



**CPED STAFF REPORT**  
Prepared for the City Planning Commission

CPC Agenda Item #5  
April 7, 2014  
BZZ-6454

**LAND USE APPLICATION SUMMARY**

*Property Location:* 81 St. Anthony Parkway  
*Project Name:* Alliance Steel Services  
*Prepared By:* [Hilary Dvorak](#), Principal Planner, (612) 673-2639  
*Applicant:* G&G Real Estate, LLC  
*Project Contact:* Jeffery Ellerd with Wenck Associates  
*Request:* 15,000 square foot addition to an existing recycling facility  
*Required Applications:*

<b>Conditional Use Permit</b>	Conditional use permit to allow for an expansion of a recycling facility in the I2, Medium Industrial District.
<b>Variance</b>	Variance to reduce the off-street parking requirement from 40 to 24 spaces. Variance of the surfacing requirements.
<b>Site Plan Review</b>	Site plan review for a 15,000 square foot addition.

**SITE DATA**

<b>Existing Zoning</b>	I2, Medium Industrial District SH Shoreland Overlay District
<b>Lot Area</b>	271,527 square feet / 6.23 acres
<b>Ward(s)</b>	I
<b>Neighborhood(s)</b>	Columbia Park; adjacent to Marshall Terrace
<b>Designated Future Land Use</b>	Industrial
<b>Land Use Features</b>	Not applicable
<b>Small Area Plan(s)</b>	<a href="#">Industrial Land Use and Employment Policy Plan</a>

<b>Date Application Deemed Complete</b>	March 12, 2014	<b>Date Extension Letter Sent</b>	Not applicable
<b>End of 60-Day Decision Period</b>	May 11, 2014	<b>End of 120-Day Decision Period</b>	Not applicable

## BACKGROUND

**SITE DESCRIPTION AND PRESENT USE.** The property is located in northeast Minneapolis along St. Anthony Parkway. The site is located between the Mississippi River and the west end of the St. Anthony Parkway bridge. The site is currently used as a recycling facility.

**SURROUNDING PROPERTIES AND NEIGHBORHOOD.** The site is surrounded by railroads and other industrial properties. There is a public park located southeasterly of the site. The site is located in the Columbia Park neighborhood and is adjacent to the Marshall Terrace neighborhood.

**PROJECT DESCRIPTION.** In 2000, the City Planning Commission approved land use applications to allow a recycling facility on the site. Today, the applicant is proposing to construct a 15,000 square foot addition on the north side of the existing building. The addition will be used for extra warehousing space within the facility. As part of the project, two stormwater ponds will be created. A stormwater infiltration pond will be constructed on the northwest corner of the site, which will capture stormwater runoff from the roof of the building. A second lined stormwater treatment pond will be constructed on the south side of the property, which will capture the stormwater runoff from the ground.

The building is used as a recycling facility. According to the applicant, Alliance Steel Services is a secure storage facility for the bulking of high-value scrap metals. Alliance Steel Services transports materials from other facilities to this location for recycling purposes. Materials are stockpiled until enough materials are collected, at which time they are then baled or otherwise bulked and shipped off for further processing. The business is not open to the public and the company does not dismantle automobiles.

When the recycling facility was approved to locate on this site in 2000, several conditions were placed on the approvals which, today, the applicant is wishing to amend. The applicant is asking to modify two of the conditions and remove one of them altogether. The first condition that the applicant wishes to modify is that there be no outdoor storage of materials. The applicant wishes to have this clarified to say that outdoor storage is allowed in containers, as that is what has been happening since the use first began operating at this location. The second condition that the applicant wishes to modify is that there be no storage of containers greater than one yard capacity on the south side of the warehouse building. In 2000, staff had conditioned that the larger containers be kept on the north side of the building in order to improve the aesthetics of the site when viewed from St. Anthony Parkway. With the addition being constructed on the north side of the building, and the placement of the stormwater infiltration pond on the northwest corner of the site, much of the area used for container storage on the north side of the site will be gone. The condition that the applicant wishes to remove altogether is that the doors must be closed prior to the unloading of materials. The applicant has indicated that the materials processed at the site are non-ferrous and relatively free of dust and debris. In addition, because the building is not a conditioned space, closing the doors especially during the summer months creates a challenging work environment.

**RELATED APPROVALS.** In 2000, the City Planning Commission approved land use applications to allow a recycling facility on the site. The applications needed at the time were a conditional use permit and site plan review. Several conditions were placed on the approvals which the applicant is today wishing to amend.

**PUBLIC COMMENTS.** Both the Columbia Park Neighborhood Association and the Concerned Citizens of Marshall Terrace organization have submitted comments regarding the project. Any other

correspondence received prior to the public meeting will be forwarded on to the City Planning Commission for consideration.

## ANALYSIS

### CONDITIONAL USE PERMIT

The Department of Community Planning and Economic Development has analyzed the application to allow for an expansion of a recycling facility in the I2, Medium Industrial District based on the following [findings](#):

1. *The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

The Department of Community Planning and Economic Development finds that allowing an expansion of the existing recycling facility on the property located at 81 St. Anthony Parkway would not be detrimental to or endanger the public health, safety, comfort or general welfare. A recycling facility has operated at this location since 2000. To CPED's knowledge there have not been any complaints with the operations of the business at this location.

2. *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

The Department of Community Planning and Economic Development finds that allowing an expansion of the existing recycling facility on the property located at 81 St. Anthony Parkway would not be injurious to the use and enjoyment of other property in the area and would not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district. The site is surrounded by railroads and other industrial properties. The addition will be constructed on the north side of the existing building. The addition will provide additional warehousing space within the building.

Aerial photos and photos of the property show storage containers, both large and small, haphazardly placed around the site. Some of the images show containers located near the entrance gates to the site. While the site is zoned industrial and the use was permitted to have storage containers located on the south side of the property dating back to 2000, CPED staff believes that, because St. Anthony Parkway is part of the Grand Rounds, nothing should be stored near the entrance gates to the site and that a storage container plan needs to be developed in order to better utilize the land outside of the building footprint.

As for the conditions that the applicant is requesting be modified or deleted altogether, CPED feels that the requests are reasonable. As part of the 2000 approvals storage containers were allowed to be kept outside. However, only containers smaller than one yard capacity were allowed to be kept on the south side of the warehouse building. This condition of approval is not being adhered to, based on a recent site visit; it does not appear that any of the storage containers are smaller than one yard. With the submittal of a storage container plan and not allowing any storage near the entrance gates to the site, CPED feels that the aesthetics of the site will be improved upon. As for requiring that the doors be closed prior to the unloading of materials, CPED feels that, given the size of the site and the location of the building, it is reasonable to allow the doors to remain open during the business's hours of operation.

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

The building is existing. All utilities, access roads and drainage are in place.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

The parking requirement for the recycling facility is 40 spaces. The applicant has indicated that the number of employees will increase from 12 full-time employees to 23 full-time employees once the addition is complete. There are currently eight parking spaces located on the site. The applicant is proposing to increase the number of spaces on the site to 24. The applicant has submitted a parking variance.

The loading requirement for the use is three large (12 feet by 50 feet) spaces. There will be a total of seven large loading spaces on the site once the addition is complete.

Both passenger vehicles and trucks will continue to utilize the access drive located on the southwest corner of the site along St. Anthony Parkway. Given the surrounding uses and the topography of the area, this is the only point at which the property can be accessed from a public street.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

The property is located in northeast Minneapolis along St. Anthony Parkway. The site is located between the Mississippi River and the west end of the St. Anthony Parkway bridge. The future land use map in *The Minneapolis Plan for Sustainable Growth* designates the site as Industrial. The site is located in one of the City's seven Industrial Employment Districts which were formed as part of the adoption of the *Industrial Land Use and Employment Plan* in 2006. The objective of Industrial Employment Districts is to protect prime employment space, provide an opportunity for the City to support targeted industrial and business clusters and to redevelop underutilized sites for economic development purposes. Industrial Employment Districts preserve properties for the retention, expansion and attraction of existing and new industrial firms in areas of the city with good transportation access, minimal conflict with nearby land uses and proximity to recent market investment. According to the principles and policies outlined in *The Minneapolis Plan for Sustainable Growth*, the following apply to this proposal:

**Land Use Policy 1.14: Maintain Industrial Employment Districts to provide appropriate locations for industrial land uses.**

1.14.5 Encourage and implement buffering through the site plan review process to mitigate potential conflicts between industrial uses and adjacent other uses.

6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

With the approval of the conditional use permit, variances, and site plan review applications, this development will meet the applicable regulations of the I2, Medium Industrial District.

## VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance to reduce the off-street parking requirement from 40 to 24 spaces based on the following [findings](#):

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The applicant is seeking a variance to reduce the off-street parking requirement from 40 to 24 spaces. The applicant has indicated that the number of employees will increase once the addition is complete from 12 full-time employees to 23 full-time employees. In addition, the facility is not open to the public. The applicant has also indicated that given the placement of the lined stormwater treatment pond on the south side of the property, the location of the loading spaces on the south side of the building and the truck movement operations of the business that there isn't any available space near the building entrance for any additional parking spaces.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The applicant is proposing to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan. Since the business is not open to the public and because there will be only 23 full-time employees, providing 24 parking spaces will be reasonable.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Granting of this variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. In addition, granting of the variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties. Since the business is not open to the public and because there will be only 23 full-time employees, providing 24 parking spaces on the site will meet the parking needs of the business.

## VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance of the surfacing requirements based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

All open off-street parking areas, all driveways leading to such parking areas, and all other areas upon which motor vehicles may be located, are required to be surfaced with a dustless all-weather hard surface material capable of carrying a wheel load of 4,000 pounds. Acceptable surfacing materials include asphalt, concrete, brick, cement pavers or similar material installed and maintained per industry standards.

Practical difficulties exist in complying with the ordinance due to circumstances unique to the property. The total site area is 271,527 square feet (6.23 acres) in size, most of which is covered by the parking area (211,077 square feet). The use is surrounded by railroads and other industrial properties. The nearest proposed unpaved drivable surface is approximately 340 feet from St. Anthony Parkway. The applicant has indicated that the majority of the site is used for storage

container and semi-trailer parking. The applicant has also indicated that, other than those areas used to access the loading areas, the surface is not driven upon as part of the daily or even routine operation of the business. Given the size of the site it would not be practical to pave it in its entirety.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The intent of the ordinance is to provide a durable parking surface that does not produce dust, erode, or allow dirt or other matter to be tracked into the public right-of-way. The applicant is proposing to pave the main parking area and the areas in front of the three loading spaces. The applicant is also proposing to construct two stormwater ponds on the site. A stormwater infiltration pond will be constructed on the northwest corner of the site and a lined stormwater treatment pond will be constructed on the south side of the property near the entrance to the site. The nearest proposed unpaved, drivable surface is approximately 340 feet from St. Anthony Parkway.

Photos of the site and a recent site visit revealed that the area where the storage containers and semi-trailers are parked is in need of repair. The surface has eroded in some areas and needs to be leveled out. CPED is recommending that additional Class 5 be brought in to level out the ground.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

The use is surrounded by railroads and other industrial properties. Because the unpaved areas are separated from the adjacent public streets by paved parking areas, granting the variance would not likely affect the character of the area and should have little effect on adjacent properties. It would not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

## SITE PLAN REVIEW

The Department of Community Planning and Economic Development has analyzed the application based on the required [findings](#) and [applicable standards](#) in the site plan review chapter:

### **I. Conformance to all applicable standards of Chapter 530, Site Plan Review.**

#### **Building Placement and Design – Requires alternative compliance**

- The location of the addition is on the north side of the building which is on the opposite side of the site from St. Anthony Parkway. The addition does not reinforce the street wall. Alternative compliance is required.
- The existing building is located approximately 190 feet from the front property line. Since the addition is being constructed on the side of the building opposite the front property line, alternative compliance is required.
- The area between the building and the front property line will be landscaped. A lined stormwater treatment pond will be constructed on the south side of the property near the entrance to the site.
- The principal entrance to the building faces St. Anthony Parkway.
- The on-site accessory parking area is located between the building and the front property line. This is an existing condition of the site.

- The north and west walls of the addition are over 25 feet in length and blank. Alternative compliance is required.
- The building addition will be made out of metal panels.
- The existing building is made out of concrete block that has been painted. While the addition is made out of metal panels, it is compatible with concrete block.
- The existing building is made out of concrete block and is visible from St. Anthony Parkway.
- The addition is being constructed on the north side of the building, away from the public street and not facing an on-site parking area. Given the location of the addition, no window requirements apply.
- The roof line of the addition will be flat which will match the flat roof of the existing building.

**Access and Circulation** – *Requires alternative compliance*

- The on-site parking area is not connected to the public sidewalk. Alternative compliance is required.
- There is no transit shelter proposed as part of this development.
- Both passenger vehicles and trucks will continue to utilize the access drive located on the southwest corner of the site along St. Anthony Parkway. Given the surrounding uses and the topography of the area this is the only point at which the property can be accessed from a public street.
- This site does not have access to a public alley.
- There is no maximum impervious surface requirement in the I2 District. According to the materials submitted by the applicant, 87 percent of the site will be impervious.

**Landscaping and Screening** – *Requires alternative compliance*

- The zoning code requires that at least 20 percent of the site not occupied by the building be landscaped. The landscaping requirement for this site is 42,215 square feet. The applicant is proposing 34,418 square feet of landscaping, or approximately 16 percent of the site not occupied by the building. The tree and shrub requirement is 84 trees and 422 shrubs. The applicant is proposing 91 trees and 104 shrubs. Alternative compliance is required.
- The 24 on-site parking spaces are divided into two areas near the front of the building. The closest parking space is located over 80 feet from the front property line along St. Anthony Parkway. Most of the parking spaces are more than 50 feet from an on-site deciduous tree. Alternative compliance is required.

**Table 1. Landscaping and Screening Requirements**

	Code Requirement	Proposed
Lot Area	--	271,527 sq. ft.
Building footprint	--	60,450 sq. ft.
Remaining Lot Area	--	211,077 sq. ft.
Landscaping Required	42,215 sq. ft.	34,418 sq. ft.
Canopy Trees (1: 500 sq. ft.)	84 trees	91 trees
Shrubs (1: 100 sq. ft.)	422 shrubs	104 shrubs

**Additional Standards** – *Meets requirements*

- The parking area will be graded so it drains towards the lined stormwater treatment pond located on the south side of the property near the entrance to the site.

- The building addition will not block views of important elements of the city.
- The building addition will not cast shadows on public spaces or adjacent properties.
- The building addition will have minimal wind effects on the surrounding area.
- The business is not open to the public. For security purposes, the site has a fence around it and is monitored by video cameras.
- This site is neither historically designated nor located in a historic district.

**2. Conformance with all applicable regulations of the zoning ordinance.**

The proposed use is *conditional* in the I2 District.

**Off-street Parking and Loading – Meets requirements with Conditions of Approval**

**Table 2. Vehicle Parking Requirements Per Use (Chapter 541)**

	Minimum Parking Requirement	Applicable Reductions	Total Minimum Requirement	Maximum Parking Allowed	Proposed
Recycling facility	40	--	40	120	24
Total	40	--	40	120	24

**Building Bulk and Height – Meets requirements**

**Table 3. Building Bulk and Height Requirements**

	Code Requirement	Proposed
Lot Area	--	271,527 square feet / 6.23 acres
Gross Floor Area (GFA)	--	60,450 sq. ft.
Maximum Floor Area Ratio (GFA/Lot Area)	2.7	.22
Maximum Building Height	4 stories or 56 feet, whichever is less	24 ft.

**Yard Requirements – Meets requirements**

- There are no setback requirements for this use since it is zoned industrial and is surrounded by industrially zoned properties.

**Signs – Meets requirements**

- Signs are subject to Chapters 531 and 543 of the Zoning Code. All new signs are required to meet the requirements of Chapter 543 of the zoning code.
- In the I2 zoning district, 1.5 square feet of signage for every 1 foot of primary building wall is allowed. However, if there is a freestanding sign on the zoning lot then there can only be 1 square foot of signage for every 1 foot of primary building wall. Wall signs are limited to 180 square feet in size. Projecting signs are limited to 20 square feet in size. The height limitation for both wall signs and projecting signs is 28 feet. Freestanding signs are limited to 80 square feet and can be no taller than 8 feet. The zoning code also limits the number of freestanding signs on a zoning lot to 1.
- The applicant is not proposing any new signage on the property. However, the sign that is located near the entrance gates to the site announces “Al Johnson Trucking,” which CPED

understands is no longer operating at this site. CPED is recommending that this sign be removed. If the applicant wishes to install a new sign, the appropriate permits will need to be applied for.

**Dumpster Screening** – *Not applicable*

- The trash dumpster is located on the east side of the building. Given all of the storage containers and semi-truck trailers stored outside the trash dumpster blends in with everything else. As part of the container management plan, the applicant shall designate an area for the trash dumpster. It shall be screened per the requirements of section 535.80 of the zoning code.

**Screening of Mechanical Equipment** – *Not applicable*

- The applicant is not proposing to add any new mechanical equipment to the site as part of the addition.

