story Commercial

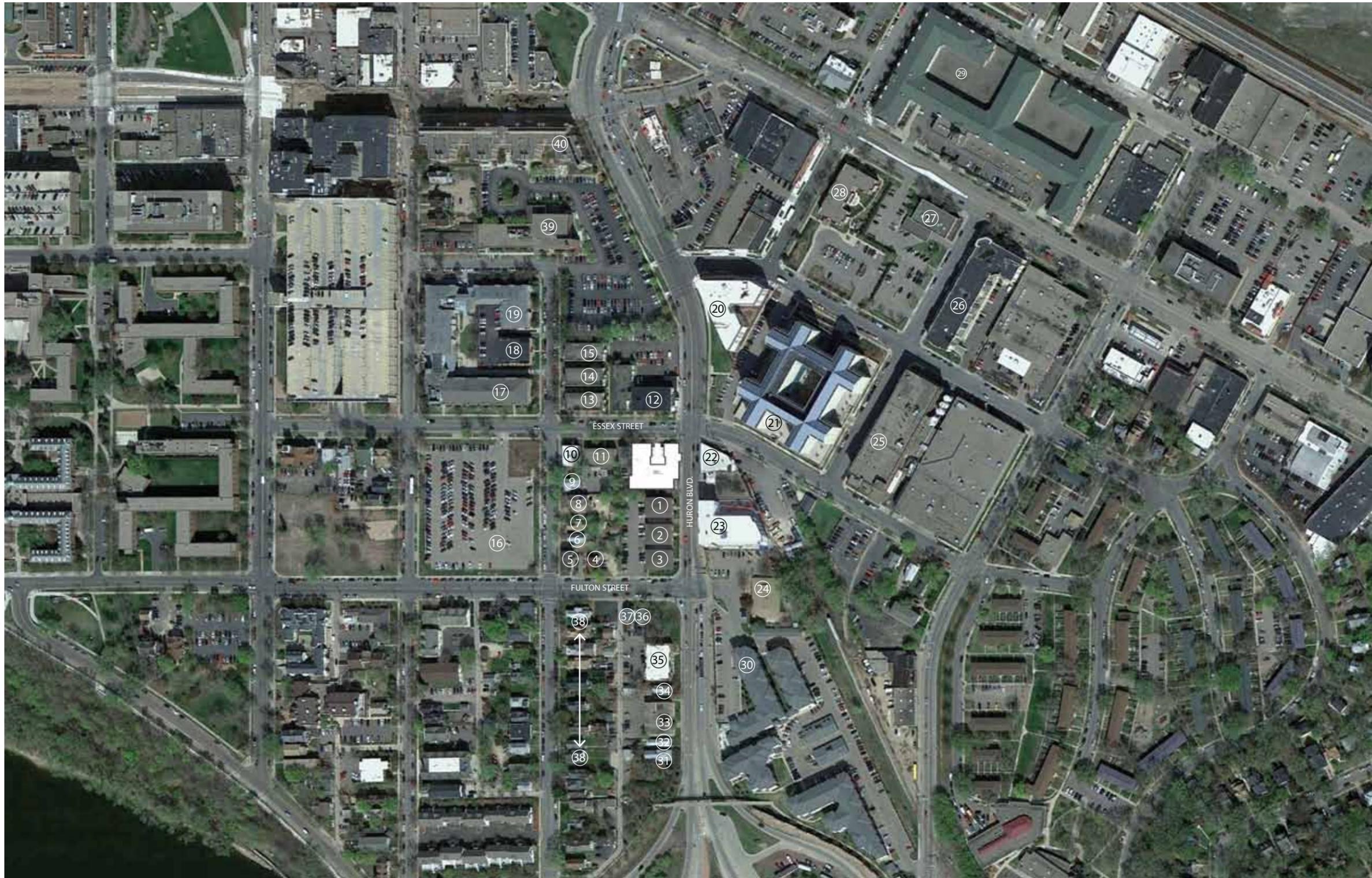
3 BLOCK NEIGHBORHOOD CONTEXT PLAN - MINNEAPOLIS

Minneapolis, Minnesota

January 2, 2014

Arial View

113-0110



1. 3-Story Apartment
2. 3-Story Apartment
3. 3-Story Apartment
4. 2-Story House
5. 2-Story House
6. 2-Story House
7. 2-Story House
8. 2-Story House
9. 2-Story House
10. 2-Story Apartment
11. 3-Story Apartment
12. 3-Story Apartment
13. 3-Story Apartment
14. 3-Story Apartment
15. 3-Story Apartment
16. Construction Site
17. 3-Story Apartment
18. 3-Story Apartment
19. 3-Story Apartment
20. 6-Story Apartment
21. 10-Story Apartment
22. 2-Story Commercial
23. 6-Story Apartment
24. 1-Story Commercial
25. 4-Story Commercial
26. 4-Story Apartment
27. 2-Story Bank
28. 3-Story Office
29. 4-Story Mixed Use
30. 4-Story Apartment
31. 2-Story House
32. 2-Story House
33. 3-Story Apartment
34. 2-Story House
35. 3-Story Apartment
36. 2-Story House
37. 2-Story House
38. 2-Story Houses
39. 3-Story U of M
40. 4-Story Office

LARGE NEIGHBORHOOD CONTEXT PLAN - MINNEAPOLIS

Minneapolis, Minnesota

January 2, 2014

Arial View

113-0110









1. Mixed - use and urban density to accommodate all the people who want work and live in the District

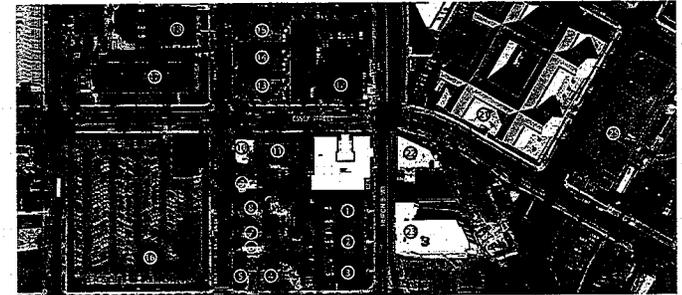
1.1 ++ + 0 - -- Provide a mix of uses for the diversity of people who live, work, learn, do business and visit in the district.

The proposed hotel brings a new use to the area between I-94 and the U of M campus. Designed as an extended stay hotel with kitchenettes in most units the hotel will provide a necessary lodging for the patients, and their families, of the new Ambulatory Care Center as well as for visiting professors and researchers working with the University.



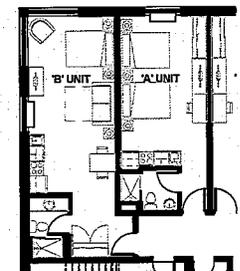
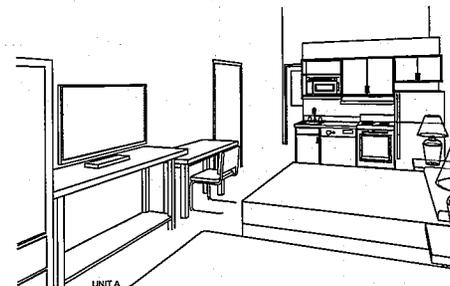
1.2 ++ + 0 - -- Provide the density to support a walkable, transit-oriented urban place, with access to services and amenities.

The proposed 122 room hotel is on a major bus transit line and is within walking distance of the University of Minnesota's East Bank Campus, and its many athletic venues. The project is also one block from the new Ambulatory Care Center. The hotel is being developed to provide guests lodging options and amenities that are unique to the needs of patients from the Care Center. These will include food and beverage options, and multiple connecting rooms for families and care givers.



1.3 ++ + 0 - -- Contribute to the variety of unit types and rents to accommodate those who want to live in the District.

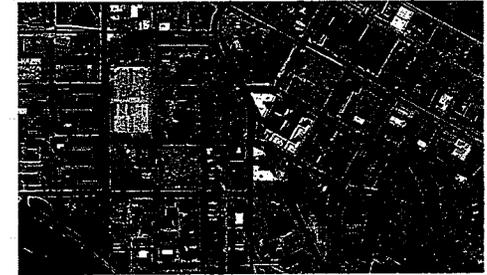
Providing an extended stay hotel creates a new lodging option for people who need regular housing in the District. This will include University staff as well as patients of the Ambulatory Care Center.



2. Connections to create a pedestrian friendly, transit-oriented community

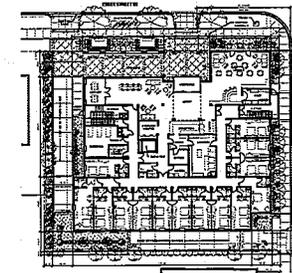
2.1 ++ + 0 - -- Create a walkable, bikeable district, with connectivity within the District and to the rest of the city and region.

The location on Essex and Huron will provide easy non-automotive connectivity to the U of M East Bank as well as a short walk to the new Ambulatory Care Center located a block away. The site is located equidistant from the East River Parkway bike path and the University of Minnesota Transitway, providing a connection to the St. Paul campus and the West Bank for residents and guests of the proposed hotel. The hotel will seamlessly tie into the existing network of bike paths and pedestrian friendly routes already available in the area by providing its main entry on the more pedestrian and bike friendly Essex Street.



2.2 ++ + 0 - -- Support a high quality pedestrian network and public realm and a network of public transit.

The hotel plan provides for active uses at the corner of Essex and Huron with outdoor seating and benches, landscaping and trash and recycling receptacles located along both streets. The streetscape will be clean, safe and well-lighted at night to contribute to and enhance a pedestrian friendly neighborhood. Four bus lines have routes along Huron, allowing for easy connection to the Stadium Village light rail station at University and Huron located two blocks north of our site.



2.3 ++ + 0 - -- Reinforce neighborhood connectivity through the site plan, the architecture and the landscape design.

The hotel's design will foster an active streetscape, separation of car and pedestrian traffic and a variety of landscape materials intended to enhance the pedestrian experience along the streets. The exterior facade of the hotel will include a combination of brick and stone at street levels to provide a visually pleasing human scale that is intrinsic to masonry.



UNIVERSITY DISTRICT DEVELOPMENT PRINCIPLES

Minneapolis, Minnesota

February 28, 2014

HURON & ESSEX HOTEL

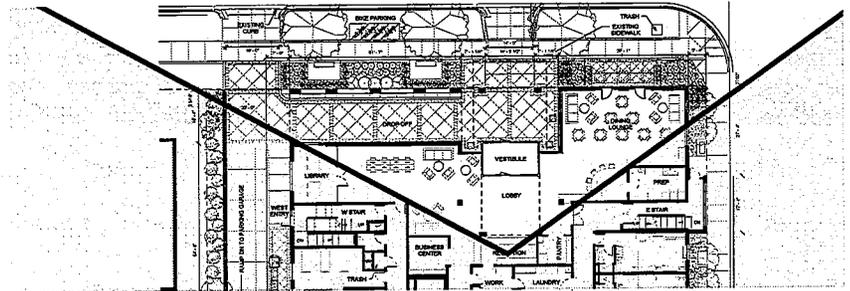
113-0110



3. Public realm enhanced and enlivened by the adjacent buildings

3.1 ++ + 0 - -- Enhance the safety and friendliness of the street through the street-level design of the building.

Providing active uses and “eyes on the street” is an important part of the hotel’s street level plan. The first floor provides large window access on the corner and Essex side. The entry is recessed to provide a safe drop off for guests and to separate traffic from the street to preserve the bike lane. This is required by the City of Minneapolis Traffic Department.



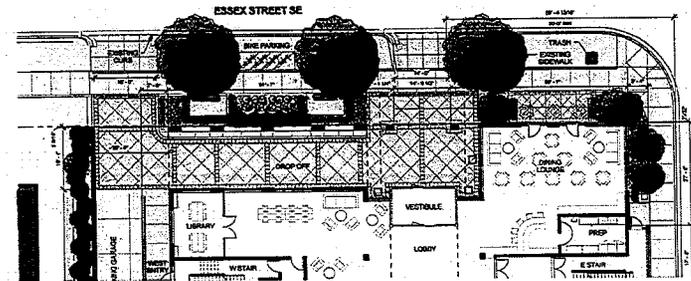
3.2 ++ + 0 - -- Enliven the street with active spaces and entry ways.

