

## Hiawatha Corridor LRT Yards and Shops Mitigation Study

In addition to precast concrete panels, a number of interesting metal panels are available that are commonly used in maintenance facilities and normally are possible in reasonable building budgets. A sketch elevation is included that illustrates a number of the suggestions as one possible approach. Also note two examples of maintenance facilities in other cities that use simple, economical forms, materials, and color to present acceptable buildings to a neighborhood. These buildings have a long life span and will impact the neighborhood for several generations.

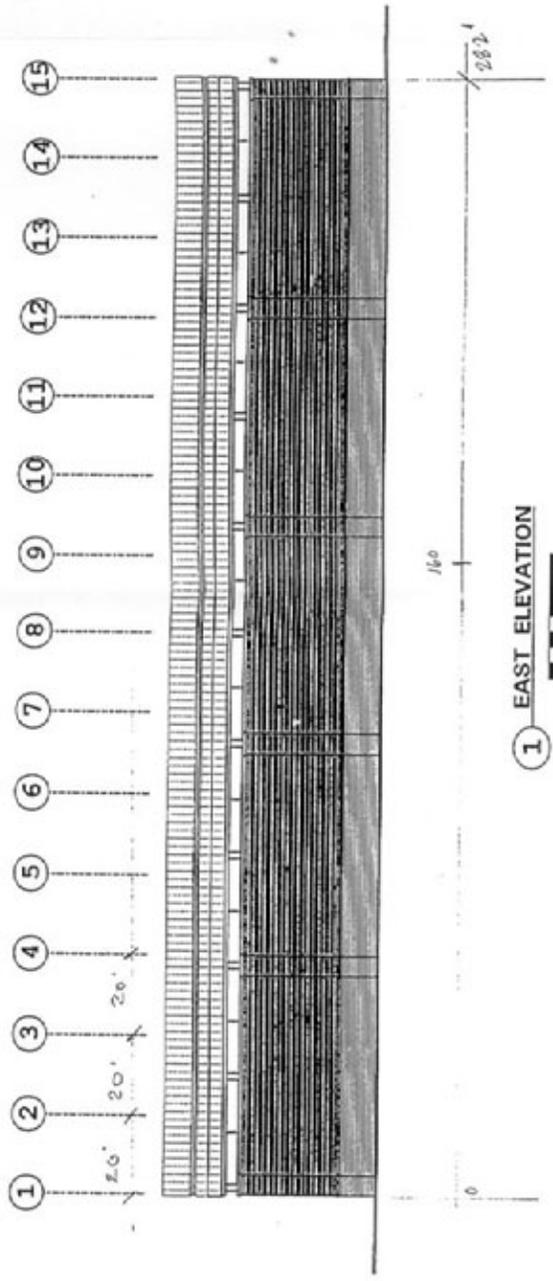
While all the facades of the building require additional design attention, priorities can be established based on the exposures and relationship to the neighborhood. The east elevation is most important since it will face the future development sites, and the existing immediate neighborhood. The long wall needs articulation. In addition to a mix of construction materials, consider planting deciduous vines that can climb the wall to add another texture and color to the wall.

The south elevation is a close second priority because it is the 'front door' of the facility and is the facade that is presented to the Franklin Avenue development sites and the transit station. This elevation offers a number of possibilities because of the overhead doors, openings, and the office functions.

The west elevation is important because it faces Hiawatha Avenue. It will be viewed mainly from vehicles at highway speeds but it is 640 feet long; approximately two city blocks. Landscape berming and tree planting will help mitigate the impact of this long unbroken wall. Deciduous vines should be planted at the base of this wall and wires attached to the wall to allow the vines to climb the wall for added texture and interest.