**Lighting** – *Not applicable*

- The applicant is not proposing to add any new lights to the site as part of the addition.

**Impervious Surface Area** – *Not applicable*

**Specific Development Standards** – *Meets requirements with Conditions of Approval*

- The specific development standard for a recycling facility is: “The use shall be performed in a fully enclosed building, except that paper and cardboard may be stored outside in fully enclosed containers or trailers. For the purposes of this requirement, "enclosed" shall mean completely enclosed with no outdoor storage, sorting or processing of materials.”

**Overlay District Standards** – *Meets requirements*

- The addition is in compliance with the regulations of the SH Shoreland Overlay District.

**3. Conformance with the applicable policies of The Minneapolis Plan for Sustainable Growth.**

*The Minneapolis Plan for Sustainable Growth* identifies the site as industrial on the future land use map. The proposed development is consistent with the following principles and policies outlined in the comprehensive plan:

**Urban Design Policy 10.12: Design industrial uses with appropriate transitions and other design features which minimize negative impacts on surrounding residential uses.**

- 10.12.1 Provide appropriate physical transition and separation using green space, fencing, setbacks or orientation between industrial uses and other surrounding uses.
- 10.12.2 Encourage site planning for new developments that orients the “back” of proposed buildings to the “back” of existing development.
- 10.12.4 Design industrial sites to ensure direct access to major truck routes and freeways as a way to minimize automobile and truck impacts on residential streets and alleys.
- 10.12.5 Promote quality design and building orientation of industrial development that is appropriate with the surrounding neighborhoods.

- 10.12.6 Use the site plan review process to ensure that lighting and signage associated with industrial uses do not create negative impacts for residential properties.

#### **4. Conformance with applicable development plans or objectives adopted by the City Council.**

The site is located in one of the City's seven Industrial Employment Districts which were formed as part of the adoption of the *Industrial Land Use and Employment Plan* in 2006. The objective of Industrial Employment Districts is to protect prime employment space, provide an opportunity for the City to support targeted industrial and business clusters and to redevelop underutilized sites for economic development purposes. Industrial Employment Districts preserve properties for the retention, expansion and attraction of existing and new industrial firms in areas of the city with good transportation access, minimal conflict with nearby land uses and proximity to recent market investment.

#### **5. Alternative compliance.**

The Planning Commission or zoning administrator may approve alternatives to any site plan review requirement upon finding that project meets one of three criteria required for [alternative compliance](#). Alternative compliance is requested for the following requirements:

- The location of the addition is on the north side of the building which is on the opposite side of the site from St. Anthony Parkway. The existing building is located approximately 190 feet from the front property line. Since the addition is being constructed on the side of the building opposite the front property line, it does not reinforce the street wall nor is it located within 8 feet of the front property line. CPED is recommending that the planning commission grant alternative compliance, as strict adherence to the requirements is impractical because of existing conditions.
- The north and west walls of the addition are over 25 feet in length and blank. The addition will be constructed on the north side of the existing building facing the railroad tracks. The addition will be used for extra warehousing space within the facility. CPED is recommending that the planning commission grant alternative compliance as strict adherence to the requirements is impractical because of these conditions.
- The on-site parking area is not connected to the public sidewalk. Since the business is not open to the public, CPED is recommending that the planning commission grant alternative compliance, as strict adherence to the requirements is impractical.
- The zoning code requires that at least 20 percent of the site not occupied by the building be landscaped. The landscaping requirement for this site is 42,215 square feet. The applicant is proposing 34,418 square feet of landscaping, or approximately 16 percent of the site not occupied by the building. The tree and shrub requirement is 84 trees and 422 shrubs. The applicant is proposing 91 trees and 104 shrubs. Given the size of the site and the nature of the business it would be impractical to increase the amount of landscaping on the site or meet the shrub requirement. However, to offset the overall amount of landscaping and number of shrubs on the site, CPED is recommending that a minimum of 20 evergreen trees be planted on the site. Evergreen trees will provide screening year-round.
- The 24 on-site parking spaces are divided into two areas near the front of the building. The closest parking space is located over 80 feet from the front property line along St. Anthony Parkway. Most of the parking spaces are more than 50 feet from an on-site deciduous tree. Given where the parking spaces are located on the site and where the lined stormwater

treatment pond is located, it is impractical to locate every parking space within 50 feet of an on-site deciduous tree.

## RECOMMENDATIONS

### **Recommendation of the Department of Community Planning and Economic Development for the Conditional Use Permit:**

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the application for a conditional use permit to allow for an expansion of a recycling facility at the property located at 81 St. Anthony Parkway, subject to the following conditions:

1. The conditional use permit shall be recorded with Hennepin County as required by Minn. Stat. 462.3595, subd. 4 before building permits may be issued or before the use or activity requiring a conditional use permit may commence. Unless extended by the zoning administrator, the conditional use permit shall expire if it is not recorded within two years of approval.
2. Nothing shall be stored near the entrance gates to the site.
3. A storage container plan shall be developed in order to better utilize the land outside of the building footprint.

### **Recommendation of the Department of Community Planning and Economic Development for the Variance:**

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the application for a variance to reduce the off-street parking requirement from 40 to 24 spaces at the property located at 81 St. Anthony Parkway.

### **Recommendation of the Department of Community Planning and Economic Development for the Variance:**

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the application for a variance of the surfacing requirements at the property located at 81 St. Anthony Parkway, subject to the following conditions:

1. Additional Class 5 shall be brought in to level out the ground where the storage containers and semi-trailers are parked.

### **Recommendation of the Department of Community Planning and Economic Development for the Site Plan Review:**

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt the above findings and **approve** the site plan review application to allow for a 15,000 square foot addition at the property located at 81 St. Anthony Parkway, subject to the following conditions:

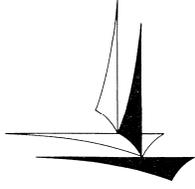
1. Approval of the final site, landscaping, elevation and lighting plans by the Department of Community Planning and Economic Development
2. All site improvements shall be completed by April 7, 2016, unless extended by the Zoning

Administrator, or the permit may be revoked for non-compliance.

3. A minimum of 20 evergreen trees shall be planted on the site.
4. The “Al Johnson Trucking” sign located near the entrance gates to the site shall be removed.
5. As part of the container management plan the applicant shall designate an area for the trash dumpster. It shall be screened per the requirements of section 535.80 of the zoning code.

## ATTACHMENTS

1. PDR report
2. Written description and findings submitted by applicant
3. Zoning map
4. Site and civil plans
5. Landscaping plans
6. Building elevations
7. Photos
8. Correspondence



Minneapolis Development Review  
250 South 4<sup>th</sup> Street  
Room 300  
Minneapolis, MN 55415

## Preliminary Development Review Report

Development Coordinator Assigned: **DONALD ZART**  
**(612) 673-2726**  
**don.zart@minneapologismn.gov**

<b>Status *</b>
<b>RESUBMISSION REQUIRED</b>

<b>Tracking Number:</b>	PDR 1001174
<b>Applicant:</b>	G & G REAL ESTATE 115 31ST AVENUE NORTH MINNEAPOLIS, MN 55411
<b>Site Address:</b>	81 ST ANTHONY PKWY
<b>Date Submitted:</b>	26-FEB-2014
<b>Date Reviewed:</b>	05-MAR-2014

### Purpose

The purpose of the Preliminary Development Review (PDR) is to provide Customers with comments about their proposed development. City personnel, who specialize in various disciplines, review site plans to identify issues and provide feedback to the Customers to assist them in developing their final site plans.

The City of Minneapolis encourages the use of green building techniques. For additional information please check out our green building web page at: [http://www.ci.minneapolis.mn.us/mdr/GreenBuildingOptions\\_home.asp](http://www.ci.minneapolis.mn.us/mdr/GreenBuildingOptions_home.asp).

***DISCLAIMER: The information in this review is based solely on the preliminary site plan submitted. The comments contained in this report are preliminary ONLY and are subject to modification.***

### Project Scope

15,000 sq ft building addition for expanded operations.

### Review Findings (by Discipline)

#### Traffic and Parking

- The plan as submitted meets the requirements of the Public Works Traffic & Parking Services Division

#### Water

- The plan as submitted meets the requirements of the Public Works Water Maintenance & Distribution Division.

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\*Approved: You may continue to the next phase of developing your project.

\*Resubmission Required: You cannot move forward or obtain permits until your plans have been resubmitted and approved.

### ❑ Zoning - Planning

- There needs to be a plan for the temporary storage of containers outside. The plans submitted for Planning Commission review should show where these containers will be located along with a written description and photo of the containers.
- Applications required for Planning Commission review:
  - Conditional use permit for an expansion to a recycling facility
  - Parking variance
  - Surfacing material variance
  - Site plan review

### ❑ Addressing

- There will not be a change in address for the proposed addition.

### ❑ Parks - Forestry

- Contact Paul Martinson (612-499-9209) regarding removal or the process for protection of trees during construction in the city right of way.
- Effective January 1, 2014, the City of Minneapolis and the Minneapolis Park and Recreation Board adopted an update to the existing Parkland Dedication Ordinance.
- The adopted City of Minneapolis Parkland Dedication ordinance is located in Section 598.340 of the City's Land Subdivision ordinance:
- <http://library.municode.com/index.aspx?clientId=11490>
- As adopted, the fee in lieu of dedication for new residential units is \$1,500 per unit (affordable units excluded per ordinance) and for commercial and industrial development it is \$200 per development employee (as defined in ordinance). Any dedication fee (if required) must be paid at the time of building permit issuance. There is also an administration fee that is 5% of the calculated park dedication fee.
- As proposed, for your project the Alliance Steel addition, the calculated dedication fee is as follows:

• Park Dedication fee based on structure addition	=	\$1,600
• 5% of \$1,600 (Administrative Fee)	=	\$ 80
• Total	=	\$1,680
- This is a preliminary calculation based on your current proposal; a final calculation will be made at the time of building permit submittal.
- For further information, please contact Don Zart at 612-673-2726.

### ❑ Right of Way

- The plan as submitted meets the requirements of the Public Works Right-Of-Way Division.
- The contact for the bridge modification is Ole Mersinger (612) 673-3537.

### ❑ Street Design

- The plan as submitted meets the requirements of the Public Works Street Design Division.

### ❑ Sidewalk

- The plan as submitted meets the requirements of the Public Works Sidewalk Inspections Division

### ❑ Fire Safety

- Maintain fire suppression system throughout new addition
- Maintain fire department access at all times

## ❑ Business Licensing

- Contact Business Licensing for license requirements.

## ❑ Environmental Health

- Documented petroleum releases have occurred on some parcels in the area that may have adversely affected the site. If the project will be disturbing identified impacted soils on the site preapproval for removal, disposal and/or reuse of must occur from the MCPA and the City of Minneapolis prior to beginning excavation activities. If impacted soil is encountered during site activities work will need to stop and notification provided to the MN State Duty officer at (615) 649-5451.
- The site Industrial Stormwater Plan for MPCA Permit # MNR0534L7, should be reviewed and updated as necessary to reflect site changes. If there are changes they should and submitted to the MCPA as required under your current permit.
- If dewatering is required during site construction see below for city permit requirements. Subgrade structures should be designed to prevent infiltration of groundwater without the need for a permanent dewatering system being installed. If a continuously operating permanent dewatering system is needed it must be approved as part of the sanitary sewer and storm drain site plan approval prior to construction beginning.
- No construction, demolition or commercial power maintenance equipment shall be operated within the city between the hours of 6:00 p.m. and 7:00 a.m. on weekdays or during any hours on Saturdays, Sundays and state and federal holidays, except under permit. Contact Environmental Services at 612-673-3867 for permit information.
- Permits and approval are required from Environmental Services for the following activities: Temporary storage of impacted soils on site prior to disposal or reuse; Reuse of impacted soils on site; Dewatering and discharge of accumulated storm water or ground water, underground or aboveground tank installation or removal, well construction or sealing. Contact Tom Frame at 612-673-5807 for permit applications and approvals.
- A review of the project, permits issued and an inspection from Environmental Service for identification of equipment and site operations that require annual registration with the City of Minneapolis will occur for this project.

## ❑ Historical Preservation Committee

- There is no HPC flag on this property. HPC review is not required at this time. HPC review is required for any wrecking permits pertaining to the removal of existing structures.

## ❑ Sewer Design

### **Stormwater Management:**

- Please provide the necessary documentation and analysis demonstrating compliance with Chapter 54, including stormwater calculations, BMP maintenance plans, etc.
- Please provide a copy of any geotechnical reports for the site. In the absence of field measured infiltration rates, the design infiltration rates from the MN Stormwater Manual ([http://stormwater.pca.state.mn.us/index.php/Design\\_infiltration\\_rates](http://stormwater.pca.state.mn.us/index.php/Design_infiltration_rates)) should be used for the design of the infiltration practice. Please note that the drawdown time for the infiltration feature should be 48 hours or less. Also provide a cross section detail of the proposed infiltration feature.
- An operations and maintenance plan is required for the stormwater treatment devices. The O&M plan shall define the maintenance regimen, including type and interval of maintenance and party to conduct such maintenance. Please provide a copy of the O&M Plan.
- Please add the following note to the appropriate plan sheet: The Contractor, property owner or responsible party shall contact Minneapolis Surface Waters and Sewers 48 hours prior to any excavation or construction related to or in the location of the proposed Stormwater Management BMP (Contact Paul Chellsen, 673-2406 or [paul.chellsen@minneapolismn.gov](mailto:paul.chellsen@minneapolismn.gov)).
- Please add the following note to the appropriate plan sheet: Upon the project's completion the General Contractor, Property Owner or Responsible Party shall provide to the Department of Public Works a Final

Stormwater Management Report including record drawings. This report will serve as a means of verification that the intent of the approved stormwater management design has been met. This final report shall substantiate that all aspects of the original design have been adequately provided for by the construction of the project.

- Please identify the location and details of the roof drains and any proposed drain tile discharges. As was previously discussed, discharge to the infiltration feature should be limited to runoff from the roof area.
- It is unclear what portion, if any, of the existing storm sewer from the site is being reconstructed. Please identify the proposed improvements to the storm sewer on the plans.
- Please provide copies of any correspondence with the MPCA, confirming they do not have any issues with the proposed infiltration feature as it relates to the site's industrial stormwater permit.
- As was previously discussed, it is the City's understanding that the south pond is proposed to be lined to prevent infiltration due to the use of the site. If this is the case, please note this on the plans along with details regarding the liner material. Please also include the NWL and HWL of the pond on the plans.
- It is recommended that notes be added to the plans identifying the construction procedure for the infiltration feature to ensure it is properly constructed.

**Surface Drainage:**

- Please correct the proposed contours in the northwest portion of the site, as they do not appear to tie in properly with the existing. Spot elevation along the building addition perimeter should also be identified on the plans to ensure proper drainage away from the building.

**Utility Connections:**

- All existing service connections to the property shall be shown and noted on the plans as being removed or remaining in place. For service connection records contact (612) 673-2451.
- For comments or questions on Public Works Surface Water & Sewers Division related requirements please contact Jeremy Strehlo, (Professional Engineer) at (612) 673-3973, or [jeremy.strehlo@minneapolismn.gov](mailto:jeremy.strehlo@minneapolismn.gov)

**□ Construction Code Services**

- Identify curb ramp with slope at accessible parking space, accessible sign location, access aisle dimensions with "No Parking" marking on access aisle as well as include an enlarge detail plan of accessible route to accessible entrance in construction permit plan set per Minnesota State Accessibility Code section 406, 502 and 503.
- Per Minnesota Accessibility Code Section 1113.9 and 1113.9.1, 20% of the cost of the alteration to the primary function area will be required for accessibility upgrades such as accessible toilet facilities, path of travel, parking facilities, telephones and drinking fountains. This information is to be provided in construction plan.
- Provide soil and watertable elevation to help determine footing construction type and depth.
- Indicate proposed setbacks from new addition to property line on a site plan. \*\*\*Reason: No setback indicated on copy of signed survey submitted for PDR\*\*\*
- Contact the Met Council for a SAC determination. See this link for additional information. [http://www.minneapolismn.gov/www/groups/public/@regservices/documents/webcontent/convert\\_281675.pdf](http://www.minneapolismn.gov/www/groups/public/@regservices/documents/webcontent/convert_281675.pdf)

END OF REPORT

## **Statement of Proposed Use and Project Description**

G&G Real Estate, LLC has prepared this application related to the expansion of the existing operations by Alliance Steel Services on the property located at 81 St. Anthony Parkway in Minneapolis. The property is located in the Columbia Park Neighborhood. The Marshall Terrace Neighborhood is located across St. Anthony Parkway to the south.

### **Site Description**

The site encompasses 271,527 square feet, of which 45,428 SF is currently occupied by a one story office and warehouse facility. The majority of the remainder of the site is asphalt or gravel area used for operations or the temporary storage of roll off containers and semi-trailers. Three trailer height loading docks are currently in operation on the south façade of the existing facility. Four trailer height loading docks are also located on the north building façade. These docks are not currently in use. Additionally overhead drive in doors are located on the south and east facades of the existing building and are used as part of current operations.