Entries to the hotel are provided on both streets. The breakfast room and lounge located on the corner, is the largest active space. It will be the “heart” of the hotel and will be occupied most of the day by guests and will also serve as a place for guest to meet friends. Sidewalk seating will also be provided on the Essex side and benches are provided on both Essex and Huron.



3.3 ++ + 0 - -- Incorporate landscaping, appropriate to local conditions, that contributes to a healthy urban ecology.

The landscape plan will feature perennial flowers and flowering shrubs that provide for a variety of seasonal colors. In addition to these, trees will be planted along both Essex and Huron to provide shade along the sidewalk for pedestrians and enhance the streetscape along the Huron Corridor.



UNIVERSITY DISTRICT DEVELOPMENT PRINCIPLES

Minneapolis, Minnesota

February 28, 2014

HURON & ESSEX HOTEL

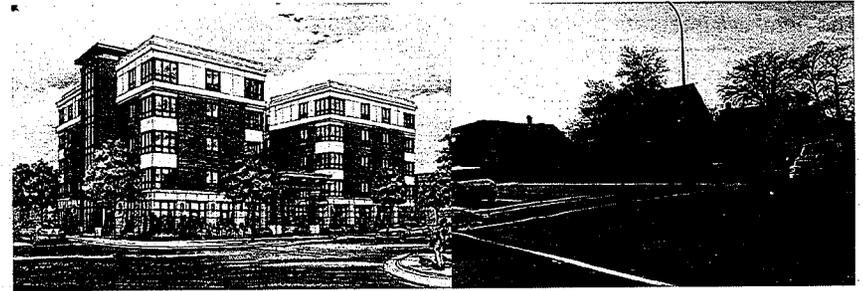
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5. Buildings and landscape design that respects adjacent conditions and strengthens neighborhood identity

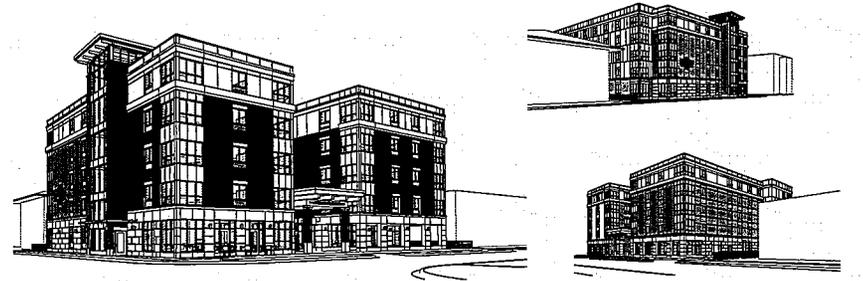
5.1 ++ + 0 - - - Complement and enhance adjacent neighborhood landscaping and architecture, for example scale, materials and rooflines.

The landscaping and streetscape provided to the neighborhood by the hotel project will greatly improve the current condition of this section of the neighborhood. Currently the existing buildings on and adjacent to the site have little or no landscaping or architectural detail. This project will hopefully set a new standard for future development along the corridor by providing a rich, colorful and varied landscape plan and quality architectural materials and detailing at the street level.



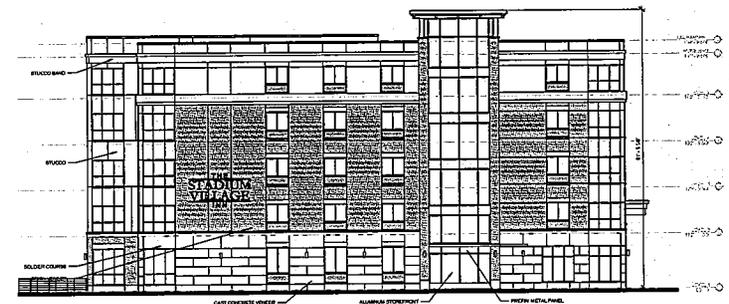
5.2 ++ + 0 - - - Contribute to the visual vitality, richness and distinctiveness of the street and neighborhood.

The hotel, with the combination of the building materials, and streetscape will provide a new use and distinct visual appeal not currently found on the Huron Corridor between I-94 and the U of M campus. The massing and choice of materials were carefully considered to relate to users and passersby on multiple scales creating a richly detailed and unique presence along the Huron Corridor.



5.3 ++ + 0 - - - Use materials and methods that assure the building will be an enduring part of the public realm.

The use of brick and stone in the exterior facade of the hotel relates to the overall feel of the University of Minnesota campus. The use of enduring masonry materials that stand the test of time, will help enhance the visual appeal of the Huron Corridor for decades to come.



UNIVERSITY DISTRICT DEVELOPMENT PRINCIPLES

Minneapolis, Minnesota

February 28, 2014

HURON & ESSEX HOTEL

113-0110

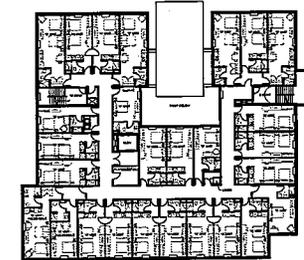


6. Mixed - use and urban density to accommodate all the people who want work and live in the District

6.1 ++ + 0 - -- Conserve energy and resources through orientation, massing, choice of materials and operating systems.

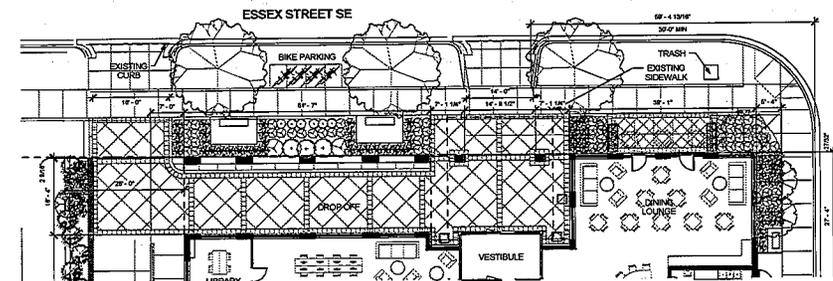
The hotel will integrate many energy efficient design features, including:

- Daylighting by way of the building's U-shaped massing
- Optimizing the site's density by allowing a greater number of units per floor on a previously under-utilized site.
- High efficiency PTAC units in all rooms
- High performance windows with Low-E glass
- Recycling space will be provided from rooms and common areas and on-site receptacles.



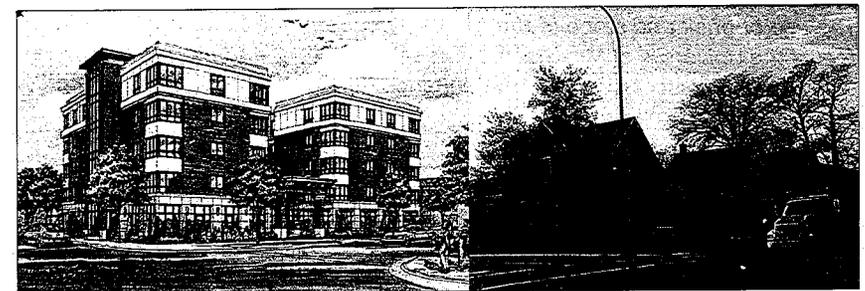
6.2 ++ + 0 - -- Promote easy evolution, maintenance and repair over time.

The exterior materials, cast stone, brick and stucco, require very little maintenance. The landscape materials including many perennials that will mature over time and also require very little maintenance. The hotel's first floor, common lobby and gathering areas are open and flexible for many future uses.



6.3 ++ + 0 - -- Protect existing ecosystems and habitat.

There is very little ecosystem or habitat on this site or on the adjacent properties. The hotel landscape materials will include new boulevard trees and shrubs and perennial plantings adjacent to the building. Such a diverse landscape will provide new locations for supporting the existing neighborhood's wildlife and ecosystem.



UNIVERSITY DISTRICT DEVELOPMENT PRINCIPLES

Minneapolis, Minnesota

February 28, 2014

HURON & ESSEX HOTEL

113-0110





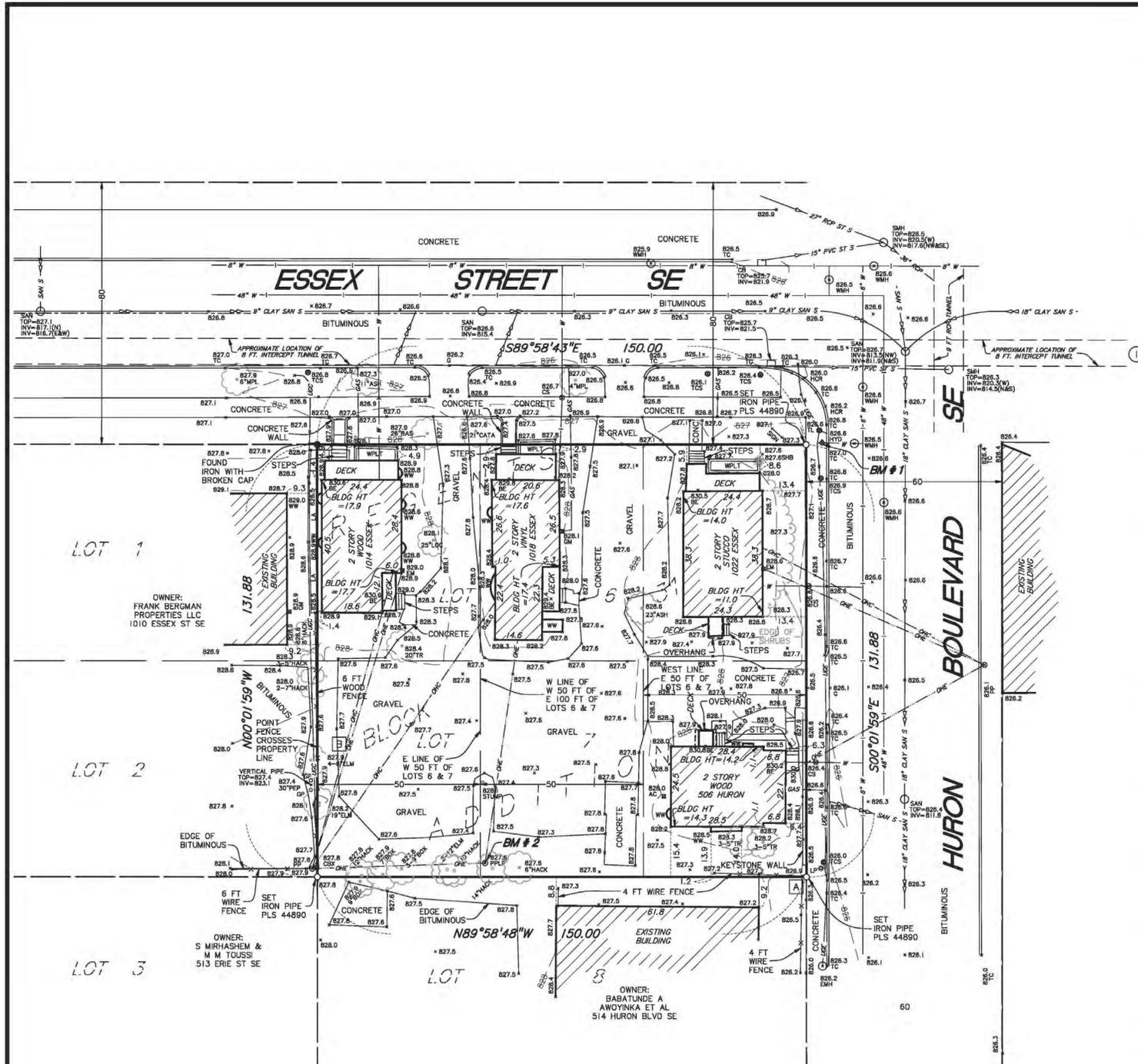
ESSEX AND HURON HOTEL

Minneapolis, Minnesota

February 28, 2014

EXTERIOR RENDERING

113-0110.0



DESCRIPTION OF PROPERTY SURVEYED

(Per First American Title Insurance Company Commitment for Title Insurance Commitment No. NCS-611382-MPLS, commitment date May 12, 2013)

- Parcel 1:
The West 50 feet of Lots 6 and 7, Block 31, Regents Addition, Hennepin County, Minnesota.
(Abstract Property)
- Parcel 2:
The West 50 feet of the East 100 feet of Lots 6 and 7, Block 31, Regents Addition, Hennepin County, Minnesota.
(Abstract Property)
- Parcel 3:
The East 50 feet of Lots 6 and 7, Block 31, Regents Addition, Hennepin County, Minnesota.
(Abstract Property)

PLAT RECORDING INFORMATION

The plat of REGENTS' ADDITION was filed of record on August 25, 1883.

