

PROPERTY ADDRESS  
**2004-2018 Lyndale Ave S**

FILE NUMBER  
**BZZ-7922**



04/01/2015

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REAL ESTATE ■ DEVELOPMENT ■ CONSTRUCTION

Master Properties Minnesota, LLC  
201 West Broadway Avenue  
Minneapolis, MN 55411  
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[www.masterpropertiesmn.com](http://www.masterpropertiesmn.com)

September 2, 2016

Mei-Ling Anderson  
City Planner  
Land Use, Design & Preservation  
250 South 4<sup>th</sup> Street – Room 300  
Minneapolis MN 55415

**Re: 2004-2018 Lyndale Avenue South, Minneapolis (Theatre Garage and Marquee Apartments) –  
Project Description / Statement of Proposed Use**

The subject property is located on the southwest quadrant of the intersection of Franklin and Lyndale Avenues in South Minneapolis in the Lowry Hill East Neighborhood. The property is comprised of five parcels containing two commercial buildings and a large, surface parking lot.

The multi-tenant commercial building situated on the northern part of the site was originally constructed in 1923 as an automotive sales and showroom building. This building is currently occupied by the Minneapolis Theatre Garage and Steeple People Thrift Store. The one story smaller multi-tenant commercial building located on the south property line of the site was constructed in 1958 and currently houses an art gallery and a dry-cleaners drop-off/pick-up and tailors. Situated between the two commercial buildings is a large, surface parking lot of approximately 54 stalls. This site and intersection are seen by many neighborhood residents and City officials as a gateway or entrance point to south Lyndale Avenue, Lyn-Lake, Uptown and South Minneapolis neighborhoods.

Master Properties Minnesota, on behalf of TGMA Developers, LLC, and Collage Architects, propose to construct a new, mixed-use building on the site that architecturally defines the intersection and establishes a prominent gateway to South Minneapolis. The proposed development would place a new, sit down restaurant at street level, anchoring the intersection. Lofted walk-up rental units will front Lyndale Avenue as well as a lobby that will provide access to the four stories of rental housing above. Structured below- and above-grade parking will support the project and will be fully enclosed within the project. Above street level, four levels of market rate apartments will front Lyndale and Franklin Avenues.

The proposed Theatre Garage development project will bring life and vibrancy to an underutilized site and prominent commercial intersection. The project will establish a significant gateway into the neighborhood, as well as make the area more pedestrian friendly by replacing the current surface parking lot with interactive residential walk-up units along Lyndale and a restaurant anchoring the intersection.

The proposed plan includes:

- Construction of:
  - 113 Alcove, One and Two bedroom units and walk-up lofted units—all market-rate, rental.
  - Approximately 8200sf of commercial space—including 1200sf fitness space and a 6800sf sit-down restaurant.
  - Residential street lobby for the residential units of the building along Lyndale Avenue
  - 144 parking stalls in a below grade garage level and an at-grade enclosed surface parking area both accessible off of Lyndale Avenue as well as a mezzanine level of parking, accessible off of Franklin Avenue.
  - A redesigned curb along Lyndale Avenue allowing for the widening of the sidewalk from 9' to 20' at the intersection.

Land Use Applications to be applied for include:

- Site Plan Review
- Rezoning 2008-2018 Lyndale Avenue South from C1 to C2
- Conditional Use Permit to increase max allowable height from 56 to 68' 11 5/8"
- Variance for a reduction in side and rear yard setbacks
  - Side yard: From 15' to 3.2'
  - Rear yard: From 15' to 3.9'
- Variance to increase the maximum allowable FAR above 2.38



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Mei-Ling Anderson  
Senior City Planner  
City of Minneapolis  
Community Planning & Economic Development  
Planning Division – Development Services  
250 South 6<sup>th</sup> Street, Room 300 PSC  
Minneapolis, MN 55414-1393

RE: General Land Use Application: **CONDITIONAL USE PERMIT APPLICATION**

Subject Property: 2004-2018 Lyndale Avenue South, Minneapolis, MN

**Conditional Use Permit (1): To allow for an increase in maximum height from 56' to 68' – 11 5/8"**

Master Properties Minnesota, LLC, on behalf of TGMA Developers, LLC, is requesting a conditional use permit to allow for an increase in maximum building height from 56' to 68'-11 5/8'

- 1. The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*  
Granting of an increase in maximum height of the proposed project from 56' to 68'-11 5/8" would not be detrimental nor endanger the public health, safety, comfort or general welfare. The topography at the subject site is such that the intersection is in a valley which rises steeply to the west and east along Franklin. When comparing the height of the proposed structure with structures in the immediate vicinity, taking into account the grade change, the structure is in line with the heights of the surrounding neighborhood. The intersection has also been identified as an area where the City would like to see increased density and mixed-use development. Allowing for increased height at the corner/intersection of this site would allow for compliance with the City's plans for increased density at commercial and transit corridors.
- 2. The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

The properties to the west of the subject site are the only residential properties to directly abut the proposed project. For the majority of the building's length along the western property line, the building will be at a height of approximately 11' with a 4' parapet (total height 15' 2"). Closer to Franklin Avenue, the building is its full height of 68' 11 5/8". Franklin's grade changes drastically from the intersection to both the west and east. The point at which the building abuts the adjacent residential uses, the grade is approximately seven feet higher than at the existing grade at the intersection. This grade change along Franklin Avenue gives the proposed building an appearance of being 61' rather than 68'. Also noteworthy is the building is visually a five story building with the first story being greater than 20' in height, requiring it to be classified as two stories.

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

Master has contracted with a civil engineering firm who has prepared a comprehensive utility and drainage plan for the subject site. The proposed project will have access both from Lyndale and Franklin Avenues.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

Master has contracted with a private traffic engineering and urban planning consultant to prepare a Travel Demand Management Plan (TDMP) to assess the proposed design and its impacts on the public infrastructure as well as how the project itself will integrate with the existing infrastructure. The analysis has shown that the proposed design will not cause traffic congestion in the public streets, rather it will provide a more efficient intersection design by closing the existing curb cuts near the intersection and replacing them with two curb cuts furthest from the intersection. In addition, Hennepin County has agreed to allow for the reconstruction of the curb alignment along Lyndale Avenue at the intersection in order to eliminate the existing taper as well as reduce the pedestrian crossing length across Lyndale.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

The City's comprehensive plan for sustainable growth identifies the intersection of Franklin and Lyndale as an area for density and mixed-use development. Allowing additional height at the intersection corner of the proposed project would allow for additional density at this location.

6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

The requested Conditional Use for increased height is consistent with the regulations in the C-1 and C-2 districts for a mixed-use development. The conditional use requested is directly related to the desire to provide additional density at the intersection of major commercial corridors and provides great benefit to the neighborhood by establishing housing at major transit intersections.

*Additional findings for conditional use permit for INCREASING MAXIMUM HEIGHT:*

1. *Access to light and air of surrounding properties.*  
The architectural team has provided a shadowing and context study for the proposed project. The proposed project will produce minimal shadowing effects to neighboring properties through the various seasons and equinoxes.
2. *Shadowing of residential properties, significant public spaces, or existing solar energy systems.*  
There are no significant public spaces or existing solar energy systems in the area that would be adversely affected by shadowing. Any shadowing of neighboring properties has been minimized through architectural design elements of reduced height and step backs in building design in areas closest to residential neighbors.
3. *The scale and character of surrounding uses.*  
The topography of the immediate area in which the subject site is located shows drastic grade changes with a valley, or low point, at the intersection and includes steep inclines to the west and east along Franklin Avenue. Taking into account these grade changes, the proposed height of the building fits appropriately within the scale and character of surrounding uses.
4. *Preservation of views of landmark buildings, significant open spaces or water bodies.*  
There are no designated landmark buildings, significant open spaces or water bodies that would be adversely affected by the requested additional height of the proposed project.



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Minneapolis, MN 55414-1393

RE: General Land Use Application: **VARIANCE APPLICATION**

Subject Property: 2004-2018 Lyndale Avenue South, Minneapolis, MN

**Variance (1): To allow for a reduction in rear and side yard setbacks from 15' to 3.9' and 3.2' respectively.**

Master Properties Minnesota, LLC, on behalf of TGMA Developers, LLC, is requesting a variance to reduce the minimum rear and side yard setbacks from 15' to 3.9' and 3.2' respectively.

- 1. Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

Several existing site conditions require Master Properties MN (Master) and TGMA Developers to request a variance to reduce the side and rear yard setbacks for this project. The most impactful condition being the soil conditions of the site. Master has completed geotechnical and environmental engineering soils analysis on the site. The existing soils are unsuitable and non-compactable and prohibit having 100 percent of the parking be located below grade. The design team was able to fit one level of parking below grade while placing the remaining project's parking enclosed within the building at street level and at a mezzanine level within the building. Due to the strict dimensions of stalls and drive aisles required to construct a parking facility, the architect has positioned the building as far from the west property line as possible to still accommodate code-required parking stalls and circulation/drive aisle dimensions, while still

allowing for the major load bearing wall separating the parking structure from the residential structure. If the building were relocated any further from the west property line, a majority of the parking would be lost due to an inability to accommodate required parking minimum dimensions.

Keeping setbacks along the continuous retail wall along Lyndale will continue the policies outlined in the City's plan for sustainable growth for strong commercial and transit corridors.

- 2. The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

Master, in working with our architectural, civil, and landscaping consultants, has proposed to maximize the proposed side and rear yard setbacks to still keep with the spirit and intent of the zoning ordinance. Along the majority of the western property line, the height of the structure is 15.2' (11' Wall height and 4" railing). Within the proposed 3.9' rear yard setback, Master proposes to fill the setback with new landscaping materials with the intent that the setback area would become an extension of the neighboring yards belonging to the properties to the west.

Additionally, the building design provides for residential window fenestrations above the commercial first floor, the architectural team has proposed a series of setbacks and insets to eliminate any commercial buildings from residential buildings future setback code requirements.

- 3. The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity nor will it be injurious or detrimental to the health, safety, or welfare of neither the general public nor those utilizing the property or nearby properties. As outlined above, the project will utilize the reduced setback by planting various shrubs, trees and other landscaping, in order to provide a buffer between the subject site and the adjacent sites. In keeping with the City's policies in the Minneapolis plan for sustainable growth, eliminating and minimizing "gaps" in the proposed building (commercial wall) design will eliminate dark or dead spots for pedestrians utilizing public sidewalks along these commercial corridors. The continuous wall will make for a more enjoyable and safe pedestrian experience as well as further foster vibrant commercial activity at the street level.

**Variance (2): To allow for an increase in the maximum allowable floor area ratio (FAR) above 2.36**

Master Properties Minnesota, LLC, on behalf of TGMA Developers, LLC, is requesting a variance to increase the maximum allowable FAR above 2.36

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

The vision for the Theater Garage project is to provide an urban infill project in keeping with the proposed development policies of the Minneapolis Plan for Sustainable Growth, and the Uptown Small Area Plan which promotes the need to: ... *“grow an independent and diverse business mix, promote a pedestrian friendly urban environment, support a dense environment with housing options for a variety of incomes and lifestyles, encourage incremental greening through development of small urban green spaces, environmentally sensitive (building) design and continue to plan for the parking needs of area businesses, residents and visitors while balancing the need for a walkable, well and visitors while balancing the need for a walkable, well developed area”.*

The Minneapolis Plan for Sustainable Growth encourages various commercial uses and high density housing along Commercial Corridors. *“When redevelopment projects are proposed, stepped down building heights, a transition of uses and buffering to adjacent residential areas is encouraged.”*

The Theater Garage project is in keeping with multiple Minneapolis policies to increase density along major Transit and Commercial Corridors. Lyndale Avenue is a major Transit and Commercial Corridor and Franklin Avenue is a major Transit and Community Corridor.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

Master Properties Minnesota, on behalf of TGMA Developers LLC has taken into consideration the City of Minneapolis comprehensive plan which calls for increased density and mixed-use development at this intersection. In order to achieve the level of density and mixed-uses that is desired by the City, a request to exceed the maximum allowable FAR is necessary.

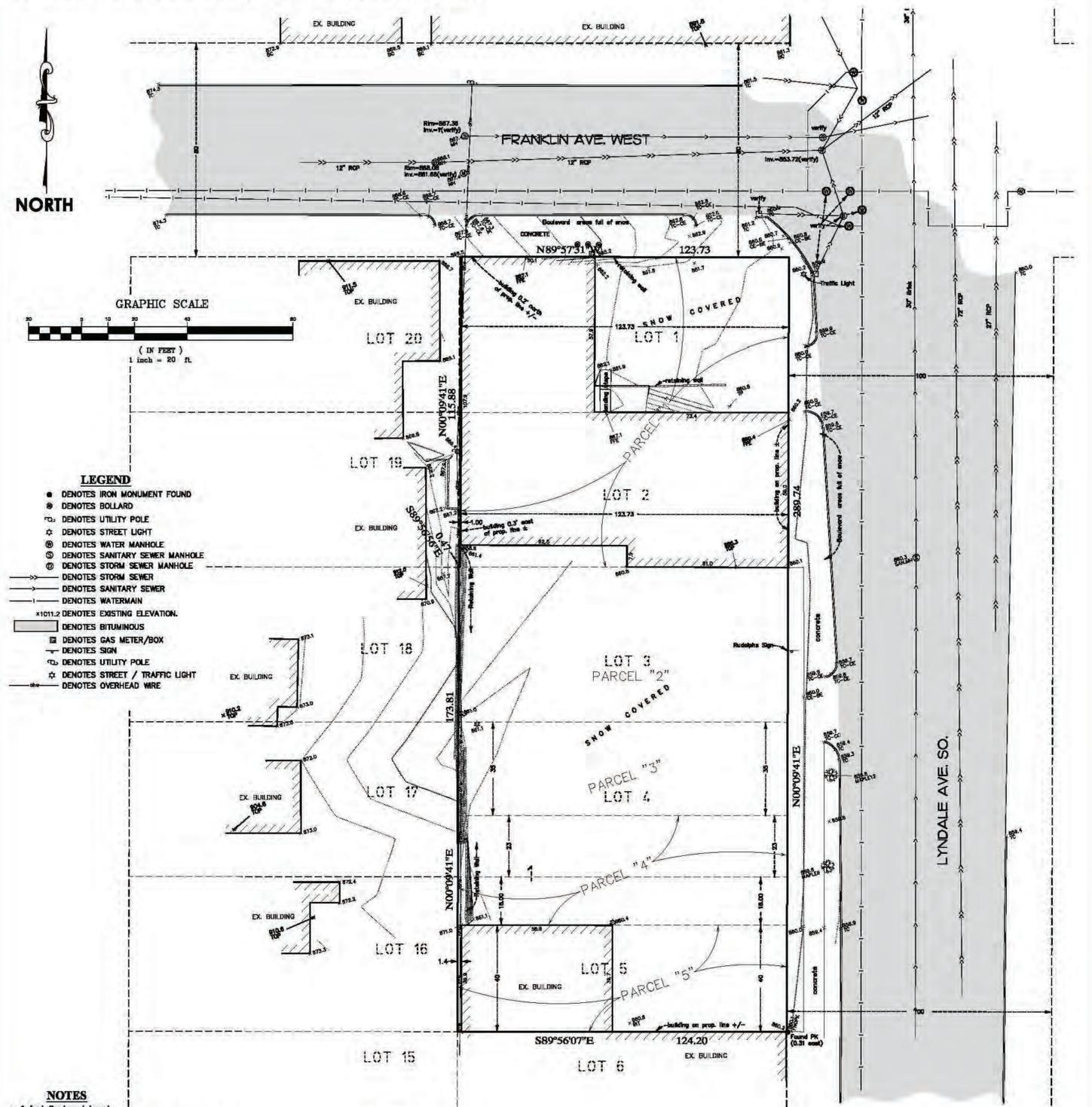
The design of the building takes the breaking up of the massing into consideration in order to meet the intent of the zoning code. The building visually from the street is a five story building, exceeding the maximum height permitted (without a CUP) in a C2 zoning district. Due to the proposed height of the first floor greater than 20 feet, this level is considered by zoning code to be two stories which make the project a six story building. Architecturally, the majority of the building massing has been placed along the Franklin and Lyndale avenues, away from the residential properties to the rear. The housing portion of the building on the floors above street level are designed in the shape of an “L”, following Franklin and Lyndale Avenues which creates large open spaces between the building and the neighboring properties. In doing so, the building massing is only 11 feet with a four foot parapet/railing along the majority of the western property line as seen from the neighboring residential properties. Above the second level, the massing of the residential building is set back an additional 50 feet from the edge of the building.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

The granting of this variance request will not be detrimental to the health, safety, or welfare of the general public or those utilizing the property or nearby properties. The topography in the surrounding areas of the site is such that Franklin Avenue rises steeply as it moves east and west from the intersection. Because of this, the grade to the west of the subject site is approximately 11 feet higher than the grade along Lyndale Avenue. The design of the building takes advantage of this grade change and the building design places the majority of the building's mass away from the private residential properties to the west and instead places the building bulk toward the street face along Franklin and Lyndale Avenues.

It is because of this grade change and these design elements that the height of the building at the property line, as viewed from the properties directly west, is 11 feet with a four foot parapet/railing along the majority of the property line. Above this height, the building is set back an additional 50 feet to the building-face on floors two and up. Shadowing studies produced by the architectural design team demonstrate minimal shadowing to the properties surrounding the site.

**ALTA/ACSM LAND TITLE SURVEY** PROPERTY ADDRESS: #2400 - #2018 LYNDALE AVE. SO., MINNEAPOLIS, MN  
 ~for~ 2004 Real Estate Company & Theros Realty II, LLC



- LEGEND**
- DENOTES IRON MONUMENT FOUND
  - DENOTES BOLLARD
  - DENOTES UTILITY POLE
  - ☆ DENOTES STREET LIGHT
  - DENOTES WATER MANHOLE
  - DENOTES SANITARY SEWER MANHOLE
  - DENOTES STORM SEWER MANHOLE
  - DENOTES STORM SEWER
  - DENOTES SANITARY SEWER
  - DENOTES WATERMAIN
  - x1011.2 DENOTES EXISTING ELEVATION.
  - DENOTES BITUMINOUS
  - DENOTES GAS METER/BOX
  - DENOTES SIGN
  - DENOTES UTILITY POLE
  - ☆ DENOTES STREET / TRAFFIC LIGHT
  - DENOTES OVERHEAD WIRE

- NOTES**
- 1 foot Contour Interval
  - N.G.V.D. Vertical Datum
  - This survey was completed Dec. 18th, 2013, in the middle of the Minnesota snow/winter season. There may be improvements/features in addition to those shown hereon that were not located due to snow/ice cover.

**BENCHMARK**  
 BASIS FOR ELEVATION: NAVD 88 (VIA REAL TIME GPS MEASUREMENTS UTILIZING MINNESOTA DEPARTMENT OF TRANSPORTATION VRS NETWORK)

**BASIS FOR BEARINGS: NAD83 (1986) (VIA REAL TIME GPS MEASUREMENTS UTILIZING MINNESOTA DEPARTMENT OF TRANSPORTATION VRS NETWORK).**

**PROPERTY DESCRIPTION**

Parcel 1:  
 The east 123.73 feet of Lots 1 and 2, Block 1, SUNNYSIDE ADDITION TO MINNEAPOLIS, Hennepin County, Minnesota.  
 #2004 Lyndale Ave. So., Minneapolis, MN

Parcel 2:  
 Lot 3, Block 1, SUNNYSIDE ADDITION TO MINNEAPOLIS, Hennepin County, Minnesota.  
 #2008 Lyndale Ave. So., Minneapolis, MN

Parcel 3:  
 The north 35 feet, front and rear, of Lot 4, Block 1, SUNNYSIDE ADDITION TO MINNEAPOLIS, Hennepin County, Minnesota.  
 #2012 Lyndale Ave. So., Minneapolis, MN

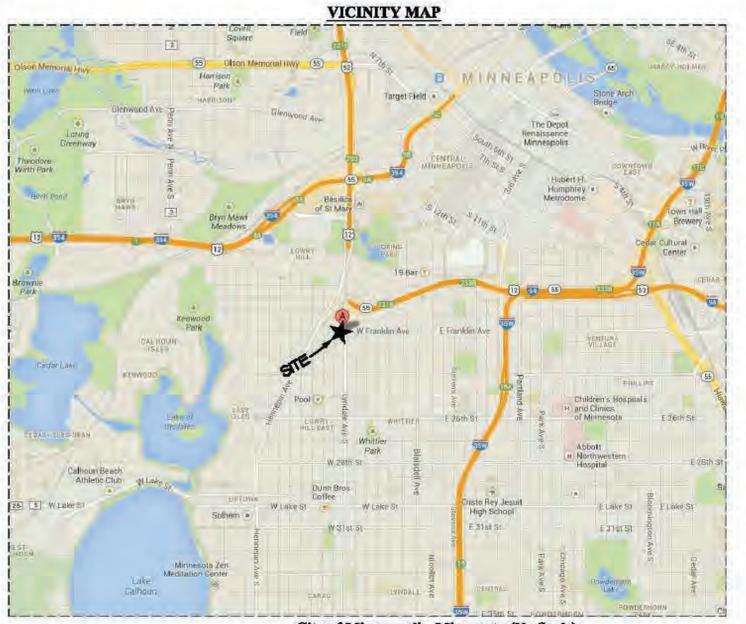
Parcel 4:  
 The south 23 feet of Lot 4 and the north 18 feet of Lot 5, Block 1, SUNNYSIDE ADDITION TO MINNEAPOLIS, Hennepin County, Minnesota.  
 #2014 Lyndale Ave. So., Minneapolis, MN

Parcel 5:  
 The south 40 feet of Lot 5, Block 1, SUNNYSIDE ADDITION TO MINNEAPOLIS, Hennepin County, Minnesota.  
 #2018 Lyndale Ave. So., Minneapolis, MN

All ABSTRACT property.

**GENERAL NOTES**

- Bearings shown hereon are on assumed datum.
- Boundary area of surveyed premises: 33,926± sq. ft. (0.82± acres)
- The surveyed premises has access to Lyndale Ave. So. & Franklin Ave. W., both publicly dedicated right-of-ways.
- Utilities shown hereon are observed. Excavations were not made during the process of this survey to locate underground utilities and/or structures. The location of underground utilities and/or structures may vary from locations shown hereon and underground utilities and/or structures may be encountered. Contact Greater State One Call Notification Center at (855) 494-0002 for verification of utility type and field location prior to excavation.
- Subsurface and environmental conditions were not examined or considered during the process of this survey. No statement is made concerning the existence of underground or overhead conditions or facilities that may affect the use or development of the surveyed property.
- This survey was completed 12/18/2013.
- There is no observable evidence of building construction or building additions within recent months. There is no observable evidence of site use as a solid waste dump, sump or sanitary landfill. There is no observable evidence of recent street or sidewalk repairs within recent months.
- Property Identification Number: 33-029-24-11-0017, 33-029-24-11-0020, 33-029-24-11-0021, 33-029-24-11-0021 & 33-029-24-11-0022.
- Property FEMA Flood map #2705300399E (unprinted map status).



I hereby certify to "2004 Real Estate Company & Theros Realty II, LLC" in their behalf, successors and assigns, that I have surveyed the property legally described hereon; that this survey is a true, correct and accurate drawing and representation of said property and the boundaries thereof; that this map or plat and the survey on which it is based were made in accordance with "Minimum Standard Detail Requirements for ALTA/ACSM Land Title Surveys," as jointly established and adopted by ALTA and ACSM in 2011, and includes Items 2, 3, 4, 5, 7 (a), 8, 11 (a & b), 16 and 18 of Table A thereof, and meets the Accuracy Standards (as adopted by ALTA and ACSM and in effect on the date of this certification), and that in locating or identifying recorded easements or other recorded documents I have relied upon the Commitment for Title Insurance issued by Old Republic National Title Insurance Company, dated Dec. 4th, 2013 (commitment no. ORTE740799).

Dated January 3rd, 2014.

*Eric R. Vickyona, P.L.S.#41425*  
 Eric R. Vickyona, P.L.S.#41425

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NO.	DATE	DESCRIPTION	BY
1	01/07/14	revise utilities	ERV
2	01/22/14	revise utilities	ERV
3			

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**JOB #13838**



# THEATER GARAGE

## MARQUEE APARTMENTS

MINNEAPOLIS, MINNESOTA



www.collagearch.com



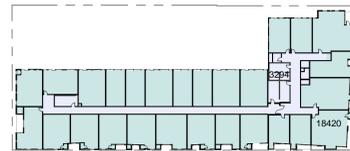
THEATER GARAGE  
LYNDALE AVENUE,  
MINNEAPOLIS



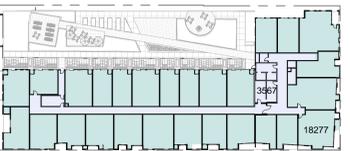
SIXTH LEVEL



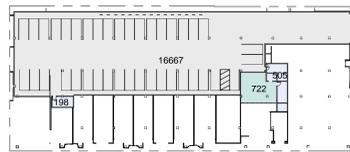
FIFTH LEVEL



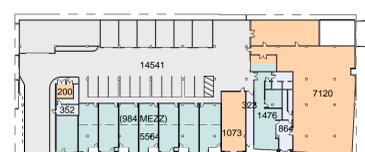
FOURTH LEVEL



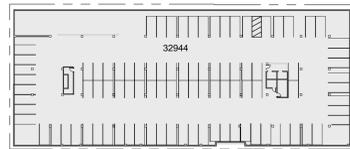
THIRD LEVEL



SECOND LEVEL



FIRST LEVEL



GARAGE LEVEL

NOTE: THESE AREAS CALCULATIONS ARE PRELIMINARY ONLY, NOT INTENDED FOR LEGAL OR FINANCIAL PURPOSES

### AREA SCHEDULE - GROSS BUILDING

Area	Level	Name
32944 SF	GARAGE LEVEL	PARKING
32944 SF		
984 SF	FIRST LEVEL	Area
14541 SF	FIRST LEVEL	PARKING
7040 SF	FIRST LEVEL	RESIDENTIAL
8393 SF	FIRST LEVEL	RESTURANT
1539 SF	FIRST LEVEL	UTILITY & CIRCULATION
32177 SF		
16667 SF	SECOND LEVEL	PARKING
722 SF	SECOND LEVEL	RESIDENTIAL
703 SF	SECOND LEVEL	UTILITY & CIRCULATION
18092 SF		
18277 SF	THIRD LEVEL	RESIDENTIAL
3567 SF	THIRD LEVEL	UTILITY & CIRCULATION
21844 SF		
18420 SF	FOURTH LEVEL	RESIDENTIAL
3294 SF	FOURTH LEVEL	UTILITY & CIRCULATION
21714 SF		
18420 SF	FIFTH LEVEL	RESIDENTIAL
3343 SF	FIFTH LEVEL	UTILITY & CIRCULATION
21763 SF		
18531 SF	SIXTH LEVEL	RESIDENTIAL
3361 SF	SIXTH LEVEL	UTILITY & CIRCULATION
18892 SF		
168427 SF		

### PARKING SCHEDULE

Count	Level	Type
1	GARAGE LEVEL	ADA - 8'0" x 18' - 90 deg
1	GARAGE LEVEL	ADA - 8' x 18' (8' Aisle)
40	GARAGE LEVEL	Compact - 8'6" x 15'
1	GARAGE LEVEL	Compact - 8' x 15'
50	GARAGE LEVEL	Standard - 8'6" x 18' - 90 deg
GARAGE LEVEL: 93		
1	FIRST LEVEL	ADA - 8' x 18' (8' Aisle)
10	FIRST LEVEL	Standard - 8'6" x 18' - 90 deg
FIRST LEVEL: 11		
1	SECOND LEVEL	ADA - 8'0" x 18' - 90 deg
1	SECOND LEVEL	ADA - 8' x 18' (8' Aisle)
37	SECOND LEVEL	Standard - 8'6" x 18' - 90 deg
SECOND LEVEL: 39		
GRAND TOTAL: 143		



PROJECT LOCATION

UNIT	BEDROOMS	BATHS	AVERAGE S.F.	QUANTITY	AREA (S.F.)
UNIT A	0	1	485	8	3880
UNIT A3	0	1	528	16	8448
UNIT A4	0	1	525	8	5000
UNIT A5	0	1	514	1	514
UNIT B	1	1	682	24	16368
UNIT B1	1	1	621	7	4347
UNIT B2	1	1	717	16	11472
UNIT B4	1	1	607	2	1214
UNIT B5	1	1	728	1	728
UNIT C	1	1	685	3	2055
UNIT D	1	1	690	4	2760
UNIT F	1	1	724	3	2172
UNIT G	2	2	1000	4	4000
UNIT H	2	2	1088	4	4352
UNIT L	2	2	1033	4	4132
UNIT M	2	1	804	7	5628
UNIT N	1	1	700	1	700
TOTAL ALCOVE UNITS					33
TOTAL ONE BEDROOM UNITS					60
TOTAL TWO BEDROOM UNITS					12
TOTAL LOFT UNITS					8
TOTAL UNITS					113
					77770

### UNIT DATA

UNIT SCHEDULE				UNIT SCHEDULE			
Name	Area	BD	BA	Name	Area	BD	BA
M UNIT	804 SF	2	1	A3 UNIT	528 SF	0	1
M UNIT	804 SF	2	1	A3 UNIT	528 SF	1	1
M UNIT	804 SF	2	1	A3 UNIT	528 SF	0	1
M UNIT	804 SF	2	1	A3 UNIT	528 SF	1	1
M UNIT	804 SF	2	1	A4 UNIT	557 SF	0	1
M UNIT	804 SF	2	1	A4 UNIT	553 SF	0	1
M UNIT	804 SF	2	1	A UNIT	487 SF	0	1
N UNIT	700 SF	1	1	A UNIT	473 SF	0	1
FIRST LEVEL: 8							
A3 UNIT	528 SF	1	1	B2 UNIT	743 SF	1	1
A3 UNIT	528 SF	0	1	B2 UNIT	740 SF	0	1
A3 UNIT	528 SF	0	1	B2 UNIT	740 SF	1	1
A3 UNIT	528 SF	1	1	B4 UNIT	639 SF	0	1
A4 UNIT	553 SF	0	1	B UNIT	652 SF	1	1
A4 UNIT	557 SF	0	1	B UNIT	800 SF	1	1
A5 UNIT	514 SF	1	1	B UNIT	700 SF	1	1
A UNIT	477 SF	0	1	B UNIT	632 SF	1	1
A UNIT	492 SF	0	1	B UNIT	684 SF	1	1
B2 UNIT	740 SF	1	1	B UNIT	670 SF	1	1
B2 UNIT	740 SF	1	1	B UNIT	723 SF	1	1
B2 UNIT	740 SF	0	1	B UNIT	645 SF	1	1
B2 UNIT	731 SF	0	1	C UNIT	684 SF	1	1
B UNIT	652 SF	1	1	D UNIT	659 SF	1	1
B UNIT	594 SF	1	1	F UNIT	695 SF	1	1
B UNIT	683 SF	1	1	G UNIT	1097 SF	2	2
B UNIT	700 SF	1	1	H UNIT	1081 SF	2	2
B UNIT	697 SF	1	1	L UNIT	1057 SF	2	2
B UNIT	645 SF	1	1	FIFTH LEVEL: 27			
B UNIT	800 SF	1	1	A3 UNIT	528 SF	1	1
B UNIT	690 SF	1	1	A3 UNIT	528 SF	0	1
C UNIT	684 SF	1	1	A3 UNIT	528 SF	0	1
D UNIT	659 SF	1	1	A3 UNIT	528 SF	1	1
F UNIT	694 SF	1	1	A4 UNIT	557 SF	0	1
G UNIT	1097 SF	2	2	A4 UNIT	553 SF	0	1
H UNIT	1081 SF	2	2	A UNIT	468 SF	0	1
L UNIT	979 SF	2	2	A UNIT	539 SF	0	1
THIRD LEVEL: 27							
A3 UNIT	528 SF	1	1	B1 UNIT	591 SF	1	1
A3 UNIT	528 SF	0	1	B1 UNIT	650 SF	1	1
A3 UNIT	528 SF	0	1	B1 UNIT	722 SF	1	1
A3 UNIT	528 SF	1	1	B1 UNIT	629 SF	1	1
A4 UNIT	553 SF	0	1	A3 UNIT	553 SF	1	1
A4 UNIT	557 SF	0	1	B1 UNIT	608 SF	1	1
A UNIT	449 SF	0	1	B1 UNIT	607 SF	1	1
A UNIT	487 SF	0	1	B2 UNIT	743 SF	1	1
A UNIT	743 SF	1	1	B2 UNIT	740 SF	1	1
B2 UNIT	740 SF	1	1	B2 UNIT	740 SF	0	1
B2 UNIT	740 SF	0	1	B5 UNIT	738 SF	1	1
B2 UNIT	731 SF	0	1	D UNIT	678 SF	1	1
B4 UNIT	639 SF	1	1	G UNIT	1050 SF	2	2
B UNIT	683 SF	1	1	H UNIT	969 SF	2	2
B UNIT	697 SF	1	1	L UNIT	1074 SF	2	2
B UNIT	700 SF	1	1	SIXTH LEVEL: 24			
B UNIT	645 SF	1	1	Grand total: 113			
B UNIT	685 SF	1	1				
B UNIT	714 SF	1	1				
B UNIT	595 SF	1	1				
B UNIT	763 SF	1	1				
C UNIT	684 SF	1	1				
D UNIT	659 SF	1	1				
F UNIT	684 SF	1	1				
G UNIT	1084 SF	2	2				
H UNIT	1081 SF	2	2				
L UNIT	979 SF	2	2				
FOURTH LEVEL: 27							

DRAWING INDEX			
Current Revision	ISSUED	#	SHEET NAME
	09/02/16	T100	TITLE SHEET AND PROJECT DATA
	09/02/16	C0.0	PROJECT TITLE SHEET
	04/29/16	C0.1	SITE SURVEY
	09/02/16	C1.0	REMOVALS PLAN
	09/02/16	C2.0	SITE PLAN
	09/02/16	C2.1	SITE PLAN ROOF DECK
	09/02/16	C3.0	GRADING PLAN
	09/02/16	C4.0	UTILITY PLAN
	09/02/16	C5.0	DETAILS
	09/02/16	C5.1	DETAILS
	09/02/16	L1.0	LANDSCAPE PLAN
	09/02/16	L1.1	ROOF DECK LANDSCAPE PLAN
	09/02/16	SW1.0	SWPP PLAN EXISTING CONDITIONS
	09/02/16	SW1.1	SWPP PLAN PROPOSED CONDITIONS
	09/02/16	SW1.2	SWPP PLAN DETAILS AND NARRATIVE
	09/02/16	A100	GARAGE LEVEL
	09/02/16	A101	FIRST LEVEL
	09/02/16	A102	SECOND LEVEL
	09/02/16	A103	THIRD LEVEL
	09/02/16	A104	FOURTH LEVEL
	09/02/16	A105	FIFTH LEVEL
	09/02/16	A106	SIXTH LEVEL
	09/02/16	A107	ROOF PLAN
	09/02/16	A200	ELEVATIONS
	09/02/16	A201	ELEVATIONS
	09/02/16	A300	BUILDING SECTIONS
	09/02/16	A303	BUILDING SECTIONS
	09/02/16	ASH00	SHADOW STUDIES
	09/02/16	A000	CONTEXT SITE PLAN
	09/02/16	A001	AERIAL VIEW
	09/02/16	A200-A	COLORLED ELEVATIONS
	09/02/16	A201-A	COLORLED ELEVATIONS
	09/02/16	A301	SITE SECTION
	09/02/16	A400	PERSPECTIVE
	09/02/16	A401	PERSPECTIVE
	09/02/16	A402	PERSPECTIVE

DATE:  
I HEREBY CERTIFY THAT THIS PLAN SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.