The site is generally sloped from northeast to southwest. A railroad berm with an elevated spur is present on the east and north side of the site providing a natural buffer from the site in those directions. St. Anthony Parkway is elevated along the majority of the site, with grade matching the site at the far west side where the only access to the site is located from public streets. A thick growth of screening vegetation including trees and shrubs is present on the slope between St. Anthony Parkway and the operating area of the site.

Stormwater on the site currently either runs off or infiltrates. There is a storm sewer inlet located to the northeast of the site entrance.

### **Site Operations**

The site is used by Alliance Steel Services and its tenants as a secure storage facility for the bulking of high-value scrap metals. Alliance transports materials from other facilities to this location for indoor storage. The materials are stockpiled in the building until sufficient volume is collected. The materials are then either baled or otherwise bulked and shipped off for further processing. The facility is not open to the public and does not purchase materials from the public. It is solely a location for storage and bulking of Alliance or affiliated company materials. The site does not accept or process automobiles at this facility, nor does it accept automobile parts. No crushing or other processing of metals, other than the bulking and baling occurs or will occur at this facility.

The facility currently houses 12 full-time employees. With the proposed addition, the employment will rise by 11 to a total of 23 full-time employees. As part of the addition, no significant changes are proposed to operations other than an increase in the warehousing and loading dock space.

### **Proposed Project**

The proposed project consists of the construction of a 15,000 SF building addition to the north façade of the existing building. As part of the construction, stormwater management upgrades and landscaping will be constructed. The addition will consist of an engineered steel building with two man doors located on the north façade and four trailer height loading docks on the east façade. The addition will replace

the majority of the existing concrete block north façade. The existing building is fully sprinkled and the addition will be as well. The existing north wall will be modified to provide better access into the new addition with cutting or expanding of existing openings. The addition floor slab will match existing, providing one continuous storage and operational space.

As part of the construction of the addition, a significant portion of the site will be regarded. Roof water will be directed to a newly constructed stormwater infiltration pond on the northwest corner of the site. This pond will accept water from the entire 15,000 SF addition and approximately two-thirds of the existing building roof. Existing roof drains will be routed through the existing building, beneath the proposed addition and discharge into the new infiltration pond. It has been sized to fully contain a 1.1 inch rainfall event for all roof water that will be diverted to it. A berm will be constructed around the pond to ensure that no contact water from the site can flow into the infiltration pond.

The remainder of the site will be graded such that water will flow to the south side of the site where a new lined stormwater treatment pond will be constructed. The treatment pond will discharge to the existing storm sewer line. During a rainfall event, the pond will retain water, allowing for suspended solids to precipitate out of the water. A manhole with an overflow structure and weir will discharge water as needed. It will be equipped with a small diameter drain which will allow for water to drain out of the pond within 48 hours so the pond does not routinely hold water.

Landscaping and greenspace are being added as part of the proposed construction project. In addition to the stormwater infiltration pond, trees, shrubs and other vegetation will be added in and around the pond. Further enhancements to the landscaping will be focused on the south side of the building and site increasing the curb appeal on the public side of the facility.

Lastly, the existing asphalt surface located at the entrance and extending to the entrance to the building will be extended to the end of the existing loading dock area on the south side of the site. This will reduce the generation of dust and tracking on the public side of the site. With the added stormwater improvements and landscaping, the site will have significantly less stormwater runoff after the project is complete than is currently occurring. Further the new treatment pond on the south side of the site will improve the water quality of any runoff leaving the site.

## Continuation of Existing Conditional Use Permit (CUP)

As part of this application, the applicant is requesting that the existing CUP be amended to include the proposed addition and changes to operations. The existing CUP contains conditions as follows:

1. Approval of the Planning Department of final plans and landscape plans for the site,
2. No outdoor storage,
3. No auto wrecking or dismantling,
4. No storage of containers greater than 1 yard on the south side of the warehouse,
5. Doors must be closed prior to the unloading of materials,
6. Planning Department approval of a sweeping program designed to minimize litter leaving the site.

With this application, the applicant is requesting the conditions of the CUP be amended as follows:

- Condition 1 remains as-is. The new site plan and landscape plan will be approved as part of the building addition project.
- Condition 2 be modified to clarify that outdoor storage is allowed within containers. No outdoor storage on the ground of any materials will occur. Further, any materials stored within containers outside will be for a short duration and will not consist of any materials that have the potential to have any oils or residues on them.
- Condition 3 remain as-is. The site does no auto wrecking or dismantling and will do no such work going forward. The site is recycling facility that aggregates materials and provides indoor storage for high value metals.
- Condition 4 be modified to allow for short-term placement (less than 72 hours) of larger containers on the south side of the building until the container can be processed into the building.
- Condition 5 should be removed. The materials processed at the site are non-ferrous and relatively free of dust and debris. The interior warehouse facility is not a conditioned space. Keeping doors closed, primarily in summer months creates a challenging work environment.
- Condition 6 remains as-is. A sweeper is located on site. Hard surfaces will be swept as-needed.
- With the construction of the building addition and the two stormwater control structures, the site storage space will significantly decrease. The applicant is enhancing the site with new landscaping, surfacing and stormwater improvements. These additions will greatly improve water quality and site aesthetics. We ask that the CUP be amended to include the addition and that condition Number 4 be modified as part of this approval process.

As part of the CUP application process, the applicant is required to address certain required findings related to the CUP. The statements associated with those necessary findings are as follows:

1. The maintenance of the existing conditional use permit will not be detrimental to or endanger the public health, safety, comfort, or general welfare. The site is located in a current industrial area. The long range planning for the City calls for this area to remain industrial for at least the next 20 plus years. The site has been operating in a manner similar to what is being requested for at least the past 15 or more years with no impact to neighbors or the public. Operations are largely conducted within the building. Traffic is not significant from the operations. The public

view of the site is currently screened by trees and shrubs which will be enhanced as part of the project.

2. The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district. The site is located in an Industrial I-2 zoning district. Neighboring uses consist of rail yards and commercial and industrial operations. As previously indicated, the site and the area are targeted to remain as industrial land in the City's long range planning documents. The operation is consistent with and a less intensive use than the rail yards located east and north of the site. Neighboring uses are not impacted by current operations which will not significantly change with the proposed project. Minimal additional traffic may be added, but street infrastructure is more than adequate with connections to main arterial routes to the east and west of the site. The added landscaping and site improvements will enhance the enjoyment of the public on St. Anthony Parkway.
3. Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided. The proposed project will have a minimal impact on utilities at the site. Stormwater demand on existing infrastructure will be reduced as part of the project. Sanitary usage may increase slightly but existing infrastructure is more than adequate for the modest increase. The site is served by other utilities (gas and electric) and does not have concerns related to capacity. The existing roadway infrastructure is more than sufficient to sustain any modest increase in traffic associated with the project. Further the construction of the new St. Anthony Parkway Bridge will provide for further access and capacity.
4. Adequate measures have been or will be taken to minimize traffic congestion in the public streets. The site has more than adequate area for truck or vehicular traffic to enter the site and not stage or stand on public streets. Truck traffic in and out of the site is controlled by the Applicant and can be coordinated such that no change will be noticed on the public streets.
5. The Conditional Use is compatible with the applicable policies of the Comprehensive Plan. The conditional use allows for the continued operation of an industrial facility in an industrial zoned area. Further the CUP will add employees at the site and increase the taxable base within the industrial district.
6. The Conditional Use shall, in all other respects, conform to the applicable regulations of the district in which it is located. The applicant intends to operate the facility in such a manner as to conform to any other applicable regulations for the district, with the exception of three variances which are sought for the project and outlined further in the next section.

## **Site Variances**

As part of the application, the Applicant is seeking relief through variance in three areas. These include:

1. Parking requirements. Per code, the site and buildings are required to have 40 parking spaces. The Applicant is requesting a variance to 24 spaces.
2. Dust-free drive surfaces. Per code, the site is required to provide a dust-free surface in all drive areas. The majority of the site is used for storage; however, drive areas to the storage and access to the proposed new loading docks on the northeast side of the site can still be considered drive areas. The Applicant is requesting a variance from providing a dust-free surface across the site. The Applicant is proposing to provide additional dust-free surface extending along the south loading area toward the site entrance/exit.

## **Parking**

The site currently has 11 full-time employees. With the addition, it is proposed to expand to 23 employees. The site does no business with the public; therefore, there is no reason to provide parking for 40 automobiles. The site will add designated parking for the 23 employees, along with a new handicap parking stall. They are provided in the front of the building with 7 standard stalls and 1 handicap stall along the front, 4 sets of double stalls immediately east of the entrance and west of the loading docks and 8 parallel parking stalls on the west side of the existing building. Due to site constraints and proposed operations, it is unreasonable to require the added parking stalls. The stalls would impede the ability of the site to operate as needed with the reduction in area due to the addition, but also the added stormwater ponding and landscaping.

## **Dust Free Surfaces**

The Applicant is proposing to create additional dust-free surfaces at the entrance and extending eastward along the front façade of the building. The additional asphalt paving will mitigate the potential for offsite tracking of dust by increasing the travel distance from the primary loading dock to the street that is paved. Further, the Applicant is proposing significant improvements to the stormwater infrastructure at the site which is another primary reason for the move to a dust-free surface. The stormwater pond added on the south side of the site will serve to capture and collect stormwater for pre-treatment prior to the water entering the City storm sewer system, a significant increase in overall stormwater quality for the site.

The majority of the site is currently used for storage, primarily of empty semi-trailers, along with some roll off boxes and other miscellaneous containers. It is not typically driven upon as part of daily or even routine operations at the Site. Most nearby properties are of an industrial nature and no evidence of dust tracking or blowing has been documented to date.

## **Alternative Compliance**

The Applicant is seeking alternative compliance for one requirement of section of Chapter 530, the greenspace requirements for the site. Per code, the site is required to have 42,215 SF of greenspace. The Applicant is requesting a variance to 34,418 SF of greenspace; an increase of 7,818 SF from current site conditions of 26,600 SF. Additionally, per code, the site is required to have 1 shrub per 100 SF of required greenspace or 422 shrubs. The Applicant is requesting a variance to 84 shrubs due to site constraints and the presence of more than 300 existing shrubs (tartarian honeysuckle and common buckthorn) in the existing greenspace on the site. The Applicant is proposing to add 84 shrubs to the site, along with 17 canopy trees to supplement the existing 87 canopy trees currently located on the

site. There is not enough physical space to accommodate the required number of shrubs on the site. Additionally, the existing screening and greenery would have to be removed and reestablished along the south side of the site to provide additional area. This would reduce or eliminate the screening value to the public that currently exists along St. Anthony Parkway.

With the project, the Applicant is proposing to increase the greenspace and add significant stormwater improvements to the site. To operate as necessary, the site cannot meet the required greenspace requirements as calculated. Adding the additional greenspace will cause significant operational difficulties predominantly associated with truck access and storage areas. The proposed added greenspace represents an increase of nearly 30% over existing conditions. Due to the site's status as a Sector N site, the stormwater treatment pond has to be lined which does not allow for it to be vegetated or counted as greenspace. However, this pond represents a significant contribution to the environmental impact of the site. The shrub requirement for the site of 422 shrubs cannot physically be accommodated on the site.

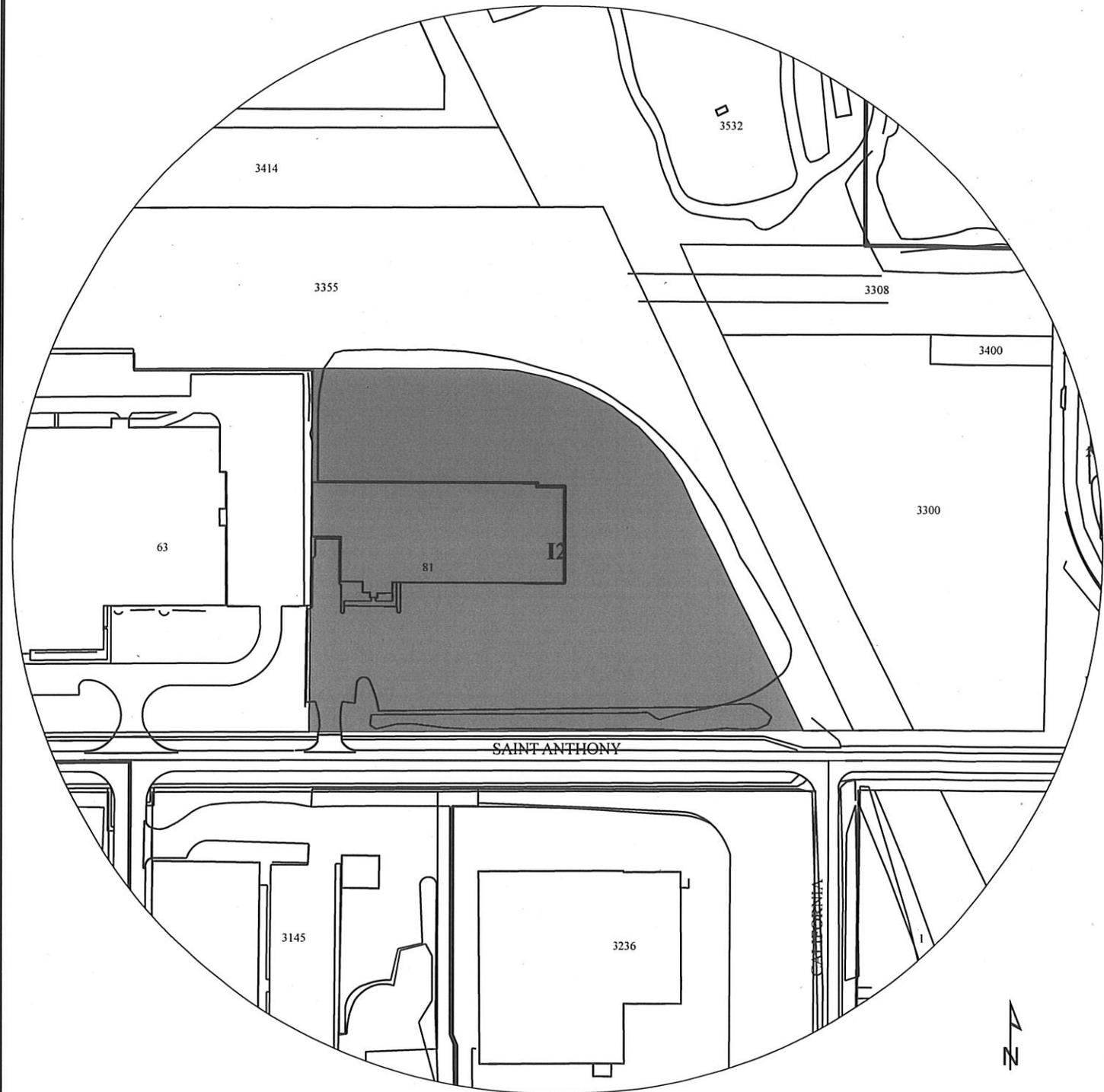
The Applicant is requesting alternative compliance as outlined in Section 530.80(2). The Site is an operating industrial facility. The proposed improvements are reducing the available site area for storage and operations. The Applicant is providing stormwater management features that go above and beyond the applicable requirements. By doing this, the Applicant is seeking relief from adding additional greenspace to the site, reducing its available operations footprint.