TITLE COMMITMENT

First American Title Insurance Company Commitment for Title Insurance Commitment No. NCS-611382-MPLS, commitment date May 12, 2013 was relied upon as to matters of record.

Schedule B Exceptions:

Exceptions are indicated on survey with circled numbers where applicable.

- 16.) Possible gaps and/or overlaps between the parcels. The descriptions all assume that Lots 6 and 7 measure exactly 150 feet from west to east. [Based on found monumentation, within Block 31, Lots 6 and 7 are 150.00 feet wide.]

FLOOD ZONE NOTE

- 1.) The subject property lies within Zone X (areas determined to be outside the 0.2% annual chance floodplain) per the National Flood Insurance Program, Flood Insurance Rate Map Community Panel No. 2701720376E, dated September 2, 2004. This information was obtained from the FEMA Map Service Center web site.

ZONING NOTES

- 1.) Zoning information obtained from the City of Minneapolis web site on June 5, 2013.

The subject property is zoned High Density Residence District (R5).

- 2.) The setbacks for zone High Density Residence District (R5) are:

Building: Front = 15 feet
Interior Side = 5 feet
Rear = 5 feet
Corner Side = 8 feet

- 3.) Parking: No designated parking spaces

AREAS

Land Area = 19,782 sq. ft. or 0.454 acres
Building Footprint Area 1022 Essex St. = 930 sq. ft.
Building Footprint Area 1014 Essex St. = 920 sq. ft.
Building Footprint Area 506 Huron Blvd. = 845 sq. ft.
Building Footprint Area 1018 Essex St. = 871 sq. ft.

GENERAL NOTES

- 1.) Adjoining ownership information shown hereon was obtained from the Hennepin County Property Tax Information web site. Ownership information is subject to revision upon receipt of a title search by a title insurance company.
- 2.) Survey coordinate basis: Hennepin County Coordinate System.
- 3.) We have shown the current zoning classification of the property in the zoning notes section of the survey based on our own research; however, we have not included item 6(a) in the survey certification because zoning information has not been provided to us by the insurer as called for in the 2011 ALTA requirements.

UTILITY NOTES

- 1.) Utility information from plans and markings was combined with observed evidence of utilities to develop a view of the underground utilities shown hereon. However, lacking excavation, the exact location of underground features cannot be accurately, completely and reliably depicted. Where additional or more detailed information is required, excavation may be necessary.
- 2.) Other underground utilities of which we are unaware may exist. Verify all utilities critical to construction or design.
- 3.) Some underground utility locations are shown as marked onsite by those utility companies whose locators responded to our Gopher State One Call, ticket numbers 131432873, 131432876 and 131432900.
- 4.) Contact GOPHER STATE ONE CALL at 651-454-0002 (800-252-1166) for precise onsite location of utilities prior to any excavation.

LEGEND

- AC Denotes air conditioner
- BE Denotes building entrance
- CB Denotes catch basin
- CBX Denotes communication box
- CS Denotes curb stop
- EM Denotes electric meter
- EMH Denotes electric manhole
- G Denotes gutter
- GM Denotes gas meter
- GP Denotes guard post
- HCR Denotes handicap ramp
- HYD Denotes fire hydrant
- INV Denotes structure invert
- LA Denotes landscaped area
- LP Denotes light pole
- OHC Denotes overhead communication line
- OHE Denotes overhead electric line
- PP Denotes power pole
- PEP Denotes polyethylene pipe
- PPLP Denotes power and light pole
- PVC Denotes polyvinylchloride pipe
- RCP Denotes reinforced concrete pipe
- SAN Denotes sanitary manhole
- SAN S Denotes sanitary sewer
- SMH Denotes storm manhole
- ST S Denotes storm sewer
- TC Denotes top of concrete curb
- TCS Denotes traffic control sign
- TL Denotes traffic light
- UGC Denotes underground communication line
- UGE Denotes underground electric line
- W Denotes water line
- WPLT Denotes water planter
- WMH Denotes water manhole
- WW Denotes window well
- BAS Denotes Basswood tree
- BOX Denotes Boxelder tree
- CATA Denotes Northern Catalpa tree
- HACK Denotes Hackberry tree
- LOC Denotes Locust tree
- MPL Denotes Maple tree
- SHB Denotes shrub
- TR Denotes deciduous tree

REVISION SUMMARY

DATE	DESCRIPTION
3-3-14	PDR SUBMITTAL

SITE SURVEY

C0.0

SURVEYOR'S CERTIFICATION

To: CPM Development and First American Title Insurance Company

This is to certify that this map or plat and the survey on which it is based were made in accordance with the 2011 Minimum Standard Detail Requirements for ALTA/ACSM Land Title Surveys, jointly established and adopted by ALTA and NSPS, and includes items 1, 2, 3, 4, 5, 7(a), 7(b)(1), 8, 9, 11 and 13 of Table A thereof. The field work was completed on June 5, 2013.

Dated this 13th day of June, 2013.

SUNDE LAND SURVEYING, LLC.

By: Leonard F. Carlson
Leonard F. Carlson, P.L.S. Minn. Lic. No. 44890



BENCH MARKS (BM)

- 1.) Top of top nut of fire hydrant at the southwest quadrant of SE Huron Boulevard and SE Essex Street.
Elevation = 829.55 feet
- 2.) Top of railroad spike on the north face of the power and light pole located easterly 51 feet 7/8 from the southwest property corner.
Elevation = 828.80 feet

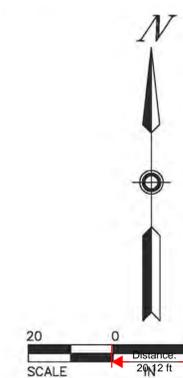
NOTE: Elevations shown are based University of Minnesota 1912 USC&G datum.

LIST OF POSSIBLE ENCROACHMENTS

The following list of possible encroachments is only the opinion of this surveyor; should not be interpreted as a legal opinion and should not be interpreted as a complete listing.

- A) Possible encroachments are indicated on survey with boxed letters as listed below.

- A.) 4 foot wire fence lying along the southerly property line.
- B.) 6 foot wood fence lying along the westerly property line.



Revision	By	Date

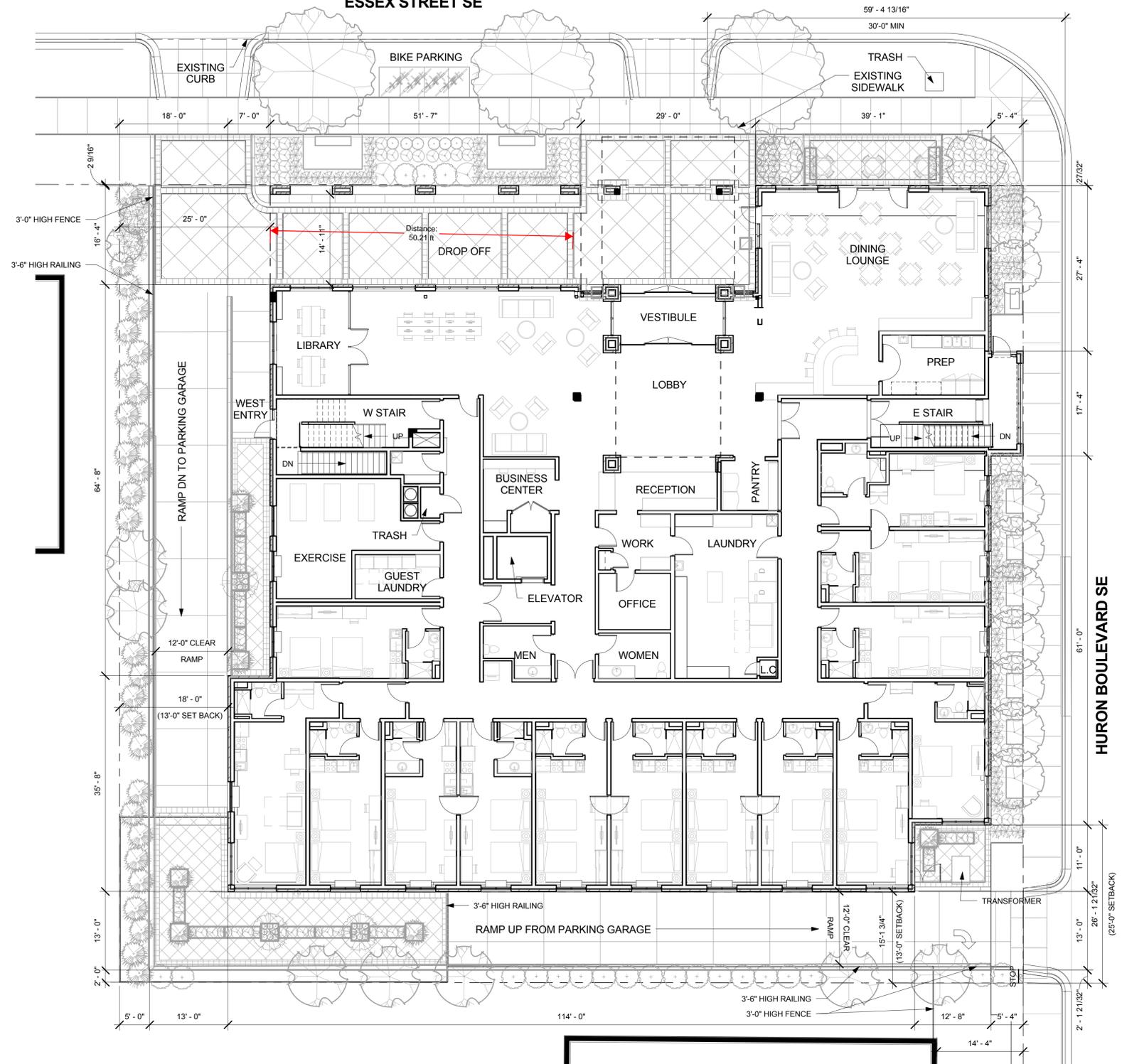
Drawing Title:
**ALTA/ACSM LAND TITLE SURVEY FOR:
CPM DEVELOPMENT**
1014, 1018 & 1022 ESSEX ST. SE, MINNEAPOLIS, MN

SUNDE LAND SURVEYING
www.sunde.com
Main Office: 3001 East Bloomington Freeway (35W) • Suite 118
Bloomington, Minnesota 55420-3435
952-881-2455 (Fax: 952-888-9526)
West Office: Mandan, North Dakota 701-663-5562

Project: 2013-102 Blk/Pg: 719/82 Date: 06/13/2013
Township: 029 Range: 23 Section: 30
File: 2013102001.dwg Sheet: 1 of 1

P:\Projects\2013\102\2013102001.dwg 6/13/2013 12:48:37 PM CST

ESSEX STREET SE



SITE DATA	
SITE AREA:	19,782 SQ. FT., 0.45 ACRE
CURRENT ZONING:	R-5
PROPOSED ZONING:	C-3A
F.A.R.: ALLOWED	2.7 (53,411 SQ. FT.)
PROPOSED F.A.R.:	3.17 (62,684 SQ. FT.)
1ST FLOOR -	12,712
2ND - 5TH -	12,493
NUMBER OF GUESTROOMS:	122
PARKING:	REQ'D 41 (1/3 STALL PER ROOM)
	PROPOSED 41
BLDG HEIGHT: ALLOWED	4 STORIES 56'-0"
PROPOSED	5 STORIES 62'-0"

A1 SITE PLAN
1" = 10'-0"