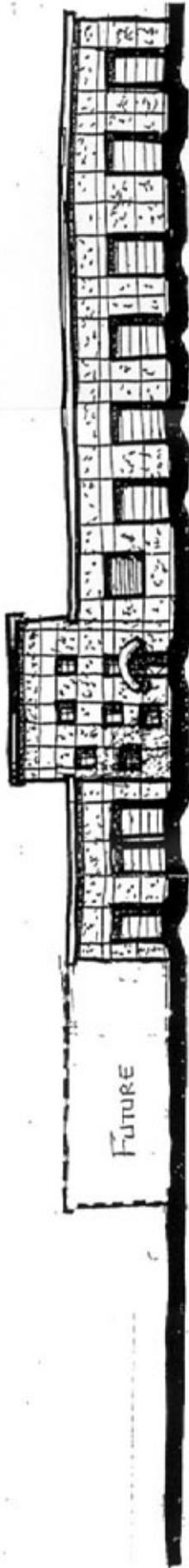
The north elevation has less of a priority because of its exposure to the I-94 overpass. However, the north elevation has the same opportunities as the south elevation for detailing and articulation. The perimeter screening outlined previously will minimize the impact of this elevation.



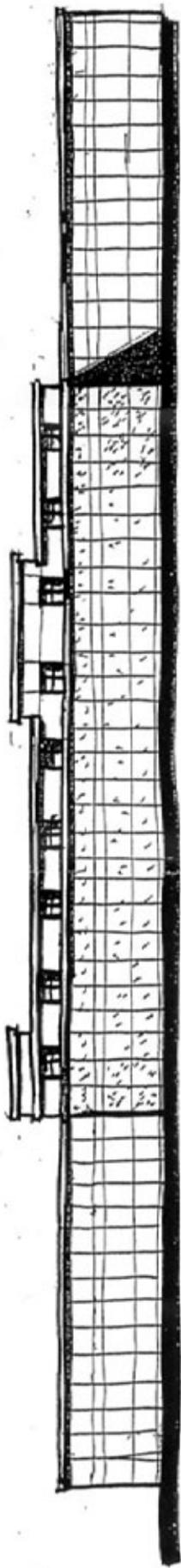


140  
120  
100

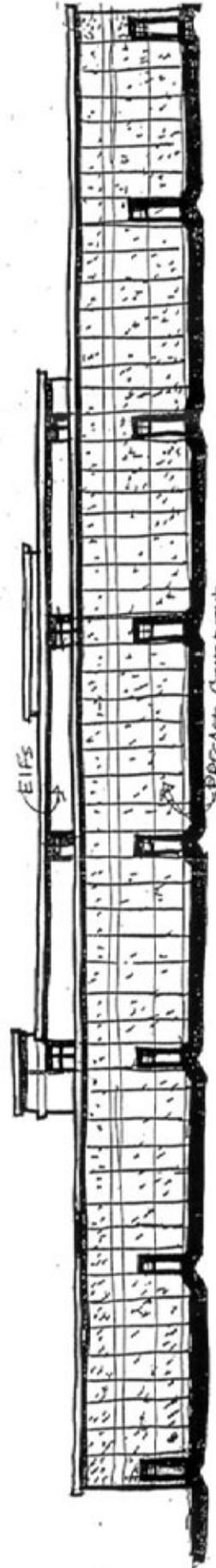
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		KL/SON															
MINNESOTA DEPARTMENT OF TRANSPORTATION SP 2724-1111 SHEET 827 OF 1382 HIAWATHA CORRIDOR LRT MAINTENANCE FACILITY YARD & SHOP EXTERIOR ELEVATIONS - SHEET 2 OF 2 SYSTEMS DRAWING NAME: M.F17																	
<b>CONTRACT DOCUMENTS</b> NOT FOR CONSTRUCTION						  LTK Engineering Services						 HPO <small>HIAWATHA MAINTENANCE</small>					