**G&G Real Estate, LLC**

**1st**

NAME OF APPLICANT

WARD



200 100 0 200 400

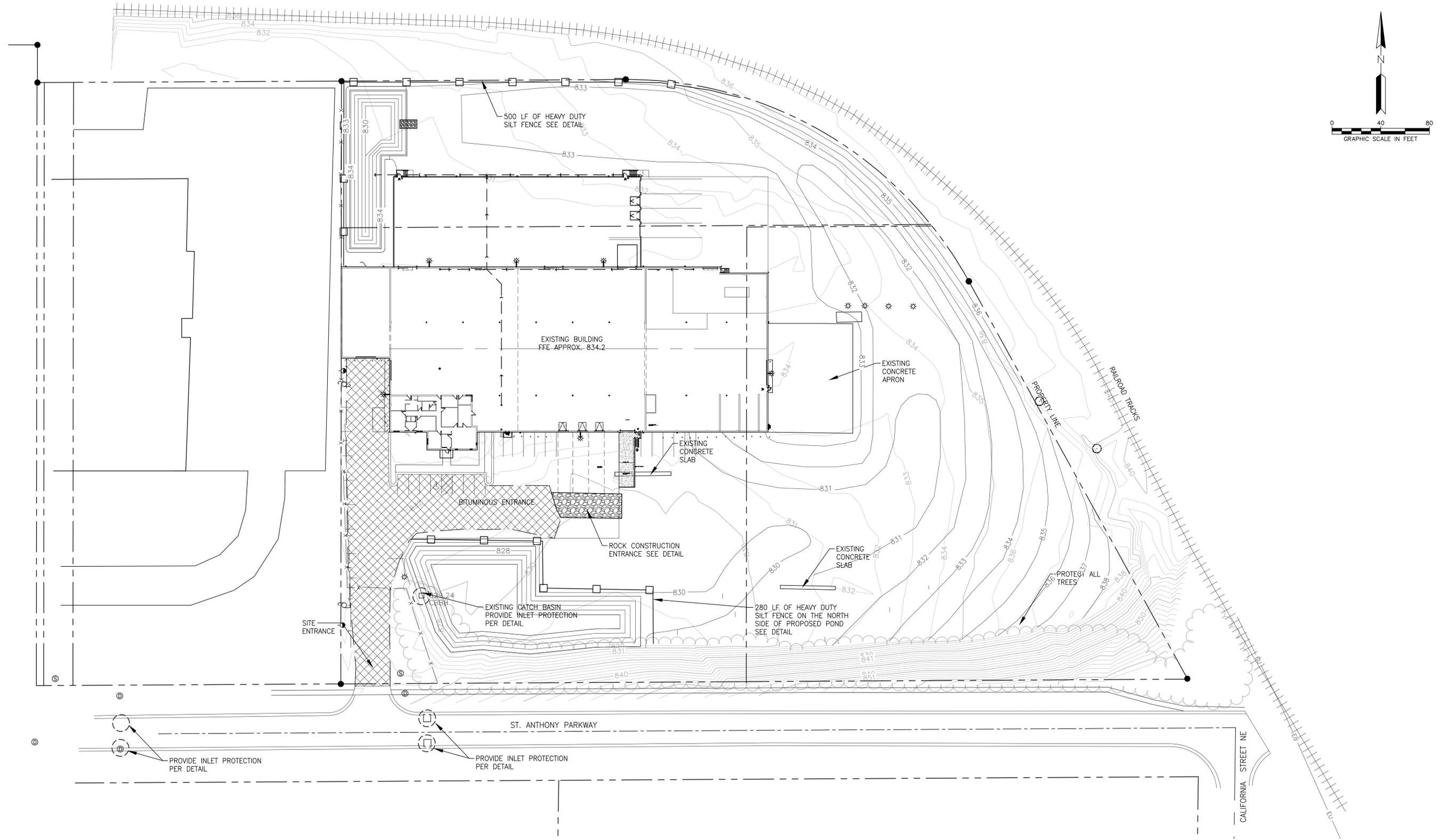
PROPERTY ADDRESS

**81 St. Anthony Parkway**

FILE NUMBER

**BZZ-6454**





REV	REVISION DESCRIPTION	DWN	APP	REV DATE
2	LAND USE RESUBMITTAL	JVB	JE	03/12/14
1	ISSUED FOR PERMIT	JVB	JE	02/19/14

SEAL

PRIME CONSULTANT



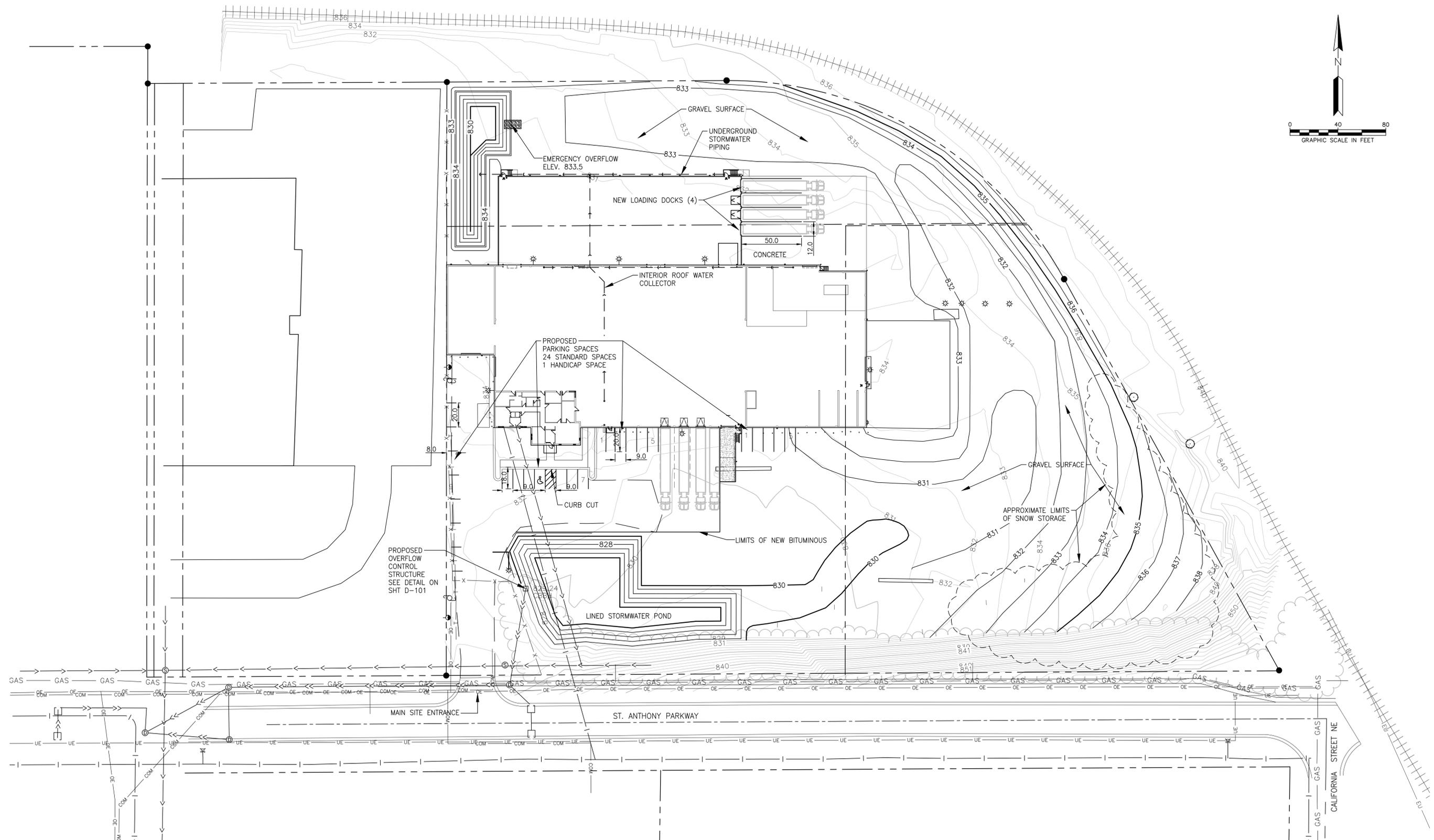
Wenck Associates, Inc.  
Consulting Engineers  
1800 PIONEER CREEK CENTER  
MAPLE PLAN, MN 55359

Web Site: www.wenck.com  
763-479-4200  
763-479-4242

PROJECT TITLE  
FACILITY IMPROVEMENT PROJECT

ALLIANCE STEEL FACILITY  
81 ST. ANTHONY PARKWAY  
MINNEAPOLIS, MINNESOTA

SHEET TITLE EROSION CONTROL PLAN			
DWN BY JVB	CHK'D JE	APP'D JE	DWG DATE NOV 2013
PROJECT NO. 0258-52	SHEET NO. C-102	SCALE AS SHOWN	REV NO. 2



REV	REVISION DESCRIPTION	DWN	APP	REV DATE
2	LAND USE RESUBMITTAL	JVB	JE	03/12/14
1	ISSUED FOR PERMIT	JVB	JE	02/19/14

SEAL

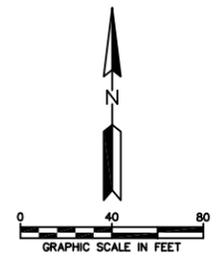
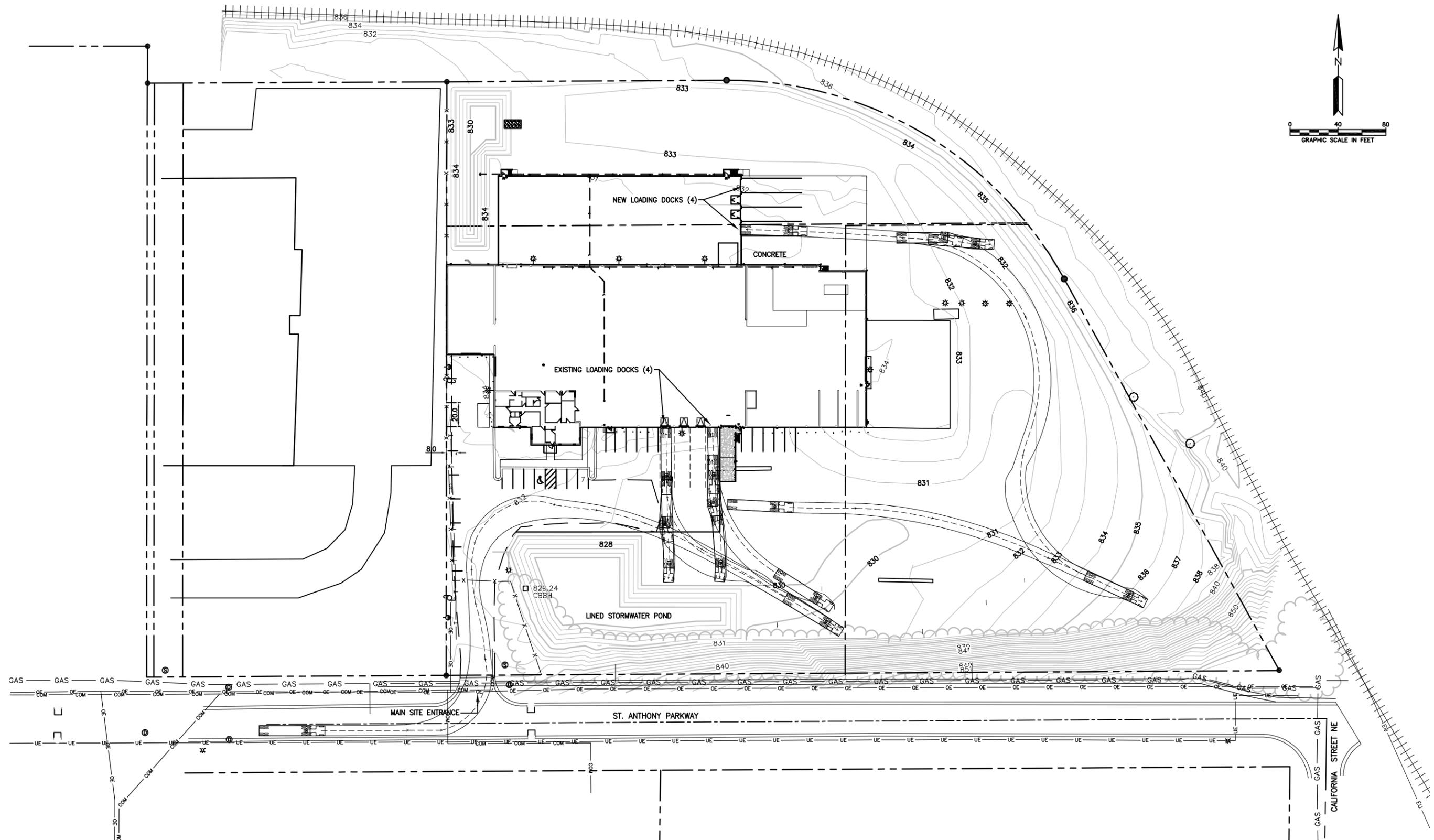
PRIME CONSULTANT

Wenck  
Consulting Engineers  
1800 PIONEER CREEK CENTER  
MAPLE PLAIN, MN 55359  
763-479-4200  
763-479-4242

PROJECT TITLE  
FACILITY IMPROVEMENT PROJECT

ALLIANCE STEEL FACILITY  
81 ST. ANTHONY PARKWAY  
MINNEAPOLIS, MINNESOTA

SHEET TITLE PROPOSED SITE IMPROVEMENTS			
DWN BY JVB	CHK'D JE	APP'D JE	DWG DATE NOV 2013
PROJECT NO. 0258-52	SHEET NO. C-103	SCALE AS SHOWN	REV NO. 2



REV	REVISION DESCRIPTION	DWN	APP	REV DATE
1	ISSUED FOR PERMIT	JVB	JE	03/28/14

SEAL

PRIME CONSULTANT

**Wenck**  
 Wenck Associates, Inc.  
 Consulting Engineers  
 1800 PIONEER CREEK CENTER  
 MAPLE PLAIN, MN 55359  
 Web Site: www.wenck.com  
 763-479-4200  
 763-479-4242

PROJECT TITLE  
**FACILITY IMPROVEMENT PROJECT**

ALLIANCE STEEL FACILITY  
 81 ST. ANTHONY PARKWAY  
 MINNEAPOLIS, MINNESOTA

SHEET TITLE		PROJECT NO.		SHEET NO.		REV NO.	
TRUCK ROUTE TURNING PLAN		0258-52		C-104		1	
DWN BY	CHK'D	APP'D	DWG DATE				
JVB	JE	JE	NOV 2013				
			SCALE				
			AS SHOWN				

### SILT FENCE INSTALLATION

**TRENCH DETAIL**  
 STEEL OR WOOD POST 36" LENGTH MAX AT 4' MAX SPACING  
 POND HEIGHT  
 DIRECTION OF RUNOFF FLOW  
 4" MIN  
 4" X 6" TRENCH WITH COMPACTED BACKFILL  
 6" MIN  
 12" MIN

**INSTALLATION WITHOUT TRENCHING**  
 STEEL OR WOOD POST 36" LENGTH MAX AT 4' MAX SPACING  
 POND HEIGHT  
 DIRECTION OF RUNOFF FLOW  
 9" MAX STORAGE HT  
 3/4" MIN DRAIN ROCK  
 12" MIN

USE HI-FLOW FILTER FABRIC, 200 GAL. PER MINUTE PER SQUARE FOOT

**NOTES:**  
 1. SILT FENCE SHALL BE PLACED ON SLOPE CONTOURS TO MAXIMIZE PONDING EFFICIENCY.  
 2. INSPECT AND REPAIR FENCE AFTER EACH STORM EVENT AND REMOVE SEDIMENT WHEN NECESSARY, REQUIRED WHEN 1/3 FULL WITHIN 24 HOURS OF DISCOVERY. 9" MAXIMUM RECOMMENDED STORAGE HEIGHT.  
 3. REMOVED SEDIMENT SHALL BE DEPOSITED TO AN AREA THAT WILL NOT CONTRIBUTE SEDIMENT OFF-SITE AND PERMANENTLY STABILIZED ALSO SEE SEWR-8009.

**REFERENCES:**  
 MINNESOTA URBAN SMALL SITES BMP MANUAL MNDOT SPEC. REF. 2573, 3886, 3889, 2 TYPE 1  
 MNDOT EROSION CONTROL HANDBOOK NOT TO SCALE

**MINNEAPOLIS DEPARTMENT OF PUBLIC WORKS**  
 DRAWN: DCD DATE: 3/03  
 APPROVED: HRS DATE: 4/07  
 SEDIMENT CONTROL - SILT FENCE  
 STANDARD PLATE NO. SEWR-8001

**ROCK ENTRANCE**  
 PUBLIC STREET  
 EX. CONCRETE APRON  
 PROPERTY LINE  
 MIN. 50"  
 6" MIN. DEPTH OF 1" TO 3" CRUSHED ROCK OR SLASH WOOD MULCH (ALTERNATE) MNDOT SPEC. 3882, TYPE 5  
 ENTRANCE WIDTH AS REQUIRED  
 12" MIN.

**ROCK ENTRANCE**  
 PUBLIC STREET  
 RADIUS AS REQUIRED  
 PROPERTY LINE  
 MIN. 50"  
 6" MIN. DEPTH OF 1" TO 3" CRUSHED ROCK OR SLASH WOOD MULCH (ALTERNATE) MNDOT SPEC. 3882, TYPE 5  
 ENTRANCE WIDTH AS REQUIRED  
 12" MIN.

REMOVE TRACKED SEDIMENTS FROM PAVED SURFACES WITHIN 24 HOURS OF DEPOSITION PICK UP OR VACUUM SWEEPER REQUIRED

**REFERENCES:**  
 MINNESOTA URBAN SMALL SITES BMP MANUAL, PP. 3-69 TO 3-73  
 MNDOT EROSION CONTROL HANDBOOK

**MINNEAPOLIS DEPARTMENT OF PUBLIC WORKS**  
 DRAWN: DCD DATE: 3/07  
 APPROVED: HRS DATE: 4/07  
 SEDIMENT CONTROL - ROCK ENTRANCE  
 STANDARD PLATE NO. SEWR-8002

**INLET PROTECTION TYPE A**  
 (SILT FENCE TO PROTECT DRAIN INLETS) USE WHERE INLET DRAINS AN AREA WITH SLOPE AT 1:3 OR LESS (TYPE A SPEC. 3891)

**INLET PROTECTION TYPE B**  
 (CAN BE INSTALLED IN ANY INLET TYPE WITH OR WITHOUT A CURB BOX)

**INLET PROTECTION TYPE C**  
 (SEE SPECIFICATIONS AS PER THE PLAN DIMENSION LENGTHS AND WIDTH TO MATCH)

**NOTES:**  
 INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER.  
 MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE MNDOT DEPARTMENT'S EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED.  
 WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.  
 1. FINISHED SIZE, INCLUDING POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 1" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.  
 2. FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2" X 4".  
 3. INSTALLATION NOTES:  
 DO NOT INSTALL PROTECTION IN INLETS SHALLOWER THAN 30" MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.  
 TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.  
 THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG, BETWEEN THE INLET WALLS AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, IF WHERE NECESSARY THE CONTRACTOR SHALL CLAMP THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MINIMUM OF 4" FROM THE BOTTOM OF THE BAG.