PETER KEELY  
REGISTRATION NO: 23570

Theater Garage Marquee Apartments  
Developers, LLC.  
612.872.9200  
1221 Nicollet Mall, Suite 310  
Minneapolis, MN 55403

Collage | architects  
Architects  
Peter Keely  
651.472.0050  
705 Raymond Avenue, Suite #200  
St. Paul, Minnesota 55114

Ericksen Roeds & Associates  
651.251.7570  
2550 University Avenue West, Suite #201  
St. Paul, Minnesota 55114

CivISITE Group  
Patrick Sarver  
952.250.2003  
4931 W. 35th Street, Suite #200  
St. Louis Park, Minnesota 55416

DATE: 09.02.2016

### PDR/LUA 9/2/2016

#### REVISIONS:

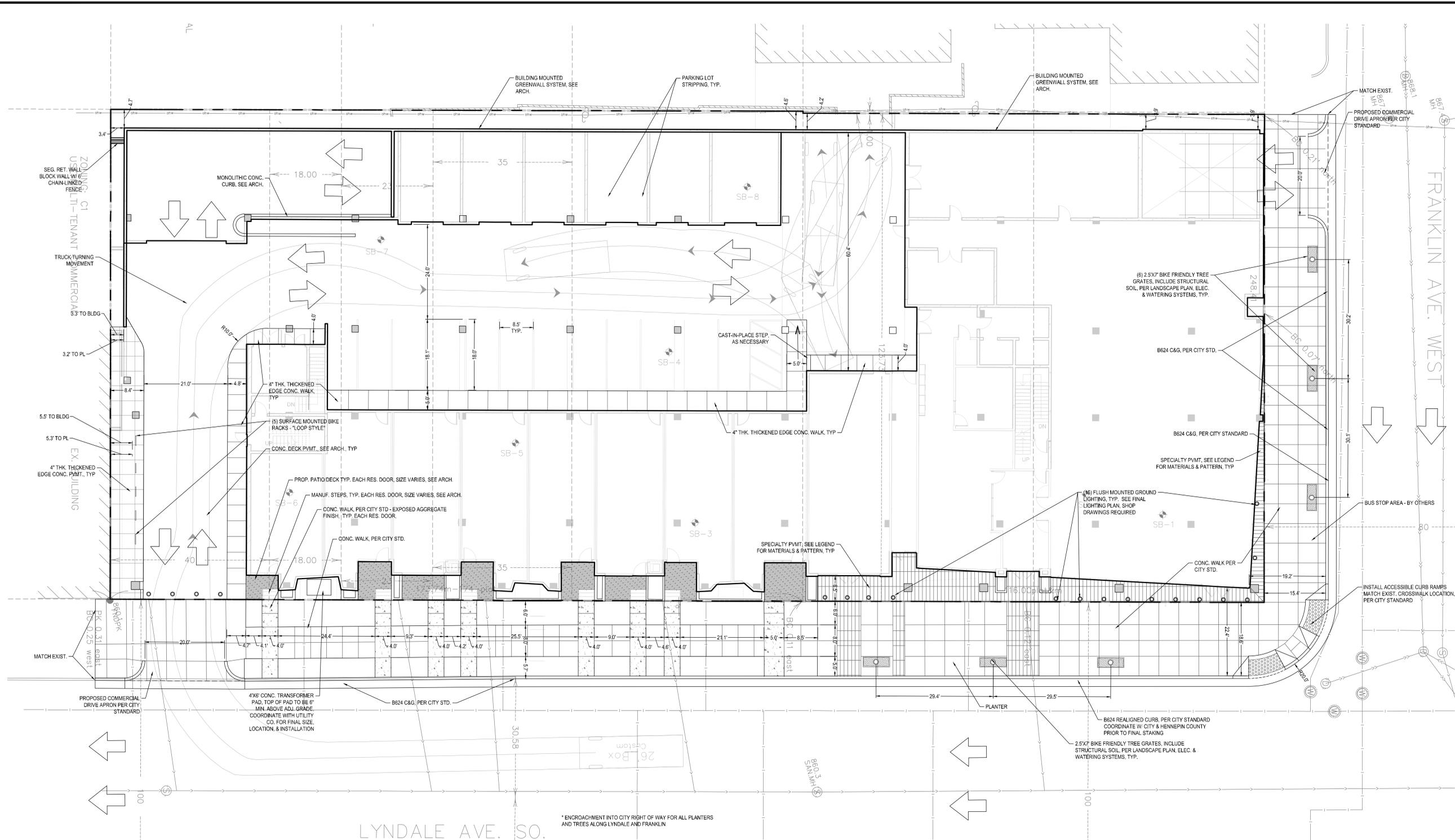
NO.	DATE	DESCRIPTION

PROJECT NO: 14\_001  
DRAWN BY: AR  
CHKD BY: Checker

SHEET TITLE

### TITLE SHEET AND PROJECT DATA

T100



**THEATER GARAGE REDEVELOPMENT**

2004-2018 LYNDALE AVE S., MINNEAPOLIS, MINNESOTA

**MASTER**

201 WEST BROADWAY AVE., MINNEAPOLIS, MN 55411

**CITY COORDINATION NOTES:**

- TRAFFIC & PARKING
  - COORDINATE WITH BILL PRINCE, CITY OF MINNEAPOLIS, (612) 673-3801 FOR ALL WORK REGARDING STREET LIGHTING AND ELECTRICAL SYSTEMS. STREET LIGHTING INSTALLED AS PART OF THE PROJECT SHALL BE INSPECTED BY THE CITY. CONTACT DAVE PRESHALL, (612) 673-5759. ANY LIGHTING INSTALLATIONS NOT MEETING CITY SPECIFICATIONS WILL BE REQUIRED TO BE REINSTALLED AT CONTRACTOR'S EXPENSE.
  - COORDINATE WITH BOB BOBLETT, CITY OF MINNEAPOLIS, (612) 673-2428 FOR ALL ISSUES REGARDING WORK IN AND ADJACENT TO CITY RIGHTS-OF-WAY. AN ENCROACHMENT PERMIT SHALL BE REQUIRED FOR ALL STREETSCAPE ELEMENTS IN THE PUBLIC ROW. ANY ELEMENTS OF AN EARTH RETENTION SYSTEM AND RELATED OPERATIONS WILL REQUIRE AN ENCROACHMENT PERMIT. ANY EXCAVATIONS THAT FALL WITHIN THE PUBLIC ROW WILL REQUIRE A ROW EXCAVATION PERMIT.
  - COORDINATE WITH SCOTT KRAMER, CITY OF MINNEAPOLIS, (612) 673-2383 REGARDING ANY WORK THAT IS PERFORMED IN THE RIGHT-OF-WAY. AN OBSTRUCTION PERMIT WILL BE REQUIRED FOR ALL WORK IN THE RIGHT-OF-WAY. CONTACT SCOTT KRAMER REGARDING DETAILS OF SIDEWALK AND LANE CLOSURES.
  - COORDINATE WITH CRAIG PINKALLA, CITY OF MINNEAPOLIS, (612) 499-9233 FOR ALL WORK REGARDING REMOVAL OR PROTECTION OF TREES DURING CONSTRUCTION IN THE CITY RIGHT-OF-WAY.
  - COORDINATE WITH PAUL CAO, CITY OF MINNEAPOLIS, (612) 673-2943 FOR ALL WORK REGARDING BIKE RACKS IN THE CITY RIGHT-OF-WAY.
  - COORDINATE WITH ALLAN KLUGMAN, CITY OF MINNEAPOLIS, (612) 673-5750 PRIOR TO CONSTRUCTION FOR THE TEMPORARY REMOVAL/RELOCATION OF ANY CITY OF MINNEAPOLIS SIGNAL SYSTEM. ALL COSTS FOR RELOCATION AND/OR REPAIR OF CITY TRAFFIC SHALL BE BORNE BY THE CONTRACTOR.
  - COORDINATE WITH DOUG MADAY, CITY OF MINNEAPOLIS, (612) 673-5755 PRIOR TO CONSTRUCTION FOR THE REMOVAL OR RELOCATION OF ANY CITY OF MINNEAPOLIS RIGHT-OF-WAY SIGNS.
- ENVIRONMENTAL HEALTH
  - COORDINATE WITH ENVIRONMENTAL SERVICES, CITY OF MINNEAPOLIS, (612) 673-3887 FOR PERMITS RELATING TO AFTER HOURS WORK, TEMPORARY STORAGE OF IMPACTED SOILS ON SITE PRIOR TO DISPOSAL OR REUSE, REMEDIATION OF CONTAMINATED SOIL AND GROUNDWATER, REUSE OF IMPACTED SOILS ON SITE, DEWATERING AND DISCHARGE OF ACCUMULATED STORM WATER OR GROUND WATER TO CITY SEWERS, FLAMMABLE WASTE TRAPS, UNDERGROUND OR ABOVEGROUND TANK INSTALLATION OR REMOVAL, WELL CONSTRUCTION OR SEALING, OR ON-SITE ROCK CRUSHING.
  - NO CONSTRUCTION, DEMOLITION OR COMMERCIAL POWER MAINTENANCE EQUIPMENT SHALL BE OPERATED WITHIN THE CITY BETWEEN THE HOURS OF 6:00 PM AND 7:00 AM ON WEEKDAYS OR DURING ANY HOURS ON SATURDAYS, SUNDAYS AND STATE AND FEDERAL HOLIDAYS, EXCEPT UNDER PERMIT.
  - IF CONTAMINATED SOIL IS ENCOUNTERED, IT MUST BE REPORTED TO THE MINNESOTA DUTY OFFICER AT (651) 649-8451. PRE-APPROVAL FOR REMOVAL OR REUSE MUST OCCUR FROM THE MRCA AND THE CITY OF MINNEAPOLIS.
- FORESTRY DEPARTMENT
  - TO PROTECT ROOT ZONES, NO CONSTRUCTION EQUIPMENT OR MATERIALS SHALL BE PLACED, PARKED, OR STORED ON ANY UNPAVED AREA WITHIN THE DRIP LINE OF ANY CITY OWNED TREE. NO CHEMICALS OR PETROLEUM PRODUCTS SHALL BE DEPOSITED ON ANY UNPAVED AREA IN THE CITY RIGHT-OF-WAY.
  - ANY TREE ROOTS ENCOUNTERED ARE TO BE CLEANLY CUT USING HAND TOOLS.
  - NO OPEN EXCAVATION OR BORE PITS ALLOWED WITHIN 8 FEET OF CITY STREET TREES.
  - CARE SHALL BE TAKEN NOT TO DAMAGE TREE TRUNKS OR BRANCHES. CONTRACTOR MUST CONTACT CRAIG PINKALLA WITH THE FORESTRY DEPARTMENT AT (612) 499-9233 REGARDING ANY QUESTIONS RELATED TO PLANTING, REMOVAL OR THE PROCESS FOR PROTECTION OF TREES DURING CONSTRUCTION IN THE CITY RIGHT-OF-WAY.

**SITE AREA CALCULATIONS:**

	EXISTING	PROPOSED
BUILDING COVERAGE	11,817.4 SF 32.9%	32,188.0 SF 89.6%
ALL PAVEMENTS	24,020.3 SF 66.9%	2,013.3 SF 5.6%
ALL NON-PAVEMENTS	90.6 SF 0.3%	1,727.0 SF 4.8%
<b>TOTAL SITE AREA</b>	<b>35,928.3 SF 100.0%</b>	<b>35,928.3 SF 100.0%</b>
IMPERVIOUS SURFACE	35,837.7 SF 99.7%	
EXISTING CONDITION	34,201.3 SF 95.2%	
DIFFERENCE	-1,636.4 SF -4.6%	

**SITE LAYOUT NOTES:**

- CONTRACTOR SHALL VERIFY LOCATIONS AND LAYOUT OF ALL SITE ELEMENTS PRIOR TO BEGINNING CONSTRUCTION, INCLUDING BUT NOT LIMITED TO, LOCATIONS OF EXISTING AND PROPOSED PROPERTY LINES, EASEMENTS, SETBACKS, UTILITIES, BUILDINGS AND PAVEMENTS. CONTRACTOR IS RESPONSIBLE FOR FINAL LOCATIONS OF ALL ELEMENTS FOR THE SITE. ANY REVISIONS REQUIRED AFTER COMMENCEMENT OF CONSTRUCTION, DUE TO LOCAL ADJUSTMENTS SHALL BE CORRECTED AT NO ADDITIONAL COST TO OWNER. ADJUSTMENTS TO THE LAYOUT SHALL BE APPROVED BY THE ENGINEER/LANDSCAPE ARCHITECT PRIOR TO INSTALLATION OF MATERIALS. STAKE LAYOUT FOR APPROVAL.
- THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS PRIOR TO CONSTRUCTION, INCLUDING A RIGHT-OF-WAY AND STREET OPENING PERMIT.
- THE CONTRACTOR SHALL VERIFY RECOMMENDATIONS NOTED IN THE GEO TECHNICAL REPORT PRIOR TO INSTALLATION OF SITE IMPROVEMENT MATERIALS.
- CONTRACTOR SHALL FIELD VERIFY COORDINATES AND LOCATION DIMENSIONS OF THE BUILDING AND STAKE FOR REVIEW AND APPROVAL BY THE OWNERS REPRESENTATIVE PRIOR TO INSTALLATION OF FOOTING MATERIALS.
- LOCATIONS OF STRUCTURES, ROADWAY PAVEMENTS, CURBS AND GUTTERS, BOLLARDS, AND WALKS ARE APPROXIMATE AND SHALL BE STAKED IN THE FIELD, PRIOR TO INSTALLATION, FOR REVIEW AND APPROVAL BY THE ENGINEER/LANDSCAPE ARCHITECT.
- CURB DIMENSIONS SHOWN ARE TO FACE OF CURB. BUILDING DIMENSIONS ARE TO FACE OF CONCRETE FOUNDATION. LOCATION OF BUILDING IS TO BUILDING FOUNDATION AND SHALL BE AS SHOWN ON THE DRAWINGS.
- THE CONTRACTOR SHALL SUBMIT SHOP DRAWINGS OR SAMPLES AS SPECIFIED FOR REVIEW AND APPROVAL BY THE ENGINEER/LANDSCAPE ARCHITECT PRIOR TO FABRICATION FOR ALL PREFABRICATED SITE IMPROVEMENT MATERIALS SUCH AS, BUT NOT LIMITED TO THE FOLLOWING, FURNISHINGS, PAVEMENTS, WALLS, RAILINGS, BENCHES, FLAGPOLES, LANDING PADS FOR CURB RAMPS, AND LIGHT AND POLES. THE OWNER RESERVES THE RIGHT TO SELECT INSTALLED MATERIALS NOT PREVIOUSLY APPROVED.
- PEDESTRIAN CURB RAMPS SHALL BE CONSTRUCTED WITH TRUNCATED DOME LANDING AREAS IN ACCORDANCE WITH A.D.A. REQUIREMENTS-SEE DETAIL.
- CROSSWALK STRIPING SHALL BE 24" WIDE WHITE PAINTED LINE, SPACED 48" ON CENTER PERPENDICULAR TO THE FLOW OF TRAFFIC. WIDTH OF CROSSWALK SHALL BE 5' WIDE. ALL OTHER PAVEMENT MARKINGS SHALL BE WHITE IN COLOR UNLESS OTHERWISE NOTED OR REQUIRED BY ADA OR LOCAL GOVERNING BODIES.
- CURB AND GUTTER TYPE SHALL BE B612 UNLESS OTHERWISE NOTED ON THE DRAWINGS-TAPER BETWEEN CURB TYPES-SEE DETAIL.

- ALL CURB RADI ARE MINIMUM 3' UNLESS OTHERWISE NOTED.
- CONTRACTOR SHALL REFER TO FINAL PLAT FOR LOT BOUNDARIES, NUMBERS, AREAS AND DIMENSIONS PRIOR TO SITE IMPROVEMENTS.
- FIELD VERIFY ALL EXISTING SITE CONDITIONS, DIMENSIONS.
- PARKING IS TO BE SET PARALLEL OR PERPENDICULAR TO EXISTING BUILDING UNLESS NOTED OTHERWISE.
- ALL PARKING LOT PAINT STRIPPING TO BE WHITE, 4" WIDE TYP.
- BITUMINOUS PAVING TO BE "LIGHT DUTY" UNLESS OTHERWISE NOTED. SEE DETAIL SHEETS FOR PAVEMENT SECTIONS.
- ALL TREES THAT ARE TO REMAIN ARE TO BE PROTECTED FROM DAMAGE WITH A CONSTRUCTION FENCE AT THE DRIP LINE. SEE LANDSCAPE DOCUMENTS.

**OWNER INFORMATION**

OWNER:  
TGMA DEVELOPERS LLC  
EIDEN BUILDING - NORTH LOOP GATEWAY  
201 WEST BROADWAY AVE.  
MINNEAPOLIS, MN 55316  
CONTACT: DONALD GERBERDING

**OPERATIONAL NOTES:**

- ALL SNOW WILL BE EITHER BE REMOVED FROM SITE AT EACH PLOWING OR STORED IN THE LANDSCAPED AREAS. NO SPECIFIC SNOW STORAGE LOCATIONS WILL BE REQUIRED.
- TRASH REMOVAL AND REGULAR DELIVERIES SHALL BE OCCUR THROUGH THE ALLEY EAST OF THE PROPOSED PROPERTY.

**CITY OF MINNEAPOLIS SITE SPECIFIC NOTES:**

- RESERVED FOR CITY SPECIFIC NOTES.

**SITE PLAN LEGEND:**

- CONCRETE PAVEMENT AS SPECIFIED (PAD OR WALK)
- PROPERTY LINE
- CURB AND GUTTER-SEE NOTES (T.O.) TIP-OUT GUTTER WHERE APPLICABLE-SEE PLAN
- TRAFFIC DIRECTIONAL ARROWS
- SIGN AND POST ASSEMBLY. SHOP DRAWINGS REQUIRED.
- HC = ACCESSIBLE SIGN
- NP = NO PARKING FIRE LANE
- ST = STOP
- CP = COMPACT CAR PARKING ONLY

**GOPHER STATE ONE CALL**  
WWW.GOPHERSTATEONECALL.ORG  
(800) 252-1166 TOLL FREE  
(612) 454-0002 LOCAL

1" = 10'-0"  
0' 0" 10' 0"

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Matthew R. Pavlek  
DATE: 8/8/16 LICENSE NO.: 44263

**ISSUE/SUBMITTAL SUMMARY**

DATE	DESCRIPTION
9/9/16	LUU / PDR SUBMITTAL

**REVISION SUMMARY**

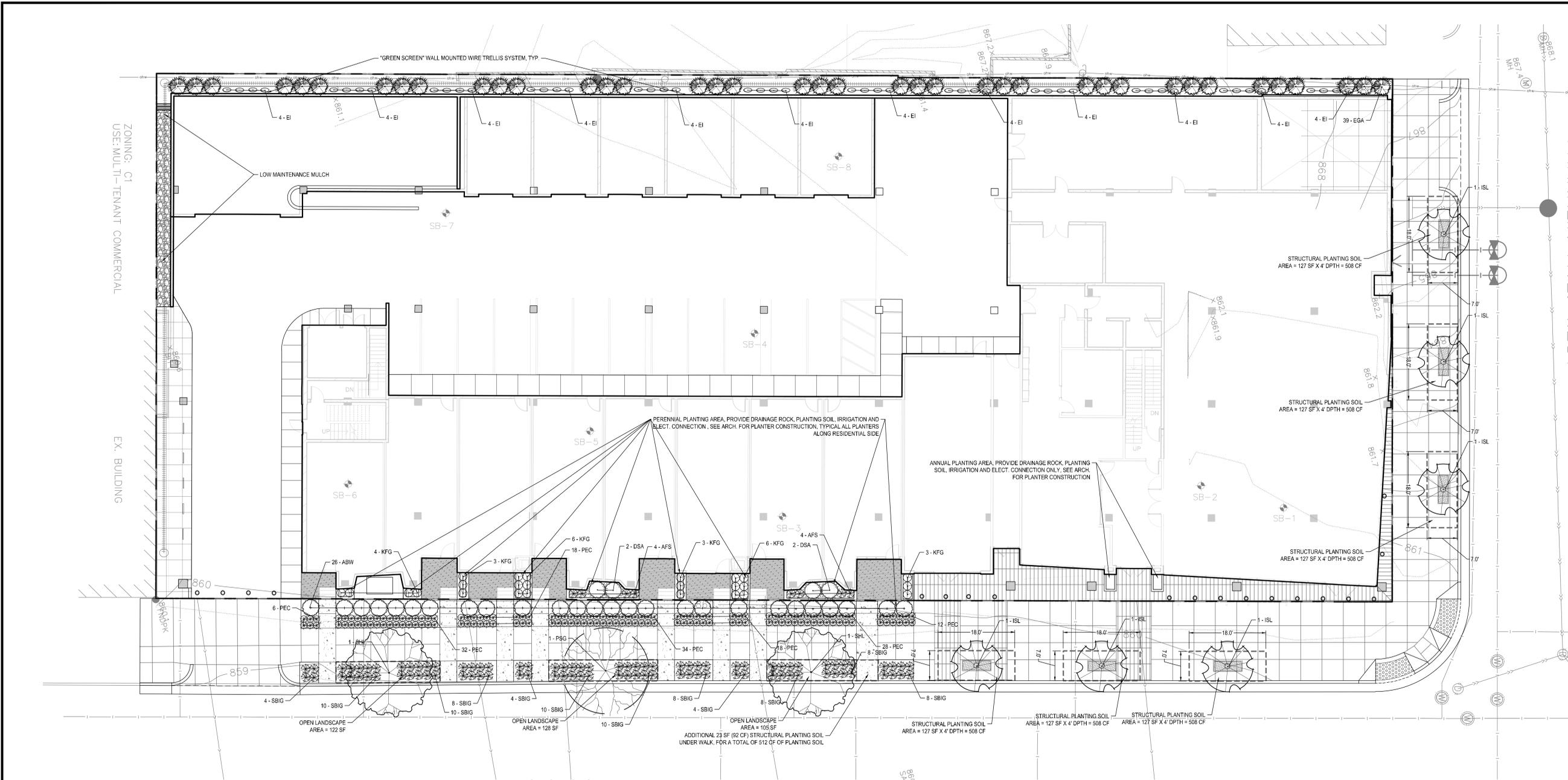
DATE	DESCRIPTION
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PROJECT NUMBER: 16005

**SITE PLAN**

**C2.0**

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**PLANT SCHEDULE - ENTIRE SITE**

SYM	ON-SITE	OFF-SITE	TOTAL	COMMON NAME	BOTANICAL NAME	SIZE	ROOT	COMMENTS	NATIVE	POLLINATOR FRIENDLY
<b>DECIDUOUS &amp; EVERGREEN TREES</b>										
SHL	2	2	4	Skyline Honeylocust	Gleditsia triacanthos 'Skycole'	2.5' CAL.	B&B		X	X
PSG	1	1	2	Princeton Sentry Ginkgo	Ginkgo biloba 'Princeton Sentry'	2.5' CAL.	B&B		X	
<b>SMALL SHADE TREES</b>										
ISL	6	6	12	Ivory Silk Tree Lilac (tree form)	Syringa reticulata 'Ivory Silk (tree form)'	1.5' CAL.	B&B		X	X
<b>DECIDUOUS &amp; EVERGREEN SHRUBS</b>										
ABW	26	26	52	Arctic Blue Leaf Willow	Salix purpurea 'Nana'	24" HT.	CONT.	DENSE BRANCHING	X	
DSA	4	4	8	Degroot's Spire Arborvitae	Thuja occidentalis 'Degroot's Spire'	24" HT.	CONT.	DENSE BRANCHING		X
EGA	39	39	78	Emerald Green Arborvitae	Thuja occidentalis 'Smaragd'	36" HT.	CONT.	DENSE BRANCHING		X
<b>PERENNIALS, GRASSES, GC &amp; VINES</b>										
KFG	25	25	50	KARL FOERSTER GRASS	Calamagrostis x acutiflora 'Karl Foerster'	#2	CONT.		X	
SBIG	92	92	184	Sioux Blue Indian Grass	Sorghastrum nutans 'Sioux Blue'	#2	CONT.		X	
AFS	8	8	16	Autumn Fire Sedum	Sedum spectabile 'Autumn Fire'	#2	CONT.		X	X
PEC	148	148	296	Purple Emperor Coneflower	Echinacea purpurea 'Purple Emperor'	#2	CONT.		X	X
EI	48	48	96	Englemann Ivy	Parthenocissus quinquefolia 'var. engelmannii'	#2	CONT.		X	

- LANDSCAPE NOTES:**
- ALL SHRUB BEDS SHALL BE MULCHED WITH 4" DEPTH OF DOUBLE SHREDDED HARDWOOD MULCH OVER WEED BARRIER. OWNER'S REP SHALL APPROVE MULCH SAMPLE PRIOR TO INSTALLATION. EDGING SHALL BE METAL EDGING OR APPROVED EQUAL.
  - PLANT MATERIALS SHALL CONFORM WITH THE AMERICAN ASSOCIATION OF NURSESPYMENT STANDARDS AND SHALL BE OF HARDY STOCK, FREE FROM DISEASE, DAMAGE AND DISFIGURATION. CONTRACTOR IS RESPONSIBLE FOR MAINTAINING PLUMBNESS OF PLANT MATERIAL FOR DURING OF ACCEPTANCE PERIOD.
  - UPON DISCOVERY OF A DISCREPANCY BETWEEN THE QUANTITY OF PLANTS SHOWN ON THE SCHEDULE AND THE QUANTITY SHOWN ON THE PLAN, THE PLAN SHALL GOVERN.
  - CONDITION OF VEGETATION SHALL BE MONITORED BY THE LANDSCAPE ARCHITECT THROUGHOUT THE DURATION OF THE CONTRACT. LANDSCAPE MATERIALS PART OF THE CONTRACT SHALL BE WARRANTED FOR ONE (1) FULL GROWING SEASONS FROM SUBSTANTIAL COMPLETION DATE.
  - ALL AREAS DISTURBED BY CONSTRUCTION ACTIVITIES SHALL RECEIVE 4" LAYER LOAM AND SOD AS SPECIFIED UNLESS OTHERWISE NOTED ON THE DRAWINGS.
  - COORDINATE LOCATION OF VEGETATION WITH UNDERGROUND AND OVERHEAD UTILITIES, LIGHTING FIXTURES, DOORS AND WINDOWS. CONTRACTOR SHALL STAKE IN THE FIELD FINAL LOCATION OF TREES AND SHRUBS FOR REVIEW AND APPROVAL BY THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
  - ALL PLANT MATERIALS SHALL BE WATERED AND MAINTAINED UNTIL ACCEPTANCE.
  - REPAIR AT NO COST TO OWNER ALL DAMAGE RESULTING FROM LANDSCAPE CONTRACTOR'S ACTIVITIES.
  - SWEEP AND MAINTAIN ALL PAVED SURFACES FREE OF DEBRIS GENERATED FROM LANDSCAPE CONTRACTOR'S ACTIVITIES.

- IRRIGATION NOTES:**
- ENTIRE SITE SHALL BE FULLY IRRIGATED. THE CONTRACTOR SHALL SUBMIT IRRIGATION SHOP DRAWINGS FOR REVIEW AND APPROVAL BY THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
  - SEE MECHANICAL AND ELECTRICAL PLANS AND SPECIFICATIONS FOR IRRIGATION WATER, METER, AND POWER CONNECTIONS.
  - CONTRACTOR TO VERIFY LOCATION OF ALL UNDERGROUND/ABOVE GROUND FACILITIES PRIOR TO ANY EXCAVATION/INSTALLATION. ANY DAMAGE TO UNDERGROUND/ABOVE GROUND FACILITIES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND COSTS ASSOCIATED WITH CORRECTING DAMAGES SHALL BE BORNE ENTIRELY BY THE CONTRACTOR.
  - SERVICE EQUIPMENT AND INSTALLATION SHALL BE PER LOCAL UTILITY COMPANY STANDARDS AND SHALL BE PER NATIONAL AND LOCAL CODES. EXACT LOCATION OF SERVICE EQUIPMENT SHALL BE COORDINATED WITH THE LANDSCAPE ARCHITECT OR EQUIVALENT AT THE JOB SITE.
  - CONTRACTOR SHALL COORDINATE WITH LOCAL UTILITY COMPANY FOR THE PROPOSED ELECTRICAL SERVICE AND METERING FACILITIES.
  - IRRIGATION WATER LINE CONNECTION SIZE IS 1-1/2" AT BUILDING. VERIFY WITH MECHANICAL PLANS FOR COVERAGE.
  - ALL MAIN LINES SHALL BE 18" BELOW FINISHED GRADE.
  - ALL LATERAL LINES SHALL BE 12" BELOW FINISHED GRADE.
  - ALL EXPOSED PVC RISERS, IF ANY, SHALL BE GRAY IN COLOR.
  - CONTRACTOR SHALL LAY ALL SLEEVES AND CONDUIT AT 2'-0" BELOW THE FINISHED GRADE OF THE TOP OF PAVEMENT. EXTEND SLEEVES TO 2'-0" BEYOND PAVEMENT.
  - CONTRACTOR SHALL MARK THE LOCATION OF ALL SLEEVES AND CONDUIT WITH THE SLEEVING MATERIAL "ELDED" TO 2'-0" ABOVE FINISHED GRADE AND CAPPED.
  - FABRICATE ALL PIPE TO MANUFACTURER'S SPECIFICATIONS WITH CLEAN AND SQUARE CUT JOINTS. USE QUALITY GRADE PRIMER AND SOLVENT CEMENT FORMULATED FOR INTENDED TYPE OF CONNECTION.
  - BACKFILL ALL TRENCHES WITH SOIL FREE OF SHARP OBJECTS AND DEBRIS.
  - ALL VALVE BOXES AND COVERS SHALL BE BLACK IN COLOR.
  - GROUP VALVE BOXES TOGETHER FOR EASE WHEN SERVICE IS REQUIRED. LOCATE IN PLANT BED AREAS WHENEVER POSSIBLE.
  - IRRIGATION CONTROLLER LOCATION SHALL BE VERIFIED ON-SITE WITH OWNERS REPRESENTATIVE.
  - CONTROL WIRES: 14 GAUGE DIRECT BURIAL, SOLID COPPER IRRIGATION WIRE. RUN UNDER MAIN LINE. USE MOISTURE-PROOF SPLICES AND SPICE ONLY AT VALVES OR PULL BOXES. RUN SEPARATE HOT AND COMMON WIRE TO EACH VALVE AND ONE (1) SPARE WIRE AND GROUND TO FURTHEST VALVE FROM CONTROLLER. LABEL OR COLOR CODE ALL WIRES.
  - AVOID OVERSPRAY ON BUILDINGS, PAVEMENT, WALLS AND ROADWAYS BY INDIVIDUALLY ADJUSTING RADIUS OR ARC ON SPRINKLER HEADS AND FLOW CONTROL ON AUTOMATIC VALVE.
  - ADJUST PRESSURE REGULATING VALVES FOR OPTIMUM PRESSURE ON SITE.
  - USE SCREENS ON ALL HEADS.
  - PRESENTATIVE, A SET OF AS-BUILT DRAWINGS SHALL BE MAINTAINED ON-SITE AT ALL TIMES IN AN UPDATED CONDITION.
  - ALL PIPE 3" AND OVER SHALL HAVE THRUST BLOCKING AT EACH TURN.
  - CONTRACTOR SHALL LAY ALL SLEEVES AND CONDUIT AT 2'-0" MINIMUM DEPTH OF 3/4" WASHED GRAVEL UNDERNEATH VALVE AND VALVE BOX. GRAVEL SHALL EXTEND 3" BEYOND PERIMETER OF VALVE BOX.
  - THERE SHALL BE 3" MINIMUM SPACE BETWEEN BOTTOM OF VALVE BOX COVER AND TOP OF VALVE STRUCTURE.