ESSEX AND HURON HOTEL

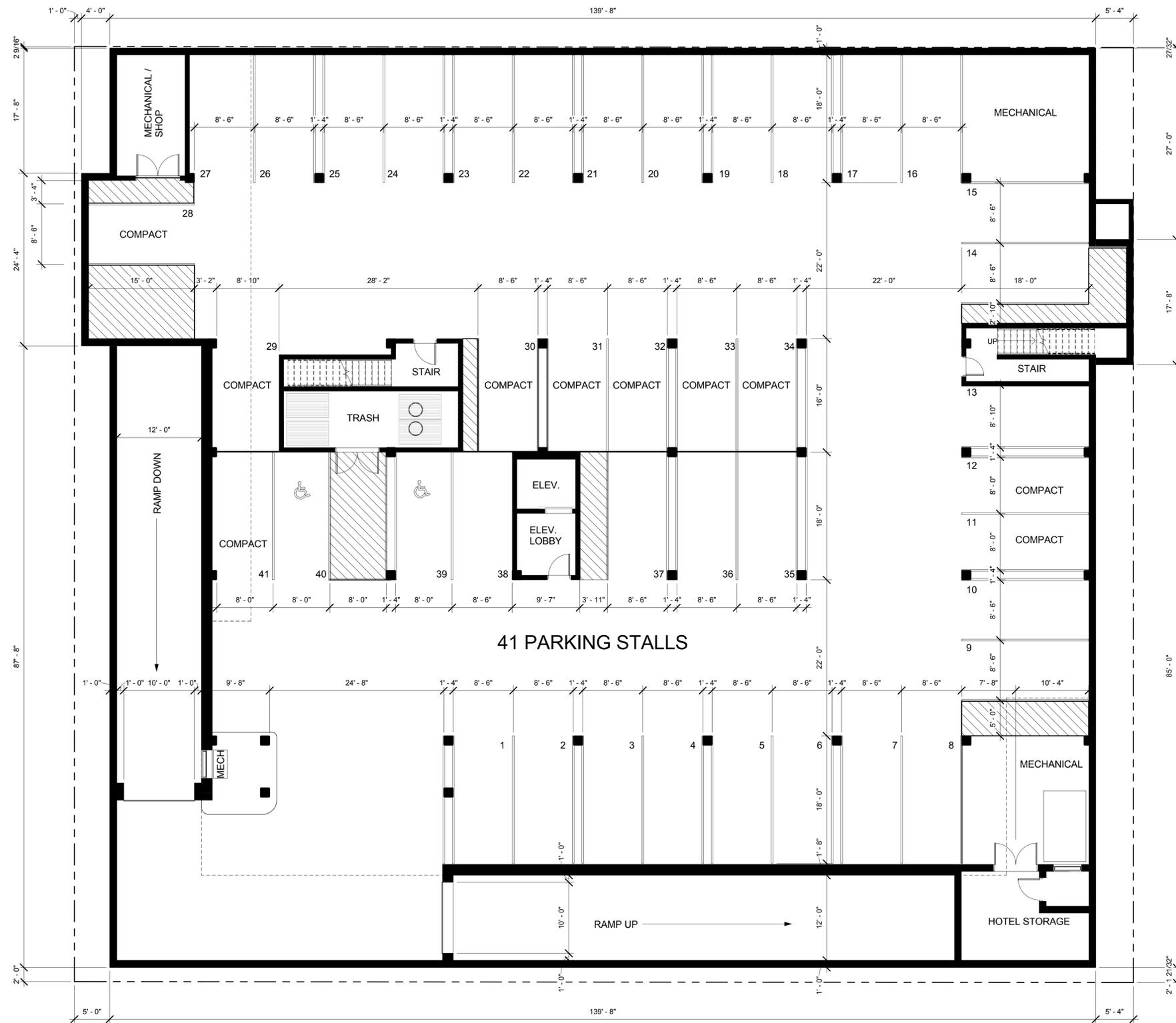
Minneapolis, Minnesota

February 28, 2014

SITE / FIRST FLOOR PLAN

113-0110.0





SCALE 1/8" = 1'-0" 

ESSEX AND HURON HOTEL

Minneapolis, Minnesota

February 28, 2014

GARAGE LEVEL FLOOR PLAN

113-0110.0



SCALE 1/8" = 1'-0" 

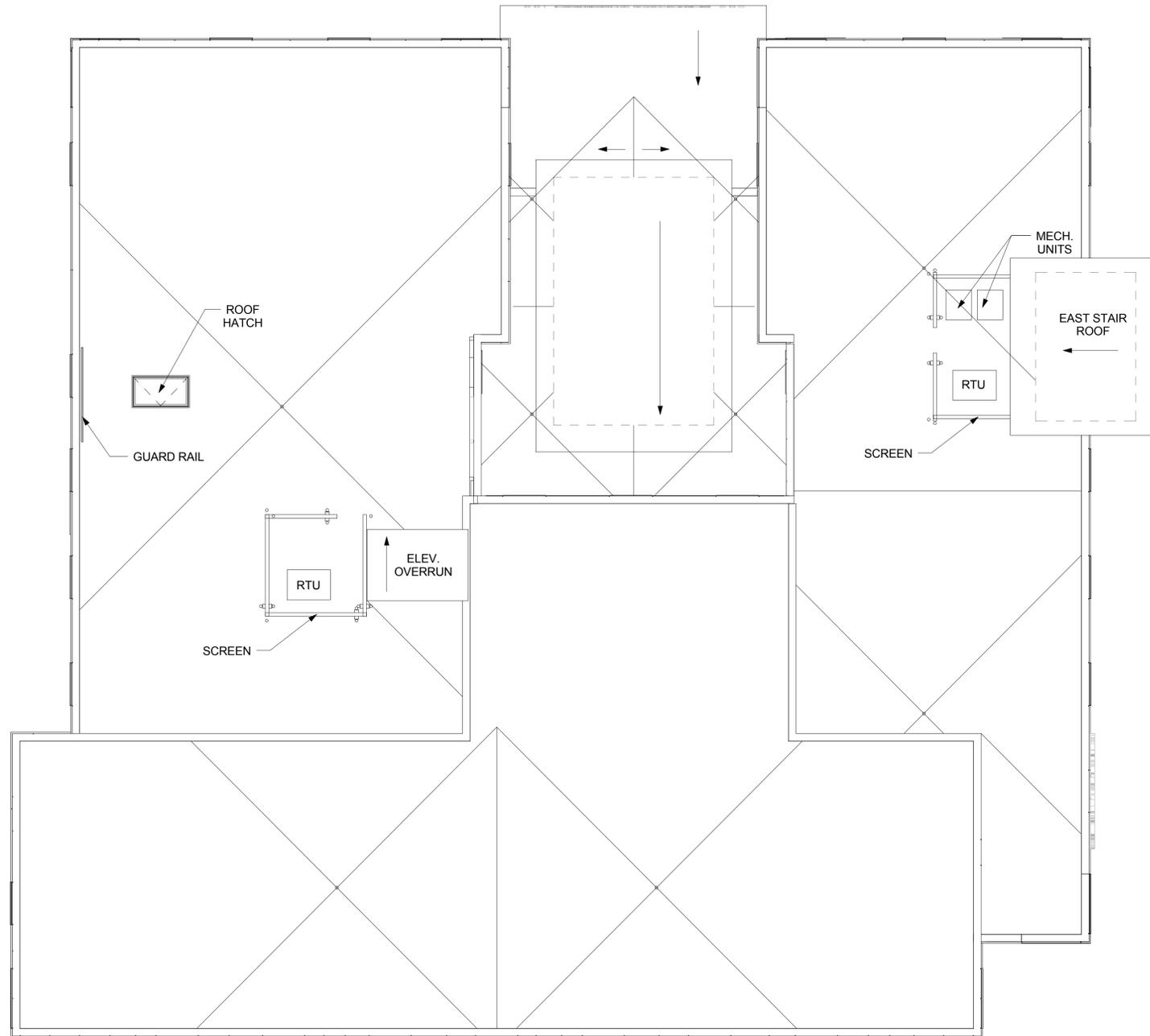
ESSEX AND HURON HOTEL

Minneapolis, Minnesota

February 28, 2014

FLOOR PLAN LEVELS 2 - 5

113-0110.0



SCALE 1/8" = 1'-0" 

ESSEX AND HURON HOTEL

Minneapolis, Minnesota

February 28, 2014

ROOF PLAN

113-0110.0



A3 SOUTH EXTERIOR ELEVATION
1/8" = 1'-0"



D1 WEST EXTERIOR ELEVATION
1/8" = 1'-0"

ESSEX AND HURON HOTEL

Minneapolis, Minnesota

February 28, 2014

EXTERIOR ELEVATIONS

113-0110.0



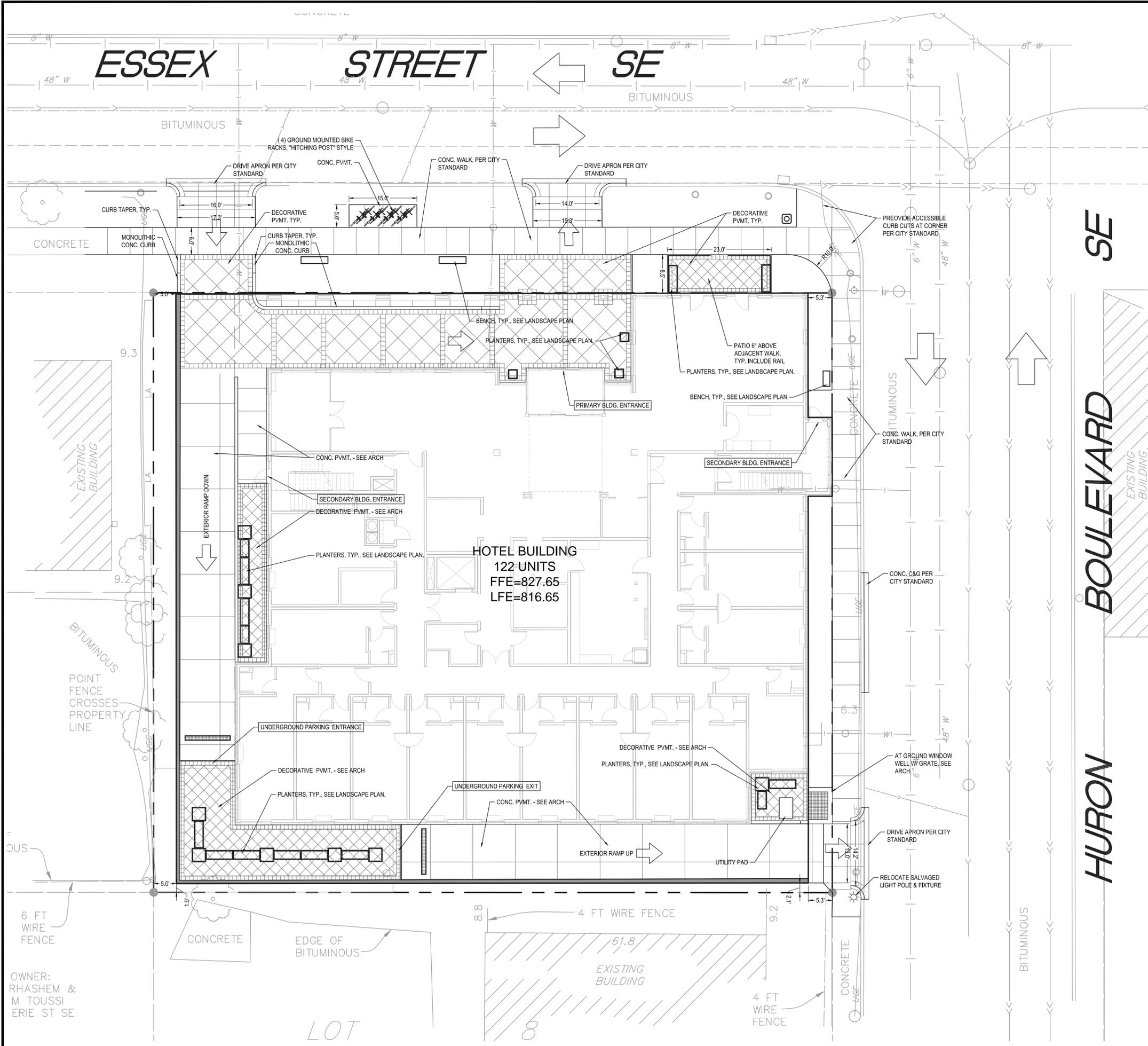
ESSEX AND HURON HOTEL

Minneapolis, Minnesota

February 28, 2014

PERSPECTIVES

113-0110.0



SITE PLAN NOTES:

1. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS PRIOR TO CONSTRUCTION, INCLUDING A RIGHT-OF-WAY AND STREET OPENING PERMIT.
2. THE CONTRACTOR SHALL VERIFY RECOMMENDATIONS NOTED IN THE GEO-TECHNICAL REPORT PRIOR TO INSTALLATION OF SITE IMPROVEMENT MATERIALS.
3. CONTRACTOR SHALL FIELD VERIFY COORDINATES AND LOCATION DIMENSIONS OF THE BUILDING AND STAKE FOR REVIEW AND APPROVAL BY THE OWNERS REPRESENTATIVE PRIOR TO INSTALLATION OF FOOTING MATERIALS.
4. LOCATIONS OF STRUCTURES, ROADWAY PAVEMENTS, CURBS AND GUTTERS, BOLLARDS, AND WALKS ARE APPROXIMATE AND SHALL BE STAKED IN THE FIELD. PRIOR TO INSTALLATION, FOR REVIEW AND APPROVAL BY THE ENGINEER/LANDSCAPE ARCHITECT.
5. CURB DIMENSIONS SHOWN ARE TO FACE OF CURB. BUILDING DIMENSIONS ARE TO FACE OF CONCRETE FOUNDATION. LOCATION OF BUILDING IS TO BUILDING FOUNDATION AND SHALL BE AS SHOWN ON THE DRAWINGS.
6. THE CONTRACTOR SHALL SUBMIT SHOP DRAWINGS OR SAMPLES AS SPECIFIED FOR REVIEW AND APPROVAL BY THE ENGINEER/LANDSCAPE ARCHITECT PRIOR TO FABRICATION FOR ALL PREFABRICATED SITE IMPROVEMENT MATERIALS SUCH AS, BUT NOT LIMITED TO THE FOLLOWING: FURNISHINGS, PAVEMENTS, WALLS, RAILINGS, BENCHES, FLAGPOLES, LANDING PADS FOR CURB RAMPS, AND LIGHT AND POLES. THE OWNER RESERVES THE RIGHT TO REJECT INSTALLED MATERIALS NOT PREVIOUSLY APPROVED.
7. CONTRACTOR SHALL REFER TO FINAL PLAT FOR LOT BOUNDARIES, NUMBERS, AREAS AND DIMENSIONS PRIOR TO SITE IMPROVEMENTS.
8. FIELD VERIFY ALL EXISTING SITE CONDITIONS, DIMENSIONS.
9. CONTRACTOR SHALL VERIFY LOCATIONS AND LAYOUT OF ALL SITE ELEMENTS PRIOR TO BEGINNING CONSTRUCTION, INCLUDING BUT NOT LIMITED TO, LOCATIONS OF EXISTING AND PROPOSED PROPERTY LINES, EASEMENTS, SETBACKS, UTILITIES, BUILDINGS AND PAVEMENTS. CONTRACTOR IS RESPONSIBLE FOR FINAL LOCATIONS OF ALL ELEMENTS FOR THE SITE. ANY REVISIONS REQUIRED AFTER COMMENCEMENT OF CONSTRUCTION, DUE TO LOCATIONAL ADJUSTMENTS SHALL BE CORRECTED AT NO ADDITIONAL COST TO OWNER. ADJUSTMENTS TO THE LAYOUT SHALL BE APPROVED BY THE ENGINEER/LANDSCAPE ARCHITECT PRIOR TO INSTALLATION OF MATERIALS. STAKE LAYOUT FOR APPROVAL.

OPERATIONAL NOTES:

1. ALL SNOW WILL BE REMOVED FROM PUBLIC WALK, AS PER CITY ORD., BY MANAGEMENT CO.
2. DRIVES ENTERING AND EXISTING UNDERGROUND PARKING SHALL BE HEATED. THEREFORE NO SNOW REMOVAL WILL BE NECESSARY.
3. TRASH WILL BE COLLECTED INSIDE THE BUILDING. TRASH BINS WILL BE REMOVED FROM BASEMENT STORAGE AREA TO THE REMOVAL TRUCK LOCATION AT TIME OF PICK-UP. NO TEMPORARY OUTDOOR STAGING OR STORAGE OF TRASH BINS WILL BE NECESSARY.

CITY COORDINATION NOTES:

1. TRAFFIC & PARKING
 - 1.1. COORDINATE WITH BILL PRINCE, CITY OF MINNEAPOLIS, (612) 673-3901 FOR ALL WORK REGARDING STREET LIGHTING AND ELECTRICAL SYSTEMS.
 - 1.2. COORDINATE WITH SCOTT KRAMER, CITY OF MINNEAPOLIS, (612) 673-5755 FOR ALL OTHER ISSUES REGARDING WORK IN AND ADJACENT TO CITY RIGHTS-OF-WAY.

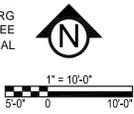
OWNER INFORMATION

OWNER: CPM DEVELOPMENT
2919 KNOX AVE SOUTH
SUITE 200
MINNEAPOLIS, MN 55408

SITE PLAN LEGEND:

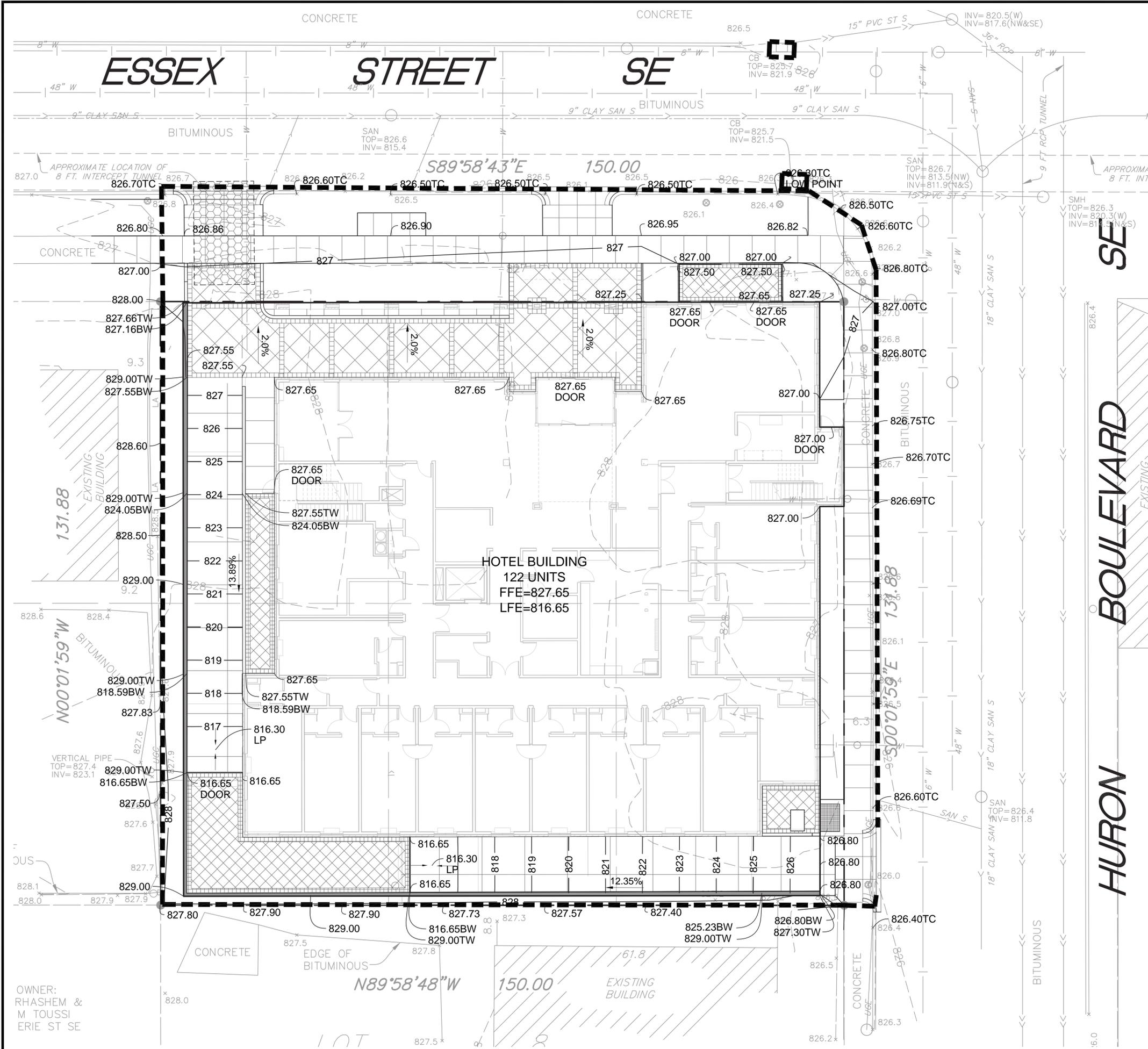
- CONCRETE PAVEMENT AS SPECIFIED (PAD OR WALK)
- PROPERTY LINE
- CURB AND GUTTER-SEE NOTES (T.O.) TIP OUT GUTTER WHERE APPLICABLE-SEE PLAN
- TRAFFIC DIRECTIONAL ARROWS

GOPHER STATE ONE CALL
WWW.GOPHERSTATEONECALL.ORG
(800) 252-1166 TOLL FREE
(651) 454-0002 LOCAL



OWNER:
RHASHEM &
M TOUSSI
ERIE ST SE

LOT



- GRADING NOTES:**
- SEE SITE PLAN FOR HORIZONTAL LAYOUT.
 - CONTRACTOR SHALL REFER TO RECOMMENDATIONS IN THE GEOTECHNICAL REPORT FOR ANY ADDITIONAL SITE PREPARATION INFORMATION, SOIL CORRECTION, TYPE OF BACKFILL, OR REQUIREMENTS.
 - EXCAVATION AND EMBANKMENT ACTIVITIES SHALL BE PERFORMED IN ACCORDANCE WITH THE GEOTECHNICAL REPORT RECOMMENDATIONS.
 - GRADING AND EXCAVATION ACTIVITIES SHALL BE PERFORMED IN ACCORDANCE WITH THE NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT REQUIREMENTS & PERMIT REQUIREMENTS OF THE CITY.
 - THE CONTRACTOR SHALL SCHEDULE SITE WORK TO MINIMIZE THE DISTURBED AREA AT ANY GIVEN TIME.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE OF GRADE STAKES THROUGHOUT THE DURATION OF CONSTRUCTION TO ESTABLISH PROPER GRADES. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR A FINAL FIELD CHECK OF FINISHED GRADES ACCEPTABLE TO THE ENGINEER/LANDSCAPE ARCHITECT PRIOR TO TOP-SOIL AND SODDING ACTIVITIES.
 - EXCESS FILL MATERIAL SHALL BE REMOVED AND LEGALLY DISPOSED OF BY THE CONTRACTOR OFF-SITE.
 - PROPOSED SPOT GRADES ARE FLOWLINE FINISHED GRADE ELEVATIONS, UNLESS OTHERWISE NOTED.
 - GRADES OF WALKS SHALL BE INSTALLED WITH 5% MAX. LONGITUDINAL SLOPE AND 1% MIN. AND 2% MAX. CROSS SLOPE, UNLESS OTHERWISE NOTED.
 - PROPOSED SLOPES SHALL NOT EXCEED 3:1 UNLESS INDICATED OTHERWISE ON THE DRAWINGS.
 - CONTRACTOR SHALL STRIP, STOCKPILE, AND RE-SPREAD SUFFICIENT TOPSOIL TO PROVIDE A MINIMUM 4" COMPACTED DEPTH TO DISTURBED AREAS TO BE SEEDED OR SODDED.
 - CONCRETE WASHOUT PROCEDURES SHALL BE COMPLETED OFF-SITE.