**REFERENCES:**  
 MNDOT DRAFT PLATES  
 MINNEAPOLIS CUSTOM MNDOT SPEC. REF. 3891 NOT TO SCALE

**MINNEAPOLIS DEPARTMENT OF PUBLIC WORKS**  
 DRAWN: DCD DATE: 1/05  
 APPROVED: HRS DATE: 4/07  
 INLET PROTECTION TYPE A & C  
 STANDARD PLATE NO. SEWR-8003

ENDS SECURELY CLOSED TO PREVENT LOSS OF OPEN GRADED AGGREGATE FILL SECURED WITH 50 PSI. ZIP TIE

5" DIAMETER GEOTEXTILE SOCK, TYPE WOVEN MONOFLAMENT CONFORMING TO MNDOT SPEC. 3886, TABLE 3886-1, MACHINE SLICE

SEAM JOINED BY TWO ROWS OF STITCHING WITH A PLASTIC MESH BACKING OR HEAT BONDED (OR APPROVED EQUIVALENT)

FILL ROCK LOG WITH 45 LBS. OF OPEN GRADED AGGREGATE CONSISTING OF SOUND, DURABLE PARTICLES OF CRUSHED QUARRY ROCK OR GRAVEL, CONFORMING TO THE FOLLOWING GRADATION

GRADATION	
SIEVE SIZE	PERCENT PASSING
1 1/2 INCH	100
1 INCH	95-100
3/4 INCH	65-95
3/8 INCH	30-65
NO. 4	10-35
NO. 10	3-20
NO. 40	0-8
NO. 200	0-3

NOTE: CRUSHED CONCRETE OR BITUMINOUS SHALL NOT BE USED FOR OPEN GRADED AGGREGATE.

PAYMENT SHALL INCLUDE ALL MATERIALS, FILLING OF LOG, PLACEMENT, MAINTENANCE, & REMOVAL. 80% OF BID PRICE SHALL BE PAID UPON PROPER PLACEMENT WITH THE FINAL 20% PAID UPON REMOVAL.

INLET PROTECTION WITH FILTER LOG  
 DO NOT USE IN LIVE TRAFFIC AREAS

BUTT JOINTS

FILTER LOGS MNDOT SPEC. 3897

STRAW BIOROLL  
 WOOD FIBER BIOROLL  
 COMPOST LOG

GRAIN AND SEED FREE OF WEEDS, 8-9 MONTH POLYPROPYLENE NETTING  
 EXCELSIOR FIBER LOG, 8-9 MONTH POLYPROPYLENE NETTING  
 30-40% WEED FREE COMPOST (3850, GRADE 2), 5" DIAMETER  
 GEOTECHNICAL CYLINDRICAL BAG  
 4"-6" DIAMETER, SEE SIEVE SIZE CHART ABOVE

ROCK LOG

FIBER LOG MNDOT SPEC. 3895  
 12" DIAMETER COCONUT FIBER (COIR)  
 OUTER COIR NETTING REQUIRED, MINIMUM 5 YEAR LIFE

**MINNEAPOLIS DEPARTMENT OF PUBLIC WORKS**  
 DRAWN: DCD DATE: 4/07  
 APPROVED: HRS DATE: 4/07  
 SEDIMENT CONTROL - FILTER LOG  
 STANDARD PLATE NO. SEWR-8008

### EROSION AND SEDIMENT CONTROL NOTES

INSTALL PERIMETER EROSION CONTROLS AS INDICATED IN PLANS PRIOR TO START OF WORK. HAY BALES ARE NOT ALLOWED AS EROSION & SEDIMENT CONTROL DEVICE IN MINNEAPOLIS.

ESTABLISH ROCK CONSTRUCTION ENTRANCES PRIOR TO BEGINNING LAND DISTURBING ACTIVITIES. 1 1/2" - 3" WASHED AGGREGATE IS RECOMMENDED FOR ROCK ENTRANCES. A GEOTEXTILE FABRIC IS REQUIRED.

REMOVE ALL SOILS AND SEDIMENTS DEPOSITED ONTO PUBLIC AND/OR PRIVATE PAVEMENT AREAS WITHIN 24 HOURS OF DEPOSITION. REMOVAL OF TRACKING MATERIALS SHALL BE COMPLETED AT THE END OF EACH WORK DAY WHEN TRACKING OCCURS. SWEEPING MAY BE ORDERED AT ANY TIME IF CONDITIONS WARRANT. SWEEPING SHALL BE MAINTAINED THROUGHOUT THE DURATION OF CONSTRUCTION AND IN SUCH A MANNER TO PREVENT DUST BEING BLOWN TO ADJACENT PROPERTIES.

INSTALL INLET PROTECTION IN ALL DOWNSTREAM CATCH BASINS WHICH RECEIVE RUNOFF FROM THE DISTURBED AREA. CATCH BASIN INSERTS ARE REQUIRED AT ALL LOCATIONS NOT WITHIN THE DISTURBED AREA WHICH RECEIVE RUNOFF (MNDOT TYPE C INLET PROTECTION). NOTE HAY BALES AND SILT FENCE WRAPPED GRATINGS ARE NOT EFFECTIVE AND ARE NOT APPROVED FOR USE AS INLET PROTECTION DEVICES.

LOCATE ALL SOIL AND DIRT PILES NO LESS THAN 25 FEET FROM ANY PUBLIC OR PRIVATE ROADWAY OR DRAINAGE CHANNEL. ALL STOCK PILES THAT REMAIN IN PLACE FOR 7 DAYS OR MORE SHALL BE STABILIZED BY MULCHING, VEGETATIVE COVER, TARPING OR OTHER MEANS. TEMPORARY STOCK PILES LOCATED ON PAVED SURFACES MUST BE AT LEAST 2 FEET OR MORE AWAY FROM THE DRAINAGE/GUTTER LINE AND SHALL BE COVERED IF REMAINING MORE THAN 24 HOURS.

MAINTAIN ALL TEMPORARY EROSION CONTROL DEVICES IN PLACE UNTIL THE CONTRIBUTING DRAINAGE AREA HAS BEEN STABILIZED. INSPECT TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES ON A WEEKLY BASIS AND REPLACE DETERIORATED, DAMAGED OR ROTTED EROSION CONTROL DEVICES IMMEDIATELY.

MAINTENANCE OF EROSION AND SEDIMENT CONTROL DEVICES SHALL BE PERFORMED WHENEVER THE DEVICE IS 30% FULL. FAILURE TO MAINTAIN EROSION CONTROL DEVICES MAY LEAD TO FURTHER ENFORCEMENT ACTION. WEEKLY INSPECTIONS REQUIRED AND AFTER EACH 1/2" OR MORE RAIN EVENT WITHIN 24 HRS.

READY MIXED CONCRETE AND BATCH PLANT WASHOUTS PROHIBITED WITHIN THE PUBLIC RIGHT OF WAY. DESIGNATE CONCRETE WASHOUT AND MIXING LOCATIONS IN THE EROSION CONTROL PLANS. UNDER NO CIRCUMSTANCES MAY WASHOUT WATER DRAIN ONTO THE PUBLIC RIGHT OF WAY OR INTO THE PUBLIC STORM DRAIN.

TEMPORARILY OR PERMANENTLY STABILIZE ALL DENUDED AREAS WHICH HAVE BEEN FINISH GRADED WITHIN 7-14 DAYS (SLOPE DEPENDENT). USE SEEDING AND MULCHING, EROSION CONTROL MATTING AND/OR SODDING WITH TEMPORARY STAKING IN GREEN SPACE AREAS. USE EARLY APPLICATION OF GRAVEL BASE FOR AREAS DESIGNATED FOR PAVED SURFACING.

REMOVE ALL TEMPORARY SYNTHETIC, STRUCTURAL AND NON-BIODEGRADABLE EROSION AND SEDIMENT CONTROL AFTER THE SITE HAS UNDERGONE FINAL STABILIZATION AND PERMANENT VEGETATION HAS BEEN ESTABLISHED. MINIMUM VEGETATION COVER OF 70% REQUIRED. ALL TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES SHALL BE MAINTAINED UNTIL THE SITE HAS 70% ESTABLISHED VEGETATIVE COVER AND ALL PAVED AREAS HAVE BEEN STABILIZED WITH THE SELECTED PAVEMENT TYPE.

ADDITIONAL EROSION CONTROL MAY BE REQUIRED BY OTHER PERMITTING AGENCIES. IT IS THE RESPONSIBILITY OF THE PROJECT ENGINEER TO VERIFY THAT THE CITY AND ALL OTHER AGENCY REQUIREMENTS ARE MET.

**MINNEAPOLIS DEPARTMENT OF PUBLIC WORKS**  
 DRAWN: DCD DATE: 8/06  
 APPROVED: HRS DATE: 9/07  
 EROSION CONTROL NOTES  
 STANDARD PLATE NO. SEWR-8007

**PLAN VIEW**  
 INLET FROM POND  
 OUTLET TO CITY

**ELEVATION VIEW**  
 TOP EL. 830.25  
 EL. 829.5  
 30 MIL TEXTURED HDPE LINER  
 ALL JOINTS TO INCLUDE A TYPE-A O-RING RUBBER GASKET PER MNDOT 3726 OR APPROVED EQUAL  
 PROPOSED POND BOTTOM EL. 825.0  
 INV. 826.33  
 OUTLET 48" DIA  
 INLET (FROM POND)  
 FES EL. 825.0  
 MONOLITHICALLY POURED CONCRETE

**NOTES:**  
 1. STANDARD PIPE BEDDING REQUIRED ON BOTH INLET AND OUTLET PIPES.  
 2. WEIR SIZE DETERMINED BY STORMWATER MODELING.  
 3. COVER WILL BE STANDARD STEEL GRATE.

OVERFLOW CONTROL STRUCTURE (OCS)

REV	REVISION DESCRIPTION	DWN	APP	REV DATE
2	LAND USE RESUBMITTAL	JVB	JE	03/12/14
1	ISSUED FOR PERMIT	JVB	JE	02/19/14

SEAL

PRIME CONSULTANT

Wenck Associates, Inc.  
 Consulting Engineers  
 1800 PIONEER CREEK CENTER  
 MAPLE PLAIN, MN 55359  
 763-479-4200  
 763-479-4242

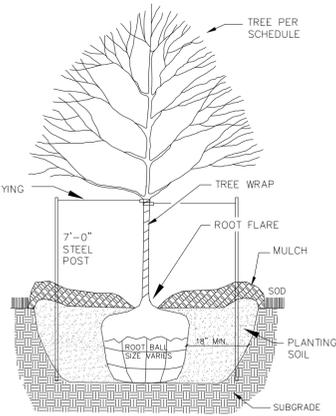
PROJECT TITLE  
 FACILITY IMPROVEMENT PROJECT

ALLIANCE STEEL FACILITY  
 81 ST. ANTHONY PARKWAY  
 MINNEAPOLIS, MINNESOTA

SHEET TITLE  
 DETAILS

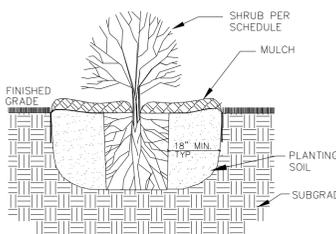
DWN BY	CHK'D	APP'D	DWG DATE
JVB	JE	JE	NOV 2013
PROJECT NO.		SCALE	
0258-52		AS SHOWN	
SHEET NO.		REV NO.	
D-101		1	

# PLANTING DETAILS



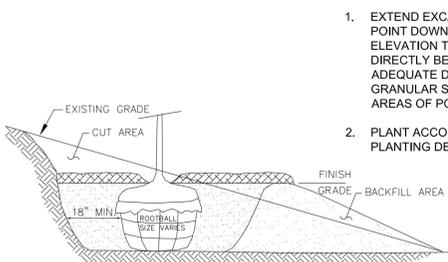
1. REMOVE DEAD OR DAMAGED BRANCHES. RETAIN THE NATURAL FORM OF THE TREE. DO NOT CUT THE LEADER.
2. WIDTH OF PLANTING HOLES: 18" MIN. LARGER THAN ROOT BALL, ON ALL SIDES.
3. DEPTH OF HOLE: ROOT FLARE TO SIT AT OR UP TO 2" ABOVE THE TOP OF THE FINISHED SOIL ELEVATION. LEAVE SOIL UNDISTURBED BENEATH THE ROOT BALL.
4. SCARIFY BOTTOM AND SIDES OF HOLE PRIOR TO PLANTING.
5. CUT ROPES AT BASE OF TRUNK AND REMOVE COMPLETELY. REMOVE TOP 1/4 (MIN.) OF THE METAL BASKET AND BURLAP FROM TOP OF ROOT BALL.
6. SLIT REMAINING BURLAP AT 6" INTERVALS.
7. BACKFILL WITH PLANTING SOIL (EXISTING SOIL THOROUGHLY MIXED WITH 4" MNDOT GRADE 2 COMPOST). CONSTRUCT 3" DEPTH WATERING BASIN THOROUGHLY WATER IN TREE WITHIN 2 HOURS TO SETTLE PLANTS AND FILL VOIDS.
8. BACKFILL VOIDS AND WATER A SECOND TIME.
9. PLACE 4" MULCH WITHIN 48 HOURS OF THE SECOND WATERING. NO MULCH TO BE IN CONTACT W/ TRUNK.
10. CONTRACTOR IS RESPONSIBLE FOR MAINTAINING TREES IN A PLUMB POSITION THROUGHOUT THE GUARANTEE PERIOD. STAKE TREES IF SITE CONDITIONS SUCH AS SOIL OR WIND PREVENT THE TREES FROM STAYING PLUMB. USE 16" LONG, 1.5" WIDE STRAPS, ATTACH TO POST WITH WIRE. INSTALL POST 3' INTO GROUND.
11. WRAP TRUNK IN FALL REMOVE IN SPRING.
12. REFER TO SPECS. FOR PRODUCTS AND OTHER INFORMATION.

1 TREE PLANTING DETAIL  
L-100 NOT TO SCALE



1. DIG SHRUB HOLE 18" MIN. LARGER THAN CONTAINER.
2. SCARIFY BOTTOM AND SIDES OF HOLE PRIOR TO PLANTING.
3. REMOVE DEAD OR DAMAGED BRANCHES. RETAIN THE NATURAL FORM OF THE SHRUB.
4. HAND LOOSEN ROOTS OF CONTAINERIZED MATERIAL. SCORE OUTSIDE OF SOIL MASS TO REDIRECT CIRCLING FIBROUS ROOTS.
5. SET SHRUB ON UNDISTURBED SOIL OR ON THOROUGHLY COMPACTED BACKFILL SOIL. DO NOT BURY ROOT FLARE. BOTTOM OF ROOT FLARE TO BE AT FINISHED SOIL ELEVATION.
6. BACKFILL WITH PLANTING SOIL (EXISTING SOIL THOROUGHLY MIXED WITH 4" MNDOT GRADE 2 COMPOST). CONSTRUCT 3" DEPTH WATERING BASIN.
7. THOROUGHLY WATER IN SHRUB PRIOR TO PLACING MULCH.
8. ALL BEDS RECEIVE 4" DEPTH MULCH.
9. REFER TO SPECS. FOR PRODUCTS AND OTHER INFORMATION.