- POLLINATOR SAFE PLANT MATERIAL:**
- THE CONTRACTOR SHALL PROVIDE ONLY PLANT MATERIAL FREE OF NEONICOTINOID BASED INSECTICIDES AND/OR TREATMENTS OF ANY KIND, INCLUDING BY NOT LIMITED TO IMIDACLOPRID (CONFIDOR, ADMIRE, GAUCHO, ADVOCATE), THIAMETHOXAM (ACTARA, PLATINUM CRUISER), CLOTHIANIDIN (PONCHO, DANTOSU, DANTOP), ACETAMIPRID (MOSPIRAN, ASSAIL, CHIPCOTRISTAR), THIAZOLIPRID (CALYPSO), DINOTEFURAN (STARKE, SAFARI, VENOM), AND NITENPYRAM (CAPSTAR, GUARDIAN).
  - CONTRACTOR SHALL CERTIFY, THROUGH SUPPLIER'S POLICY STATEMENT OR AFFIDAVIT, THAT NO NEONICOTINOID BASED INSECTICIDES HAVE BEEN USED ON SITE OR DIRECTLY ADJACENT TO THE GROWING OR STORAGE PLOTS OF THE SUPPLIED PLANT MATERIAL, INCLUDING THE PLANTING OF AGRICULTURAL (OR OTHER) SEED TREATED WITH NEONICS.

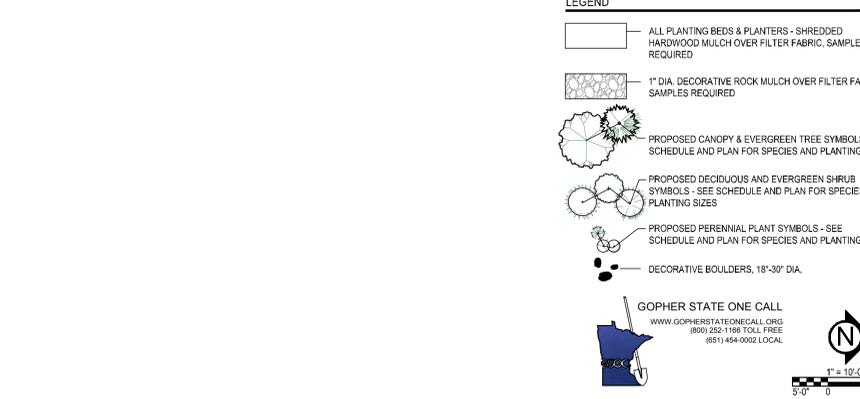
**LANDSCAPE CALCULATIONS:**

PROPOSED LANDSCAPE AREA CALCULATION:  
SITE AREA - BUILDING COVERAGE = OPEN SPACE  
35,928.3 SF - 32,188.0 SF = 3,740.3 SF

20% OF OPEN SPACE = LANDSCAPE AREA  
20% X 3,740.3 SF = 748.1 SF REQUIRED  
46% X 3,740.3 SF = 1,727.0 SF PROVIDED

95.2% PROPOSED IMPERVIOUS RATIO

PROPOSED LANDSCAPE:  
1 TREE / 500 SF OF "LANDSCAPE AREA"  
748.1 SF / 500 = 1 TREE REQUIRED  
1 SHRUB / 100 SF OR "LANDSCAPE AREA"  
748.1 SF / 100 = 7 SHRUBS REQUIRED  
SEE PLANT SCHEDULE  
SEE PLANT SCHEDULE







**THEATER GARAGE**  
LYNDALE AVENUE,  
MINNEAPOLIS

DATE:  
I HEREBY CERTIFY THAT THIS PLAN SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.

PETER KEELY  
REGISTRATION NO: 23570

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Architects  
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Patrick Sarver  
952.250.2003  
4931 W. 35th Street, Suite #200  
St. Louis Park, Minnesota 55416

DATE: 09.02.2016

**PDR/LUA 9/2/2016**

REVISIONS:

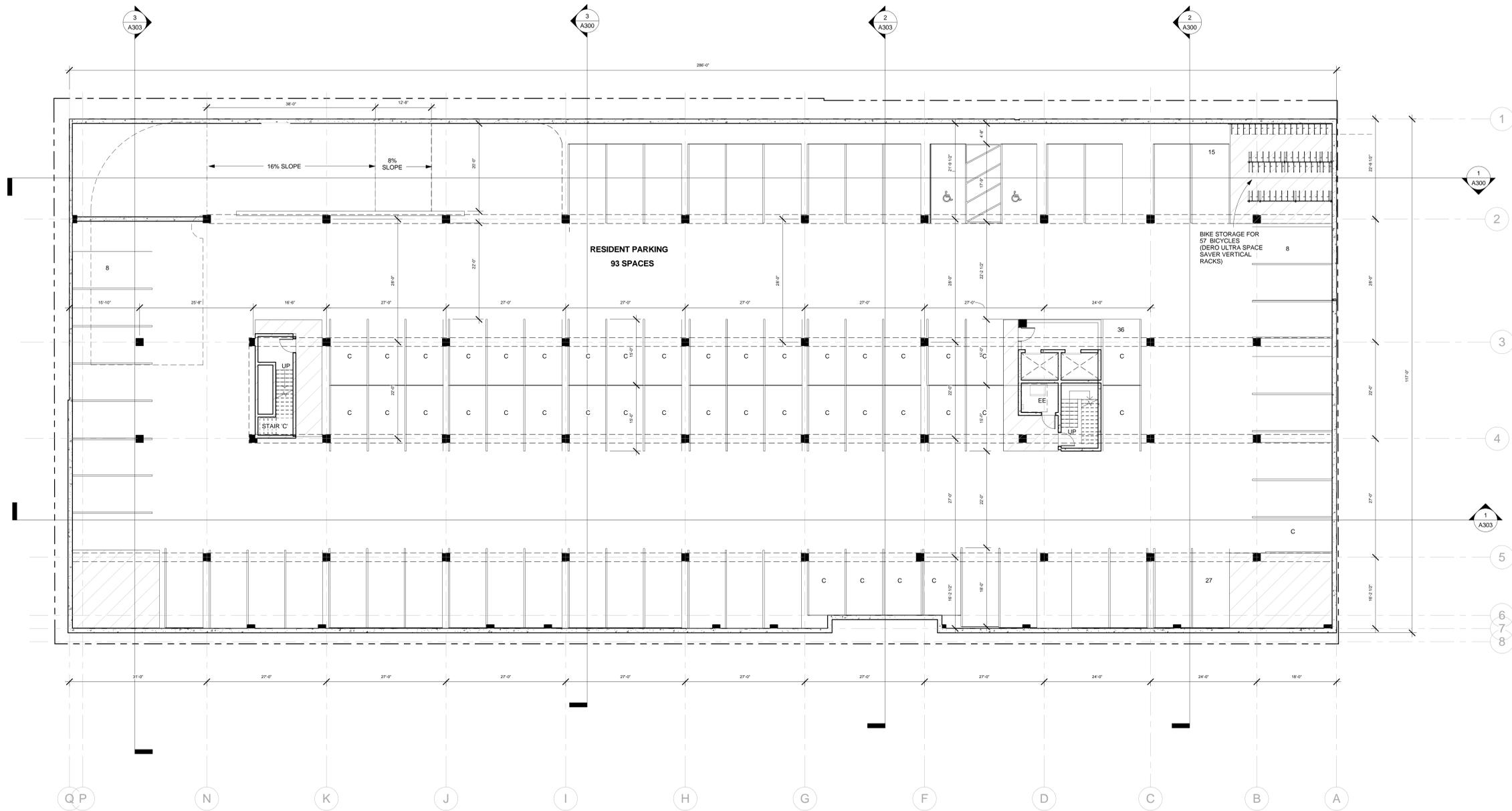
NO.	DATE	DESCRIPTION

PROJECT NO: 14\_001  
DRAWN BY: Author  
CHKD BY: Checker

SHEET TITLE

**GARAGE LEVEL**

**A100**



**1 GARAGE LEVEL**  
3/32" = 1'-0"



**THEATER GARAGE**  
LYNDALE AVENUE,  
MINNEAPOLIS

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**PDR/LUA 9/2/2016**

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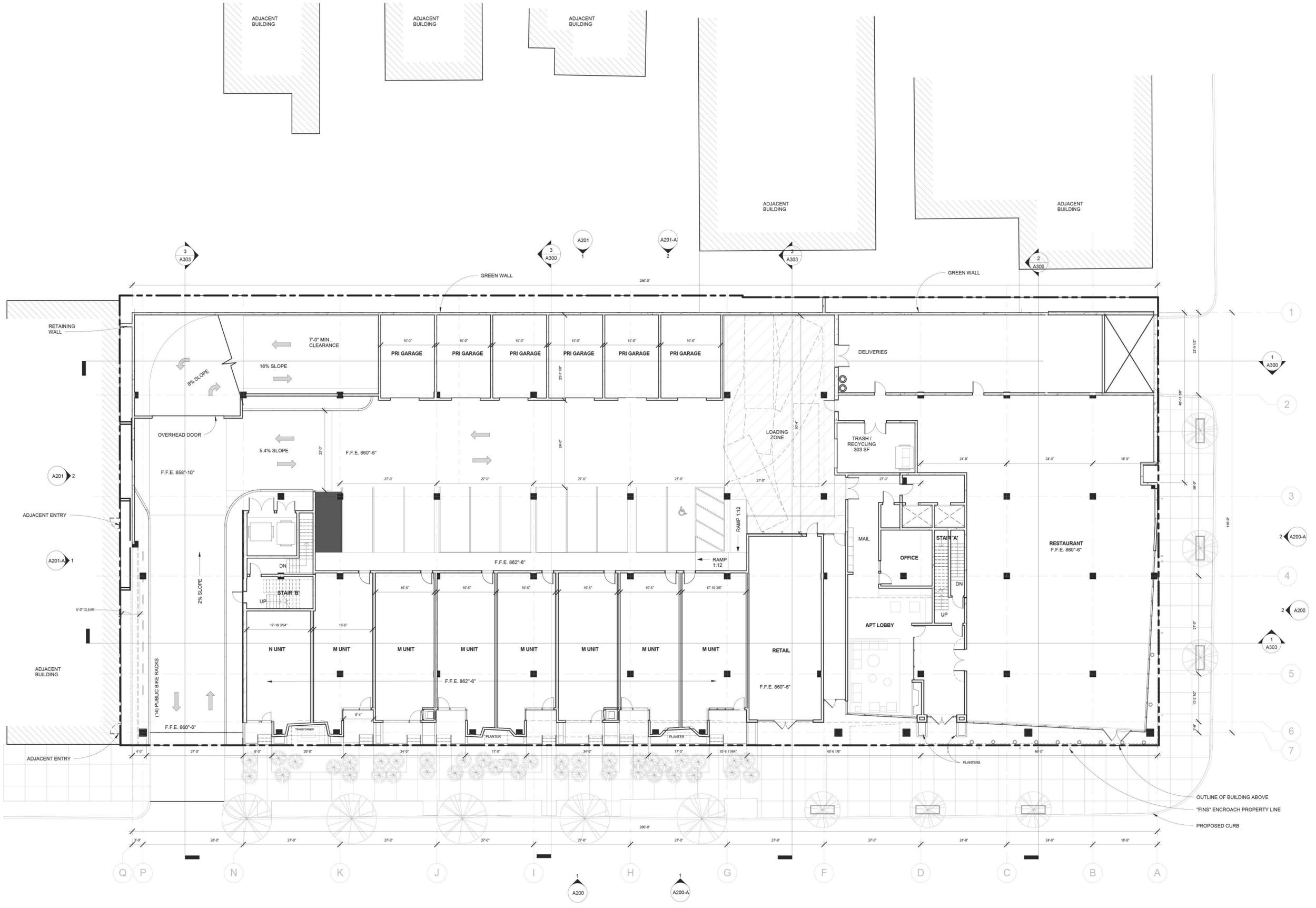
NO.	DATE	DESCRIPTION

PROJECT NO: 14\_001  
DRAWN BY: ACR  
CHKD BY: Checker

SHEET TITLE

**FIRST LEVEL**

**A101**



**1 FIRST LEVEL**  
3/32" = 1'-0"



**THEATER GARAGE**  
LYNDALE AVENUE,  
MINNEAPOLIS

DATE:  
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DATE: 09/02/2016

**PDR/LUA 9/2/2016**

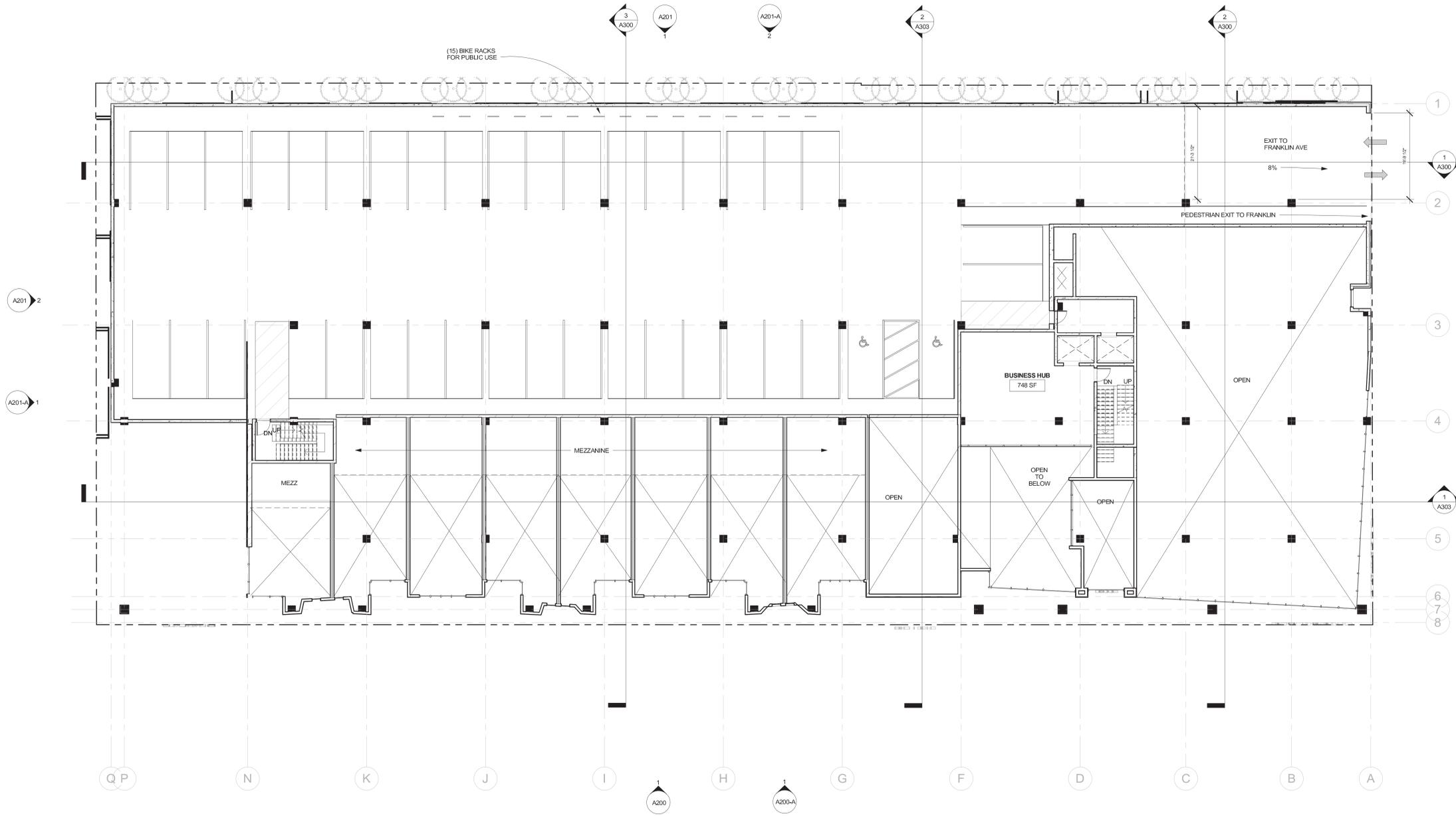
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NO.	DATE	DESCRIPTION

PROJECT NO: 14,001  
DRAWN BY: Author  
CHK'D BY: Checker

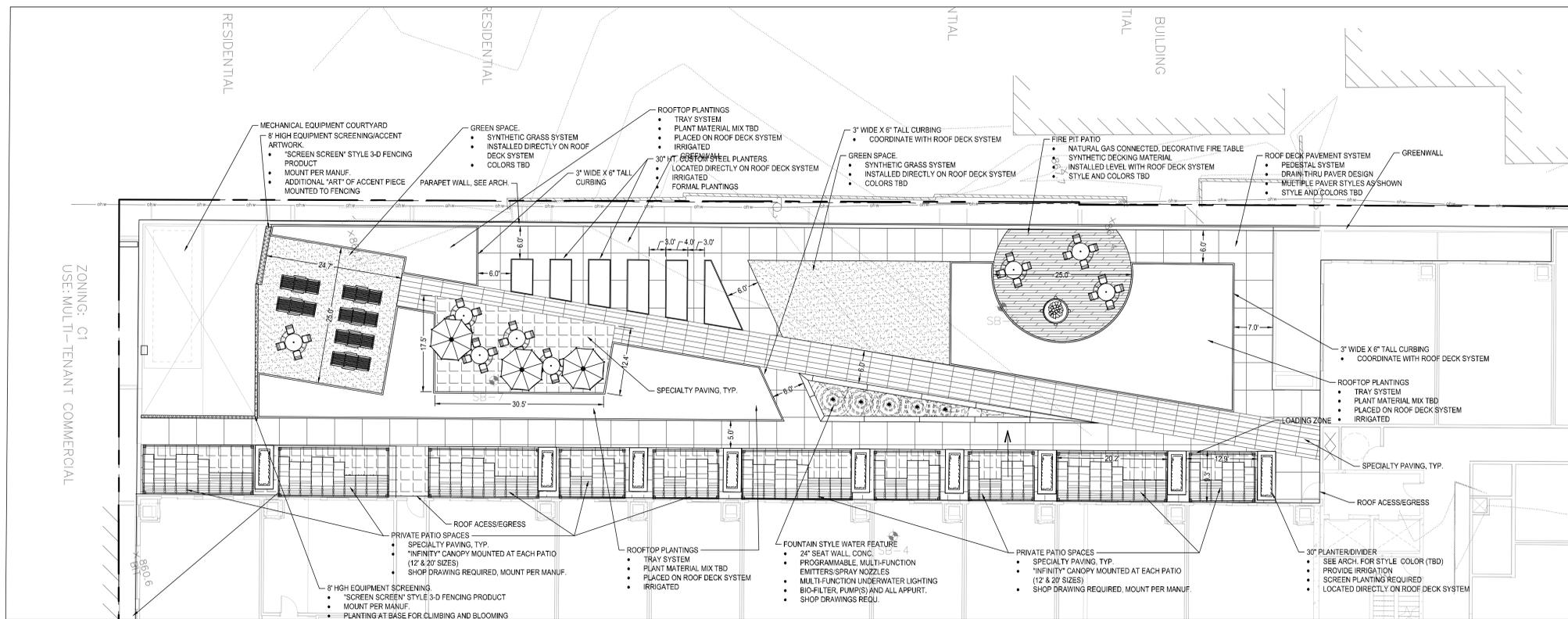
SHEET TITLE

**SECOND LEVEL**

**A102**



**1 SECOND LEVEL**  
3/32" = 1'-0"



1 2ND FL. ROOF TOP DECK SITE PLAN 1" = 10'-0" N

- SITE PLAN LEGEND:**
- CONCRETE PAVEMENT AS SPECIFIED (PAD OR WALK)
  - PROPERTY LINE
  - CURB AND GUTTER-SEE NOTES (T.O.) TIP OUT GUTTER WHERE APPLICABLE-SEE PLAN
  - TRAFFIC DIRECTIONAL ARROWS
  - SIGN AND POST ASSEMBLY. SHOP DRAWINGS REQUIRED.  
 HC = ACCESSIBLE SIGN  
 NP = NO PARKING FIRE LANE  
 ST = STOP  
 CP = COMPACT CAR PARKING ONLY

GOPHER STATE ONE CALL  
 WWW.GOPHERSTATEONECALL.ORG  
 (800) 252-1166 TOLL FREE  
 (651) 454-0002 LOCAL

**CivilSite GROUP**  
 4931 W. 35TH ST. SUITE 200  
 ST. LOUIS PARK, MN 55416  
 CIVILSITEGROUP.COM  
 Matt Pavlek 763-213-3844 Pat Sarver 952-205-2003

**Collage architects**

**master**  
 REAL ESTATE DEVELOPMENT CONSTRUCTION  
 Master Properties Minnesota LLC  
 Eden Building North Loop Gateway  
 201 W Broadway Avenue  
 Minneapolis, MN 55411  
 P 612-332-0200  
 www.masterproperties.com

**THEATER GARAGE REDEVELOPMENT**

2004-2018 LYNDALE AVE S., MINNEAPOLIS, MINNESOTA

**MASTER**

201 WEST BROADWAY AVE., MINNEAPOLIS, MN 55411

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

*Matthew R. Pavlek*  
 Matthew R. Pavlek  
 DATE 8/8/16 LICENSE NO. 44263

ISSUE/SUBMITTAL SUMMARY	
DATE	DESCRIPTION
8/8/16	LUU / PDR SUBMITTAL

REVISION SUMMARY	
DATE	DESCRIPTION

PROJECT NUMBER: 16055

**SITE PLAN ROOF DECK**

**C2.1**

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**THEATER GARAGE**  
LYNDALE AVENUE,  
MINNEAPOLIS

DATE:  
I HEREBY CERTIFY THAT THIS PLAN SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.

PETER KEELY  
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St. Louis Park, Minnesota 55416

DATE: 09/02/2016

**PDR/LUA 9/2/2016**

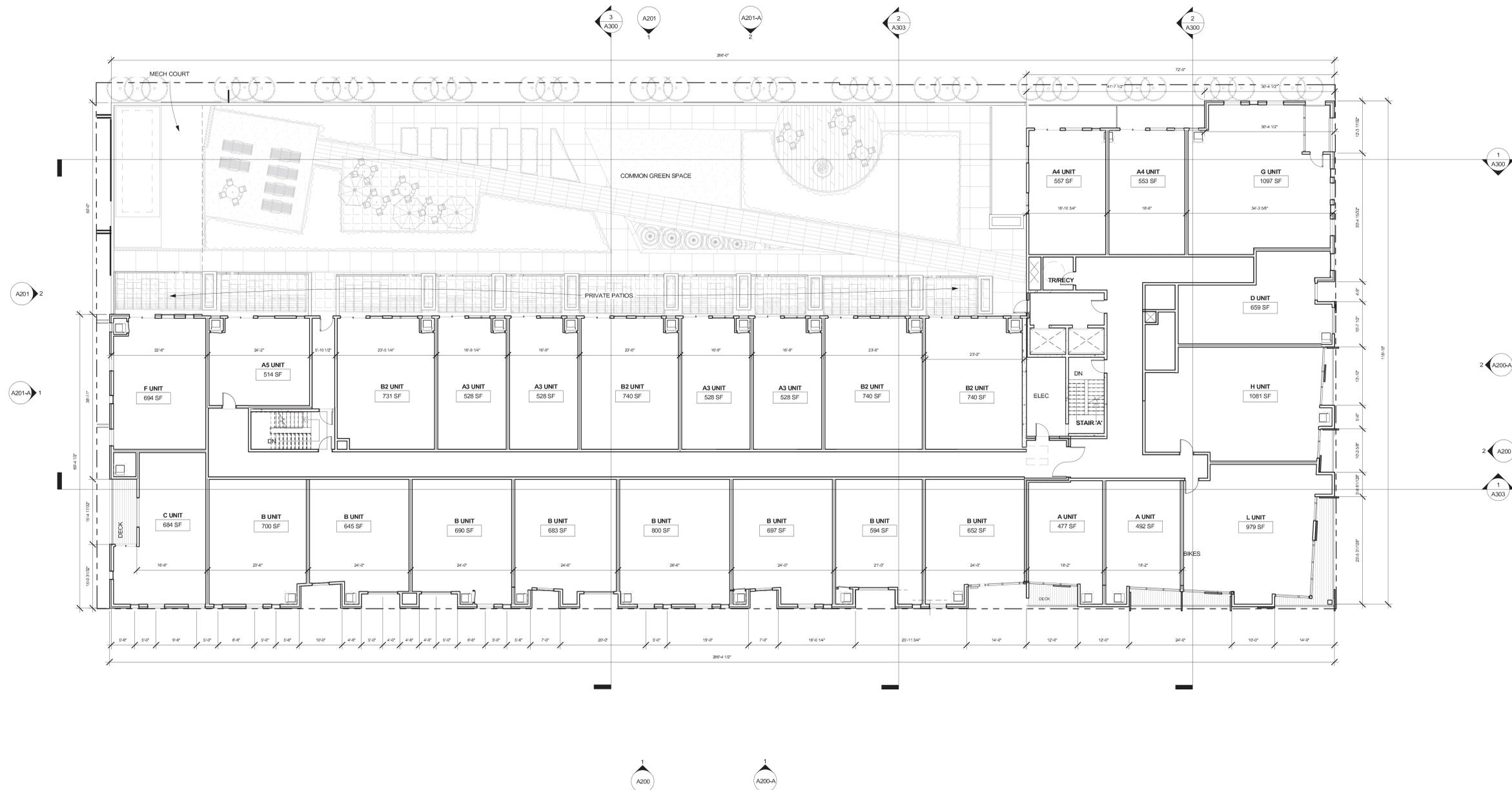
REVISIONS:		
NO.	DATE	DESCRIPTION

PROJECT NO: 14.003  
DRAWN BY: ACR  
CHK'D BY: Checker

SHEET TITLE

**THIRD LEVEL**

**A103**



**1 THIRD LEVEL**  
3/32" = 1'-0"



**THEATER GARAGE**  
LYNDALE AVENUE,  
MINNEAPOLIS

DATE:  
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PETER KEELY  
REGISTRATION NO: 23570

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4931 W. 35th Street, Suite #200  
St. Louis Park, Minnesota 55416

DATE: 09/02/2016

**PDR/LUA 9/2/2016**

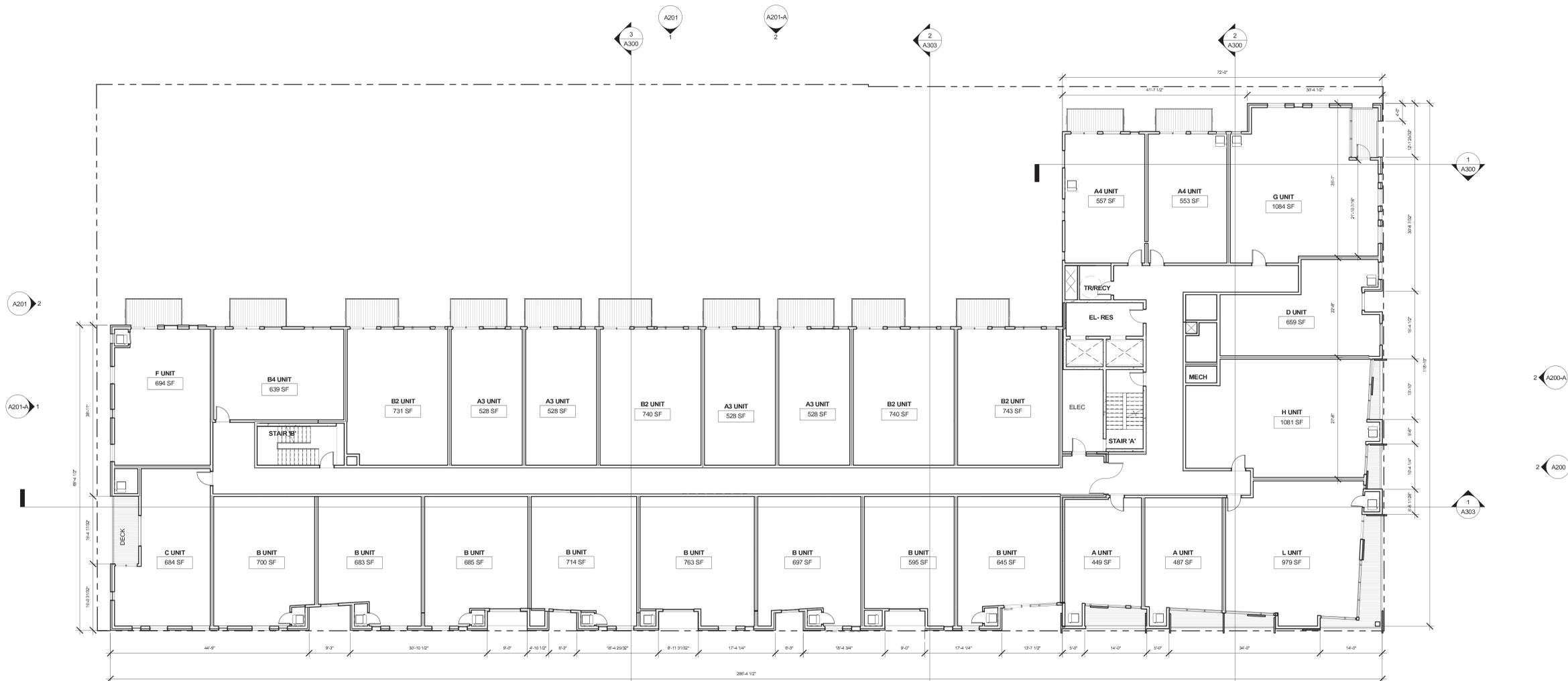
REVISIONS:		
NO.	DATE	DESCRIPTION

PROJECT NO: 14.001  
DRAWN BY: ACR  
CHK'D BY: Checker

SHEET TITLE

**FOURTH LEVEL**

**A104**



**1 FOURTH LEVEL**  
3/32" = 1'-0"

9/22/16 9:46:09 AM



**THEATER GARAGE**  
LYNDALE AVENUE,  
MINNEAPOLIS

DATE:  
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REGISTRATION NO: 23570