- NON STORM WATER DISCHARGES:**
- THERE ARE NO KNOWN NON-STORM WATER DISCHARGES ON THE EXISTING SITE AND NONE ARE PROPOSED AS PART OF THIS DEVELOPMENT.

City of Minneapolis Standard Erosion Control Notes

- These notes may be used for performance standard or requirement for projects not meeting the threshold for formal plans.*
- Contractor must call a construction start 48 hours prior to any land disturbance 612-673-3867. Failure to do so may result in fines, the revocation of permit and a stop work order being issued.
 - Install perimeter erosion control at the locations shown on the plan prior to the commencement of any land disturbance or construction activities.
 - Before beginning construction, install a temporary rock construction entrance at each point where vehicles exit the construction site. Use 2 inch or greater diameter rock in a layer at least 6 inches thick across the entire width of the entrance. Extend the rock entrance at least 50 feet into the construction zone using a geotextile fabric beneath the aggregate to prevent migration of soil into the rock from below.
 - Remove all soil and sediment tracks from vehicles. Deposit tracks into public and private pavement areas. Removal shall be on a daily basis when tracking occurs and may be ordered by Minneapolis inspectors at any time if conditions warrant. Sweeping shall be maintained throughout the duration of the construction and done in a manner to prevent dust being blown to adjacent properties.
 - Install silt protection at all public and private catch basin inlets, which receive runoff from the disturbed area. Contractor shall clean, remove sediment or replace storm drain inlet protection devices on a routine basis such that the devices are fully functional for the next rain event. Sediment deposited in and/or plugging drainage systems is the responsibility of the contractor. Use baskets or filter fabric wrapped grates not allowed for inlet protection.
 - Locate soil or dirt stockpiles no less than 25 feet from any public or private roadway or drainage channel. If remaining for more than seven days, stabilize the stockpiles by mulching, vegetative cover, tarps, or other means. Control erosion from all stockpiles by placing silt barriers around the piles. Temporary stockpiles located on paved surfaces must be no less than two feet from the demarcation line and shall be covered if left more than 24 hours.
 - Maintain all temporary erosion and sediment control devices in place until the contributing drainage area has been stabilized. Inspect temporary erosion and sediment control devices on a daily basis and replace deteriorated, damaged, or rotted erosion control devices immediately.
 - Temporarily or permanently stabilize all construction areas which have undergone final grading, and all areas in which grading or site building construction operations are not actively underway against erosion due to rain, wind and meeting water within 7-14 days. Use seed and mulch, erosion control matting, and/or sodding and raising in grass space areas. Remove all temporary systems, structures, non-biodegradable erosion and sediment control devices after the site has undergone final stabilization with permanent vegetation establishment. Final stabilization for purposes of this note is 70% established cover over disturbed area.
 - Ready mixed concrete and concrete batch mix plants are prohibited within the public right of way. All concrete related production, cleaning and mixing activities shall be done in the designated concrete mixing/loading location as shown in the approved site plan. Under no circumstances may washed water drain onto the public right of way or into any public or private storm drain conveyance.
 - Changes to approved erosion control plans may be approved by the erosion control inspector prior to implementation. Contractor to provide installation and details for all proposed alternate type devices.
 - If dewatering or pumping of water is necessary, the contractor is responsible for obtaining any necessary permits and approvals prior to discharge of any water from the site. If the discharge from the dewatering or pumping process is turbid or contains sediment laden water, it must be treated through the use of sediment traps, vegetative filter strips, or other sediment reducing measures such that the discharge is not visibly different from the receiving water. Additional erosion control measures may be required at the discharge point to prevent silt erosion. The contractor shall provide a dewatering/pumping plan to the Erosion Control Inspector prior to initiating dewatering activities.
- Prepared by: The City of Minneapolis Public Works Services and Sewer www.cityofminneapolis.gov/Environment
January 2, 2011.

- GRADING PLAN LEGEND:**
- 891 --- EX. 1' CONTOUR ELEVATION INTERVAL
 - 819 --- 1.0' CONTOUR ELEVATION INTERVAL
 - 891.0 --- SPOT GRADE ELEVATION (FLOW LINE UNLESS OTHERWISE NOTED)
 - 891.0 G --- SPOT GRADE ELEVATION GUTTER
 - 891.0 BC --- SPOT GRADE ELEVATION BACK OF CURB (TOP OF CURB)
 - 891.0 BS/TS --- SPOT GRADE ELEVATION BOTTOM OF STAIRS/TOP OF STAIRS
 - TIP --- TIP UP (T.O.) CURB AND GUTTER WHERE APPLICABLE-TAPER GUTTERS TO DRAIN AS SHOWN
 - > --- EXISTING AND PROPOSED DRAINAGE ARROWS

- EROSION CONTROL LEGEND:**
- COMBINATION BIO-ROLL, CHAIN LINK CONSTRUCTION FENCE
 - INLET PROTECTION CURB INLET FILTER
 - TREE PROTECTION

OWNER, DEVELOPER, CONTRACTOR SIGNED STATEMENT

ALL CLEARING, GRADING, CONSTRUCTION OR DEVELOPMENT WILL BE DONE PURSUANT TO THE PLAN, SIGNED BY PARTIES BELOW:

OWNER: _____

DEVELOPER: _____

CONTRACTOR: _____

GOPHER STATE ONE CALL
 WWW.GOPHERSTATEONECALL.ORG
 (800) 252-1166 TOLL FREE
 (651) 454-0002 LOCAL

CivilSite GROUP
 481 W. 35TH ST. SUITE 200
 ST. LOUIS PARK, MN 55416
 CivilSiteGroup.com
 Matt Pavlek Pat Santer
 763-213-3844 952-250-2003

DJR ARCHITECTURE, INC
 333 Washington Ave N, Suite 210
 Minneapolis, Minnesota 55401
 612.676.2700 www.djr-inc.com

CPM COMPANIES

HURON & ESSEX HOTEL
 HURON BLVD. & ESSEX ST. SE, MINNEAPOLIS, MN
 CPM DEVELOPMENT
 2919 KNOX AVENUE SOUTH, SUITE 200, MINNEAPOLIS, MN 55408

PROJECT

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Matthew R. Pavlek
 DATE 03/03/14 LICENSE NO. 44263

ISSUE/SUBMITTAL SUMMARY

DATE	DESCRIPTION
3/3/2014	PDR SUBMITTAL

REVISION SUMMARY

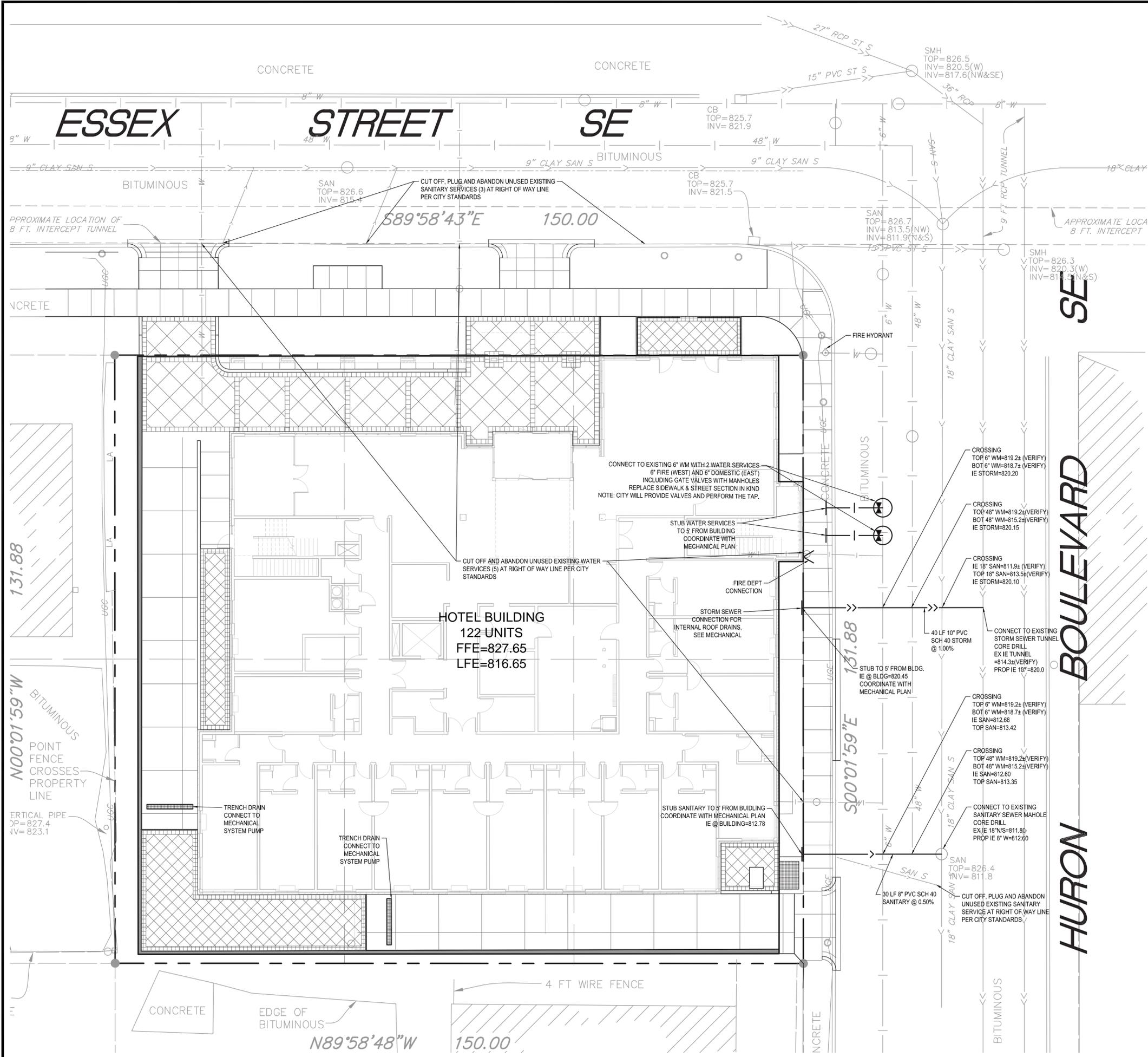
DATE	DESCRIPTION
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GRADING PLAN

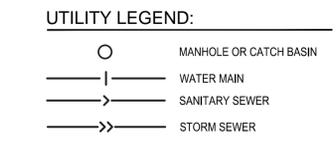
C3.0

© COPYRIGHT 2013 CIVIL SITE GROUP, INC.