2 SHRUB PLANTING DETAIL  
L-100 NOT TO SCALE



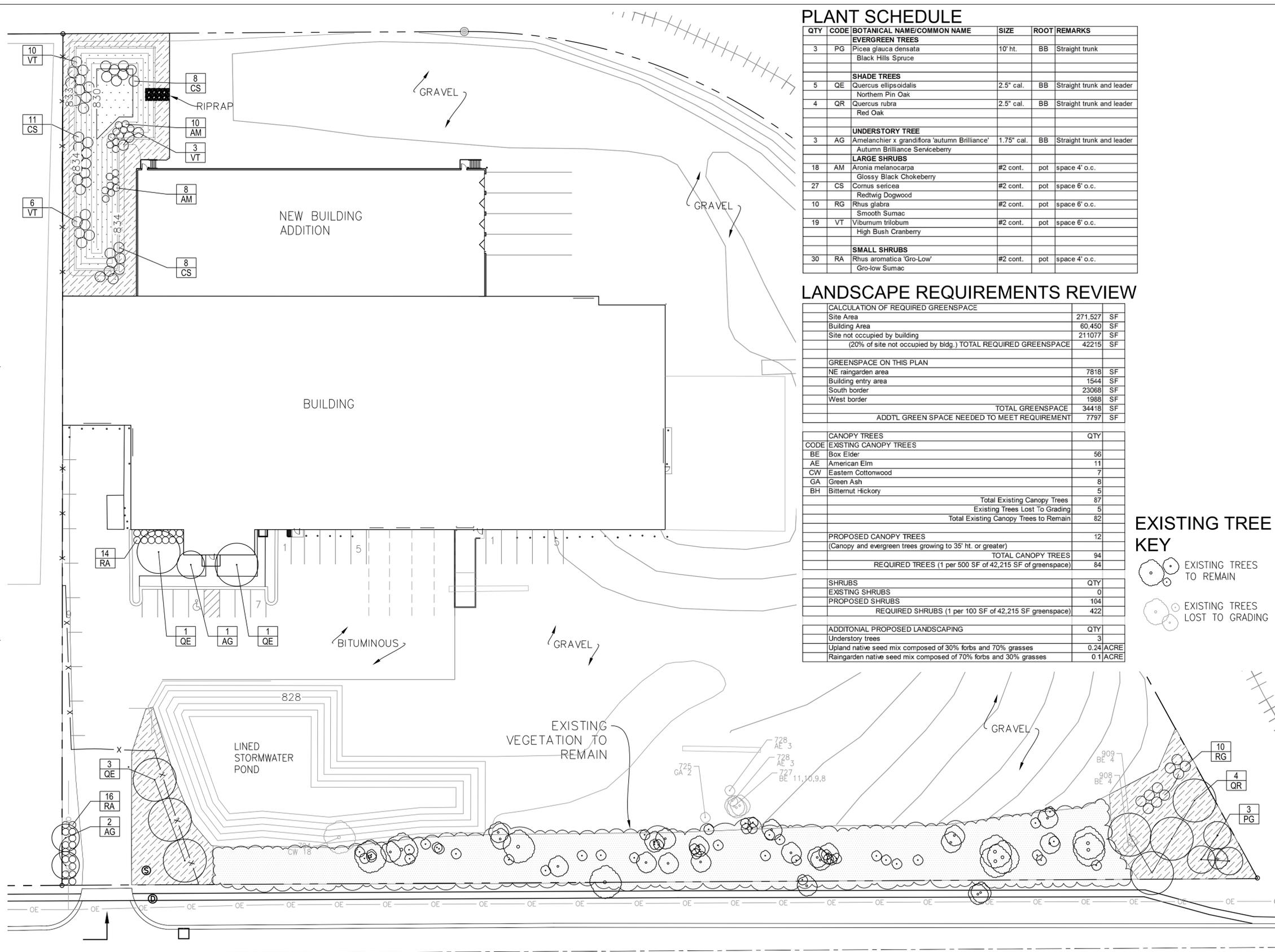
1. EXTEND EXCAVATION AND BACKFILL SOIL TO A POINT DOWNSLOPE EQUAL TO OR LOWER IN ELEVATION THAN THE BOTTOM OF THE HOLE DIRECTLY BENEATH THE PLANT TO INSURE ADEQUATE DRAINAGE IN HEAVY SOILS. GRANULAR SOIL MUST BE ADDED AS BACKFILL IN AREAS OF POOR DRAINAGE.
2. PLANT ACCORDING TO SHRUB AND TREE PLANTING DETAILS ON THIS SHEET.

3 PLANTING ON A SLOPE DETAIL  
L-100 NOT TO SCALE

## SEEDING KEY

- UPLAND NATIVE SEED MIX, SEE SPECS.
- RAINGARDEN NATIVE SEED MIX, SEE SPECS.

SEE LANDSCAPE SPECIFICATIONS  
SHEET L-101



## PLANT SCHEDULE

QTY	CODE	BOTANICAL NAME/COMMON NAME	SIZE	ROOT	REMARKS
<b>EVERGREEN TREES</b>					
3	PG	Picea glauca densata Black Hills Spruce	10' ht.	BB	Straight trunk
<b>SHADE TREES</b>					
5	QE	Quercus ellipsoidalis Northern Pin Oak	2.5" cal.	BB	Straight trunk and leader
4	QR	Quercus rubra Red Oak	2.5" cal.	BB	Straight trunk and leader
<b>UNDERSTORY TREE</b>					
3	AG	Amelanchier x grandiflora 'autumn Brilliance' Autumn Brilliance Serviceberry	1.75" cal.	BB	Straight trunk and leader
<b>LARGE SHRUBS</b>					
18	AM	Aronia melanocarpa Glossy Black Chokeberry	#2 cont.	pot	space 4' o.c.
27	CS	Cornus sericea Redtwig Dogwood	#2 cont.	pot	space 6' o.c.
10	RG	Rhus glabra Smooth Sumac	#2 cont.	pot	space 6' o.c.
19	VT	Viburnum trilobum High Bush Cranberry	#2 cont.	pot	space 6' o.c.
<b>SMALL SHRUBS</b>					
30	RA	Rhus aromatica 'Gro-Low' Gro-low Sumac	#2 cont.	pot	space 4' o.c.

## LANDSCAPE REQUIREMENTS REVIEW

CALCULATION OF REQUIRED GREENSPACE	
Site Area	271,527 SF
Building Area	60,450 SF
Site not occupied by building	211,077 SF
(20% of site not occupied by bldg.) TOTAL REQUIRED GREENSPACE	42,215 SF
<b>GREENSPACE ON THIS PLAN</b>	
NE raingarden area	7818 SF
Building entry area	1544 SF
South border	23068 SF
West border	1988 SF
<b>TOTAL GREENSPACE</b>	<b>34418 SF</b>
<b>ADDTL GREEN SPACE NEEDED TO MEET REQUIREMENT</b>	<b>7797 SF</b>
<b>CANOPY TREES</b>	
CODE	EXISTING CANOPY TREES
BE	Box Elder
AE	American Elm
CW	Eastern Cottonwood
GA	Green Ash
BH	Bitternut Hickory
	<b>Total Existing Canopy Trees</b>
	Existing Trees Lost To Grading
	<b>Total Existing Canopy Trees to Remain</b>
	12
<b>PROPOSED CANOPY TREES</b>	
(Canopy and evergreen trees growing to 35' ht. or greater)	
	<b>TOTAL CANOPY TREES</b>
	94
	<b>REQUIRED TREES (1 per 500 SF of 42,215 SF of greenspace)</b>
	84
<b>SHRUBS</b>	
	EXISTING SHRUBS
	PROPOSED SHRUBS
	<b>REQUIRED SHRUBS (1 per 100 SF of 42,215 SF greenspace)</b>
	422
<b>ADDITIONAL PROPOSED LANDSCAPING</b>	
	Understory trees
	Upland native seed mix composed of 30% forbs and 70% grasses
	Raingarden native seed mix composed of 70% forbs and 30% grasses
	3
	0.24 ACRE
	0.1 ACRE

## EXISTING TREE KEY

- EXISTING TREES TO REMAIN
- EXISTING TREES LOST TO GRADING

REV	REVISION DESCRIPTION	DWN	APP	REV DATE

SEAL  
Carmen Simonet Design LLC  
www.simonetdesign.com  
Landscape Architecture  
carmen@simonetdesign.com  
651.495.0273  
354 Stonebridge Blvd.  
Saint Paul, MN 55105

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Landscape Architect under the laws of the State of Minnesota.

Print Name: Carmen Simonet  
Signature: *Carmen Simonet*  
Date: 03.12.14 License # 24236

PRIME CONSULTANT  
**Wenck**  
Wenck Associates, Inc.  
Consulting Engineers  
1800 PIONEER CREEK CENTER  
MAPLE PLAIN, MN 55359  
763-479-4200  
763-479-4242  
Web Site: www.wenck.com

PROJECT TITLE  
FACILITY IMPROVEMENT PROJECT  
**ALLIANCE STEEL FACILITY**  
81 ST. ANTHONY PARKWAY  
MINNEAPOLIS, MINNESOTA

SHEET TITLE			
LANDSCAPE PLAN			
DWN BY	CHK'D	APP'D	DWG DATE
CMS	JE	CMS	FEB 2014
PROJECT NO.		SCALE	AS SHOWN
0258-52		L-100	
SHEET NO.		REV NO.	



DESIGNED	DJM
DRAWN	NES
CHECKED	DJM
PROJECT	13049
REVISIONS	

RELEASED FOR PERMIT  
13 DECEMBER 2013

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly licensed architect under the laws of the state of Minnesota.  
(architectural only)

DAVID J. MEDIN

License No. 0202

Date Jan 16, 2014

**Larson Engineering, Inc.**  
3524 Labore Road  
White Bear Lake, MN 55110

(P) 651-481-9120  
(F) 651-481-9201  
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I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

PRINT NAME: HENRY VOTH

SIGNATURE

DATE: Jan 16, 2014

REG. NO. 15247

(STRUCTURAL ONLY)

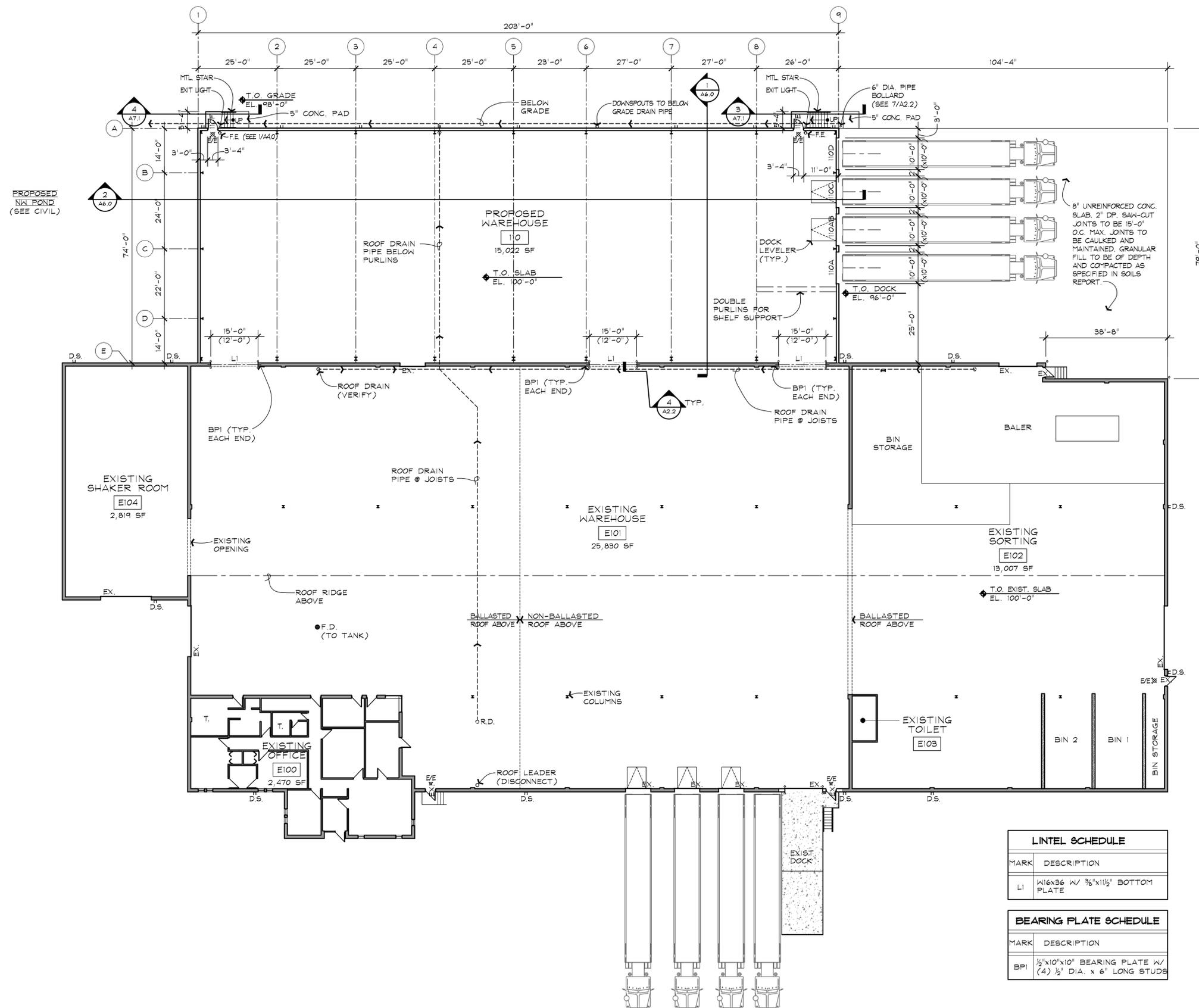
**ALLIANCE STEEL ADDITION**  
MINNEAPOLIS, MINNESOTA

203 n.w. First Ave.  
Faribault, MN 55021  
507/334-2252

**DJ MEDIN**  
architects, inc.

OVERALL FLOOR PLAN

A3.0

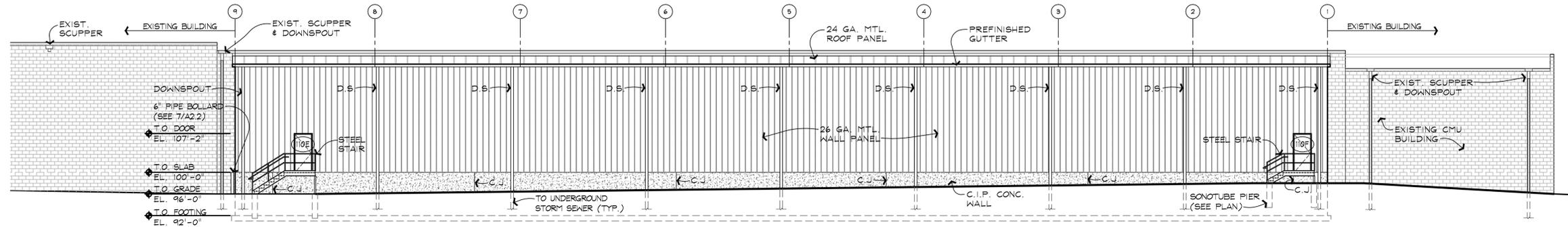


LINTEL SCHEDULE	
MARK	DESCRIPTION
L1	W16x36 W/ 3/8"x11 1/2" BOTTOM PLATE

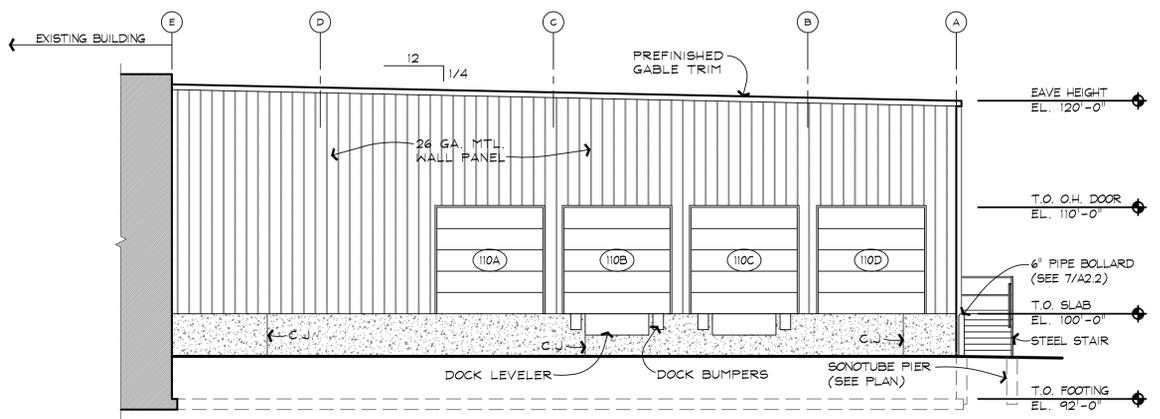
BEARING PLATE SCHEDULE	
MARK	DESCRIPTION
BPI	1/2"x10"x10" BEARING PLATE W/ (4) 1/2" DIA. x 6" LONG STUDS

**OVERALL FLOOR PLAN**  
SCALE: 1/16" = 1'-0"  
NORTH

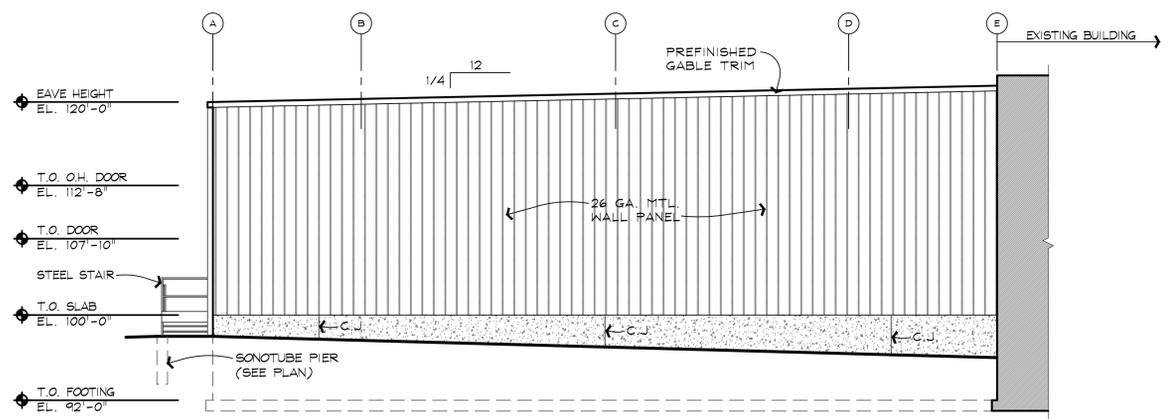
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**1**  
A5.0  
**NORTH ELEVATION**  
SCALE: 3/32" = 1'-0"