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DATE: 09/02/2016

**PDR/LUA 9/2/2016**

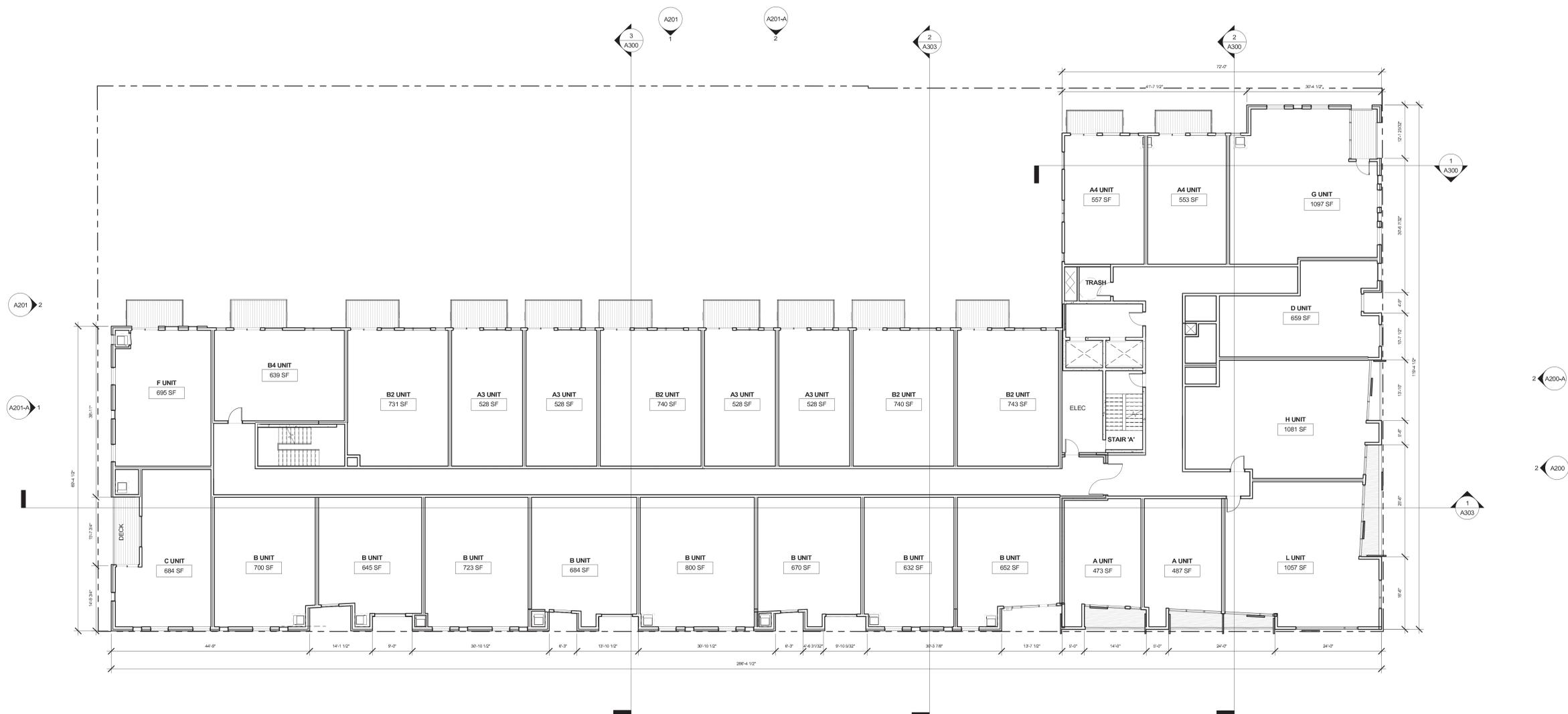
REVISIONS:		
NO.	DATE	DESCRIPTION

PROJECT NO: 14,001  
DRAWN BY: ACR  
CHK'D BY: Checker

SHEET TITLE

**FIFTH LEVEL**

**A105**



**1 FIFTH LEVEL**  
3/32" = 1'-0"



**THEATER GARAGE**  
LYNDALE AVENUE,  
MINNEAPOLIS



DATE:  
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REGISTRATION NO: 23570

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DATE: 09/02/2016

**PDR/LUA 9/2/2016**

REVISIONS:		
NO.	DATE	DESCRIPTION

PROJECT NO: 14.001  
DRAWN BY: ACR  
CHK'D BY: Checker

SHEET TITLE

**SIXTH LEVEL**

**A106**



**1 SIXTH LEVEL**  
3/32" = 1'-0"



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THEATER GARAGE  
LYNDALE AVENUE,  
MINNEAPOLIS



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4931 W. 39th Street, Suite #200  
St. Louis Park, Minnesota 55416

DATE: 09/02/2016

PDR/LUA 9/2/2016

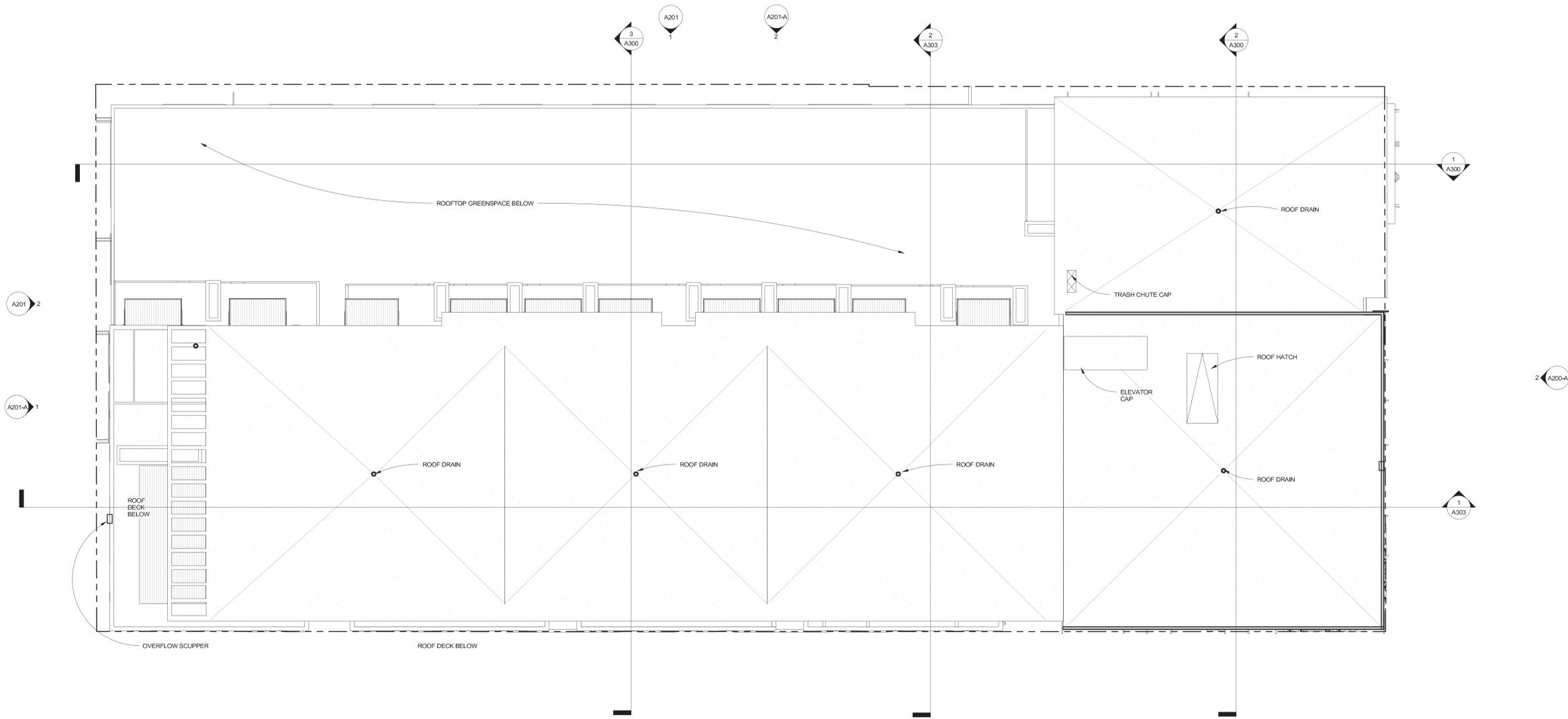
REVISIONS:		
NO.	DATE	DESCRIPTION

PROJECT NO: 14.001  
DRAWN BY: ACR  
CHK'D BY: Checker

SHEET TITLE

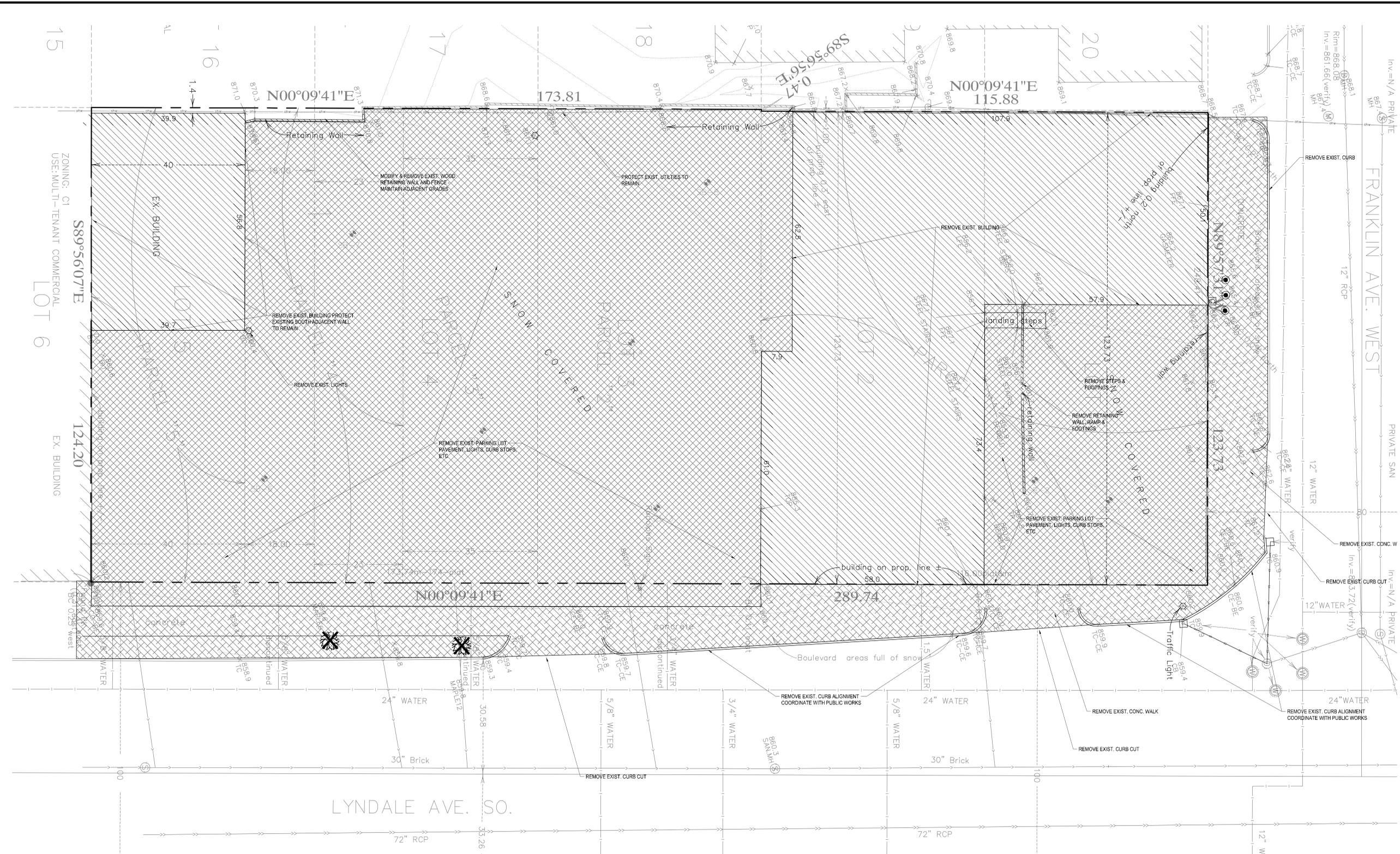
ROOF PLAN

A107



1 ROOF PLAN  
3/32" = 1'-0"

9/2/2016 9:51:32AM



**REMOVAL NOTES:**

- SEE STORM WATER POLLUTION PREVENTION PLAN (SWPPP) PLAN FOR CONSTRUCTION STORM WATER MANAGEMENT PLAN.
- REMOVAL OF MATERIALS NOTED ON THE DRAWINGS SHALL BE IN ACCORDANCE WITH MNDOT, STATE AND LOCAL REGULATIONS.
- REMOVAL OF PRIVATE UTILITIES SHALL BE COORDINATED WITH UTILITY OWNER PRIOR TO CONSTRUCTION ACTIVITIES.
- EXISTING PAVEMENTS SHALL BE SAWCUT IN LOCATIONS AS SHOWN ON THE DRAWINGS OR THE NEAREST JOINT FOR PROPOSED PAVEMENT CONNECTIONS.
- REMOVED MATERIALS SHALL BE DISPOSED OF TO A LEGAL OFF-SITE LOCATION AND IN ACCORDANCE WITH STATE AND LOCAL REGULATIONS.
- ABANDON, REMOVAL, CONNECTION, AND PROTECTION NOTES SHOWN ON THE DRAWINGS ARE APPROXIMATE. COORDINATE WITH PROPOSED PLANS.
- EXISTING ON-SITE FEATURES NOT NOTED FOR REMOVAL SHALL BE PROTECTED THROUGHOUT THE DURATION OF THE CONTRACT.
- PROPERTY LINES SHALL BE CONSIDERED GENERAL CONSTRUCTION LIMITS UNLESS OTHERWISE NOTED ON THE DRAWINGS. WORK WITHIN THE GENERAL CONSTRUCTION LIMITS SHALL INCLUDE STAGING, DEMOLITION AND CLEAN-UP OPERATIONS AS WELL AS CONSTRUCTION SHOWN ON THE DRAWINGS.
- MINOR WORK OUTSIDE OF THE GENERAL CONSTRUCTION LIMITS SHALL BE ALLOWED AS SHOWN ON THE PLAN AND PER CITY REQUIREMENTS.
- DAMAGE BEYOND THE PROPERTY LIMITS CAUSED BY CONSTRUCTION ACTIVITY SHALL BE REPAIRED IN A MANNER APPROVED BY THE ENGINEER/LANDSCAPE ARCHITECT OR IN ACCORDANCE WITH THE CITY.
- PROPOSED WORK (BUILDING AND CIVIL) SHALL NOT DISTURB EXISTING UTILITIES UNLESS OTHERWISE SHOWN ON THE DRAWINGS AND APPROVED BY THE CITY PRIOR TO CONSTRUCTION.
- SITE SECURITY MAY BE NECESSARY AND PROVIDED IN A MANNER TO PROHIBIT VANDALISM, AND THEFT, DURING AND AFTER NORMAL WORK HOURS, THROUGHOUT THE DURATION OF THE CONTRACT. SECURITY MATERIALS SHALL BE IN ACCORDANCE WITH THE CITY.
- VEHICULAR ACCESS TO THE SITE SHALL BE MAINTAINED FOR DELIVERY AND INSPECTION ACCESS DURING NORMAL OPERATING HOURS. AT NO POINT THROUGHOUT THE DURATION OF THE CONTRACT SHALL CIRCULATION OF ADJACENT STREETS BE BLOCKED WITHOUT APPROVAL BY THE CITY PRIOR TO CONSTRUCTION ACTIVITIES.
- ALL TRAFFIC CONTROLS SHALL BE PROVIDED AND ESTABLISHED PER THE REQUIREMENTS OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MMUTCD) AND THE CITY. THIS SHALL INCLUDE, BUT NOT BE LIMITED TO, SIGNAGE, BARRICADES, FLASHERS, AND FLAGGERS AS NEEDED. ALL PUBLIC STREETS SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES. NO ROAD CLOSURES SHALL BE PERMITTED WITHOUT APPROVAL BY THE CITY.
- SHORING FOR BUILDING EXCAVATION MAY BE USED AT THE DISCRETION OF THE CONTRACTOR AND AS APPROVED BY THE OWNERS REPRESENTATIVE AND THE CITY PRIOR TO CONSTRUCTION ACTIVITIES.
- STAGING, DEMOLITION, AND CLEAN-UP AREAS SHALL BE WITHIN THE PROPERTY LIMITS AS SHOWN ON THE DRAWINGS AND MAINTAINED IN A MANNER AS REQUIRED BY THE CITY.

**CITY OF MINNEAPOLIS REMOVAL NOTES:**

- RESERVED FOR CITY SPECIFIC REMOVAL NOTES.

**EROSION CONTROL NOTES:**

SEE SWPPP ON SHEETS SW1.0-SW1.2

**OWNER, DEVELOPER, CONTRACTOR SIGNED STATEMENT**

ALL CLEARING, GRADING, CONSTRUCTION OR DEVELOPMENT WILL BE DONE PURSUANT TO THE PLAN, SIGNED BY PARTIES BELOW:

OWNER: \_\_\_\_\_  
 DEVELOPER: \_\_\_\_\_  
 CONTRACTOR: \_\_\_\_\_

**OWNER INFORMATION**

OWNER:  
 TOMA DEVELOPERS LLC  
 EIDEN BUILDING - NORTH LOOP GATEWAY  
 201 WEST BROADWAY AVE.  
 MINNEAPOLIS, MN 55316  
 CONTACT: DONALD GERBERDING

**REMOVALS LEGEND:**

- 932 --- EX. 1' CONTOUR ELEVATION INTERVAL
- [Hatched Box] REMOVAL OF PAVEMENT AND ALL BASE MATERIAL, INCLUDING BIT., CONC., AND GRAVEL PAVTS.
- [Hatched Box] REMOVAL OF STRUCTURE INCLUDING ALL FOOTINGS AND FOUNDATIONS.
- [Hatched Box] REMOVAL OF UTILITY LINES - COORDINATE WITH UTILITY COMPANY PRIOR TO CONSTRUCTION
- [X Symbol] TREE REMOVAL - INCLUDING ROOTS AND STUMPS

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

*Matthew R. Pavlek*  
 Matthew R. Pavlek  
 DATE: 8/8/16 LICENSE NO.: 44263

**ISSUE/SUBMITTAL SUMMARY**

DATE	DESCRIPTION
9/9/16	LUU / PDR SUBMITTAL

**REVISION SUMMARY**

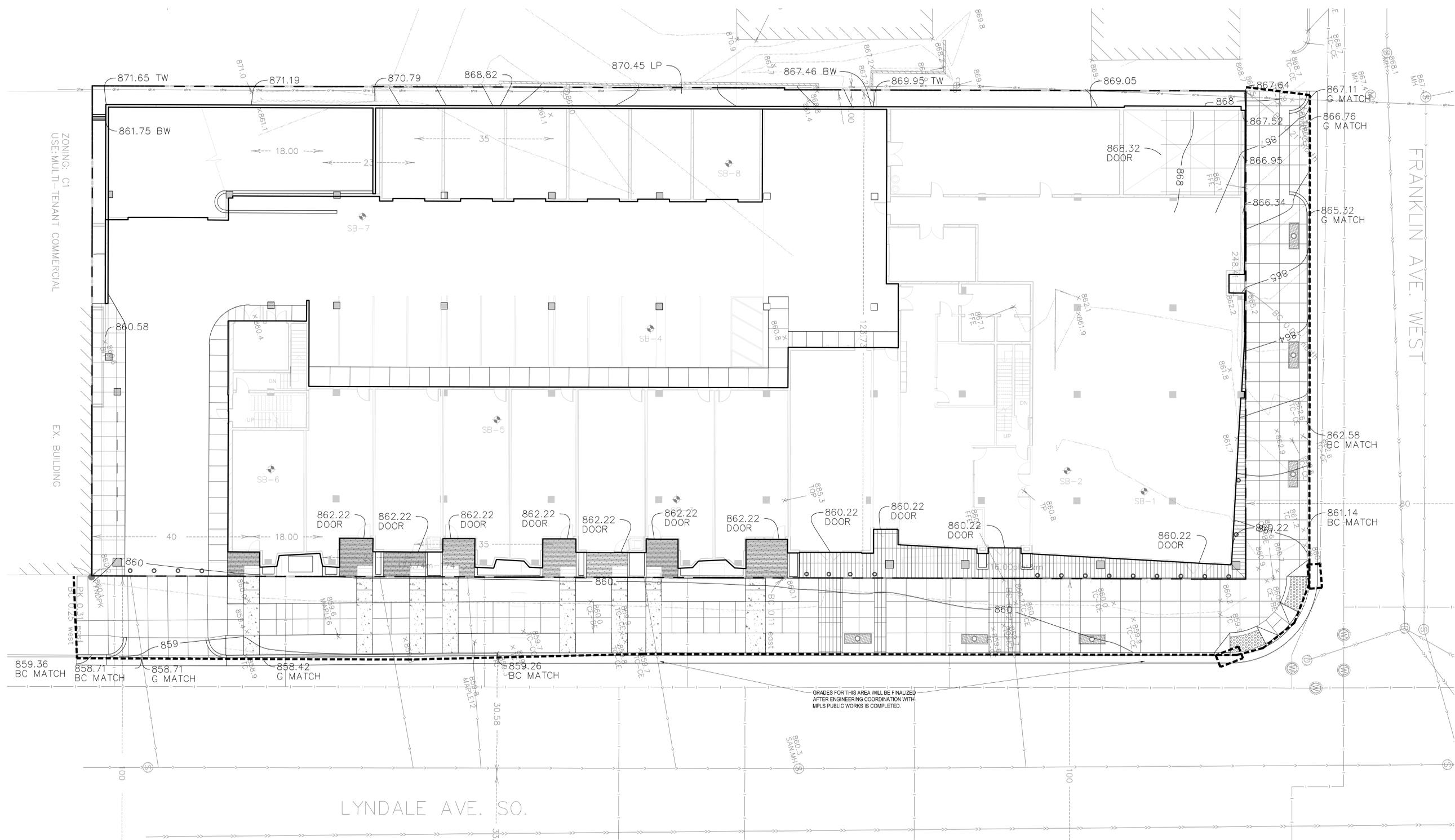
DATE	DESCRIPTION
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PROJECT NUMBER: 16055

**REMOVALS PLAN.**

**C1.0**





**GENERAL GRADING NOTES:**

- SEE SITE PLAN FOR HORIZONTAL LAYOUT & GENERAL GRADING NOTES.
- THE CONTRACTOR SHALL COMPLETE THE SITE GRADING CONSTRUCTION (INCLUDING BUT NOT LIMITED TO SITE PREPARATION, SOIL CORRECTION, EXCAVATION, EMBANKMENT, ETC.) IN ACCORDANCE WITH THE REQUIREMENTS OF THE OWNER'S SOILS ENGINEER. ALL SOIL TESTING SHALL BE COMPLETED BY THE OWNER'S SOILS ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL REQUIRED SOIL TESTS AND INSPECTIONS WITH THE SOILS ENGINEER.
- GRADING AND EXCAVATION ACTIVITIES SHALL BE PERFORMED IN ACCORDANCE WITH THE NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT REQUIREMENTS & PERMIT REQUIREMENTS OF THE CITY.
- PROPOSED SPOT GRADES ARE FLOW-LINE FINISHED GRADE ELEVATIONS, UNLESS OTHERWISE NOTED.
- GRADES OF WALKS SHALL BE INSTALLED WITH 5% MAX. LONGITUDINAL SLOPE AND 1% MIN. AND 2% MAX. CROSS SLOPE, UNLESS OTHERWISE NOTED.
- PROPOSED SLOPES SHALL NOT EXCEED 3:1 UNLESS INDICATED OTHERWISE ON THE DRAWINGS. MAXIMUM SLOPES IN MAINTAINED AREAS IS 4:1
- PROPOSED RETAINING WALLS, FREESTANDING WALLS, OR COMBINATION OF WALL TYPES GREATER THAN 4' IN HEIGHT SHALL BE DESIGNED AND ENGINEERED BY A REGISTERED RETAINING WALL ENGINEER. DESIGN DRAWINGS SHALL BE SUBMITTED FOR REVIEW AND APPROVAL PRIOR TO CONSTRUCTION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE OF GRADE STAKES THROUGHOUT THE DURATION OF CONSTRUCTION TO ESTABLISH PROPER GRADES. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR A FINAL FIELD CHECK OF FINISHED GRADES ACCEPTABLE TO THE ENGINEER/LANDSCAPE ARCHITECT PRIOR TO TOPSOIL AND SODDING ACTIVITIES.
- IF EXCESS OR SHORTAGE OF SOIL MATERIAL EXISTS, THE CONTRACTOR SHALL TRANSPORT ALL EXCESS SOIL MATERIAL OFF THE SITE TO AN AREA SELECTED BY THE CONTRACTOR, OR IMPORT SUITABLE MATERIAL TO THE SITE.
- EXCAVATE TOPSOIL FROM AREAS TO BE FURTHER EXCAVATED OR REGRADED AND STOCKPILE IN AREAS DESIGNATED ON THE SITE. THE CONTRACTOR SHALL SALVAGE ENOUGH TOPSOIL FOR RESPREADING ON THE SITE AS SPECIFIED. EXCESS TOPSOIL SHALL BE PLACED IN EMBANKMENT AREAS, OUTSIDE OF BUILDING PADS, ROADWAYS AND PARKING AREAS. THE CONTRACTOR SHALL SUBCUT CUT AREAS, WHERE TURF IS TO BE ESTABLISHED, TO A DEPTH OF 6 INCHES. RESPREAD TOPSOIL IN AREAS WHERE TURF IS TO BE ESTABLISHED TO A MINIMUM DEPTH OF 6 INCHES.
- FINISHED GRADING SHALL BE COMPLETED. THE CONTRACTOR SHALL UNIFORMLY GRADE AREAS WITHIN LIMITS OF

- GRADING, INCLUDING ADJACENT TRANSITION AREAS, PROVIDE A SMOOTH FINISHED SURFACE WITHIN SPECIFIED TOLERANCES, WITH UNIFORM LEVELS OR SLOPES BETWEEN POINTS WHERE ELEVATIONS ARE SHOWN, OR BETWEEN SUECH POINTS AND EXISTING GRADES. AREAS THAT HAVE BEEN FINISH GRADED SHALL BE PROTECTED FROM SUBSEQUENT CONSTRUCTION OPERATIONS, TRAFFIC AND EROSION. REPAIR ALL AREAS THAT HAVE BECOME RUTTED BY TRAFFIC OR ERODED BY WATER OR HAS SETTLED BELOW THE CORRECT GRADE. ALL AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED TO EQUAL OR BETTER THAN ORIGINAL CONDITION OR TO THE REQUIREMENTS OF THE NEW WORK.
- PRIOR TO PLACEMENT OF THE AGGREGATE BASE, A TEST ROLL WILL BE REQUIRED ON THE STREET AND/OR PARKING AREA SUBGRADE. THE CONTRACTOR SHALL PROVIDE A LOADED TANDEN AXLE TRUCK WITH A GROSS WEIGHT OF 25 TONS. THE TEST ROLLING SHALL BE AT THE DIRECTION OF THE SOILS ENGINEER AND SHALL BE COMPLETED IN AREAS AS DIRECTED BY THE SOILS ENGINEER. THE SOILS ENGINEER SHALL DETERMINE WHICH SECTIONS OF THE STREET OR PARKING AREA ARE UNSTABLE. CORRECTION OF THE SUBGRADE SOILS SHALL BE COMPLETED IN ACCORDANCE WITH THE REQUIREMENTS OF THE SOILS ENGINEER.
- TOLERANCES
  - THE BUILDING SUBGRADE FINISHED SURFACE ELEVATION SHALL NOT VARY BY MORE THAN 0.30 FOOT ABOVE, OR 0.30 FOOT BELOW, THE PRESCRIBED ELEVATION AT ANY POINT WHERE MEASUREMENT IS MADE.
  - THE STREET OR PARKING AREA SUBGRADE FINISHED SURFACE ELEVATION SHALL NOT VARY BY MORE THAN 0.05 FOOT ABOVE, OR 0.10 FOOT BELOW, THE PRESCRIBED ELEVATION OF ANY POINT WHERE MEASUREMENT IS MADE.
  - AREAS WHICH ARE TO RECEIVE TOPSOIL SHALL BE GRADED TO WITHIN 0.30 FOOT ABOVE OR BELOW THE REQUIRED ELEVATION, UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
  - TOPSOIL SHALL BE GRADED TO PLUS OR MINUS 1/2 INCH OF THE SPECIFIED THICKNESS.
- MAINTENANCE
  - THE CONTRACTOR SHALL PROTECT NEWLY GRADED AREAS FROM TRAFFIC AND EROSION, AND KEEP AREA FREE OF TRASH AND DEBRIS.
  - CONTRACTOR SHALL REPAIR AND REESTABLISH GRADES IN SETTLED, ERODED AND RUTTED AREAS TO SPECIFIED TOLERANCES, DURING THE CONSTRUCTION, IF REQUIRED, AND DURING THE WARRANTY PERIOD. ERODED AREAS WHERE TURF IS TO BE ESTABLISHED SHALL BE RESEEDDED AND MULCHED.
  - WHERE COMPLETED COMPACTED AREAS ARE DISTURBED BY SUBSEQUENT CONSTRUCTION OPERATIONS OR ADVERSE WEATHER, CONTRACTOR SHALL SCARIFY, SURFACE, RESHAPE, AND COMPACT TO REQUIRED DENSITY PRIOR TO FURTHER CONSTRUCTION.

**NON STORM WATER DISCHARGES:**

THERE ARE NO KNOWN NON-STORM WATER DISCHARGES ON THE EXISTING SITE AND NONE ARE PROPOSED AS PART OF THIS DEVELOPMENT.

**GROUNDWATER STATEMENT:**

THIS PROJECT DOES NOT PROPOSE ANY PERMANENT GROUNDWATER DISCHARGE TO THE STORM WATER SYSTEM.

**OWNER, DEVELOPER, CONTRACTOR SIGNED STATEMENT**

ALL CLEARING, GRADING, CONSTRUCTION OR DEVELOPMENT WILL BE DONE PURSUANT TO THE PLAN, SIGNED BY PARTIES BELOW:

OWNER: \_\_\_\_\_  
DEVELOPER: \_\_\_\_\_  
CONTRACTOR: \_\_\_\_\_

**CITY OF MINNEAPOLIS GRADING NOTES:**

- RESERVED FOR CITY SPECIFIC GRADING NOTES.

**EROSION CONTROL NOTES:**

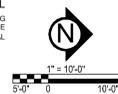
SEE SWPPP ON SHEETS SW1.0-SW1.2

**GROUNDWATER INFORMATION:**

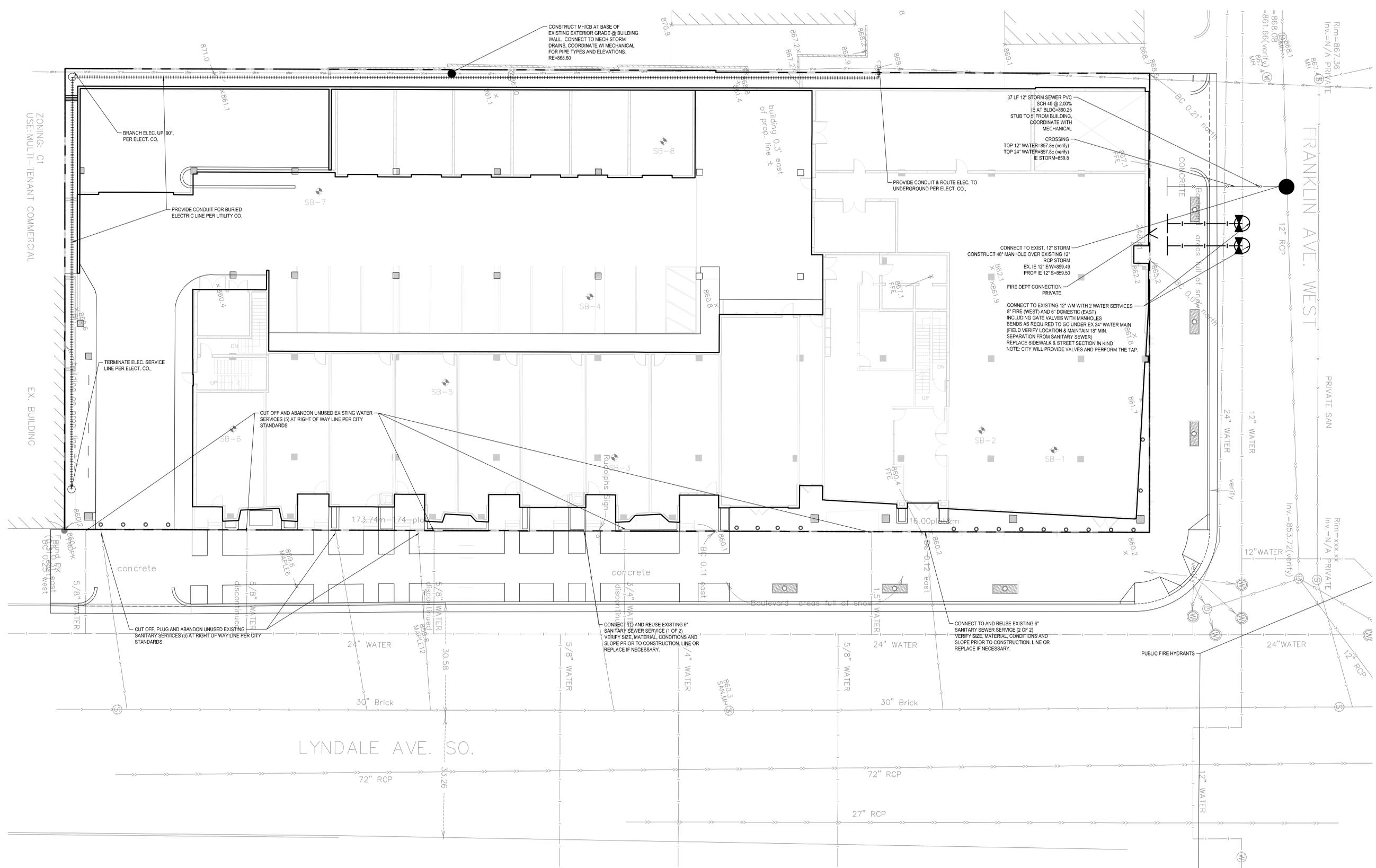
PER GEOTECHNICAL REPORT/MEMORANDUM BY NORTHERN TECHNOLOGIES, LLC., DATED 06/15/2016 GROUNDWATER WAS OBSERVED AT APPROX. 13.9 AND 13.4 FEET BELOW SURFACE ELEVATION (ELEVATIONS OF 846.1 AND 845.6 RESPECTIVELY).