OWNER:
 RHASHEM &
 M TOUSSI
 ERIE ST SE

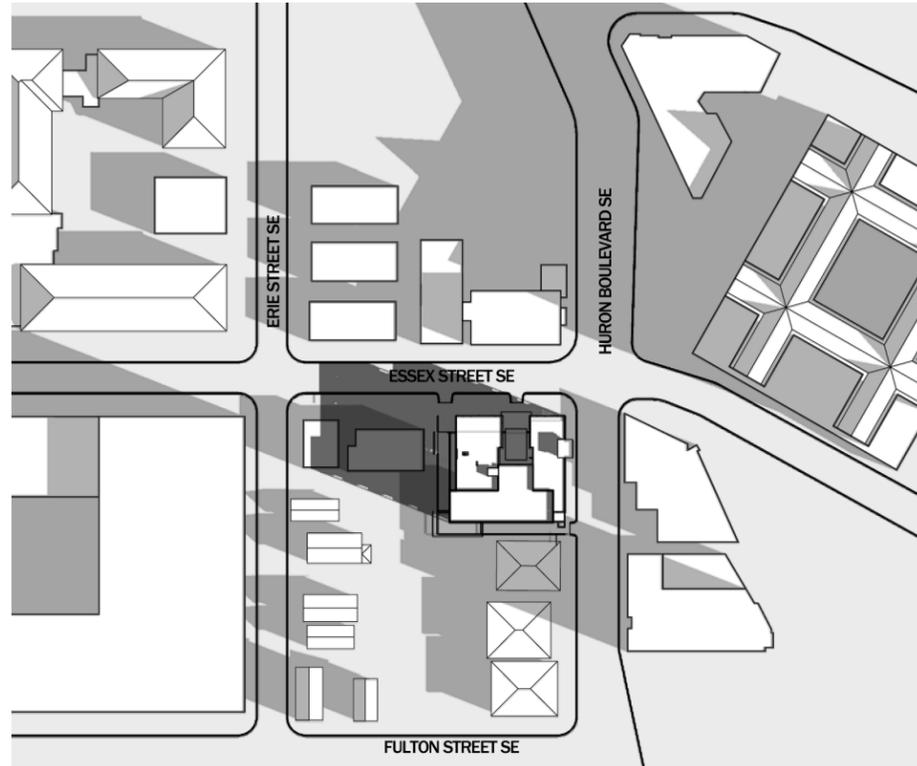


- UTILITY NOTES:**
- SEE SITE PLAN FOR HORIZONTAL DIMENSIONS AND LAYOUT.
 - CONTRACTOR SHALL FIELD VERIFY LOCATION AND ELEVATION OF EXISTING UTILITIES AND TOPOGRAPHIC FEATURES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER OF DISCREPANCIES OR VARIATIONS FROM THE PLANS.
 - ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT "GOPHER STATE ONE CALL" (651-454-0002 OR 800-252-1166) FOR UTILITY LOCATIONS. 48 HOURS PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL REPAIR OR REPLACE ANY UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.
 - UTILITY INSTALLATION SHALL CONFORM TO THE CURRENT EDITION OF "STANDARD SPECIFICATIONS FOR WATER MAIN AND SERVICE LINE INSTALLATION" AS PREPARED BY THE CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM), AND SHALL CONFORM WITH THE REQUIREMENTS OF THE CITY AND THE PROJECT SPECIFICATIONS.
 - ALL WATER PIPE SHALL BE CLASS S2 DUCTILE IRON PIPE (DIP) UNLESS OTHERWISE NOTED.
 - UTILITIES ON THE PLAN ARE SHOWN TO WITHIN 5' OF THE BUILDING FOOTPRINT. THE CONTRACTOR IS ULTIMATELY RESPONSIBLE FOR THE FINAL CONNECTION TO BUILDING LINES. COORDINATE WITH ARCHITECTURAL AND MECHANICAL PLANS.
 - A MINIMUM OF 8 FEET OF COVER IS REQUIRED OVER ALL WATERMAIN, UNLESS OTHERWISE NOTED. EXTRA DEPTH MAY BE REQUIRED TO MAINTAIN A MINIMUM OF 18" VERTICAL SEPARATION TO SANITARY OR STORM SEWER LINES. EXTRA DEPTH WATERMAIN IS INCIDENTAL.
 - A MINIMUM OF 18 INCHES OF VERTICAL SEPARATION AND 10 FEET OF HORIZONTAL SEPARATION IS REQUIRED FOR ALL UTILITIES, UNLESS OTHERWISE NOTED.
 - ALL CONNECTIONS TO EXISTING UTILITIES SHALL BE IN ACCORDANCE WITH CITY STANDARDS AND COORDINATED WITH THE CITY PRIOR TO CONSTRUCTION.
 - COORDINATE LOCATIONS AND SIZES OF SERVICE CONNECTIONS WITH THE MECHANICAL DRAWINGS.
 - COORDINATE INSTALLATION AND SCHEDULING OF THE INSTALLATION OF UTILITIES WITH ADJACENT CONTRACTORS AND CITY STAFF.
 - ALL STREET REPAIRS AND PATCHING SHALL BE PERFORMED PER THE REQUIREMENTS OF THE CITY. ALL PAVEMENT CONNECTIONS SHALL BE SAWCUT. ALL TRAFFIC CONTROLS SHALL BE PROVIDED BY THE CONTRACTOR AND SHALL BE ESTABLISHED PER THE REQUIREMENTS OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND THE CITY. THIS SHALL INCLUDE BUT NOT BE LIMITED TO SIGNAGE, BARRICADES, FLASHERS, AND FLAGGERS AS NEEDED. ALL PUBLIC STREETS SHALL BE OPEN TO TRAFFIC AT ALL TIMES. NO ROAD CLOSURES SHALL BE PERMITTED WITHOUT APPROVAL BY THE CITY.
 - CONTRACTOR SHALL COORDINATE ALL WORK WITH PRIVATE UTILITY COMPANIES.
 - CONTRACTOR SHALL MAINTAIN AS-BUILT PLANS THROUGHOUT CONSTRUCTION AND SUBMIT THESE PLANS TO ENGINEER UPON COMPLETION OF WORK.

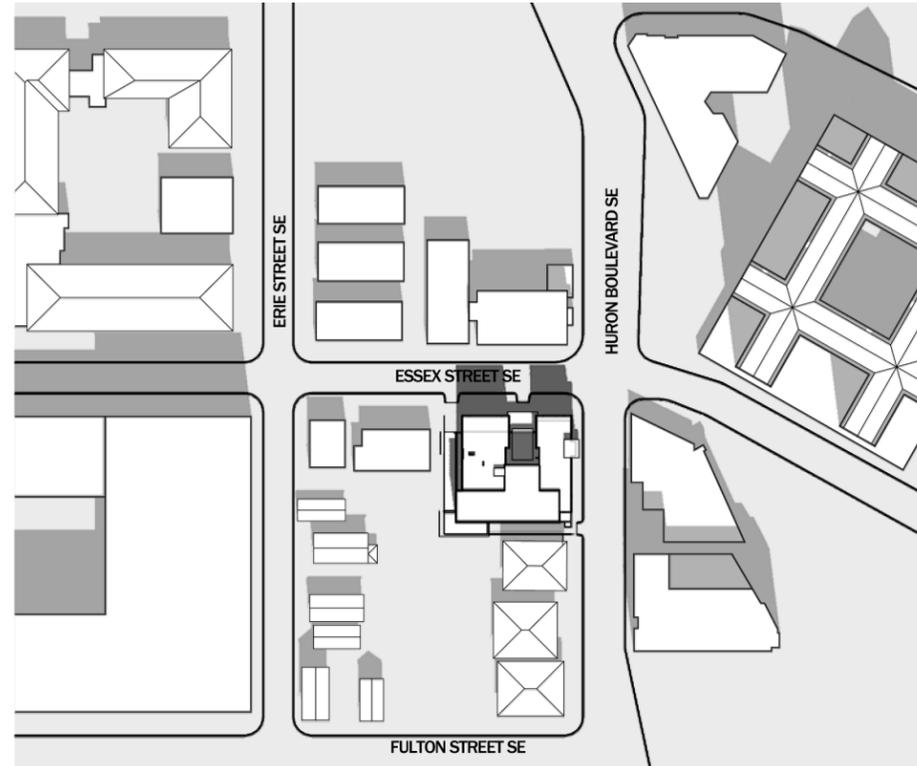


GOPHER STATE ONE CALL
WWW.GOPHERSTATEONECALL.ORG
(800) 252-1166 TOLL FREE
(651) 454-0002 LOCAL

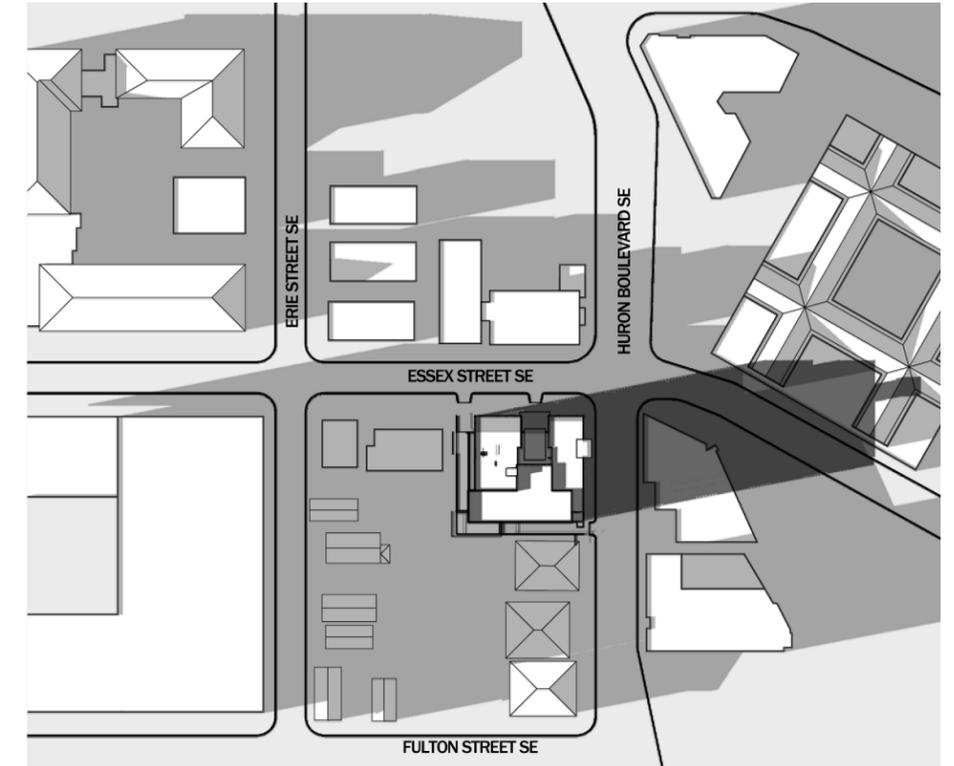
1" = 10'-0"
5'-0" 0 10'-0"



8:16 AM (HOUR AFTER SUNRISE)
SCALE 1"=200'-0"



NOON
SCALE 1"=200'-0"



6:26 PM (HOUR BEFORE SUNSET)
SCALE 1"=200'-0"