**2**  
A5.0  
**EAST ELEVATION**  
SCALE: 1/8" = 1'-0"



**3**  
A5.0  
**WEST ELEVATION**  
SCALE: 1/8" = 1'-0"

DESIGNED	DJM
DRAWN	NES
CHECKED	DJM
PROJECT	13049
REVISIONS	

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*David J. Medin*  
DAVID J. MEDIN

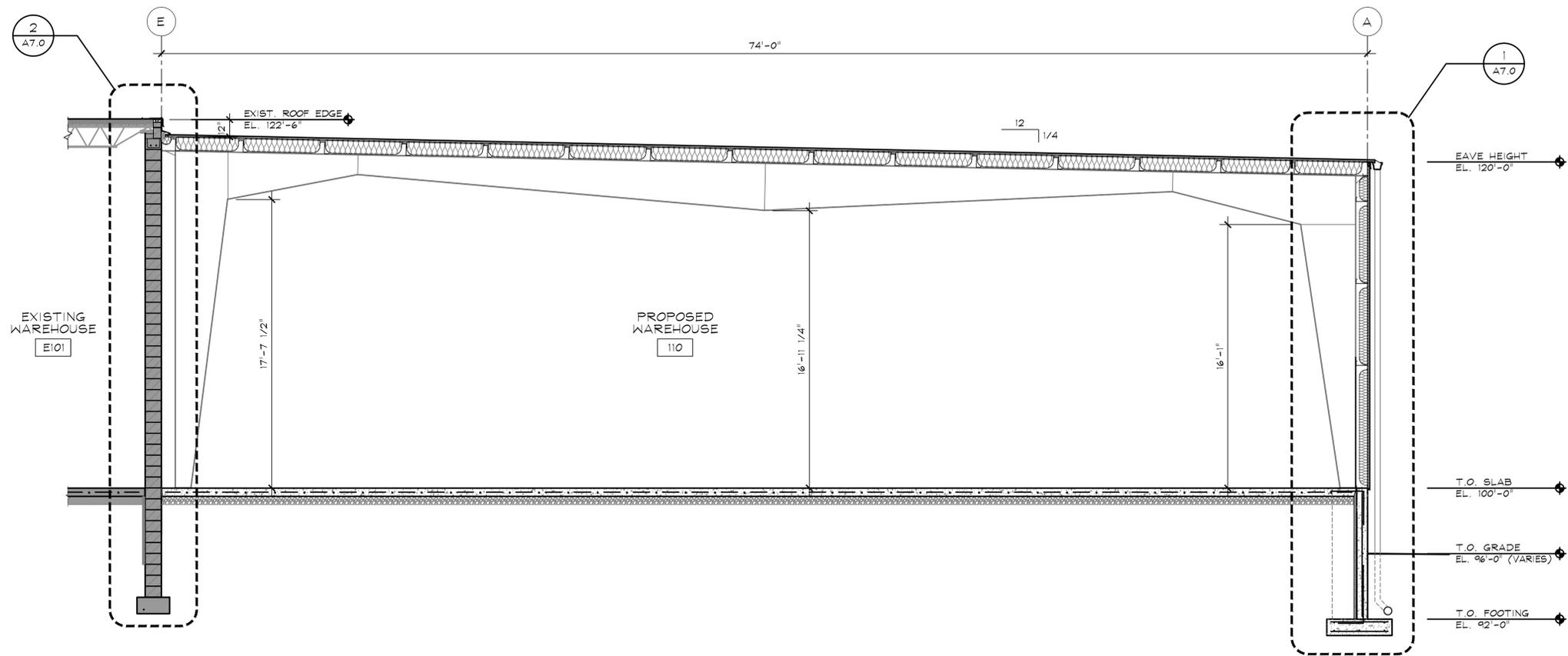
License No. 0202  
Date Jan 21, 2014

**ALLIANCE STEEL  
ADDITION**  
MINNEAPOLIS, MINNESOTA

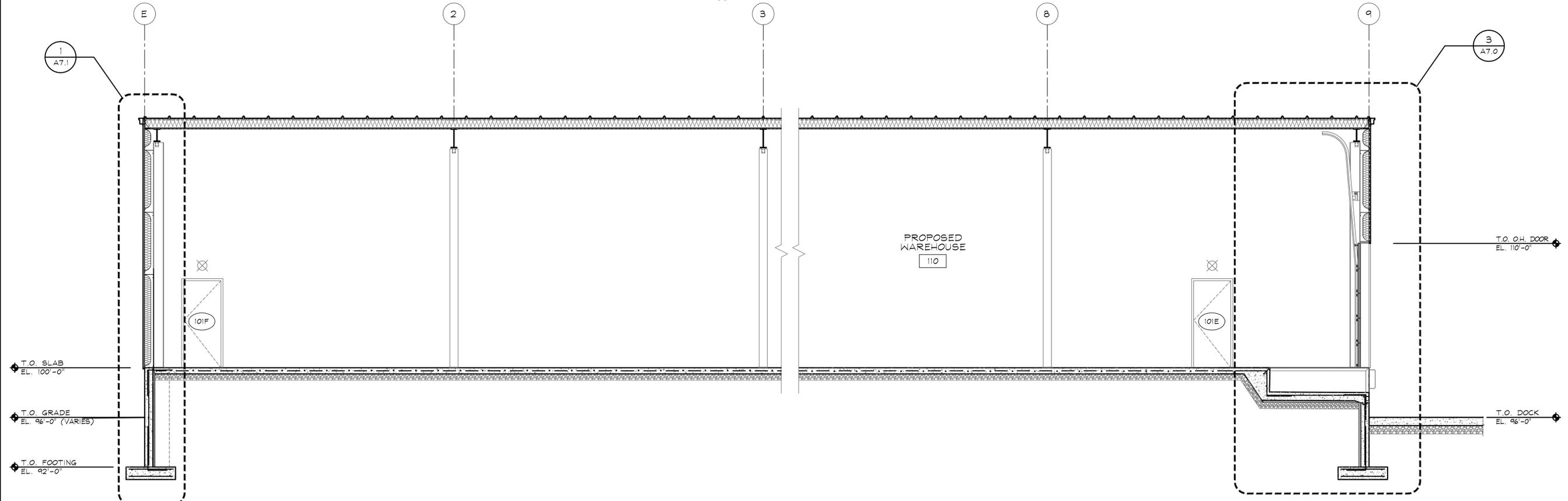
203 n.w. First Ave.  
Faribault, MN 55021  
507/334-2252

**DJ** MEDIN  
architects, inc.

EXTERIOR ELEVATIONS  
**A5.0**



**BUILDING SECTION (VERIFY FRAME PROFILE)**  
 SCALE: 1/4" = 1'-0"



**BUILDING SECTION**  
 SCALE: 1/4" = 1'-0"

DESIGNED	DJM
DRAWN	NES
CHECKED	DJM
PROJECT	13049
REVISIONS	

RELEASED FOR PERMIT 1-20-14

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*David J. Medin*  
 DAVID J. MEDIN

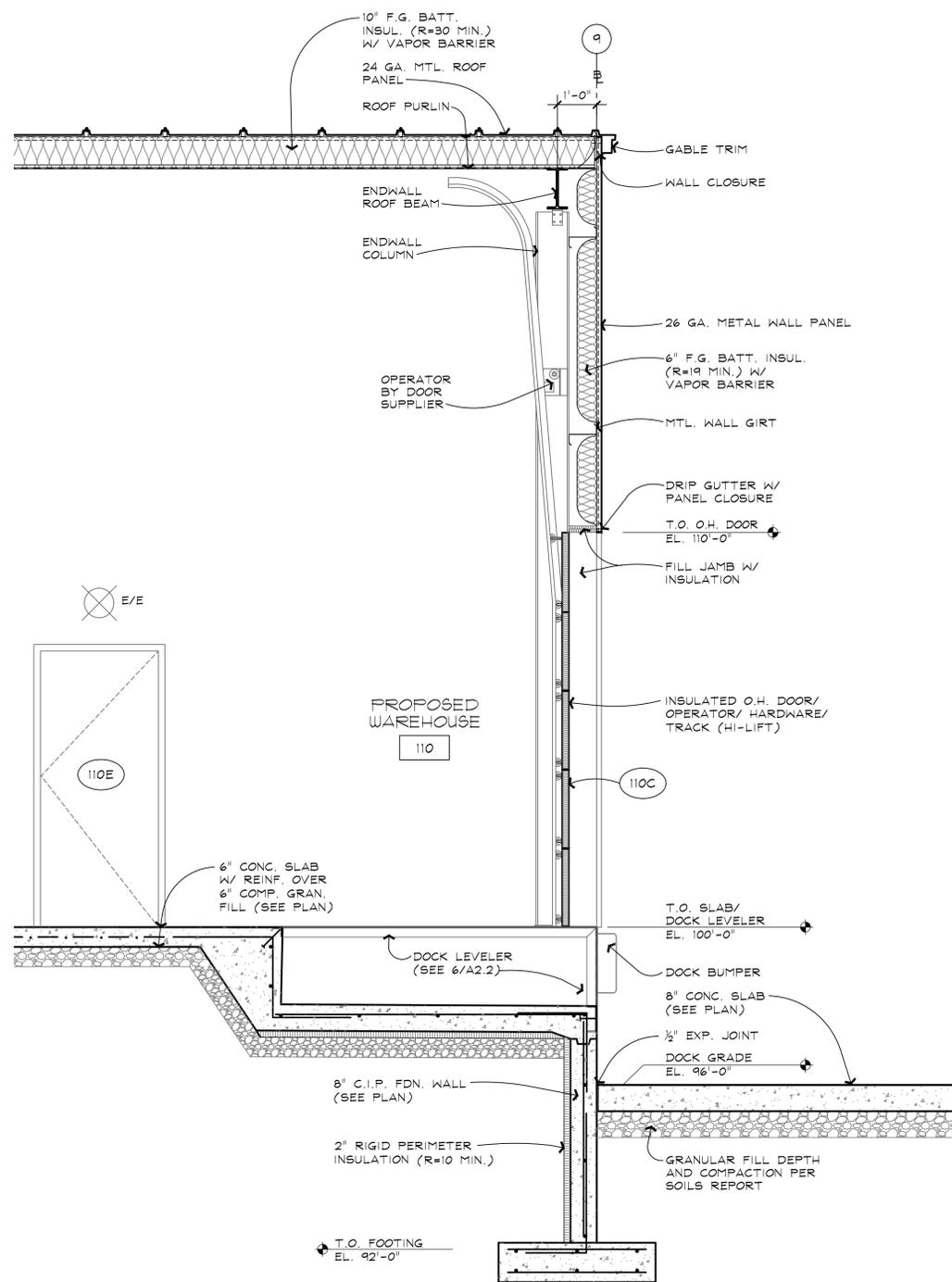
License No. 0202  
 Date Jan 21, 2014

**ALLIANCE STEEL  
 ADDITION**  
 MINNEAPOLIS, MINNESOTA

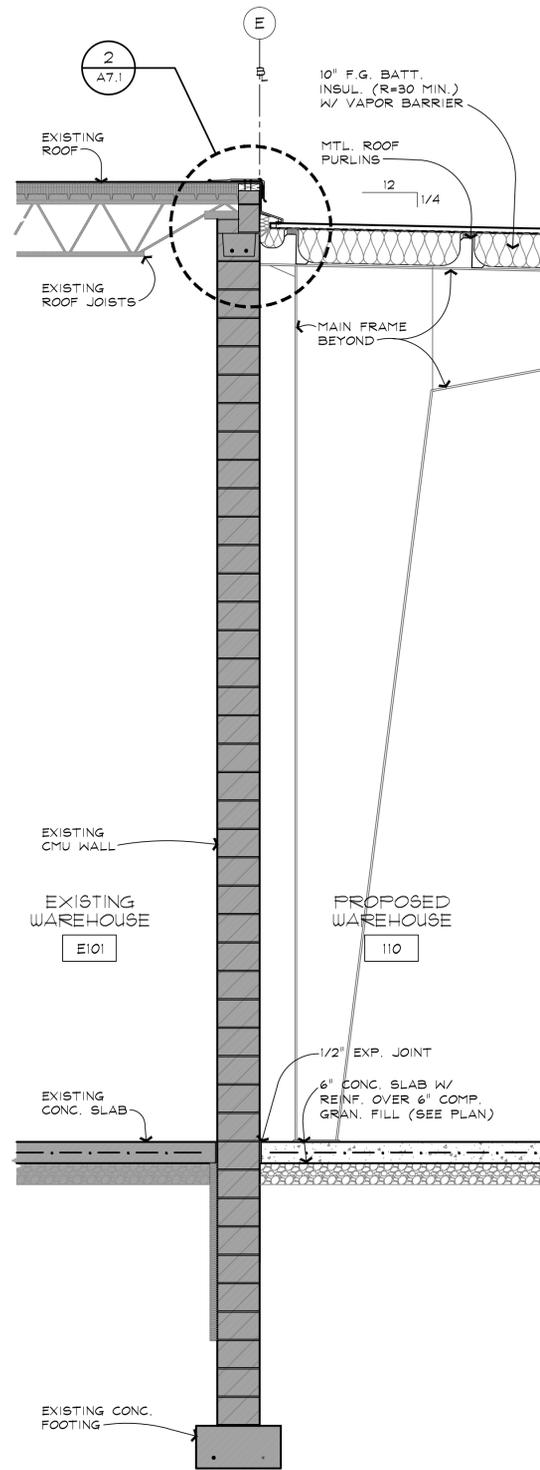
203 n.w. First Ave.  
 Faribault, MN 55021  
 507/334-2252



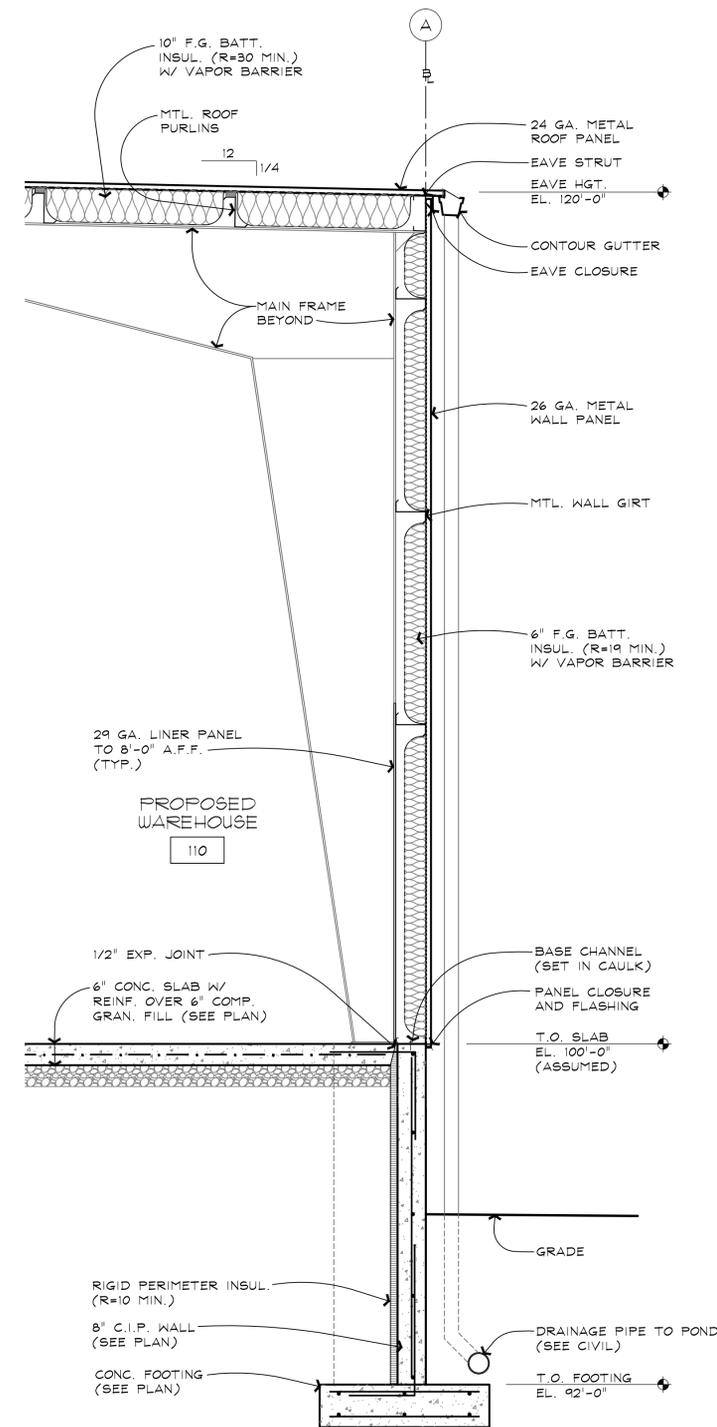
BUILDING SECTIONS  
**A6.0**



3  
A7.0  
**WALL SECTION**  
SCALE: 1/2" = 1'-0"



2  
A7.0  
**WALL SECTION**  
SCALE: 1/2" = 1'-0"



1  
A7.0  
**WALL SECTION**  
SCALE: 1/2" = 1'-0"

DESIGNED	DJM
DRAWN	NES
CHECKED	DJM
PROJECT	13049
REVISIONS	

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*David J. Medin*  
DAVID J. MEDIN

License No. 08998  
Date Jan 21, 2014

**ALLIANCE STEEL  
ADDITION**  
MINNEAPOLIS, MINNESOTA

203 n.w. First Ave.  
Faribault, MN 55021  
507/334-2252

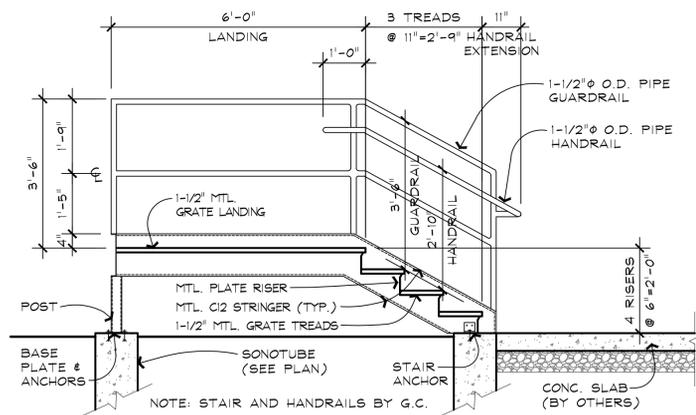
**DJ MEDIN**  
architects, inc.