**GRADING PLAN LEGEND:**

- 891 --- EX. 1' CONTOUR ELEVATION INTERVAL
- 819 --- 1/2" CONTOUR ELEVATION INTERVAL
- 891.0 --- SPOT GRADE ELEVATION (FLOW LINE UNLESS OTHERWISE NOTED)
- 891.0 G --- SPOT GRADE ELEVATION GUTTER
- 891.0 BC --- SPOT GRADE ELEVATION BACK OF CURB (TOP OF CURB)
- 891.0 BS/TS --- SPOT GRADE ELEVATION BOTTOM OF STAIRS/TOP OF STAIRS
- TIP OUT (T.O.) CURB AND GUTTER WHERE APPLICABLE - TAPER GUTTERS TO DRAIN AS SHOWN
- EXISTING AND PROPOSED DRAINAGE ARROWS



PROJECT: \_\_\_\_\_  
I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.  
Matthew R. Pavik  
DATE: 8/8/16 LICENSE NO.: 44263  
ISSUE/SUBMITTAL SUMMARY  
DATE DESCRIPTION  
9/9/16 LUA / PDR SUBMITTAL  
REVISION SUMMARY  
DATE DESCRIPTION  
PROJECT NUMBER: 16005  
**GRADING PLAN**  
**C3.0**  
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- GENERAL UTILITY NOTES:**
- SEE SITE PLAN FOR HORIZONTAL DIMENSIONS AND LAYOUT.
  - CONTRACTOR SHALL FIELD VERIFY LOCATION AND ELEVATION OF EXISTING UTILITIES AND TOPOGRAPHIC FEATURES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER OF DISCREPANCIES OR VARIATIONS FROM THE PLANS.
  - ALL EXISTING UTILITY LOCATIONS SHOWN ARE APPROXIMATE. CONTACT "GOPHER STATE ONE CALL" (851-454-0002 OR 800-252-1166) FOR UTILITY LOCATIONS, 48 HOURS PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY UTILITIES THAT ARE DAMAGED DURING CONSTRUCTION AT NO COST TO THE OWNER.
  - UTILITY INSTALLATION SHALL CONFORM TO THE CURRENT EDITION OF "STANDARD SPECIFICATIONS FOR WATER MAIN AND SERVICE LINE INSTALLATION" AND "SANITARY SEWER AND STORM SEWER INSTALLATION" AS PREPARED BY THE CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM), AND SHALL CONFORM WITH THE REQUIREMENTS OF THE CITY AND THE PROJECT SPECIFICATIONS.
  - CASTINGS SHALL BE SALVAGED FROM STRUCTURE REMOVALS AND RE-USED OR PLACED AT THE DIRECTION OF THE OWNER.
  - ALL WATER PIPE SHALL BE CLASS S2 DUCTILE IRON PIPE (DIP) UNLESS OTHERWISE NOTED.
  - ALL SANITARY SEWER SHALL BE SDR 26 POLYVINYL CHLORIDE (PVC) UNLESS OTHERWISE NOTED.
  - ALL STORM SEWER PIPE SHALL BE HDPE UNLESS OTHERWISE NOTED.
  - PIPE LENGTHS SHOWN ARE FROM CENTER TO CENTER OF STRUCTURE OR TO END OF FLARED END SECTION.
  - UTILITIES ON THE PLAN ARE SHOWN TO WITHIN 5' OF THE BUILDING FOOTPRINT. THE CONTRACTOR IS ULTIMATELY RESPONSIBLE FOR THE FINAL CONNECTION TO BUILDING LINES. COORDINATE WITH

- ARCHITECTURAL AND MECHANICAL PLANS.**
- CATCH BASINS AND MANHOLES IN PAVED AREAS SHALL BE SUMPED 0.04 FEET. ALL CATCH BASINS IN GUTTERS SHALL BE SUMPED 0.15 FEET PER DETAILS. RIM ELEVATIONS SHOWN ON THIS PLAN DO NOT REFLECT SUMPED ELEVATIONS.
  - ALL FIRE HYDRANTS SHALL BE LOCATED 5 FEET BEHIND BACK OF CURB UNLESS OTHERWISE NOTED.
  - HYDRANT TYPE, VALVE, AND CONNECTION SHALL BE IN ACCORDANCE WITH CITY REQUIREMENTS. HYDRANT EXTENSIONS ARE INCIDENTAL.
  - A MINIMUM OF 8 FEET OF COVER IS REQUIRED OVER ALL WATERMAIN, UNLESS OTHERWISE NOTED. EXTRA DEPTH MAY BE REQUIRED TO MAINTAIN A MINIMUM OF 18" VERTICAL SEPARATION TO SANITARY OR STORM SEWER LINES. EXTRA DEPTH WATERMAIN IS INCIDENTAL.
  - A MINIMUM OF 18 INCHES OF VERTICAL SEPARATION AND 10 FEET OF HORIZONTAL SEPARATION IS REQUIRED FOR ALL UTILITIES, UNLESS OTHERWISE NOTED.
  - ALL CONNECTIONS TO EXISTING UTILITIES SHALL BE IN ACCORDANCE WITH CITY STANDARDS AND COORDINATED WITH THE CITY PRIOR TO CONSTRUCTION.
  - CONNECTIONS TO EXISTING STRUCTURES SHALL BE CORE-DRILLED.
  - COORDINATE LOCATIONS AND SIZES OF SERVICE CONNECTIONS WITH THE MECHANICAL DRAWINGS.
  - COORDINATE INSTALLATION AND SCHEDULING OF THE INSTALLATION OF UTILITIES WITH ADJACENT CONTRACTORS AND CITY STAFF.
  - ALL STREET REPAIRS AND PATCHING SHALL BE PERFORMED PER THE REQUIREMENTS OF THE CITY. ALL PAVEMENT CONNECTIONS SHALL BE SAWCUT. ALL TRAFFIC CONTROLS SHALL BE PROVIDED BY THE

- CONTRACTOR AND SHALL BE ESTABLISHED PER THE REQUIREMENTS OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MMUTCD) AND THE CITY. THIS SHALL INCLUDE BUT NOT BE LIMITED TO SIGNAGE, BARRICADES, FLASHERS, AND FLAGGERS AS NEEDED. ALL PUBLIC STREETS SHALL BE OPEN TO TRAFFIC AT ALL TIMES. NO ROAD CLOSURES SHALL BE PERMITTED WITHOUT APPROVAL BY THE CITY.**
- ALL STRUCTURES, PUBLIC AND PRIVATE, SHALL BE ADJUSTED TO PROPOSED GRADES WHERE REQUIRED. THE REQUIREMENTS OF ALL OWNERS MUST BE COMPLIED WITH. STRUCTURES BEING RESET TO PAVED AREAS MUST MEET OWNERS REQUIREMENTS FOR TRAFFIC LOADING.
  - CONTRACTOR SHALL COORDINATE ALL WORK WITH PRIVATE UTILITY COMPANIES.
  - CONTRACTOR SHALL COORDINATE CONNECTION OF IRRIGATION SERVICE TO UTILITIES. COORDINATE THE INSTALLATION OF IRRIGATION SLEEVES NECESSARY AS TO NOT IMPACT INSTALLATION OF UTILITIES.
  - CONTRACTOR SHALL MAINTAIN AS-BUILT PLANS THROUGHOUT CONSTRUCTION AND SUBMIT THESE PLANS TO ENGINEER UPON COMPLETION OF WORK.
  - ALL JOINTS AND CONNECTIONS IN STORM SEWER SYSTEM SHALL BE GASTIGHT OR WATERTIGHT. APPROVED RESILIENT RUBBER JOINTS MUST BE USED TO MAKE WATERTIGHT CONNECTIONS TO MANHOLES, CATCHBASINS, OR OTHER STRUCTURES.

**UTILITY LEGEND:**

- MANHOLE OR CATCH BASIN
- MANHOLE OR CATCH BASIN
- WATER MAIN
- SANITARY SEWER
- STORM SEWER

**CITY OF MINNEAPOLIS UTILITY NOTES:**

- RESERVED FOR CITY SPECIFIC UTILITY NOTES.

GOPHER STATE ONE CALL  
WWW.GOPHERSTATEONECALL.ORG  
(800) 252-1166 TOLL FREE  
(612) 454-0002 LOCAL

1" = 10'-0"  
5'-0" 0 10'-0"



**THEATER GARAGE**  
LYNDALE AVENUE,  
MINNEAPOLIS



**MATERIALS INDEX**

- 1 FACE BRICK - NORMAN BRICK, COLOR TBD
- 2 PREFINISHED METAL PANEL TYPE 1 - COLOR TBD
- 3 PREFINISHED METAL PANEL TYPE 2 - COLOR TBD
- 4 PREFINISHED METAL PANEL TYPE 3 - COLOR TBD
- 5 SPECIAL METAL WALL PANEL TYPE 4 - COLOR TBD
- 6 STONE PANEL - STOREFRONT BASE
- 7 PRECAST WALL PANEL - COLOR AND TEXTURE TBD
- 8 WRAPPED PRECAST CONCRETE PIER
- 9 PREFINISHED METAL CORNICE/CAP FLASHING - COLOR TO MATCH
- 10 METAL GROWING "MESH"
- 11 PAINTED STEEL SIGNAGE BAND - COLOR TBD
- 12 PREFINISHED METAL MESH "FIN" - COLOR TBD
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- 14 FROSTED GLASS RAILING - COLOR TBD
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- 16 METAL GUARDRAIL - COLOR TBD
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- 21 PREFAB ALUMINIUM BALCONIES
- 22 ARCHITECTURAL LIGHT SCONCE
- 23 STEEL TRELIS
- 24 FIBERGLASS PATIO DOORS



**2 NORTH ELEVATION**  
3/32" = 1'-0"



**1 EAST ELEVATION**  
3/32" = 1'-0"

DATE:  
I HEREBY CERTIFY THAT THIS PLAN SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.

PETER KEELY  
REGISTRATION NO: 23570

Theater Garage Marquee Apartments  
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Minneapolis, MN 55403

Collage | architects  
Architects  
Pete Keely  
651.472.0050  
705 Raymond Avenue, Suite #200  
St. Paul, Minnesota 55114

Ericksen Roeds & Associates  
651.251.7570  
2550 University Avenue West, Suite #201  
St. Paul, Minnesota 55114

CivISITE Group  
Patrick Sarver  
952-250-2003  
4931 W. 35th Street, Suite #200  
St. Louis Park, Minnesota 55416

DATE: 09.02.2016

**PDR/LUA 9/2/2016**

REVISIONS:		
NO.	DATE	DESCRIPTION

PROJECT NO: 14\_001  
DRAWN BY: Author  
CHK'D BY: Checker

SHEET TITLE

**ELEVATIONS**

**A200**



**THEATER GARAGE**  
LYNDALE AVENUE,  
MINNEAPOLIS



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SHEET TITLE

**ELEVATIONS**

**A201**

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**2 SOUTH ELEVATION**  
3/32" = 1'-0"



**1 WEST ELEVATION**  
3/32" = 1'-0"



**THEATER GARAGE**  
LYNDALE AVENUE,  
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REVISIONS:

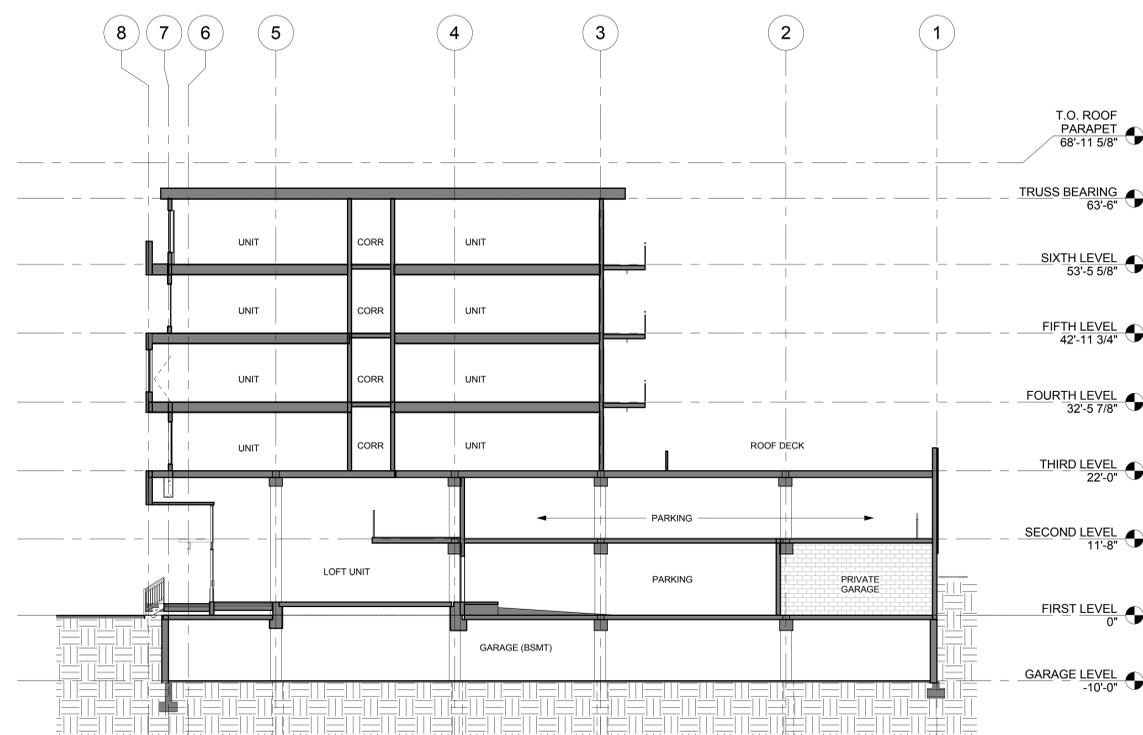
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CHKD BY: Checker

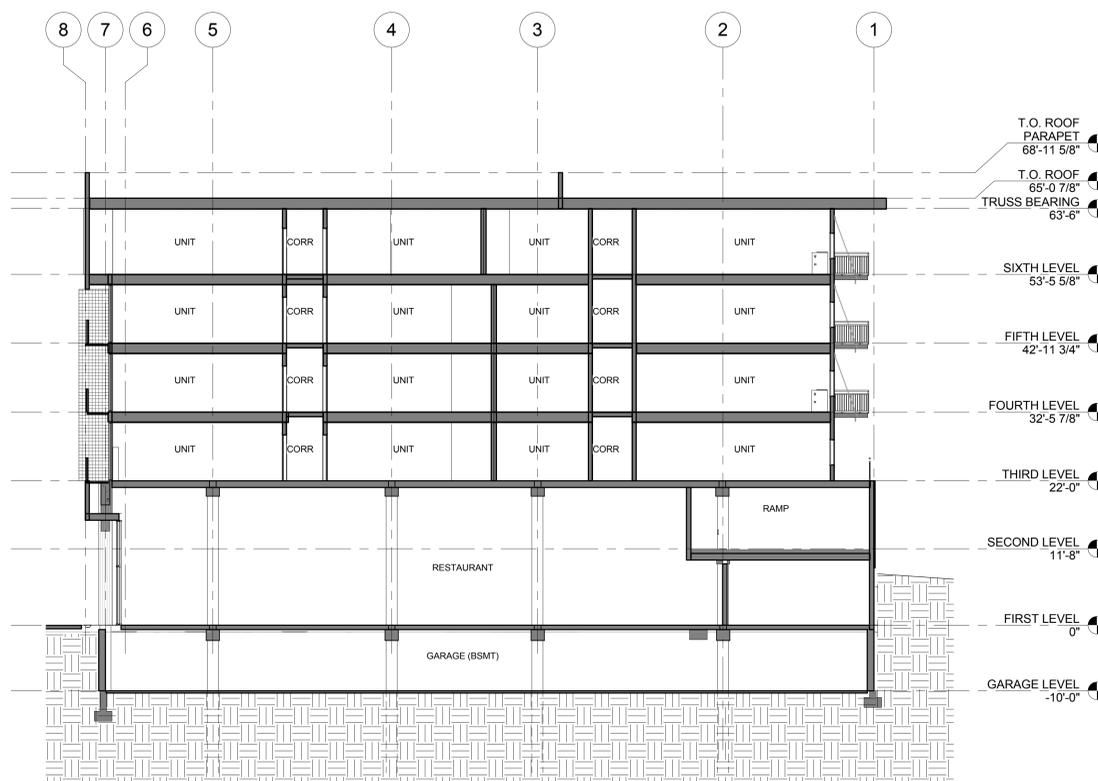
SHEET TITLE

**BUILDING SECTIONS**

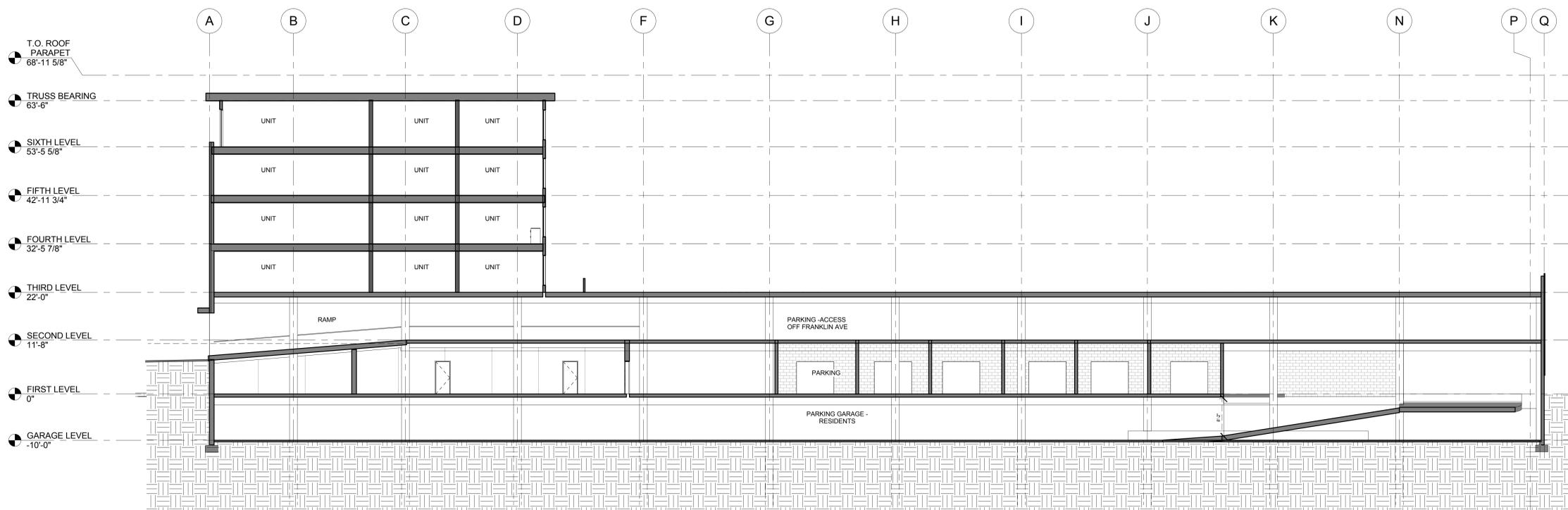
**A300**



**3 SECTION -LOFT UNITS**  
3/32" = 1'-0"



**2 BUILDING SECTION-TRANSVERSE RESTAURANT**  
3/32" = 1'-0"



**1 SECTION-LONGITUDINAL RAMP**  
3/32" = 1'-0"



**THEATER GARAGE**  
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**PDR/LUA 9/2/2016**

REVISIONS:

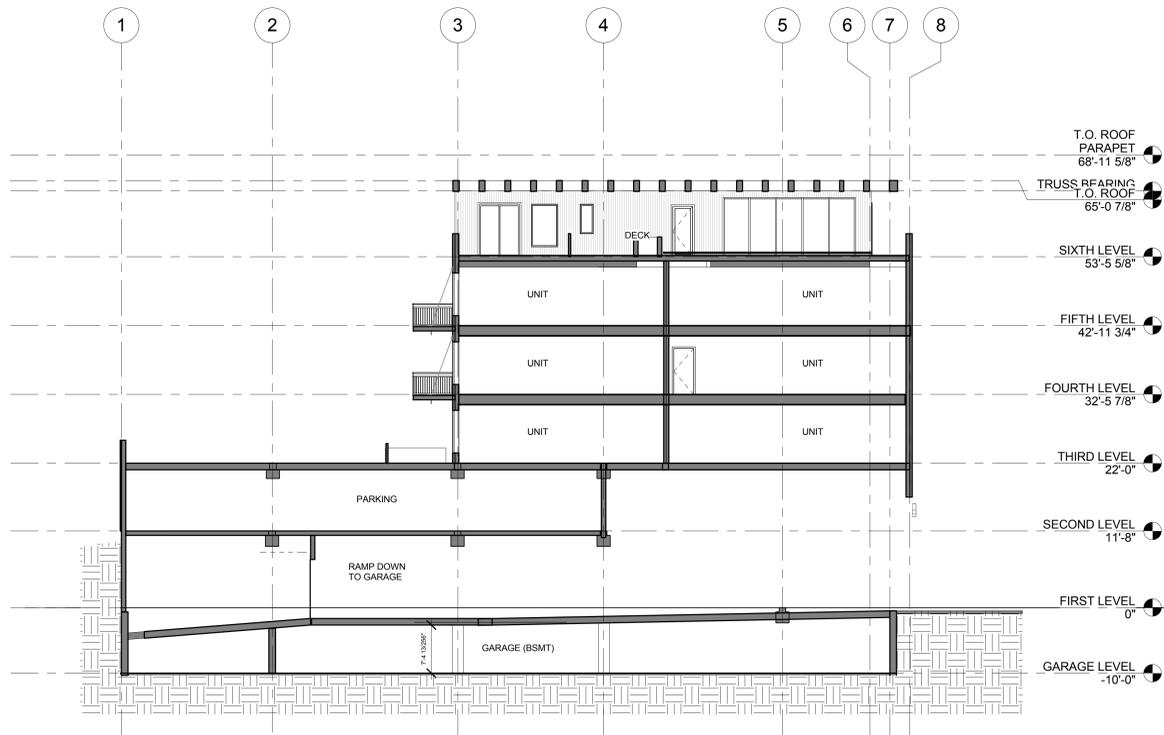
NO.	DATE	DESCRIPTION

PROJECT NO: 14\_001  
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CHKD BY: Checker

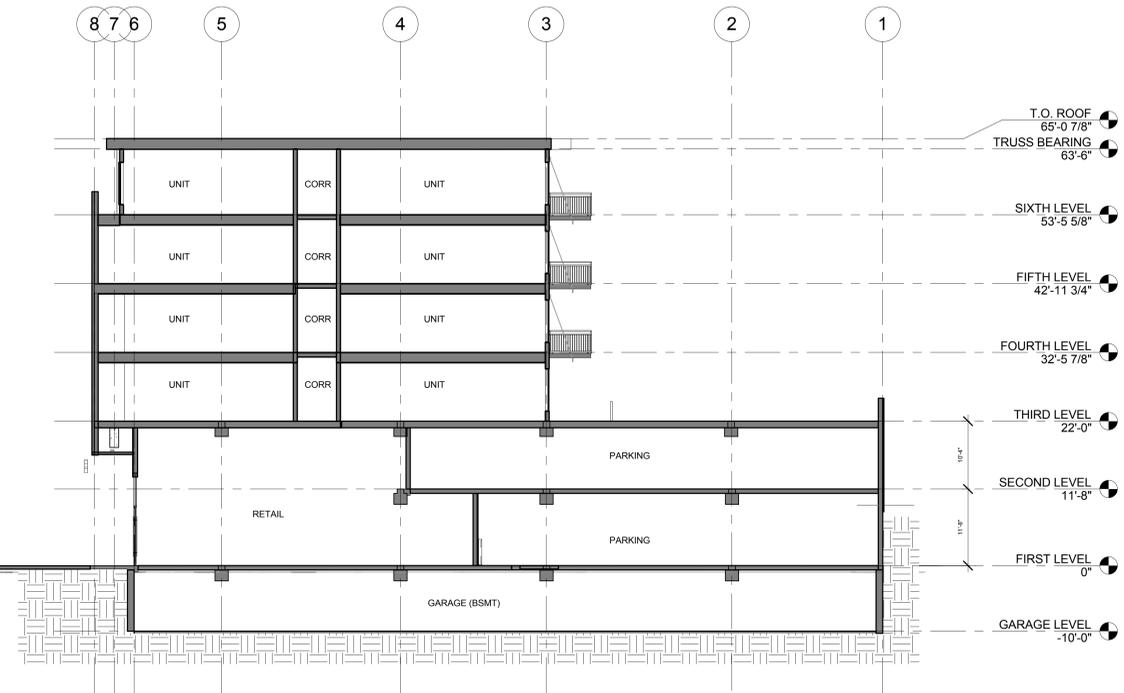
SHEET TITLE

**BUILDING SECTIONS**

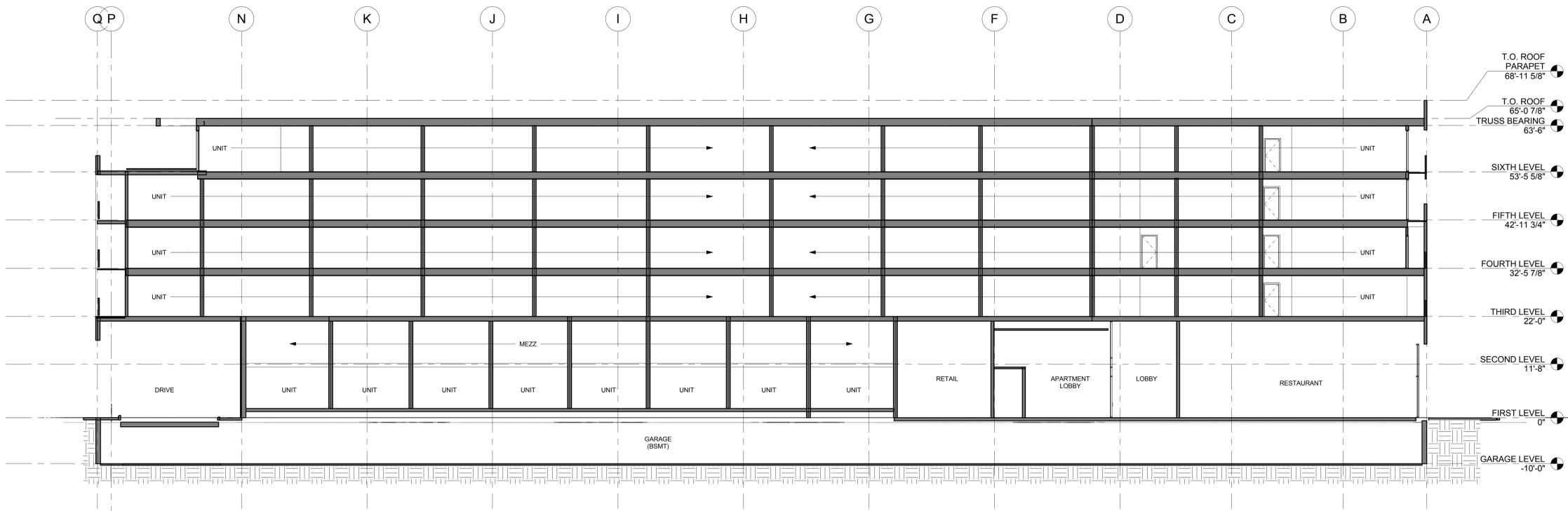
**A303**



**3 SECTION- ACCESS DRIVE**  
3/32" = 1'-0"



**2 SECTION THRU RETAIL**  
3/32" = 1'-0"



**1 SECTION -LONGITUDINAL AT UNITS**  
3/32" = 1'-0"



**1 SOUTH ELEVATION**  
3/32" = 1'-0"

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**MATERIAL CALCULATION - WEST**

ORIENTATION	MATERIAL NAME	MATERIAL AREA	PERCENT
WEST	EXTERIOR - BRICK	1616 SF	11.6%
WEST	EXTERIOR - GLASS	3976 SF	28.6%
WEST	EXTERIOR - METAL PANEL TYPE 1	3384 SF	24.3%
WEST	EXTERIOR - METAL PANEL TYPE 3	1898 SF	13.6%
WEST	EXTERIOR - PRECAST WITH METAL GROWING MESH	3048 SF	21.9%
WEST		13,922 SF	

**MATERIAL CALCULATION - SOUTH**

ORIENTATION	MATERIAL NAME	MATERIAL AREA	PERCENT
SOUTH	EXTERIOR - BRICK	2261 SF	27.7%
SOUTH	EXTERIOR - GLASS	910 SF	11.2%
SOUTH	EXTERIOR - METAL PANEL TYPE 1	3028 SF	37.1%
SOUTH	EXTERIOR - METAL PANEL TYPE 2	297 SF	3.6%
SOUTH	EXTERIOR - PRECAST	580 SF	7.1%
SOUTH	EXTERIOR - METAL GROWING MESH	1081 SF	13.3%
SOUTH		8,155 SF	



**2 WEST ELEVATION**  
3/32" = 1'-0"



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**THEATER GARAGE**  
LYNDALE AVENUE,  
MINNEAPOLIS



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St. Louis Park, Minnesota 55416

DATE: 09.02.2016

**LAND USE SUBMISSION SET**

REVISIONS:

NO.	DATE	DESCRIPTION

PROJECT NO: 14\_001  
DRAWN BY: BGAO  
CHKD BY: Checker

SHEET TITLE

**COLORED ELEVATIONS**

**A201-A**



**THEATER GARAGE**  
LYNDALE AVENUE,  
MINNEAPOLIS



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**PDR/LUA 9/2/2016**

REVISIONS:		
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SHEET TITLE

**COLORED ELEVATIONS**

**A200-A**

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**MATERIAL CALCULATION - EAST**

ORIENTATION	MATERIAL NAME	MATERIAL AREA	PERCENT
EAST	EXTERIOR - BRICK	6426 SF	36.1%
EAST	EXTERIOR - GLASS	5734 SF	32.2%
EAST	EXTERIOR - METAL PANEL TYPE 1	3210 SF	18%
EAST	EXTERIOR - METAL PANEL TYPE 2	310 SF	1.7%
EAST	EXTERIOR - METAL PANEL TYPE 4	2134 SF	12%
EAST		17,814 SF	

**MATERIAL CALCULATION - NORTH**

ORIENTATION	MATERIAL NAME	MATERIAL AREA	PERCENT
NORTH	EXTERIOR - BRICK	1486 SF	24.3%
NORTH	EXTERIOR - GLASS	2354 SF	38.2%
NORTH	EXTERIOR - METAL PANEL TYPE 1	376 SF	6.1%
NORTH	EXTERIOR - METAL PANEL TYPE 4	1937 SF	31.4%
NORTH		6153 SF	

**BUILDING SIGNAGE (INTERNALLY AND EXTERNALLY LIT WALL SIGNS)**

EAST ELEVATION	225 SF (266 X 1.5 = 429 SF ALLOWED)
NORTH ELEVATION	90 SF (118 X 1.5 = 177 SF ALLOWED)



**2 NORTH ELEVATION**  
3/32" = 1'-0"



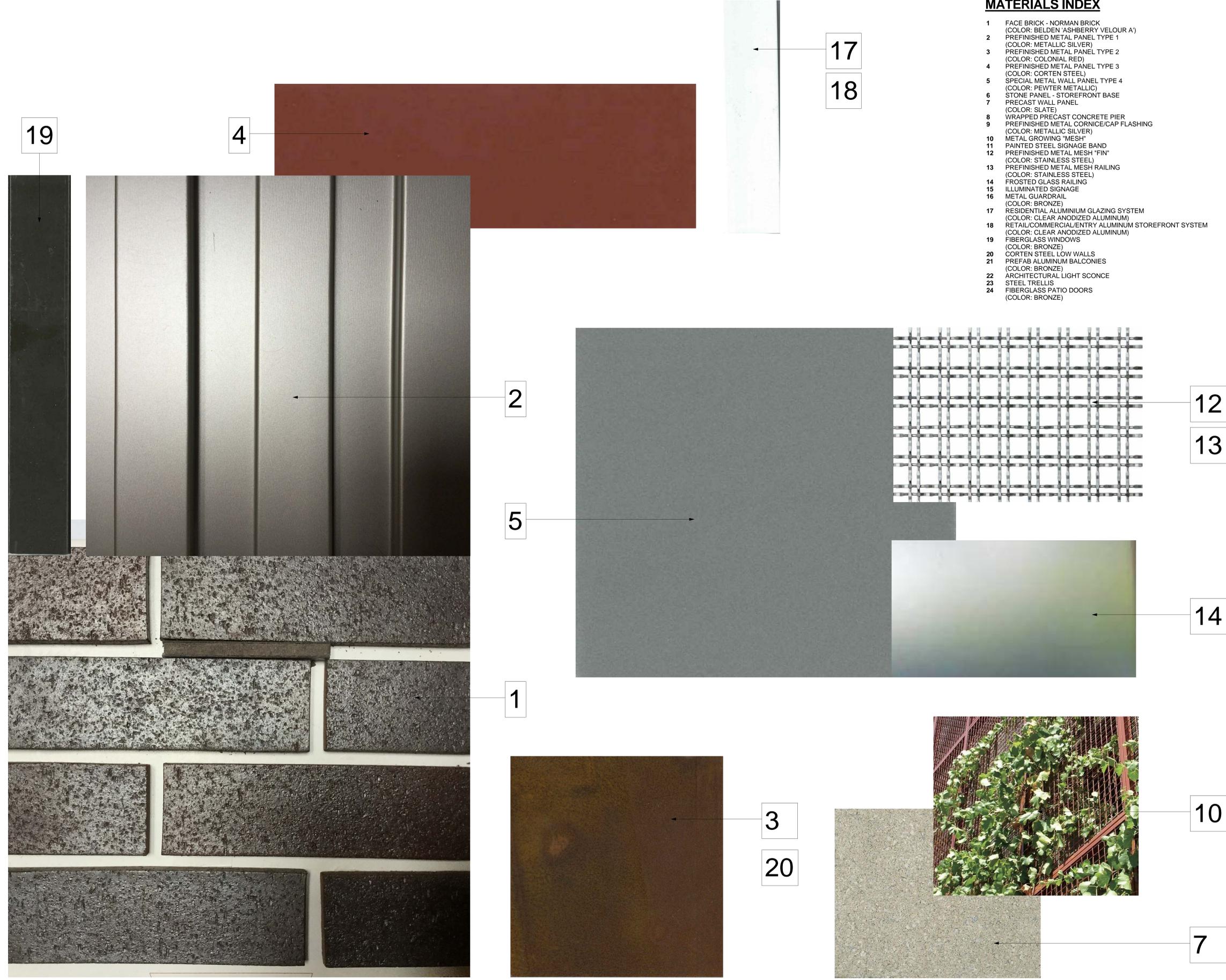
**1 EAST ELEVATION**  
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**THEATER GARAGE**  
LYNDALE AVENUE,  
MINNEAPOLIS