SOUTH ELEVATION



EAST ELEVATION

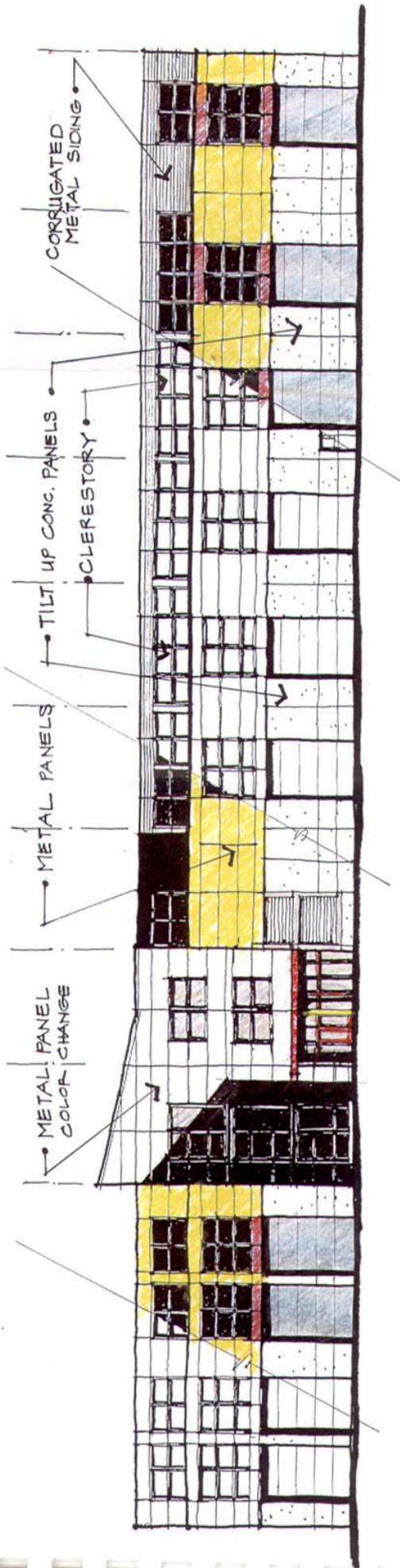


WEST ELEVATION

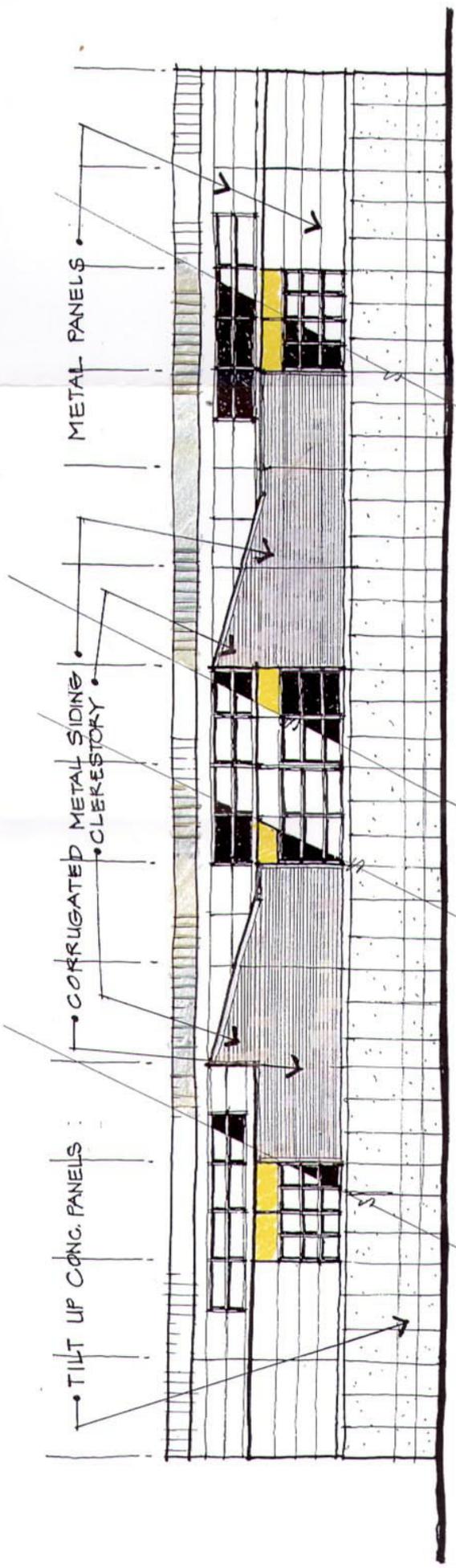


8/18/00

Current Concept  
Elevations  
Yards & Shops



ELEVATION STUDY - SOUTH ELEVATION  
1/8" = 1'-0"  
otak architects p.c.  
11.20.00.



ELEVATION STUDY. EAST ELEVATION  
 1/8" = 1' - 0"  
 otak architects, ps.  
 11.20.00