WALL SECTIONS

A7.0

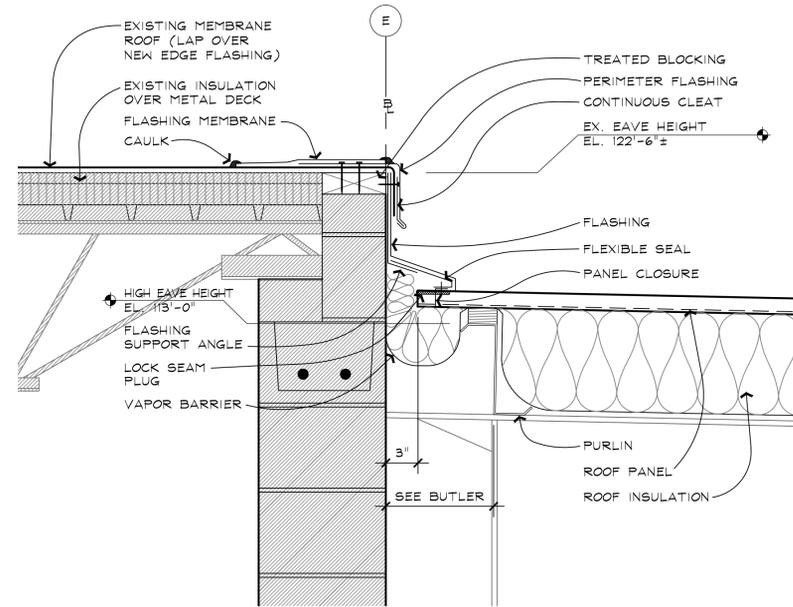
SUCCESSFUL STAIR SUPPLIER TO PROVIDE ENGINEERED SHOP DRAWINGS FOR ENGINEER'S REVIEW PRIOR TO FABRICATION

NOTE: GRATES SHALL HAVE SPACES NO GREATER THAN 1/2" WIDE IN ONE DIRECTION. IF SPACE ARE ELONGATED, LONG DIRECTION TO BE PERPENDICULAR TO DIRECTION OF TRAVEL.



**STAIR SECTION**

SCALE: 1/2" = 1'-0"



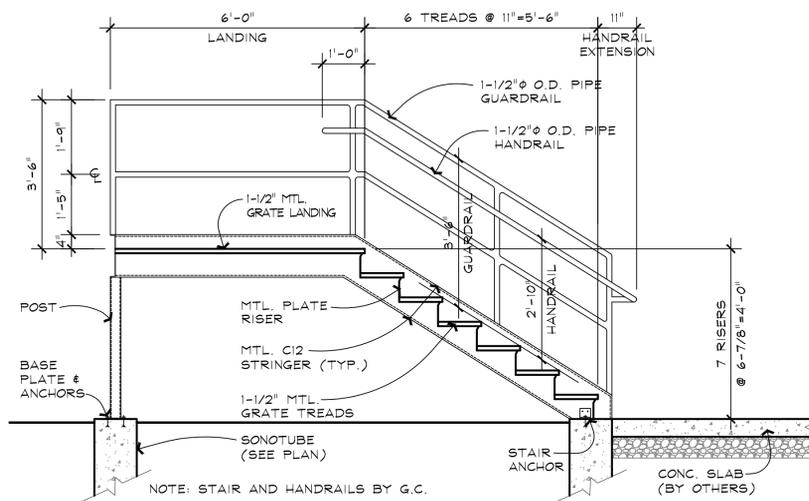
**NEW/EXIST ROOF TRANSITION**

(VERIFY DETAILS W/ ROOFING CONTRACTOR)

SCALE: NONE

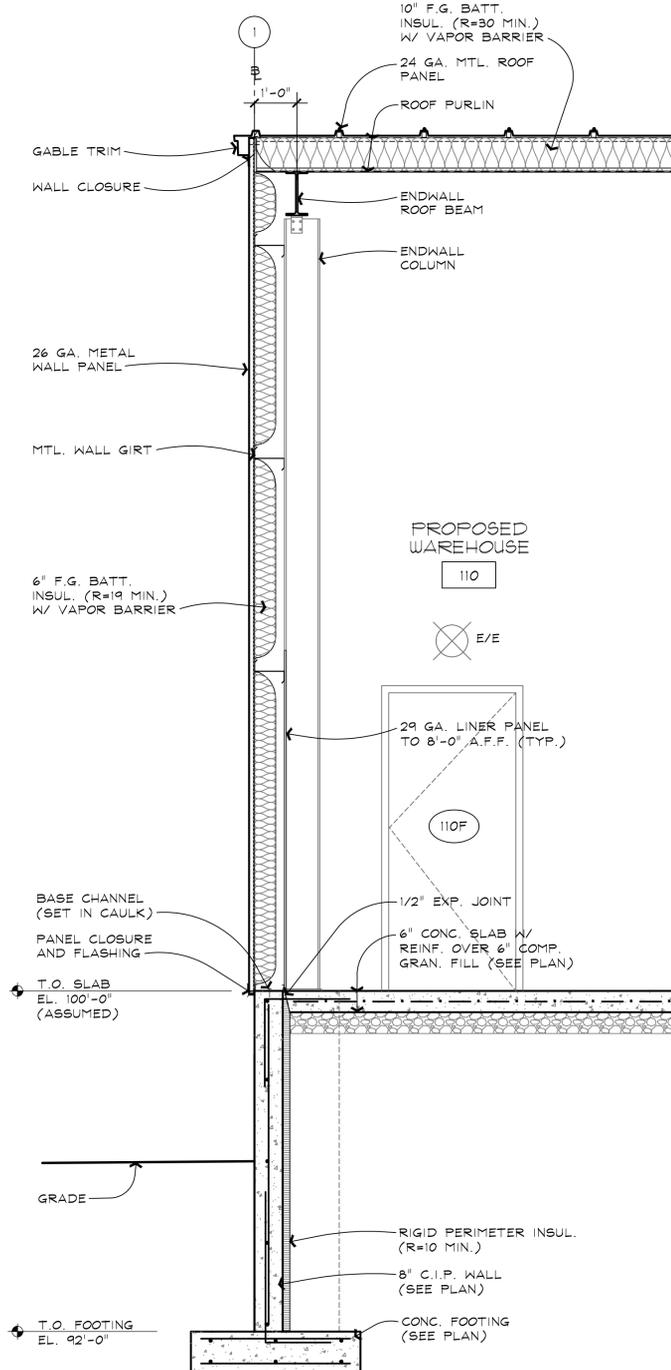
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**STAIR SECTION**

SCALE: 1/2" = 1'-0"



**WALL SECTION**

SCALE: 1/2" = 1'-0"

DESIGNED	DJM
DRAWN	NES
CHECKED	DJM
PROJECT	13049
REVISIONS	

RELEASED FOR PERMIT 1-20-14

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DAVID J. MEDIN  
License No. 0898  
Date Jan 21, 2014

**ALLIANCE STEEL ADDITION**  
MINNEAPOLIS, MINNESOTA

203 n.w. First Ave.  
Faribault, MN 55021  
507/334-2252

**DJ MEDIN**  
architects, inc.

WALL SECTION/  
STAIR SECTIONS

A7.1



Site Entrance for St. Anthony Parkway



Building South Facade



South Side of Site Looking East



East Side of Site Looking West Along North Facade



North and West Facade



East Side of Site Looking SE toward St. Anthony Bridge



Site Entrance and Sign from West



Sign From East

## Jeff A. Ellerd

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**From:** Jeff A. Ellerd  
**Sent:** Friday, February 21, 2014 1:38 PM  
**To:** contact@columbiapark.org; marshallterrace@msn.com  
**Cc:** kevin.reich@minneapolismn.gov; Jeff A. Ellerd  
**Subject:** Proposed Building Addition Project - 81 St. Anthony Parkway

The purpose of this email is to inform you of a proposed building addition to the property located at 81 St. Anthony Parkway in the Columbia Park neighborhood, adjacent the Marshall Terrace neighborhood.

The Applicant is G&G Real Estate, Larry Zweigbaum, who can be reached at [larryz@alliancesteelco.com](mailto:larryz@alliancesteelco.com) or at (612) 588.2724. My contact information is below and I would be happy to accept comments or questions on behalf of the applicant.

The proposed project consists of the construction of a 15,000 SF building addition to the north façade of the existing building. As part of the construction, storm water management upgrades and landscaping will be constructed. The addition will consist of an engineered steel building with two man doors located on the north façade and four trailer height loading docks on the east façade. The addition will replace the majority of the existing concrete block north façade. The existing north wall will be modified to provide better access into the new addition with cutting or expanding of existing openings. The addition floor slab will match existing, providing one continuous storage and operational space.

As part of the construction of the addition, a significant portion of the site will be regarded. Roof water will be directed to a newly constructed storm water infiltration pond on the northwest corner of the site. This pond will accept water from the entire 15,000 SF addition and approximately two-thirds of the existing building roof. Existing roof drains will be routed through the existing building, beneath the proposed addition and discharge into the new infiltration pond.

The remainder of the site will be graded such that water will flow to the south side of the site where a new lined storm water treatment pond will be constructed. The treatment pond will discharge to the existing storm sewer line. During a rainfall event, the pond will retain water, allowing for suspended solids to precipitate out of the water.

Landscaping and green space are being added as part of the proposed construction project. In addition to the storm water infiltration pond, trees, shrubs and other vegetation will be added in and around the pond. Further enhancements to the landscaping will be focused on the south side of the building and site increasing the curb appeal on the public side of the facility.

The Applicant is requesting an amendment to the existing Conditional Use Permit to allow for the expansion, along with three variances associated with the new construction. The variances are for a reduction in required green space on the site, along with a reduction in the required shrub count for the project, a variance from the required number of parking spaces from 40 to 25, and a variance from the requirement that all drive areas be dust-free.

The existing site conditions do not allow for the addition of 400 plus shrubs. As part of the project, the Applicant is adding trees and shrubs to the site, primarily along St. Anthony Parkway, but also in the northwest corner of the site. There is not enough physical space to plant the required number of shrubs.

The building will employ 23 people after the expansion with no plans for further employment. The facility is not open to the public and does not need to accommodate third-party traffic and parking. There is no need for 40 parking stalls to be constructed on the site.

The Applicant will add additional dust-free surfacing on the south side of the site and will make significant storm water improvements that will address potential runoff and tracking issues from currently and remaining gravel surfaces.

Please do not hesitate to call or email with questions or comments.

Thanks,

**Jeff**

Jeffrey A. Ellerd, P.E.

Wenck Associates Inc., 1802 Wooddale Drive, Suite 100, Woodbury, MN 55125-2937

[www.wenck.com](http://www.wenck.com) | [jellerd@wenck.com](mailto:jellerd@wenck.com) | D 651.395.5202 | F 651.228.1969 | G 651.294.4580 | C 612.709.7906

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# CPNA

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## Columbia Park Neighborhood Association

P.O. Box 21593

Minneapolis, MN 55421

Telephone: 612-788-5688

### **Board of Directors**

Jane McCarney  
Co-Chair

Darielle Dannon  
Co-Chair

Irro Mohamed  
Treasurer

Jeff Schalles  
Board Member

Matt Stellphlug  
Board Member

### **Staff**

Liz Wielinski

March 20, 2014

Chair Ted Tucker and Commissioners  
Minneapolis Planning Commission  
Public Service Center  
250 S 4th St, Room 300  
Minneapolis, MN 55415

Chair Tucker and Commissioners,

The Columbia Park Neighborhood Association (CPNA) would like to add the following to the record regarding the development at 81 St. Anthony Parkway. On March 17, 2014, Jeff Ellard from Wenck Assoc. presented the plans for the addition and other improvements at the site. While CPNA took no action to either support or deny the applicant's request for variances to the number of parking spots or the dust reducing surfacing of the parking lot, or the alternative compliance for the plantings, we would like you to consider adding the following to any conditions of approval.

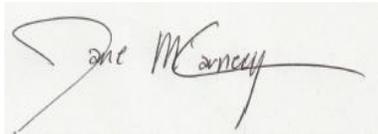
1. Some type of container management plan so that trailers, pallets, and other containers do not get placed in the entrance area and are stored in a less visible place on the lot. This would also include a more presentable appearance over the entire site consistent with an industrial business.

2. Appropriate signage that is appealing and identifies the site more appropriately (remove the Al Johnson Trucking sign). A monument type sign is preferred if a new one is added.

3. Addition of more evergreen trees along the southern edge of the property for more seasonal screening of the site. We do realize this is a steep slope and that a pipeline runs along the upper edge, but less visibility along the Grand Rounds Scenic Byway route of St. Anthony Parkway is preferred.

Thank you for your consideration of these matters.

Sincerely,



Jane McCarney, Co-Chair,



Darielle Dannon, Co-Chair  
Columbia Park Neighborhood Association



## **CONCERNED CITIZENS OF MARSHALL TERRACE (CCMT)**

P.O. Box 18180  
MINNEAPOLIS, MN 55418  
612-706-1460

[www.marshallterrace.org](http://www.marshallterrace.org)

email: [CCMT568@msn.com](mailto:CCMT568@msn.com)

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March 25, 2014

Chair Ted Tucker and Commissioners  
Minneapolis Planning Commission  
Public Service Center  
250 S 4th St, Room 300  
Minneapolis, MN 55415

Chair Tucker and Commissioners,

**The Concerned Citizens of Marshall Terrace (CCMT) would like to add the following to the record regarding the development at 81 St. Anthony Parkway. On March 17, 2014, Jeff Ellard from Wenck Assoc. presented the plans for the addition and other improvements at the site. While CCMT took no action to either support or deny the applicant's requests for variances to the number of parking spots or the dust reducing surfacing of the parking lot, or the alternative compliance for the plantings, we ask that you to consider adding the following to any conditions of approval.**

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1. **Some type of container management** plan so that trailers, pallets, and other containers do not get placed in the entrance area, and they are stored in a less visible place on the lot. This would also include a more presentable appearance over the entire site consistent with an industrial business.
2. **Appropriate signage** that is appealing and identifies the site more appropriately (remove the Al Johnson Trucking sign). A monument type sign is preferred if a new one is added.
3. **Addition of more evergreen trees** along the southern edge of the property for more seasonal screening of the site. We do realize this is a steep slope, but less visibility along the Grand Rounds Scenic Byway route of Saint Anthony Parkway is preferred. CCMT has approximately 1 dozen pine trees in their nursery that could be used and would be donated for that purpose.
4. It was noted that the company, Alliance Steel, has sites in North Minneapolis that apparently shred and do things other than "bale" scrap metal; we were assured those operations were not to be moved. **Those operations cannot be moved to this site without complete adherence to the site requirements and without variances and additional "Dust" abatement plans and procedures.** As it is, a site dust control/sweeping management plan and its monitoring are also requested by CCMT.

Thank you for your consideration of these matters.

Sincerely,

Greg Langason CCMT Chair

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THE STRENGTH OF THE PEOPLE IS IN THEIR NEIGHBORHOOD.  
THE STRENGTH OF THE NEIGHBORHOOD IS IN ITS PEOPLE.