**MATERIALS INDEX**

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- 2 PREFINISHED METAL PANEL TYPE 1 (COLOR: METALLIC SILVER)
- 3 PREFINISHED METAL PANEL TYPE 2 (COLOR: COLONIAL RED)
- 4 PREFINISHED METAL PANEL TYPE 3 (COLOR: CORTEN STEEL)
- 5 SPECIAL METAL WALL PANEL TYPE 4 (COLOR: PEWTER METALLIC)
- 6 STONE PANEL - STOREFRONT BASE
- 7 PRECAST WALL PANEL (COLOR: SLATE)
- 8 WRAPPED PRECAST CONCRETE PIER
- 9 PREFINISHED METAL CORNICE/CAP FLASHING (COLOR: METALLIC SILVER)
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- 11 PAINTED STEEL SIGNAGE BAND
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- 13 PREFINISHED METAL MESH RAILING (COLOR: STAINLESS STEEL)
- 14 FROSTED GLASS RAILING
- 15 ILLUMINATED SIGNAGE
- 16 METAL GUARDRAIL (COLOR: BRONZE)
- 17 RESIDENTIAL ALUMINIUM GLAZING SYSTEM (COLOR: CLEAR ANODIZED ALUMINUM)
- 18 RETAIL/COMMERCIAL/ENTRY ALUMINIUM STOREFRONT SYSTEM (COLOR: CLEAR ANODIZED ALUMINUM)
- 19 FIBERGLASS WINDOWS (COLOR: BRONZE)
- 20 CORTEN STEEL LOW WALLS
- 21 PREFAB ALUMINUM BALCONIES (COLOR: BRONZE)
- 22 ARCHITECTURAL LIGHT SCONCE
- 23 STEEL TRELIS
- 24 FIBERGLASS PATIO DOORS (COLOR: BRONZE)



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AERIAL VIEW

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PERSPECTIVE

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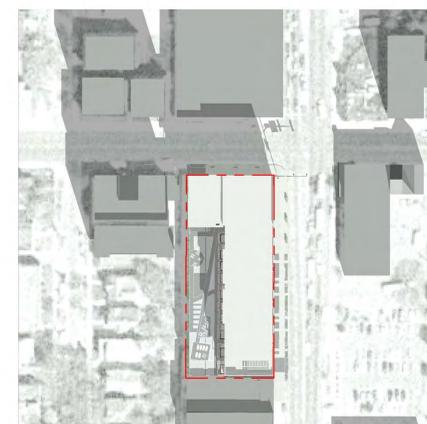
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**SHADOW STUDIES**

**AS100**



WINTER SOLSTICE - 4PM



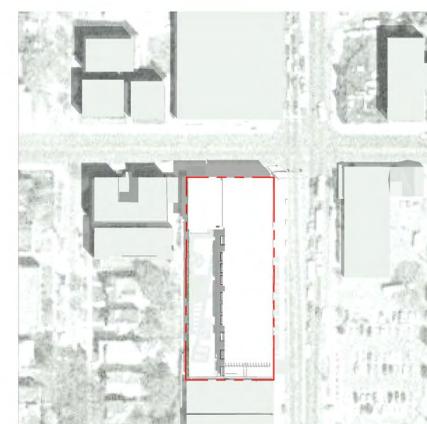
WINTER SOLSTICE - 12PM



WINTER SOLSTICE - 10AM



SUMMER SOLSTICE - 4PM



SUMMER SOLSTICE - 12PM



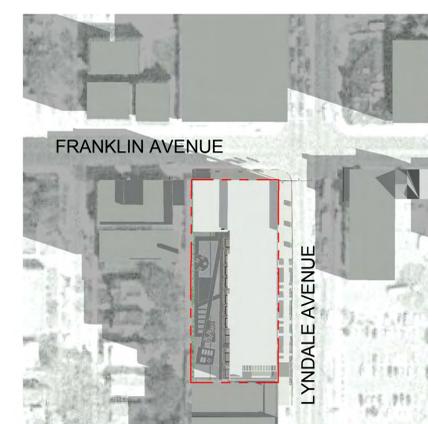
SUMMER SOLSTICE - 9AM



SPRING EQUINOX - 4PM



SPRING EQUINOX - 12PM



SPRING EQUINOX - 9AM



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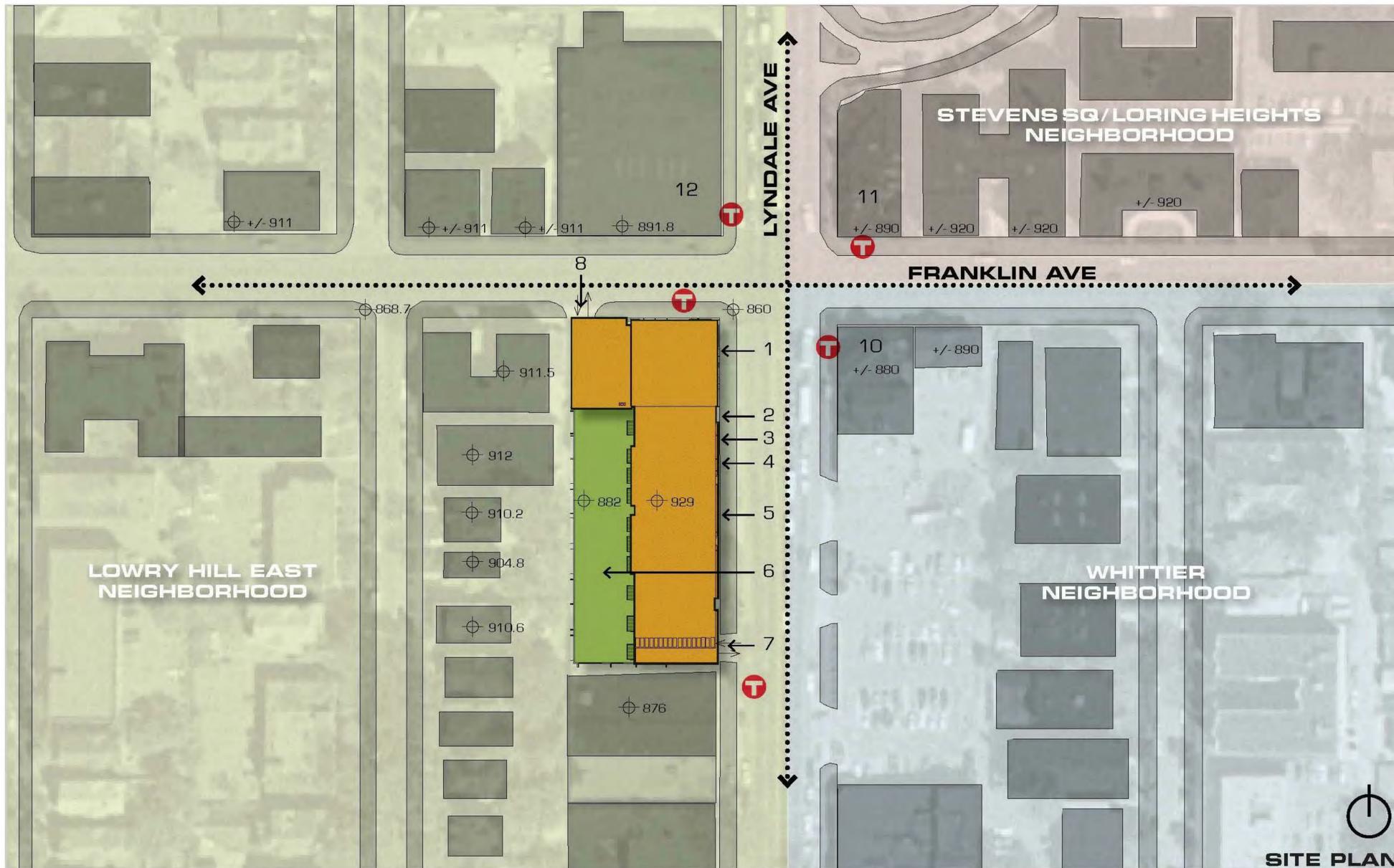
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**CONTEXT SITE PLAN**

**A000**



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2. RESIDENTIAL ENTRY
3. APT. LOBBY
4. RETAIL
5. WALK-UP UNITS
6. ROOFTOP GREEN SPACE
7. PARKING ENTRANCE
8. MID-LEVEL PARKING
9. THE WEDGE
10. MOTRIMER'S
11. RUDOLPH'S
12. VISION LOSS CENTER



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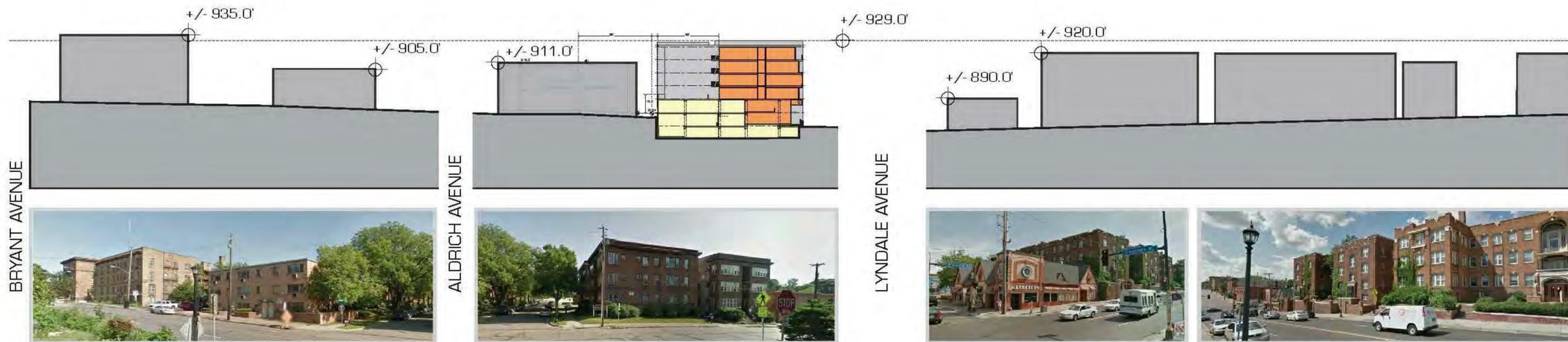
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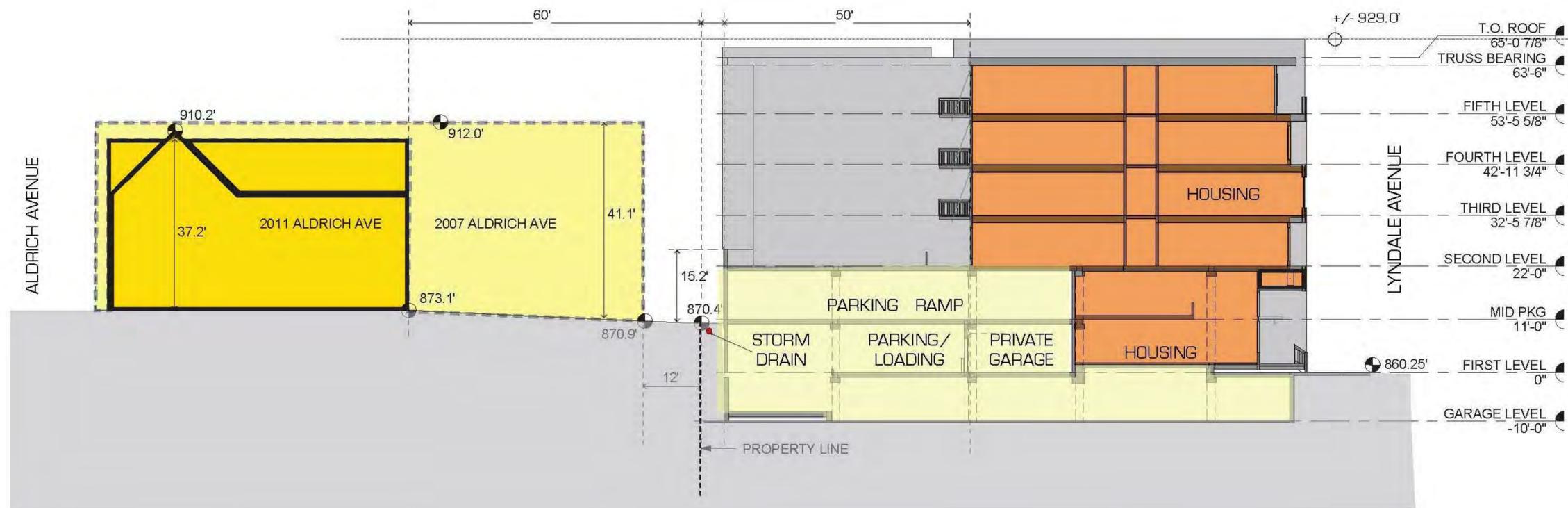
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**SITE SECTION**

**A301**



**FRANKLIN AVENUE COMPARATIVE SECTION**



**SITE SECTION**

Photos of Existing Site:



Image 1: 2004 Lyndale Avenue S, Minneapolis, MN (view from Franklin Avenue facing SE)

Image 2: 2004 Lyndale Avenue S, Minneapolis, MN (view from Franklin Avenue facing SW)



Image 3: 2004 Lyndale Avenue S, Minneapolis, MN (view from intersection of Franklin and Lyndale Avenues, facing SW)



Image 4: 2004 Lyndale Avenue South, Minneapolis, MN (view from the south across parking lot)



Image 5: 2018 Lyndale Avenue South, Minneapolis, MN (view from parking lot, looking SW)



Image 6: 2018 Lyndale Avenue South, Minneapolis, MN (view from parking lot, looking south)

# TRAVEL DEMAND MANAGEMENT PLAN

*for the*

## THEATER GARAGE MARQUEE APARTMENTS



REAL ESTATE ■ DEVELOPMENT ■ CONSTRUCTION



September 5, 2016

**TRAVEL MANAGEMENT PLAN**  
*for the*  
**THEATER GARAGE MARQUEE APARTMENTS**

**Prepared for:**

**TGMA Developers, LLC  
201 West Broadway Avenue  
Minneapolis, MN 55411**

**Prepared by:**

**Biko Associates, Inc.  
79 - 13th Avenue Northeast  
Studio 107  
Minneapolis, MN 55413**

September 5, 2016

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# INTRODUCTION

## PURPOSE

This report presents a Travel Demand Management Plan (TDMP) for the Theater Garage Marquee Apartments, a mixed-use residential and commercial development proposed to be constructed in the southwest quadrant of the Lyndale Avenue/Franklin Avenue intersection in south Minneapolis. The project proposer is Theater Garage Marquee Apartments (TGMA) Developers, LLC.

TDMPs are an element of the City of Minneapolis' development review process. TDMPs address the following issues.

1. Potential traffic and parking impacts that may result from project implementation,
2. Opportunities for the development's end-users to access alternative transportation modes, such as transit, bicycle and pedestrian facilities, and
3. TDMP action steps the development's owners and/or property managers will follow to ensure that the completed project will contribute to the city's transportation goals (refer to Chapter 8 (Movement) of the *Minneapolis Plan*). The goals are that for a minimum of three days of the week, a maximum of 50 percent of the residents will drive alone, 35 percent will use transit, and 15 percent will bike or walk.

## REGIONAL AND SITE LOCATIONS

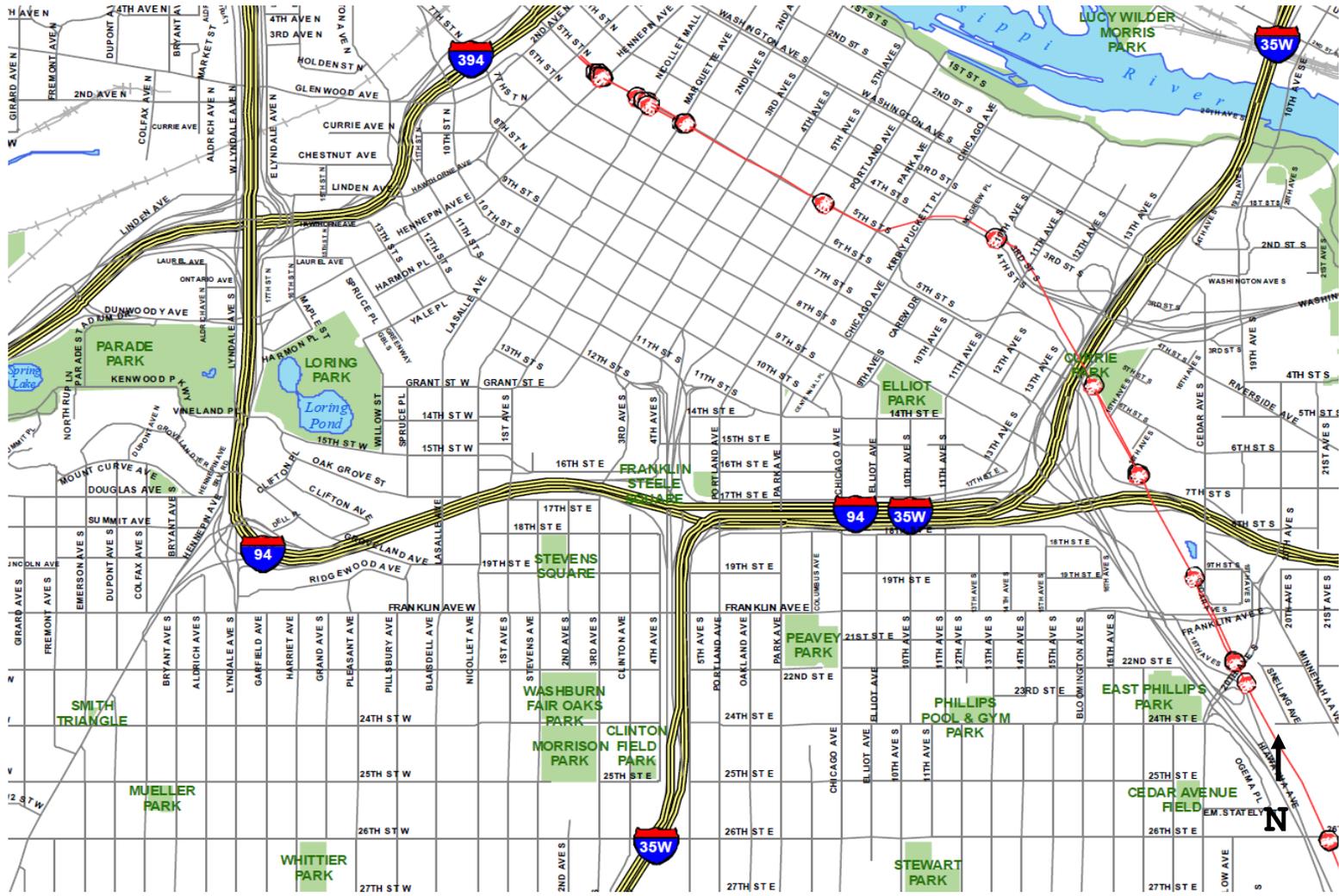
### ***Regional Location:***

The Theater Garage Marquee Apartments (TGMA) development site is located in south Minneapolis on the southwest corner of the intersection formed by Franklin and Lyndale Avenues. The development site is within an area known as the Wedge, and the Lyndale/Franklin intersection is built out as a 100 percent commercial corner.

The proposed site is located to capture both local and regional markets. Sub-regional and regional roads serving the site include:

- Hennepin, Lyndale, and Franklin Avenues are three Hennepin County roads with direct connections to the interstate highway system. They also provide cross-Minneapolis transportation service.
- I-94 and I-35W are principal arterials, with ramps north of the development site on Lyndale Avenue and east of the site on Franklin Avenue. I-94 provides access to Saint Paul and I-394 and I-694 two facilities that provide transportation service to the Twin Cities' western and northwestern suburbs. I-35W links the northern and southern suburbs to downtown Minneapolis
- I-394 is an east/west Principal Arterial that provides access between downtown Minneapolis and the western suburbs.

Figure 1 shows the location of the site relative to the roadways described above and downtown Minneapolis.



**Theater Garage Marquee Apartments  
Travel Demand Management Plan**

**Figure 1  
Regional Location**

**TGMA  
Developers LLC**



**Site Location Zoning, Parking, Transit, and Bicycle Services :**

The TGMA development site is within a split zone, where the northern parcel of the site is zoned C2 for uses within a neighborhood corridor commercial district, and the southern parcels are zoned C1 for neighborhood commercial uses. The developer of the project is proposing to rezone the C1 parcels to C2. No overlay zoning districts were identified for the development site.

Existing uses at the Franklin/Lyndale corner include:

- Rudolph’s Barbeque on the northeast corner
- Vision Loss Resources on the northwest corner
- Theater Garage and Steeple People Surplus Store on the southwest corner
- Mortimer’s Bar and Restaurant on the southeast corner

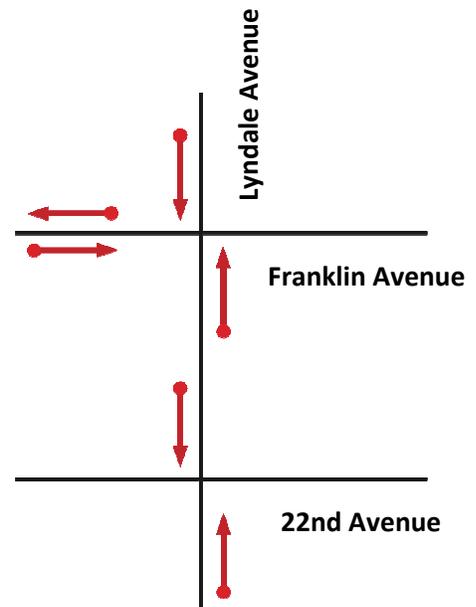
With the exception of Steeple People, these businesses largely attract a customer base that is both local and regional. Steeple People’s customer base is predominantly local.

**Parking within the Study Area:**

On-street parking is not permitted along Franklin Avenue or along Lyndale Avenue adjacent to the development site. Existing uses that occupy the development site (Theater Garage, Steeple People Surplus Store, the hair salon, and art gallery) rely on off-street parking facilities. Theater Garage and Steeple People both have small surface lots that are in the immediate southwest corner of the Franklin/Lyndale intersection. They, and other existing uses on the southwest corner and some existing uses along the west side of Lyndale Avenue, are also permitted to park in the Rudolph’s Barbeque surface lot.

**Transit Routes Serving the Study Area:**

The study area is well served by transit. Bus stops serving the project area, shown to the right, are located both on the near side and far side of intersections. The north/south routes have near side bus stops along Lyndale Avenue at Franklin and 22nd Avenues. The eastbound route (Route 2) that runs along Franklin Avenue has a near side bus stop, and westbound Route 2 along Franklin Avenue has a far side stop. Table 1 on the following page outlines bus routes serving the site and identifies the locations of bus stops. Figure 2 illustrates the routes.



These routes provide a high level of transit service to the proposed development. All of the routes listed above, except Route 2, serve the Minneapolis CBD. Other trip destinations served by these routes, either directly or by transfer, include downtown Saint Paul, the University of Minnesota, the Midway area, and the airport/Mall of America.

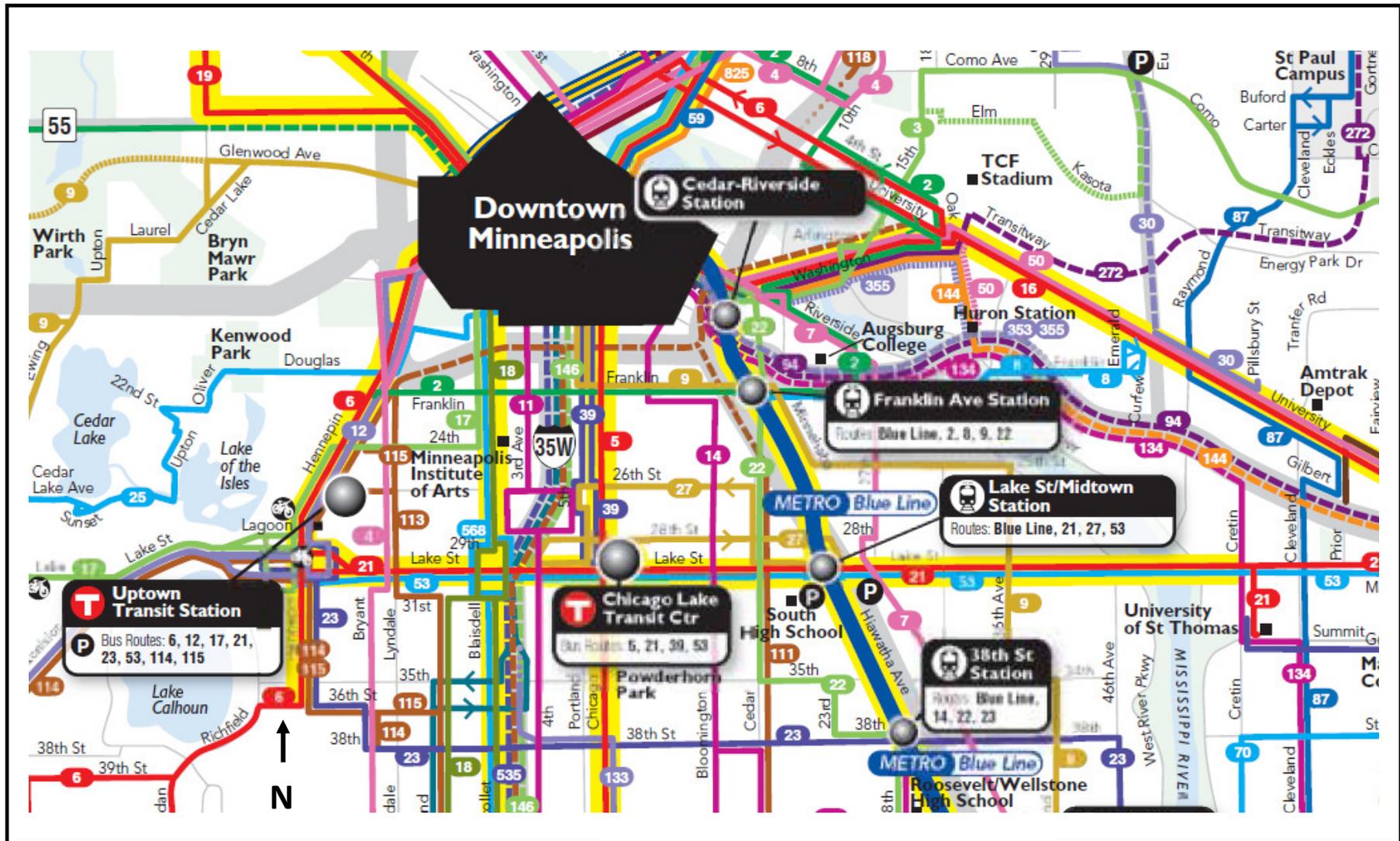
**Bicycle Routes Serving the Study Area:**

Figure 3 shows existing and proposed bike paths that serve the project area. The project area is shown to be served by a large number of on-street bike paths. It should be noted, however, that Lyndale Avenue is not an official bike path.

**TABLE 1  
TGMA DEVELOPMENT TRAVEL DEMAND MANAGEMENT PLAN  
TRANSIT ROUTES**

TRANSIT ROUTE	Type of Service	Nearest Bus Stop to TGMA Development	Route Characteristics		
			Major Line-Haul Street and Destination	Peak Hour Frequency	Off Peak Frequency
Route 2	Local	Franklin/Lyndale Ave	Hennepin Ave/22nd St to Hennepin Ave/6th St SE	10 to 17 min	20 to 30 min
Route 4	Local	Franklin/Lyndale Ave	1st Ave NW/Old Hwy 8 to 82nd St/I-35W	20 min	20 min
Route 6	Local	Franklin/Lyndale Ave	Ontario/Beacon St SE to Picture Dr/78th St SW	20 min	30 min
Route 12	Local	Franklin/Hennepin Ave	Opportunity Partners (Hopkins) to Hennepin/Washington Ave	20 min	30 min
Route 17	Local	24th St/Lyndale Ave	Lake St NE (Hopkins) to 27th NE/Washington St NE	12 - 15 min	20 min
Route 113	Limited Stop	Franklin/Lyndale Ave	Oak/4th St SE to Lyndale Ave/50th St	1 hour	1 hour
Route 115	Limited Stop	Franklin/Lyndale Ave	Oak/4th St SE to Grand Ave/46th St	30 min	30 min

Source: Metro Transit 2016 Interactive Service Map.