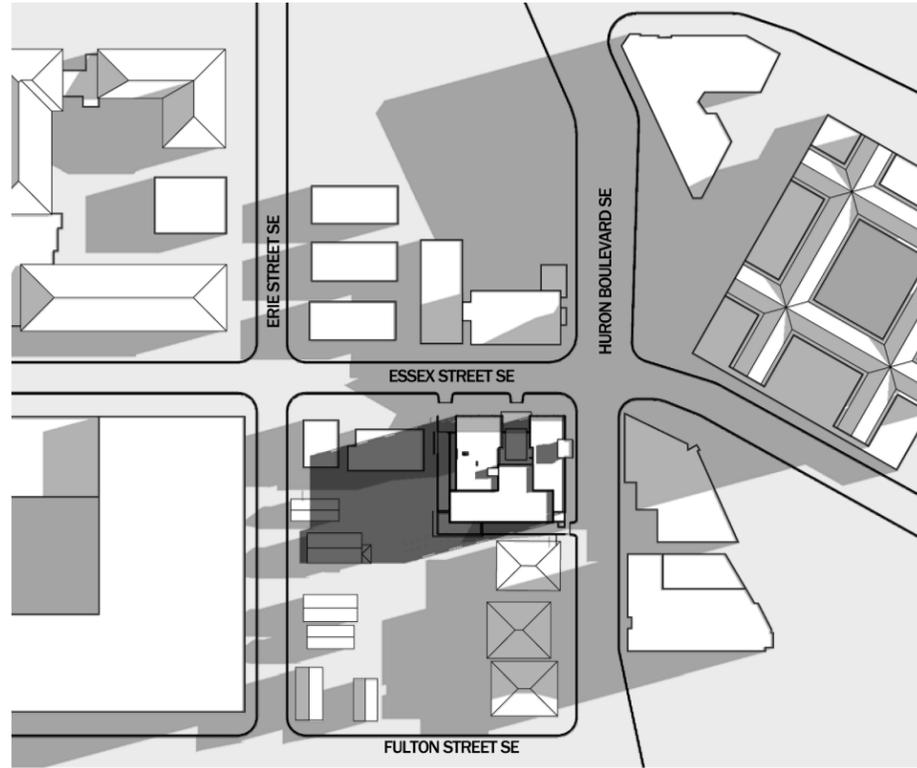
ESSEX AND HURON HOTEL

Minneapolis, Minnesota

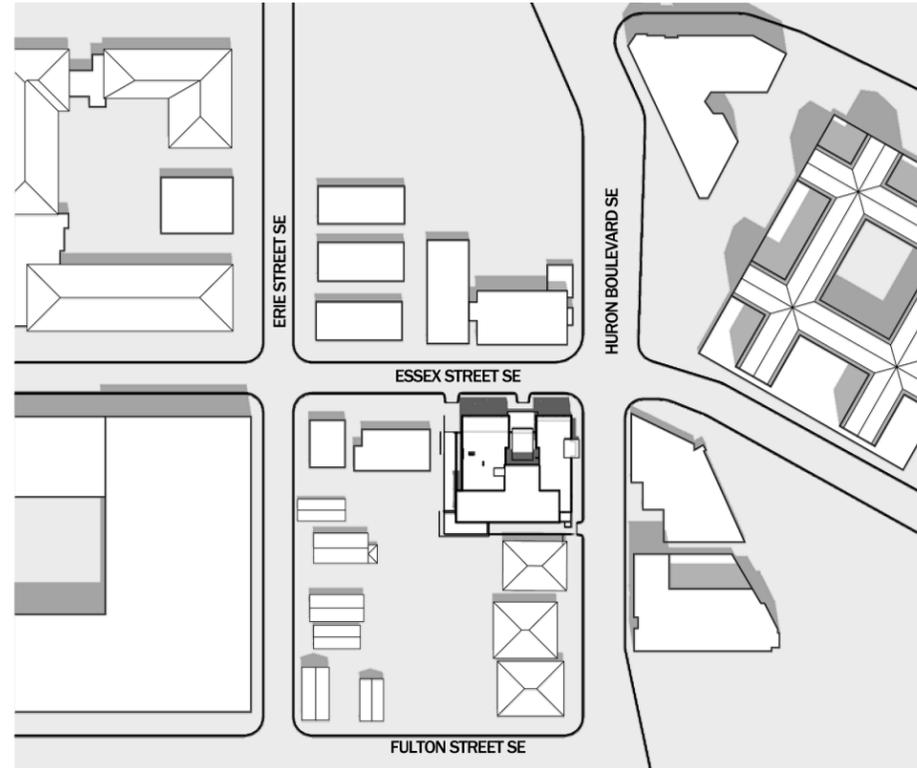
March 24, 2014

SHADOW STUDY MARCH 20

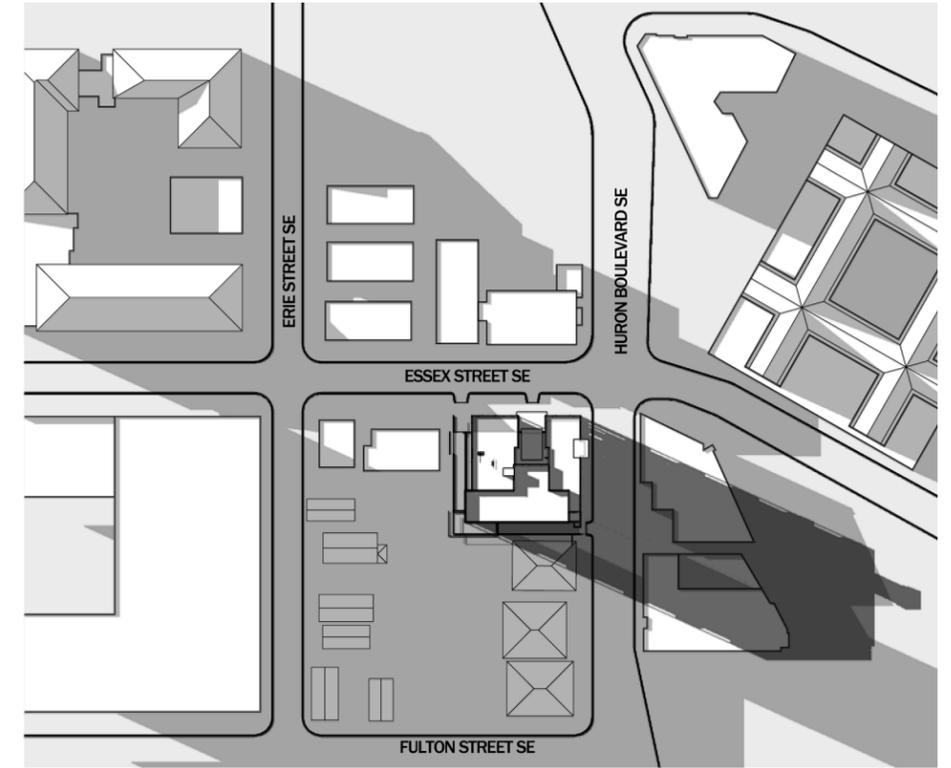
113-0110.0



6:26 AM (HOUR AFTER SUNRISE)
SCALE 1"=200'-0"



NOON
SCALE 1"=200'-0"



8:03 PM (HOUR BEFORE SUNSET)
SCALE 1"=200'-0"



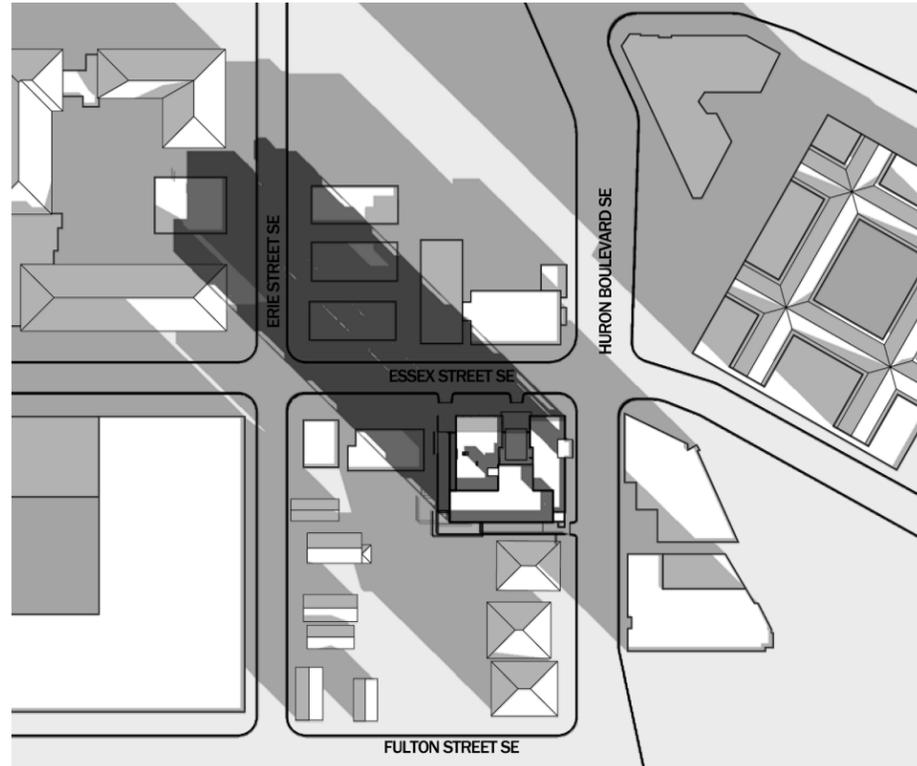
ESSEX AND HURON HOTEL

Minneapolis, Minnesota

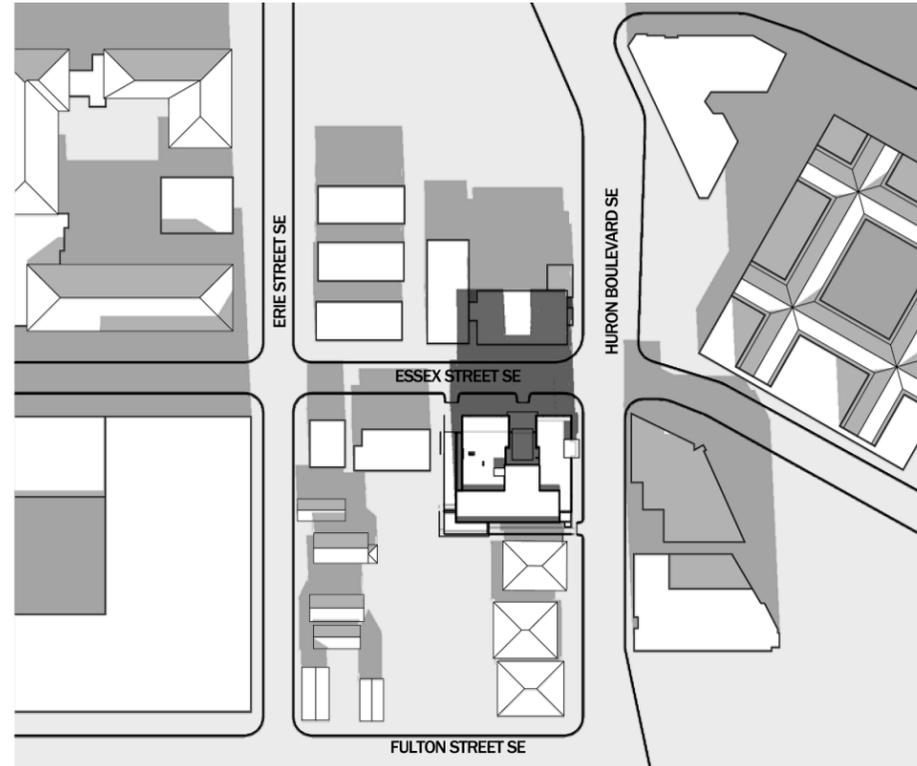
March 24, 2014

SHADOW STUDY **JUNE 21**

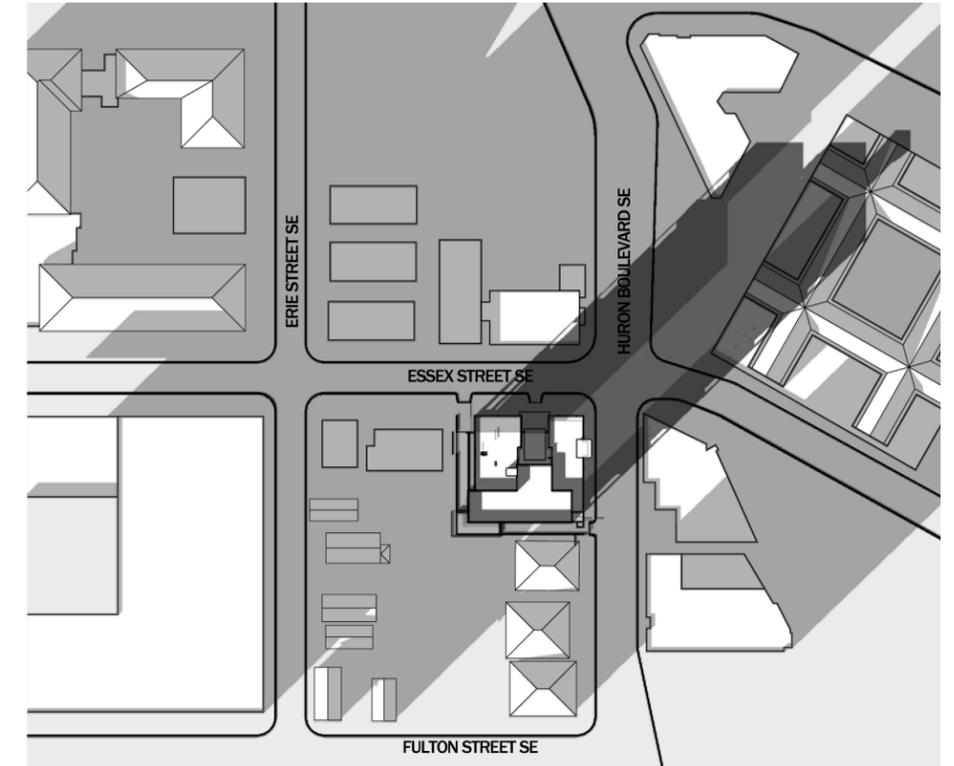
113-0110.0



8:49 AM (HOUR AFTER SUNRISE)
SCALE 1"=200'-0"



NOON
SCALE 1"=200'-0"



3:35 PM (HOUR BEFORE SUNSET)
SCALE 1"=200'-0"



ESSEX AND HURON HOTEL

Minneapolis, Minnesota

March 24, 2014

SHADOW STUDY **DECEMBER 22**

113-0110.0