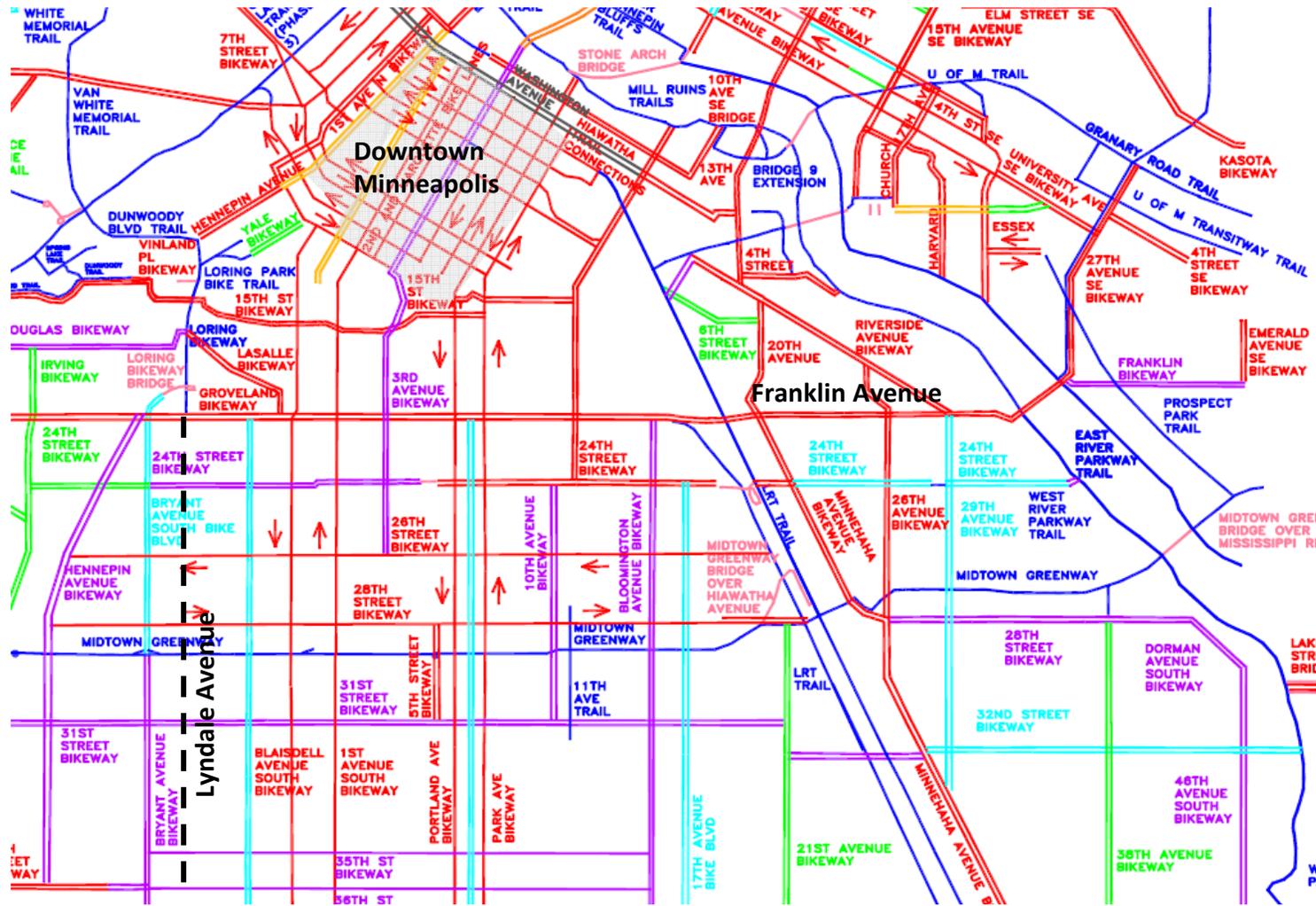
Theater Garage Marquee Apartments  
Travel Demand Management Plan

TGMA  
Developers LLC



Figure 2  
Project Area Transit Routes

Source: Metro Transit  
2016 Service Map



Theater Garage Marquee Apartments  
Travel Demand Management Plan

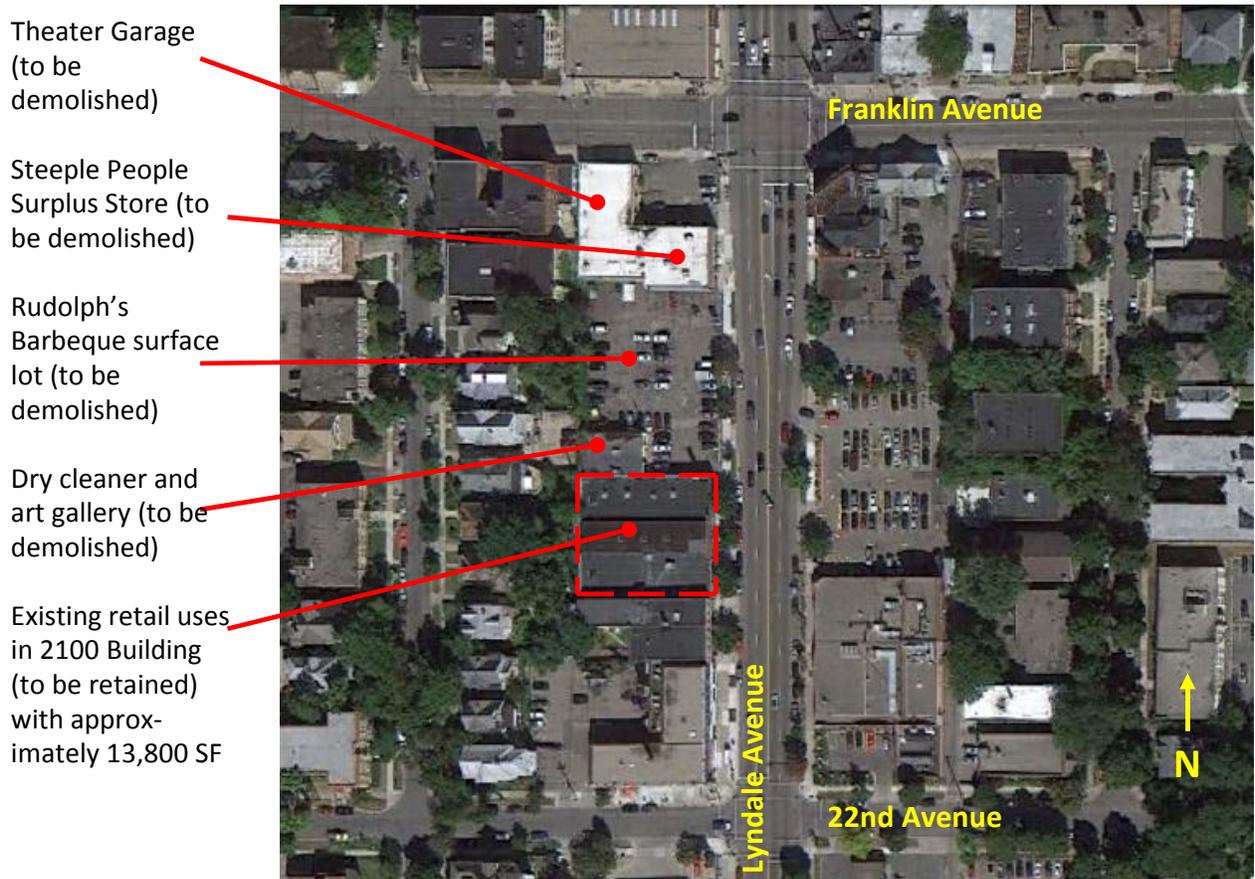
TGMA  
Developers LLC



Figure 3  
Project Area Bicycle Routes  
Source: City of Minneapolis  
Bicycle Map 2011

**TGMA DEVELOPMENT PROGRAM**

The proposed development will be constructed on the southwest corner of the block formed by Franklin and Lyndale Avenues. Existing uses on the corner are Theater Garage, Steeple People Surplus Store, Rudolph’s Barbeque surface lot, a dry cleaner and an art gallery. Each of these uses will be demolished to make way for the development. The 2100 Building, which is attached to and south of the dry cleaner, will not be demolished.



Uses programmed for the new development are detailed in Table 2.

**TABLE 2  
TGMA DEVELOPMENT TRAVEL DEMAND MANAGEMENT PLAN  
PROPOSED DEVELOPMENT PROGRAM**

USE	QUANTITY	LOCATION
Quality restaurant (front of the house)	5,000 SF 300 seats	Ground level
Fitness center	1,100 SF	Ground level
Residences	113 Total DUs: 7 DUs on ground level, walk-up with reserved parking 1 DU on ground level, walk-up without parking 105 DUs in apartments on floors 2 through 5	Levels 1st through 5th floors

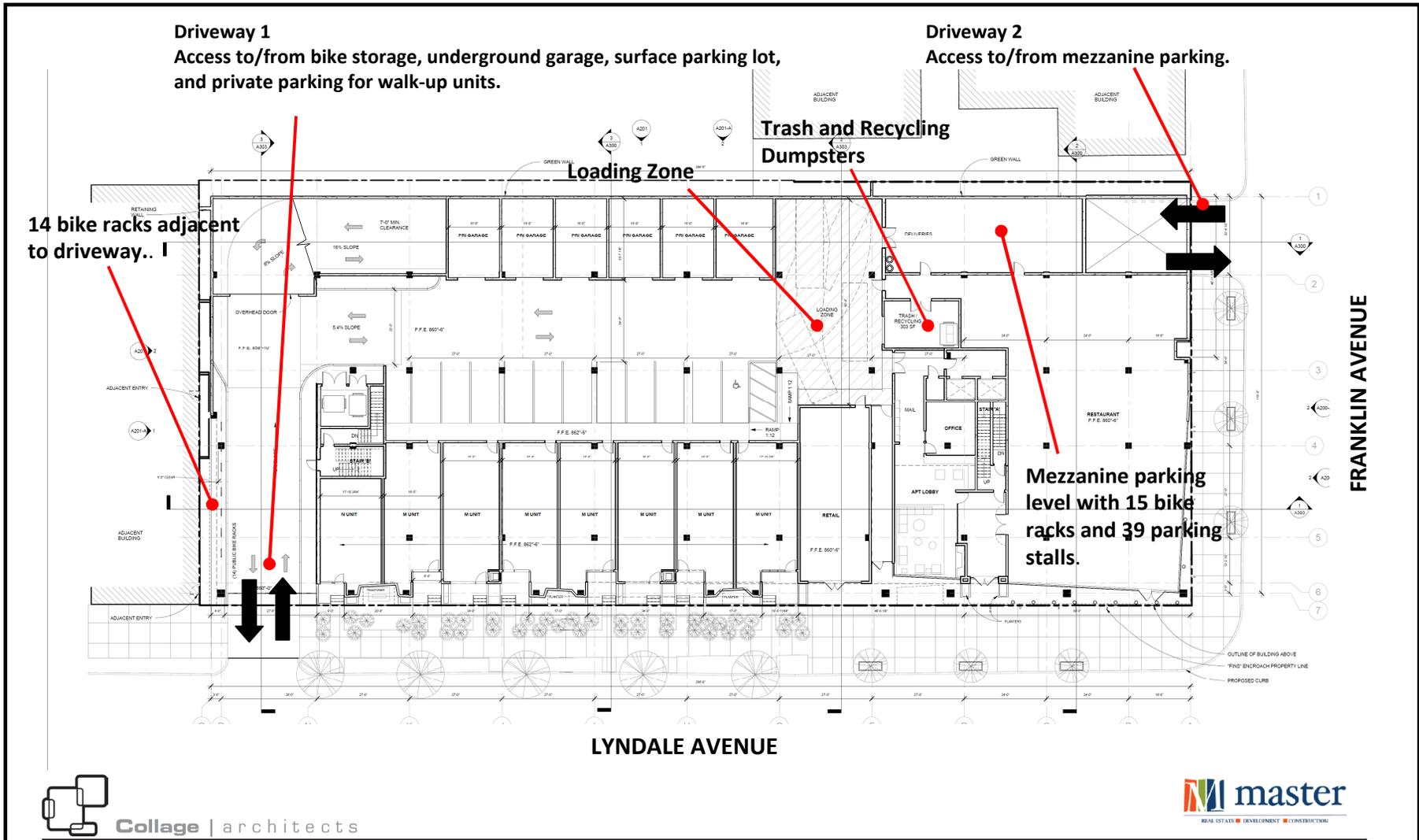
Source: Collage Architects

On-site parking for the new development will be accommodated through provision of 86 bike racks and 147 parking stalls. A minimum of 57 bike racks will be located in the garage for residents. In addition, there will be 15 bike racks on the mezzanine parking level and 14 bike racks along the on-site sidewalk that is adjacent to the Lyndale Avenue driveway.

The 147 parking stalls are defined as follows:

- 91 stalls in an underground garage for apartment residents.
- 6 private stalls in private garages, along the western edge of the development site, for residents of walk-up dwelling units on the main level
- 39 stalls for retail customers in the parking mezzanine. A pay box will be installed on the mezzanine so that parkers can pay for parking.
- 11 stalls in a surface parking lot behind the walk-up apartments. These stalls will be available for 30-minute parking at no charge.

As shown on Figure 4 (Street Level Footprint), two driveways will serve the site. The first is a driveway on Lyndale Avenue that will provide access to: a) a ramp that descends to the 91-stall underground garage, b) the six private stalls for residents in the walk-up units, and c) the 11 stalls in the surface parking lot. The second driveway will be located on Franklin Avenue. It will provide access to the parking mezzanine where there will be a total of 39 parking stalls.



Theater Garage Marquee Apartments  
Travel Demand Management Plan

TGMA  
Developers LLC

BIKO ASSOCIATES  
INCORPORATED

Figure 4  
Street Level Site Plan

## DESIGN REVIEW

This section of the TDMP addresses design-related issues that bear on transportation, both for the general travelling public using the system of sidewalks, streets and intersections and travelers who will access the development site for the purposes of dining, residing, and providing services to on-site uses for deliveries and on-site trash/garbage collection

### PEDESTRIAN CIRCULATION AND TRANSIT ACCESSIBILITY

Data collected by the City of Minneapolis shows that the proposed development area has a high level of pedestrian and bicycle traffic. Table 3 details non-motorized (pedestrian and bicycle) circulation during the AM and PM peak periods at two intersections with crosswalks in the immediate project area.

**TABLE 3  
TGMA DEVELOPMENT TRAVEL DEMAND MANAGEMENT PLAN  
NON-MOTORIZED CIRCULATION ADJACENT TO THE PROPOSED DEVELOPMENT DURING PEAK TRAVEL PERIODS**

Intersection	Year	Peak Period	Northside East/West Movement	Eastside North/South Movement	Southside East/West Movement	Westside North/South Movement	Total Non-Motorized Movements
Franklin/Lyndale	1994	AM	0	0	0	0	0
Franklin/Lyndale	1994	PM	6	0	0	0	6
Franklin/Lyndale	2005	AM	15	23	20	13	71
Franklin/Lyndale	2005	PM	31	23	59	42	155
Franklin/Lyndale	2011	AM	3	7	9	31	50
Franklin/Lyndale	2011	PM	44	48	42	78	212
22nd/Lyndale	1995	AM	29	15	17	13	74
22nd/Lyndale	1995	PM	0	0	0	0	0
22nd/Lyndale	2011	AM	21	11	6	4	42
22nd/Lyndale	2011	PM	79	63	131	91	364
22nd/Lyndale	2012	AM	20	14	21	23	78
22nd/Lyndale	2012	PM	142	30	43	44	259

Source: City of Minneapolis Traffic Count Management System

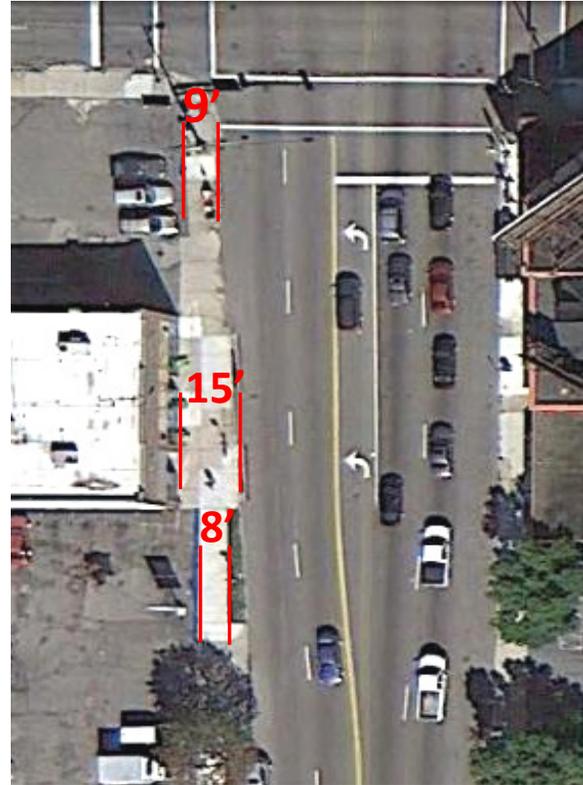
These data show the importance of the two intersections within the project area to a growing number of travelers who are not using automobiles. Keeping in mind the fact that transit riders are pedestrians, the importance of the bus stops in the immediate project area is also brought to light by the data in Table 3.

### LYNDALE AVENUE SIDEWALK WIDTH

Shown in the aerial photograph to the right is the existing southwest corner of the Franklin Avenue/Lyndale Avenue intersection. Field measurements showed that the sidewalk on the west side of Lyndale Avenue, south of Franklin Avenue, is approximately 9 feet wide at the curb. This current width likely resulted from the construction of the northbound, exclusive left-turn lane or the construction of a bus pull-out when a far-side bus stop was located on Lyndale Avenue, south of Franklin Avenue.

The 9 foot width presents an unacceptable condition for an area where pedestrian activity has been observed to be at a very high level. In addition to the narrow sidewalk, two traffic signal poles, a parking regulation sign and pole, a bus stop bench for eastbound transit riders on Route 2, and other street furniture (newspaper vending machines and a mail box) further impede pedestrian circulation in the area.

Moving south from the intersection, the sidewalk on the west side of Lyndale Avenue widens to a maximum width of 15 feet at the south end of the 2100 Building. It then abruptly narrows to 8 feet.



Given its location in the Wedge (an area served by transit routes), pedestrian circulation adjacent to the proposed development will continue to be high, and sidewalk widths should be wide enough to ensure that pedestrian circulation can be accommodated safely and comfortably.

As shown on the street level floor plan (Figure 4 on page 9), the sidewalk on Lyndale Avenue will be reconstructed as part of the proposed development project to provide a sidewalk that is consistently 20 feet wide from the intersection of Franklin/Lyndale to the southern edge of the project, a distance of approximately 350 feet. Figure 4 shows that the sidewalk on Franklin Avenue will be reconstructed to a width of 15 feet adjacent to the project. The Franklin Avenue sidewalk is currently only 8 feet wide.

### DRIVEWAY LOCATIONS

Two driveways are included in the proposed design. (Refer to Figure 4 on page 9.) The first, Drive 1, is a two-way drive that will provide access between the development site and Lyndale Avenue. It is proposed to be located approximately 273 feet south of the Franklin/Lyndale intersection (corner clearance). The second, Drive 2, is a two-way driveway providing access between the development site and Franklin

Avenue. It is proposed to be located approximately 125 feet west of the Franklin/Lyndale intersection (corner clearance). The locations of these driveways, as proposed, will help ensure efficient traffic operations, pedestrian circulation and safety, and transit accessibility on both Lyndale and Franklin Avenues, because:

- Their locations do not interfere with transit boarding and alighting operations.
- Both Lyndale and Franklin Avenues have two travel lanes in each direction. This configuration will allow through vehicles to overtake vehicles turning left into the development site.

## **ON-SITE CIRCULATION**

As shown on Figure 4, Driveway 1 on Lyndale Avenue will provide access for bicycles, vehicles parking in the 11 surface lot stalls, and vehicles parking in the six private stalls. It will also provide access to the underground garage. Long wheelbase vehicles destined for receiving/loading facilities and trash bins on the north end of the development will also enter and exit the site at Drive 1.

A truck maneuvering study was conducted to inform the design of on-site circulation for 25 foot-long (WB-25) trucks. As shown on Figure 4, the loading zone, which will provide access to the restaurant's 'back of the house' and the freight elevator for residential move-ins, has been designed to accommodate trucks.

Garbage and recyclables (trash) will be wheeled out from the restaurant and the residences during collection times. The trash will be loaded into dumpsters using a motorized lift, and will be picked up by haulers that will enter and exit the site through Driveway 1.

## ANALYSIS OF TRAFFIC CONDITIONS

### HISTORICAL TRAFFIC GROWTH RATES AND CALCULATED 2016 TRAFFIC VOLUMES

Findings from an analysis of historical turning movement counts collected by the City of Minneapolis at the Franklin/Lyndale and 22nd/Lyndale intersection are outlined below.

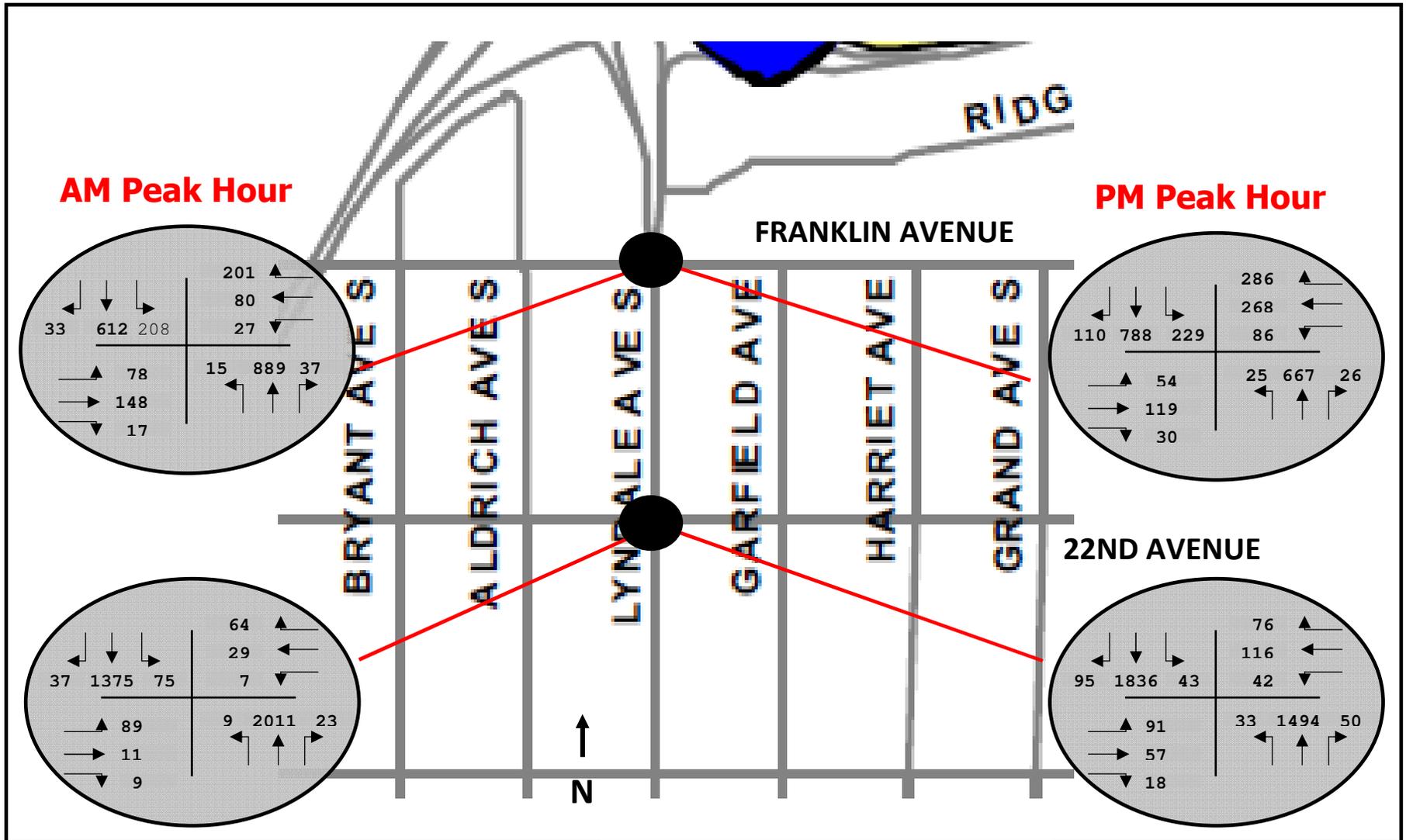
- Franklin/Lyndale intersection (comparison of 2005 and 2011 data):
  - East/west and north/south traffic volumes moving through the Franklin/Lyndale intersection have decreased, both during the AM and PM peak hours.
  - East/west traffic during the AM peak hour has decreased by approximately 40 percent between 2005 and 2011, approximately 7 percent per year.
  - North/south traffic during the AM peak hour has decreased by approximately 25 percent, approximately 4 percent per year.
  - East/west traffic during the PM peak hour has decreased by approximately 26 percent, approximately 4 percent per year.
  - North/south traffic during the PM peak hour has decreased by approximately 19 percent, approximately 3 percent per year.
- 22nd/Lyndale intersection (comparison of 1995 and 2012 data):
  - East/west traffic volumes through the 22nd/Lyndale intersection are growing slowly (less than 1 percent per year).
  - North/south traffic volumes through the 22nd/Lyndale intersection have increased, both during the AM and PM peak hours.
  - North/south traffic during the AM peak hour has grown by approximately 82 percent, approximately 5 percent per year.
  - North/south traffic during the PM peak hour has grown by approximately 25 percent, approximately 2 percent per year.

The annual growth rates outlined above were applied to the most current turning movements at the two intersections to derive calculated 2016 turning movements. Table 4 presents calculated 2016 AM and PM peak hour turning movements. Figure 5 illustrates the calculated AM and PM turning movements.

**TABLE 4**  
**TGMA DEVELOPMENT TRAVEL DEMAND MANAGEMENT PLAN**  
**CALCULATED 2016 PEAK HOUR TURNING MOVEMENTS**

Intersection/ Peak Hour	Southbound			Westbound			Northbound			Eastbound			
	L	T	R	L	T	R	L	T	R	L	T	R	
Franklin/Lyndale AM	208	612	33	27	80	201	15	889	37	78	14	8	17
Franklin/Lyndale PM	229	788	110	86	268	286	25	667	26	54	11	9	30
22nd/Lyndale AM	75	1375	37	7	29	64	9	2011	23	89	11	11	9
22nd/Lyndale PM	43	1836	95	42	116	76	33	1494	50	91	57	18	18

Source: Biko Associates, Inc., August 24, 2016



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Figure 5  
Calculated 2016  
Turning Movements

## EXISTING INTERSECTION CAPACITY ANALYSIS

### **Analysis Inputs:**

Inputs in the intersection capacity analysis included:

- AM and PM peak hour turns shown above in Table 4 and illustrated on Figure 5 and
- Intersection lane configurations shown in Table 5.

**TABLE 5  
TGMA DEVELOPMENT TRAVEL DEMAND MANAGEMENT PLAN  
EXISTING INTERSECTION LANE CONFIGURATIONS**

Intersection	Southbound Approach			Westbound Approach			Northbound Approach			Eastbound Approach		
	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Franklin/Lyndale	1	2	++	>	2	++	1	2	++	>	2	++
22nd/Lyndale	>	2	++	>	1	<	>	2	++	>	1	<

> : Left-turn lane is shared with adjacent through lane.

< : Right-turn lane is shared with adjacent through lane.

++ : Defacto right-turn lane.

Source : Field reconnaissance conducted by Biko Associates, Inc.

### **2016 Intersection Level of Service:**

Intersection capacity is a measurement of average vehicle delay and is rated in terms of Level of Service (LOS). LOS A, B, C, and D are acceptable operational conditions. LOS E and F, which represent long periods of average vehicle delay and congestion, are unacceptable during the majority of a 24-hour day.

By policy, the City of Minneapolis has identified LOS E as an acceptable condition for the AM and PM peak travel periods. An intersection that displays LOS E or LOS F operations during non-peak periods and LOS F operations during peak periods is identified for mitigation through: a) travel demand reduction strategies, b) improved intersection control devices (installation or modification of signage, installation of a traffic signal, or improved timing and/or phasing for an existing traffic signal), c) addition of turn lanes, or d) a combination of the three.

Table 6 presents results of the intersection capacity analysis for the existing, 2016 condition. Computer printouts that detail results of the analysis can be found in the Appendix. As shown in Table 6, both of the intersections currently operate at acceptable, peak hour LOS, both for the AM and PM peak hours.

**TABLE 6  
 TGMA DEVELOPMENT TRAVEL DEMAND MANAGEMENT PLAN  
 2016 AM AND PM PEAK HOUR INTERSECTION CAPACITY**

Intersection	Overall LOS	Signal Timing (sec)	Southbound Approach			Westbound Approach			Northbound Approach			Eastbound Approach		
			Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Franklin/Lyndale AM	B	60	B	B	B	B	C	C	A	B	B	B	B	B
Franklin/Lyndale PM	C	55	B	B	B	B	C	C	B	D	D	B	B	B
22nd/Lyndale AM	C	90	A	A	A	C	C	C	A	C	C	C	C	C
22nd/Lyndale PM	C	90	C	C	C	C	C	C	A	B	B	C	C	C

Source: Biko Associates, Inc.,

## FORECAST 2019 NO-BUILD CONDITIONS

### **Introduction:**

The No-Build analysis was conducted for the purpose of identifying traffic conditions within the influence area of the proposed development at a future time (Year 2019), where the proposed redevelopment has not been implemented. Moreover, the No-Build condition is a future extension of the existing condition where any growth in traffic volume would be attributed to the general growth in traffic and other factors not related to the proposed development. As such, the No-Build analysis can then be used as a base condition, against which the Build condition can be evaluated. This approach to preparing traffic studies enables analysts and decision-makers to accurately evaluate the incremental traffic impacts of a proposed project.

Year 2019 was selected as the analysis forecast year for this traffic study in accordance with rules for preparing Environmental Assessment Worksheets. These rules state that traffic impacts should be evaluated to reflect conditions that will occur one-year after the proposed development's build-out. As the TGMA development project will be completed and ready for occupancy in 2018, forecast 2019 conditions will be evaluated for the No-Build and Build conditions in this traffic study.

### **Forecast 2019 No-Build Peak Hour Traffic:**

Although the growth rates identified on page 15 indicated a reduction in peak hour traffic at the Franklin/Lyndale intersection, it was assumed for the purposes of forecasting 2019 peak hour traffic that the peak hour volumes would grow at a slow rate (1 percent to 2 percent per year). This conservative approach, which may actually overstate peak hour traffic, was used to rigorously test forecast 2019 conditions. Table 7 shows AM and PM peak hour turning movements for TGMA development' 2019 No-Build condition, and Figure 6 illustrates them.

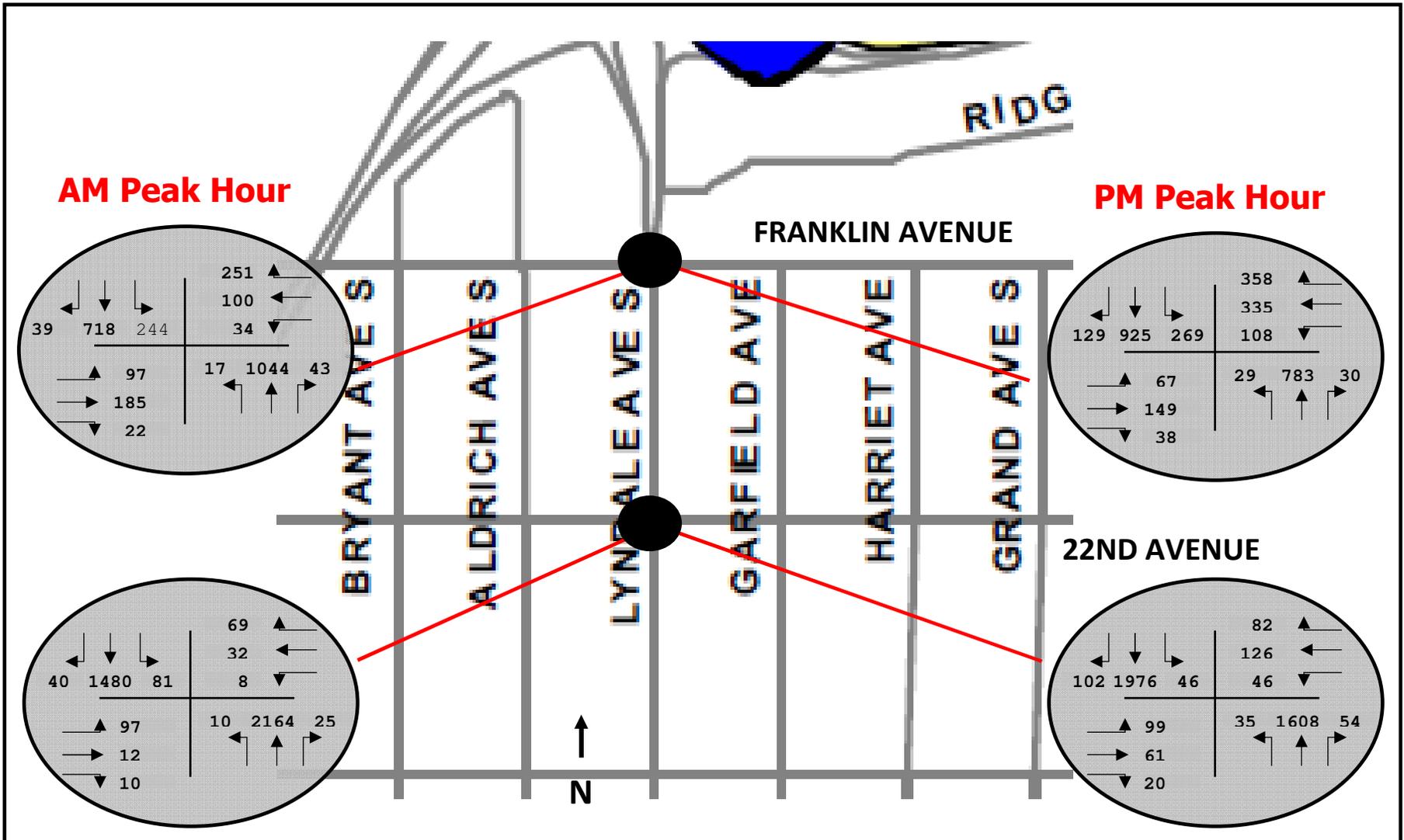
**TABLE 7  
TGMA DEVELOPMENT TRAVEL DEMAND MANAGEMENT PLAN  
FORECAST 2019 NO-BUILD AM AND PM PEAK HOUR TURNS**

Intersection/ Peak Hour	Southbound			Westbound			Northbound			Eastbound				
	L	T	R	L	T	R	L	T	R	L	T	R		
Franklin/Lyndale AM	244	718	39	34	100	251	17	1044	43	97	18	5	22	
Franklin/Lyndale PM	269	925	129	10	8	335	358	29	783	30	67	14	9	38
22nd/Lyndale AM	81	1480	40	8	32	69	10	2164	25	97	12	10		
22nd/Lyndale PM	46	1976	102	46	126	82	35	1608	54	99	61	20		

Source: Biko Associates, Inc.

### **Forecast 2019 No-Build Intersection Capacity Analysis:**

Table 8, on page 21, shows results of the capacity analysis conducted for the forecast 2017 No-Build condition. As shown, the intersections are forecast to operate at acceptable LOS under this condition.



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 **BIKO ASSOCIATES**  
INCORPORATED

Figure 6  
Forecast 2019  
No-Build Turning Movements

**TABLE 8  
TGMA DEVELOPMENT TRAVEL DEMAND MANAGEMENT PLAN  
FORECAST 2019 NO-BUILD AM AND PM PEAK HOUR INTERSECTION CAPACITY**

Intersection	Overall LOS	Signal Timing (sec)	Southbound Approach			Westbound Approach			Northbound Approach			Eastbound Approach		
			Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Franklin/Lyndale AM	B	60	A	B	B	B	C	C	A	B	B	B	B	B
Franklin/Lyndale PM	C	55	B	C	C	B	D	D	B	D	D	B	B	B
22nd/Lyndale AM	C	90	B	A	A	C	C	C	A	D	D	C	C	C
22nd/Lyndale PM	D	90	A	D	D	C	C	C	A	C	C	C	C	C

Source: Biko Associates, Inc.,

## FORECAST 2019 BUILD CONDITIONS

A trip generation analysis was conducted to quantify the volume of traffic that will be generated by the proposed development. Table 9 shows the results of the analysis.

As shown in Table 9, after taking a 10 percent credit because of the development's proximity to bus stops, the new uses proposed for the site will generate a total of 1,044 daily trips, 90 AM peak hour trips, and 95 PM peak hour trips.

**TABLE 9  
TGMA DEVELOPMENT TRAVEL DEMAND MANAGEMENT PLAN  
ESTIMATED TRIP GENERATION**

Land Use	Size	Daily Trips	AM Peak Hour			PM Peak Hour		
			In	Out	Total	In	Out	Total
Residential Apartments	113 DUs	480	10	25	35	25	20	45
Restaurant	5,000 SF	640	30	30	60	35	20	55
Fitness Center	1,100 SF	40	2	3	5	3	2	5
Subtotal		1160	42	58	100	63	42	105
Reductions:								
10% Transit Accessibility		(116)	(4)	(6)	(10)	(6)	(4)	(10)
<b>Grand Total</b>		<b>1044</b>	<b>38</b>	<b>52</b>	<b>90</b>	<b>57</b>	<b>38</b>	<b>95</b>

Source: *Trip Generation, 9th Edition*, Institute of Transportation Engineers, August 3, 2016.

### **Forecast 2019 Build Peak Hour Traffic:**

Trips estimated to be generated by the development were assigned to the network of streets and intersections serving the site, and forecast 2019 Build traffic assignments were derived. These are detailed in Table 10 and illustrated on Figure 7.

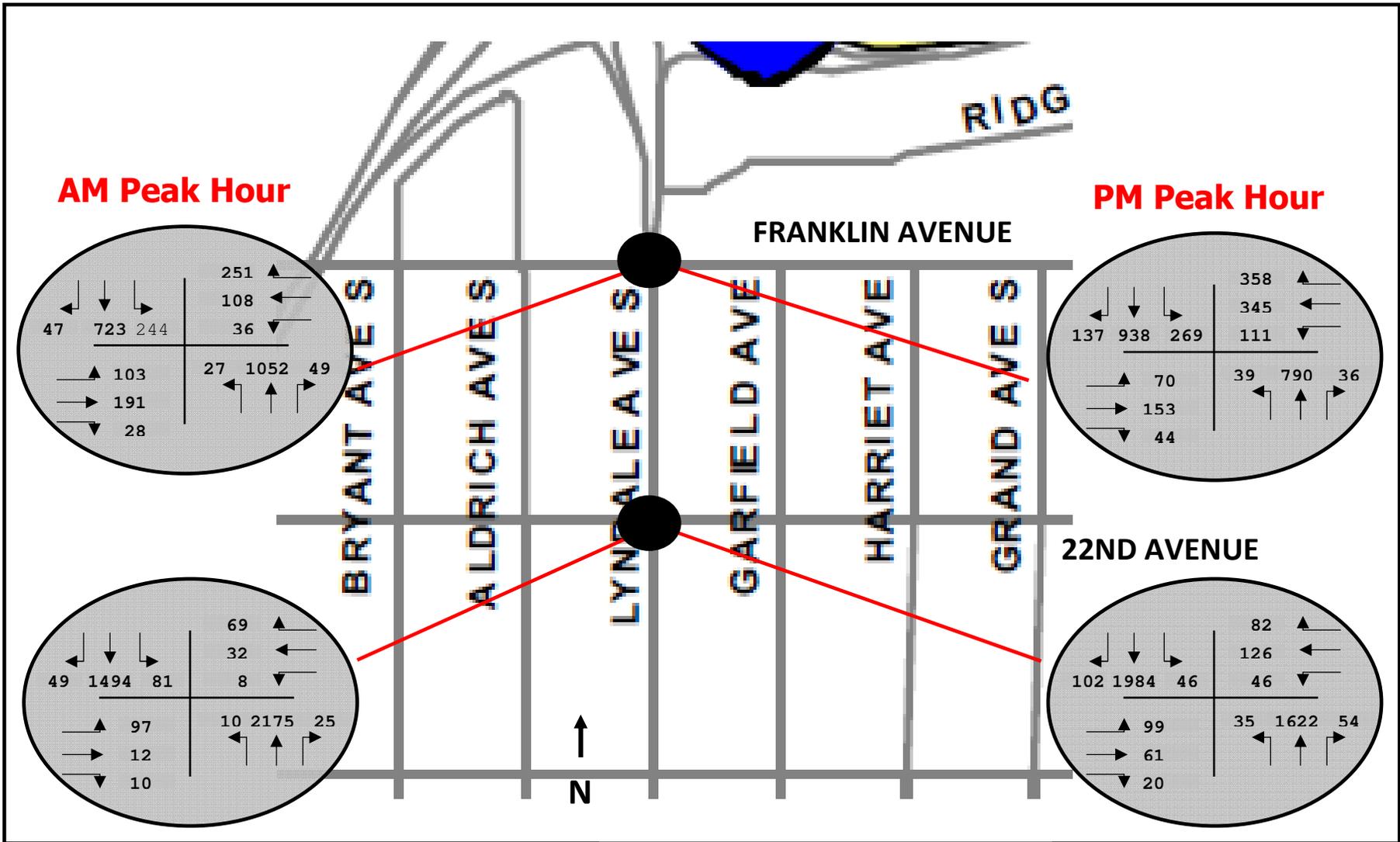
### **Forecast 2019 Build Intersection Capacity:**

Table 11 shows results of the intersection capacity analysis that was conducted for the forecast 2019 Build condition. As shown, both intersections will operate with acceptable LOS after the proposed development has been implemented.

**TABLE 10**  
**TGMA DEVELOPMENT TRAVEL DEMAND MANAGEMENT PLAN**  
**FORECAST 2019 BUILD AM AND PM PEAK HOUR TURNS**

Intersection/ Peak Hour	Southbound			Westbound			Northbound			Eastbound		
	L	T	R	L	T	R	L	T	R	L	T	R
Franklin/Lyndale AM	244	723	47	36	108	251	27	1052	49	10 3	19 1	28
Franklin/Lyndale PM	269	938	137	11 1	345	358	39	790	36	70	15 3	44
22nd/Lyndale AM	81	1494	40	8	32	69	10	2175	25	97	12	10
22nd/Lyndale PM	46	1984	102	46	126	82	35	1622	54	99	61	20

Source: Biko Associates, Inc.



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Figure 7  
Forecast 2019  
Build Turning Movements

**TABLE 11**  
**TGMA DEVELOPMENT TRAVEL DEMAND MANAGEMENT PLAN**  
**FORECAST 2019 BUILD AM AND PM PEAK HOUR INTERSECTION CAPACITY**

Intersection	Overall LOS	Signal Timing (sec)	Southbound Approach			Westbound Approach			Northbound Approach			Eastbound Approach		
			Left	Thru	Right	Left	Thru	Right	Left	Thru	Right	Left	Thru	Right
Franklin/Lyndale AM	B	60	A	B	B	B	C	C	A	B	B	B	B	B
Franklin/Lyndale PM	C	55	B	C	C	B	D	D	B	D	D	B	B	B
22nd/Lyndale AM	C	90	B	A	A	C	D	D	A	D	D	C	C	C
22nd/Lyndale PM	D	90	A	D	D	C	C	C	A	C	C	C	C	C

Source: Biko Associates, Inc.

**Driveway Intersection Level of Service:**

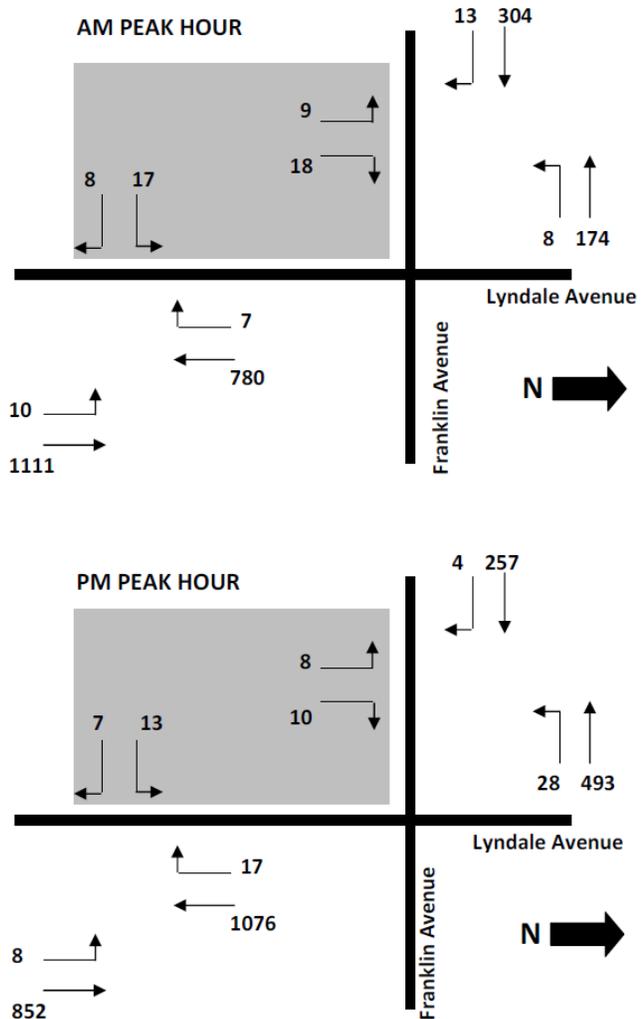
The two driveways serving the site will operate as unsignalized intersections. Analyses were conducted to determine how the driveways will operate and how traffic entering and leaving the site will impact traffic on the adjacent streets. The schematic diagrams below illustrate AM and PM peak hour turns entering and exiting the site.

The unsignalized intersection capacity analyses showed that the two driveways will not negatively impact traffic on Franklin Avenue or Lyndale Avenue. Through movements on these streets, as well as left- and right-turning movements into the proposed development from these streets, will operate at acceptable levels of service (LOS).

Right-turn movements from the development's first driveway (Driveway 1) on Lyndale Avenue will operate at LOS B during the AM and PM peak hours. Left-turns from the development will operate at LOS E during these time periods.

During the AM peak hour, both left- and right-turning movements from the development will operate at LOS B at Driveway 2 on Franklin Avenue. With the increase in traffic volume during the PM peak hour, these turns from the development will operate at LOS C.

Table 12, below details these findings. Computer printouts from the analyses of driveway capacity can be found in the appendices.



**TABLE 12  
UNSIGNALIZED DRIVEWAY INTERSECTION CAPACITY**

Driveway/ Peak Period	Overall ICU LOS	Southbound			Westbound			Northbound			Eastbound		
		L	T	R	L	T	R	L	T	R	L	T	R
Driveway 1 --- AM	A		A	A				A	A		E		B
Driveway 1 --- PM	A		A	A				A	A		E		B
Driveway 2 --- AM	A				A	A		B		B		A	A
Driveway 2 --- PM	A				A	A		C		C		A	A

Source: Biko Associates, Inc.

## PARKING CHARACTERISTICS OF THE PROPOSED DEVELOPMENT

### CITY OF MINNEAPOLIS PARKING REQUIREMENTS

According to the Municipal Code, a minimum of 71 off-street parking stalls will be required for proposed uses on the site. Table 12 details the code-specified, required parking supply for each on-site use and presents a proposed parking supply for each use. As shown in Table 13, a total of 147 stalls are proposed for the development.

**TABLE 13  
TGMA DEVELOPMENT TRAVEL DEMAND MANAGEMENT PLAN  
PROPOSED PARKING SUPPLY**

On-Site Use	Independent Variable	Required Parking Supply	Proposed Supply
Restaurant*	300 seats 5,000 SF	14	39 on Mezzanine
Fitness center**	1,100 SF	0	0
Apartments***	113 dus	57	91 in Underground Garage 6 in Private Garages
			11 in Surface Lot
<b>Total</b>		<b>71</b>	<b>147</b>

\* The proposed supply of 39 stalls more than satisfies the required number of stalls for the proposed restaurant and 17 stalls the developers have agreed to share with Rudolph's Barbeque. A fee will be charged for parking in the mezzanine stalls. A Pay-Box will be installed on the mezzanine to collect the parking fees.

\*\* The proposed fitness center will be provided for use by residents, who will not generate auto trips and neighborhood residents, the majority of whom are expected to walk or ride the bus.

\*\*\* Includes 50 percent reduction in minimum parking requirement for residential uses for the "Transit Incentive" where multi-family uses with 50 or more dwelling units are located within one-quarter mile of transit service with 15-minute headways.

Source: Biko Associates, Inc.

### BICYCLE PARKING

According to the Municipal Code, bicycle parking stalls are required for the proposed development. The Code specifies a minimum of 60 bicycle parking stalls, as detailed below, for the residential and restaurant retail uses.

- 57 bicycle parking stalls for the residences
- 3 stalls for the restaurant use

The developers are proposing to install 86 bike parking stalls; 57 for residents in the garage, 14 for public use along the driveway on Lyndale Avenue, public use on the mezzanine level.

## CONCLUSIONS

### OBSERVATIONS

Observations from the TDMP are:

1. The Traffic Impact Analysis showed that the two intersections that were analyzed will exhibit acceptable Levels of Service (LOS) under all conditions, including:
  - 2016 AM and PM peak hours
  - 2019 No-Build AM and PM peak hours
  - 2019 Build AM and PM peak hours

2. While conducting the unsignalized intersection capacity analysis for Driveway 1 (on Lyndale Avenue), the lane configuration input in the SYNCHRO model did not include an exclusive northbound left-turn lane but, instead, a shared left/through lane and a through lane for the northbound approach. Nevertheless, the analysis indicated LOS A intersection operations for all southbound and northbound turning movements on Lyndale Avenue and LOS E for eastbound left-turns from the development onto northbound Lyndale Avenue, for both the AM and PM peak hours.

While not necessary, because Lyndale Avenue has been constructed with two northbound lanes for general traffic, and vehicles accomplishing northbound left-turns from Lyndale Avenue into the site can be overtaken by vehicles travelling north on Lyndale, provision of an exclusive northbound left-turn bay for vehicles turning left into the site would enhance safety and efficiency and further ensure high levels of service. If determined that this approach has merit, it could be accomplished by painting an approximate 60-foot long southern extension onto the existing northbound left-turn bay.

On-street parking is not permitted along the west side of Lyndale Avenue adjacent to the development site. The extension of the northbound left-turn bay could possibly impact two or three on-street parking stalls on the west side of Lyndale Avenue that are located south of the development site's southern border. .

3. Combined with the increase in sidewalk width (which is part of the development), the driveways defined for the site (one on Lyndale Avenue and one on Franklin Avenue) are located so as to ensure safety and efficiency for automobile traffic and pedestrians.
4. A trip generation analysis showed that the proposed development will generate a low volume of trips. A 10 percent alternative mode allowance was applied to the gross trip generation to account for the availability of transit routes directly adjacent to the development on Lyndale and Franklin Avenues
5. The parking supply proposed for the development, 147 off-street stalls, exceeds City of Minneapolis requirements. According to the Municipal Code, 71 stalls will be required. Likewise, the proposed bicycle parking supply exceeds the City's requirements. The developer proposes to install 86 bike parking stalls, where the City's Code requires 60.

## **TRAVEL DEMAND MANAGEMENT PLAN**

The following Travel Demand Management Plan presents action steps for management at TGMA development.

1. The management team will support bicycle transportation for residents of the 113 apartment dwelling units and restaurant patrons by providing bicycle racks in the underground garage, in the surface parking lot, and on the mezzanine level. A minimum of 86 bike parking spaces will be provided.
2. The management team will support transit use by providing information on transit services to residents in Welcome Packets. In addition to distributing this information in Welcome Packets, the management team will maintain this information in packets in conspicuous locations in the lobby of the apartment building. The packets shall include information on:
  - Transit routes and schedules
  - Guaranteed Ride Home (GRH) program sponsored by Metro Commuter Services
  - Forming and joining car-pool programs
  - High Occupancy Vehicle (HOV) facilities
  - New employee incentives to ride the bus
  - Metro Mobility
3. The management team will enroll TGMA development as an associate in the Transit Works Bus Pass Program, which allows associates to purchase bus passes at discounted prices, and inform residents that they are eligible to participate in the program.
4. The management will not subsidize single occupant vehicle use. Parking spaces will not be free but, instead, sold to residents with or in addition to the cost of a dwelling unit.

Residents of TGMA development will not be able to participate in future Critical Parking Areas (CPA). Extra parking stalls may be sold to residents of the TGMA development at the going market rate.

5. A Transportation Coordinator (TC) must be designated by the management. The TC will be responsible for monitoring travel demand and parking measures and will act as a liaison between the project and the Commuter Connection, a downtown Minneapolis Transportation Management Organization that provides information and services on commuting to/from the downtown area.
6. Provisions must be made for monitoring the travel and parking behavior of residents in the development. The purpose of monitoring will be to: a) track residents' modal choices and resulting mode splits in relation to the project's goals, b) identify additional travel demand measures that may be necessary to implement in the future to ensure goals are met, and c) control the volume of parking spillover in adjacent neighborhoods from residents owning more than one car.

**SIGNATURE PAGE**

**TGMA DEVELOPERS, LLC**

**Dated:** \_\_\_\_\_

**By:** \_\_\_\_\_

**Its:** \_\_\_\_\_

**City of Minneapolis Community Planning and Economic Development Department**

**Dated:** \_\_\_\_\_

**By:** \_\_\_\_\_

**Its:** \_\_\_\_\_

**City of Minneapolis Public Works Department – Traffic and Parking Services**

**Dated:** \_\_\_\_\_

**By:** \_\_\_\_\_

**Its:** \_\_\_\_\_

## **APPENDIX**

- I. 2016 Calculated 2016 Capacity Analysis Computer Printouts**
- II. 2019 Forecast No-Build Intersection Capacity Analysis Computer Printouts**
- III. 2019 Forecast Build Intersection Capacity Analysis Computer Printouts**
- IV. 2019 Forecast Build Driveway Capacity Analysis Computer Printouts**

	↖	→	↘	↙	←	↖	↘	↑	↗	↘	↓	↙
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕↕			↕↕	↗	↘	↕↕		↘	↕↕	
Traffic Volume (vph)	78	148	17	27	80	201	15	889	37	208	612	33
Future Volume (vph)	78	148	17	27	80	201	15	889	37	208	612	33
Satd. Flow (prot)	0	3930	0	0	4002	1599	1787	3870	0	1787	3866	0
Flt Permitted		0.819			0.820		0.385			0.167		
Satd. Flow (perm)	0	3249	0	0	3295	1534	718	3870	0	311	3866	0
Satd. Flow (RTOR)		13						10			13	
Lane Group Flow (vph)	0	264	0	0	116	218	16	1006	0	226	701	0
Turn Type	Perm	NA		Perm	NA	Perm	pm+pt	NA		pm+pt	NA	
Protected Phases		4			8		5	2		1	6	
Permitted Phases	4			8		8	2			6		
Total Split (s)	26.5	26.5		26.5	26.5	26.5	-2.5	36.0		-2.5	36.0	
Total Lost Time (s)		6.5			6.5	6.5	6.5	6.5		6.5	6.5	
Act Effct Green (s)		13.8			13.8	13.8	30.1	29.5		38.0	30.8	
Actuated g/C Ratio		0.23			0.23	0.23	0.50	0.49		0.63	0.51	
v/c Ratio		0.35			0.15	0.62	0.04	0.53		0.49	0.35	
Control Delay		18.5			17.2	27.7	8.4	11.6		19.4	11.4	
Queue Delay		0.0			0.0	0.0	0.0	0.0		0.0	0.0	
Total Delay		18.5			17.2	27.7	8.4	11.6		19.4	11.4	
LOS		B			B	C	A	B		B	B	
Approach Delay		18.5			24.1			11.6			13.4	
Approach LOS		B			C			B			B	
Queue Length 50th (ft)		40			17	71	3	120		~109	63	
Queue Length 95th (ft)		58			30	114	11	167		10	172	
Internal Link Dist (ft)		515			195			245			241	
Turn Bay Length (ft)						150	145			190		
Base Capacity (vph)		1091			1098	511	390	1907		462	2174	
Starvation Cap Reductn		0			0	0	0	0		0	0	
Spillback Cap Reductn		0			0	0	0	0		0	0	
Storage Cap Reductn		0			0	0	0	0		0	0	
Reduced v/c Ratio		0.24			0.11	0.43	0.04	0.53		0.49	0.32	

**Intersection Summary**

Cycle Length: 60  
 Actuated Cycle Length: 60  
 Offset: 0 (0%), Referenced to phase 2:NBTL and 6:SBTL, Start of Green  
 Control Type: Actuated-Coordinated  
 Maximum v/c Ratio: 0.62  
 Intersection Signal Delay: 14.6  
 Intersection Capacity Utilization 77.4%  
 Analysis Period (min) 15  
 Intersection LOS: B  
 ICU Level of Service D  
 ~ Volume exceeds capacity, queue is theoretically infinite.  
 Queue shown is maximum after two cycles.

Splits and Phases: 3: Lyndale Avenue & Franklin Avenue







	↖	→	↘	↙	←	↖	↙	↑	↗	↘	↓	↙
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕↕			↕↕	↗	↘	↕↕		↗	↕↕	
Traffic Volume (vph)	91	57	18	42	116	76	33	1494	50	43	1836	95
Future Volume (vph)	91	57	18	42	116	76	33	1494	50	43	1836	95
Satd. Flow (prot)	0	3804	0	0	3998	1599	1787	3863	0	1787	3856	0
Flt Permitted		0.748			0.824		0.119			0.116		
Satd. Flow (perm)	0	2808	0	0	3231	1428	224	3863	0	218	3856	0
Satd. Flow (RTOR)		17						4			6	
Lane Group Flow (vph)	0	181	0	0	172	83	36	1678	0	47	2099	0
Turn Type	Perm	NA		Perm	NA	custom	pm+pt	NA		pm+pt	NA	
Protected Phases		4			8		5	2		1	6	
Permitted Phases	4			8		4	2			6		
Total Split (s)	40.5	40.5		40.5	40.5	40.5	11.5	38.0		11.5	38.0	
Total Lost Time (s)		6.5			6.5	6.5	6.5	6.5		6.5	6.5	
Act Effct Green (s)		9.2			9.2	9.2	35.0	32.2		36.2	34.4	
Actuated g/C Ratio		0.15			0.15	0.15	0.57	0.53		0.59	0.56	
v/c Ratio		0.41			0.35	0.39	0.14	0.82		0.18	0.96	
Control Delay		25.1			26.3	30.2	6.1	18.8		6.5	29.6	
Queue Delay		0.0			0.0	0.0	0.0	0.0		0.0	0.0	
Total Delay		25.1			26.3	30.2	6.1	18.8		6.5	29.6	
LOS		C			C	C	A	B		A	C	
Approach Delay		25.1			27.6			18.5			29.1	
Approach LOS		C			C			B			C	
Queue Length 50th (ft)		31			33	31	4	299		5	273	
Queue Length 95th (ft)		58			59	68	14	#504		17	#715	
Internal Link Dist (ft)		515			195			245			241	
Turn Bay Length (ft)						150	145			190		
Base Capacity (vph)		1597			1829	808	258	2046		260	2179	
Starvation Cap Reductn		0			0	0	0	0		0	0	
Spillback Cap Reductn		0			0	0	0	0		0	0	
Storage Cap Reductn		0			0	0	0	0		0	0	
Reduced v/c Ratio		0.11			0.09	0.10	0.14	0.82		0.18	0.96	

**Intersection Summary**

Cycle Length: 90  
 Actuated Cycle Length: 60.9  
 Control Type: Semi Act-Uncoord  
 Maximum v/c Ratio: 0.96  
 Intersection Signal Delay: 24.6  
 Intersection LOS: C  
 Intersection Capacity Utilization 96.8%  
 ICU Level of Service F  
 Analysis Period (min) 15  
 # 95th percentile volume exceeds capacity, queue may be longer.  
 Queue shown is maximum after two cycles.

Splits and Phases: 3: Lyndale Avenue & 22nd

















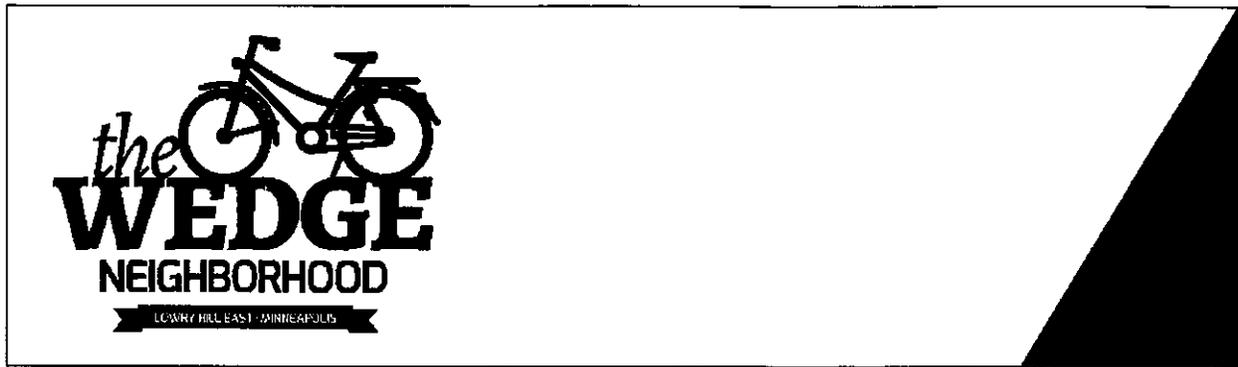


						
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	17	8	10	1111	780	7
Future Volume (Veh/h)	17	8	10	1111	780	7
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	18	9	11	1208	848	8
Pedestrians	15			17	29	
Lane Width (ft)	12.0			16.0	16.0	
Walking Speed (ft/s)	3.5			3.5	3.5	
Percent Blockage	1			2	4	
Right turn flare (veh)						
Median type				None	None	
Median storage (veh)						
Upstream signal (ft)						
pX, platoon unblocked						
vC, conflicting volume	1522	460	871			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	1522	460	871			
tC, single (s)	6.8	6.9	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	83	98	99			
cM capacity (veh/h)	103	531	765			
Direction, Lane #	EB 1	EB 2	NB 1	NB 2	SB 1	SB 2
Volume Total	18	9	414	805	565	291
Volume Left	18	0	11	0	0	0
Volume Right	0	9	0	0	0	8
cSH	103	531	765	1700	1700	1700
Volume to Capacity	0.17	0.02	0.01	0.47	0.33	0.17
Queue Length 95th (ft)	15	1	1	0	0	0
Control Delay (s)	47.2	11.9	0.4	0.0	0.0	0.0
Lane LOS	E	B	A			
Approach Delay (s)	35.4		0.1	0.0		
Approach LOS	E					
Intersection Summary						
Average Delay			0.5			
Intersection Capacity Utilization			52.1%	ICU Level of Service	A	
Analysis Period (min)			15			

						
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Volume (veh/h)	13	7	8	852	1076	17
Future Volume (Veh/h)	13	7	8	852	1076	17
Sign Control	Stop			Free	Free	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	14	8	9	926	1170	18
Pedestrians	15			17	29	
Lane Width (ft)	12.0			16.0	16.0	
Walking Speed (ft/s)	3.5			3.5	3.5	
Percent Blockage	1			2	4	
Right turn flare (veh)						
Median type				None	None	
Median storage veh						
Upstream signal (ft)						
pX, platoon unblocked						
vC, conflicting volume	1704	626	1203			
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol	1704	626	1203			
tC, single (s)	6.8	6.9	4.1			
tC, 2 stage (s)						
tF (s)	3.5	3.3	2.2			
p0 queue free %	82	98	98			
cM capacity (veh/h)	78	414	573			
Direction, Lane #	EB 1	EB 2	NB 1	NB 2	SB 1	SB 2
Volume Total	14	8	318	617	780	408
Volume Left	14	0	9	0	0	0
Volume Right	0	8	0	0	0	18
cSH	78	414	573	1700	1700	1700
Volume to Capacity	0.18	0.02	0.02	0.36	0.46	0.24
Queue Length 95th (ft)	15	1	1	0	0	0
Control Delay (s)	61.1	13.9	0.5	0.0	0.0	0.0
Lane LOS	F	B	A			
Approach Delay (s)	43.9		0.2	0.0		
Approach LOS	E					
Intersection Summary						
Average Delay			0.5			
Intersection Capacity Utilization			44.6%	ICU Level of Service	A	
Analysis Period (min)			15			

	→	↘	↙	←	↖	↗
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↕↕			↕↕	↘	↗
Traffic Volume (veh/h)	304	13	8	174	9	18
Future Volume (Veh/h)	304	13	8	174	9	18
Sign Control	Free			Free	Stop	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	330	14	9	189	10	20
Pedestrians	63			91	131	
Lane Width (ft)	16.0			16.0	12.0	
Walking Speed (ft/s)	3.5			3.5	3.5	
Percent Blockage	8			12	12	
Right turn flare (veh)						4
Median type	None			None		
Median storage (veh)						
Upstream signal (ft)						
pX, platoon unblocked						
vC, conflicting volume			475		644	394
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol			475		644	394
tC, single (s)			4.1		6.8	6.9
tC, 2 stage (s)						
tF (s)			2.2		3.5	3.3
p0 queue free %			99		97	96
cM capacity (veh/h)			954		325	471
<b>Direction, Lane #</b>	<b>EB 1</b>	<b>EB 2</b>	<b>WB 1</b>	<b>WB 2</b>	<b>NB 1</b>	
Volume Total	220	124	72	126	30	
Volume Left	0	0	9	0	10	
Volume Right	0	14	0	0	20	
cSH	1700	1700	954	1700	706	
Volume to Capacity	0.13	0.07	0.01	0.07	0.04	
Queue Length 95th (ft)	0	0	1	0	3	
Control Delay (s)	0.0	0.0	1.2	0.0	14.1	
Lane LOS			A		B	
Approach Delay (s)	0.0		0.4		14.1	
Approach LOS					B	
<b>Intersection Summary</b>						
Average Delay			0.9			
Intersection Capacity Utilization			33.0%		ICU Level of Service	A
Analysis Period (min)			15			

	→	↘	↙	←	↖	↗
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑			↑↑	↘	↗
Traffic Volume (veh/h)	257	4	28	493	8	10
Future Volume (Veh/h)	257	4	28	493	8	10
Sign Control	Free			Free	Stop	
Grade	0%			0%	0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	279	4	30	536	9	11
Pedestrians	63			91	131	
Lane Width (ft)	16.0			16.0	12.0	
Walking Speed (ft/s)	3.5			3.5	3.5	
Percent Blockage	8			12	12	
Right turn flare (veh)						4
Median type	None			None		
Median storage (veh)						
Upstream signal (ft)						
pX, platoon unblocked						
vC, conflicting volume			414			803 364
vC1, stage 1 conf vol						
vC2, stage 2 conf vol						
vCu, unblocked vol			414			803 364
tC, single (s)			4.1			6.8 6.9
tC, 2 stage (s)						
tF (s)			2.2			3.5 3.3
p0 queue free %			97			96 98
cM capacity (veh/h)			1005			252 492
Direction, Lane #	EB 1	EB 2	WB 1	WB 2	NB 1	
Volume Total	186	97	209	357	20	
Volume Left	0	0	30	0	9	
Volume Right	0	4	0	0	11	
cSH	1700	1700	1005	1700	561	
Volume to Capacity	0.11	0.06	0.03	0.21	0.04	
Queue Length 95th (ft)	0	0	2	0	3	
Control Delay (s)	0.0	0.0	1.5	0.0	15.8	
Lane LOS	A			C		
Approach Delay (s)	0.0		0.6	15.8		
Approach LOS						C
Intersection Summary						
Average Delay			0.7			
Intersection Capacity Utilization			50.5%	ICU Level of Service	A	
Analysis Period (min)			15			



To: Mei-Ling Smith, CPED, City of Minneapolis  
From: Lowry Hill East Neighborhood Association  
Cc: Council Member Lisa Bender, Policy Aide Ron Harris

21 September, 2016

Re: Marquee Apartments  
2014 Lyndale Ave S  
BZZ-7922

Don Gerberding of Master Properties presented his proposed development to the Lowry Hill East Neighborhood Association (LHENA) at our Zoning and Planning Committee meeting in August, 2016.

The committee discussed it, and has decided that some key questions remain. Therefore, LHENA does not have sufficient information to take a formal position at this time about requested variances/permits or any other proposal elements for which our views should be taken into consideration.

Please keep us informed on the status of this project; when the application is fully accepted and when it is scheduled for the Planning Commission Public Hearing.

Thank you,

Frank Brown

President, Lowry Hill East Neighborhood Association