CPED STAFF REPORT

Prepared for the City Planning Commission
CPC Agenda Item #6
May 23, 2016
BZZ-7676 and PL-306

LAND USE APPLICATION SUMMARY

Property Location: 1220 South 1st Street and 121 12th Avenue South

Project Name: Legacy

Prepared By: Hilary Dvorak, Principal Planner, (612) 673-2639

Applicant: Riverdale Ventures, LLC

Project Contact: Lynn Leegard with Riverdale Ventures, LLC

Request: To construct a planned unit development containing 374 dwelling units

Required Applications:

Rezoning	Petition to rezone the properties from the I1 Light Industrial District to the C3A Commercial Activity Center District and remove the IL Industrial Living Overlay District.
Conditional Use Permit for a Planned Unit Development	To allow 374 dwelling units.
Site Plan Review	For a new 374-unit residential building.
Preliminary Plat	PL-306

SITE DATA

Existing Zoning	I1 Light Industrial District IL Industrial Living Overlay District DP Downtown Parking Overlay District DH Downtown Height Overlay District
Lot Area	186,121 square feet / 4.27 acres
Ward(s)	3
Neighborhood(s)	Downtown East
Designated Future Land Use	Mixed Use
Land Use Features	Growth Center (Downtown)
Small Area Plan(s)	*Historic Mills District Plan* (1998) *Update to the Historic Mills District Master Plan* (2001)

Date Application Deemed Complete	April 28, 2016	**Date Extension Letter Sent**	May 5, 2016
End of 60-Day Decision Period	June 27, 2016	**End of 120-Day Decision Period**	August 26, 2016

BACKGROUND

SITE DESCRIPTION AND PRESENT USE. The site is located on the northeast corner of South 2nd Street and 13th Avenue South. The site is currently occupied by a vacant industrial building and an accessory surface parking lot.

SURROUNDING PROPERTIES AND NEIGHBORHOOD. The site is surrounded by residential developments of varying densities, office and commercial establishments, cultural destinations, Gold Medal Park and the Mississippi River. The site is located in the Downtown East neighborhood.

PROJECT DESCRIPTION. The applicant is proposing to construct a new 374-unit residential building with 704 enclosed parking spaces. The building would have an overall height of 17-stories and would be approximately 748,000 square feet in size. The building will be broken up into different volumes; a 17-story volume near South 2nd Street, an eight-story volume in the middle of the site and a 14-story volume closest to the Mississippi River. There will be amenity space within the building for the residents including a pet grooming area, community rooms, exercise space, a pool and roof terrace areas. Outside, there will be a playground and a pet exercise area.

RELATED APPROVALS. Not applicable.

PUBLIC COMMENTS. Public comments have been submitted regarding the project. Any additional correspondence received prior to the public meeting will be forwarded on to the Heritage Preservation Commission for consideration.

ANALYSIS

REZONING

The Department of Community Planning and Economic Development has analyzed the application for a petition to rezone the property from the I1 Light Industrial District to the C3A Commercial Activity Center District and remove the IL Industrial Living Overlay District based on the following findings:

1. *Whether the amendment is consistent with the applicable policies of the comprehensive plan.*

   The proposed zoning would be consistent with the applicable policies of *The Minneapolis Plan for Sustainable Growth*. The property is designated as Mixed Use on the future land use map. Mixed use allows for mixed use development, including mixed use with residential. Mixed use may include either a mix of retail, office or residential uses within a building or within a district. There is no requirement that every building be mixed use. The site is located in Downtown Minneapolis which is a designed Growth Center

   The following principles and policies outlined in the comprehensive plan apply to this proposal:

   Land Use Policy 1.15: Support development of Growth Centers as locations for concentration of jobs and housing, and supporting services.

   1.15.3 Encourage the development of high- to very high-density housing within Growth Centers.

   Urban Design Policy 10.5: Support the development of multi-family residential dwellings of appropriate form and scale.

10.5.3 Large-scale, high-rise, multi-family residential development is more appropriate in the Downtown Minneapolis Growth Center.

The City Council approved the *Historic Mills District Master Plan* in 1998. In the plan, the future land use for this property is Technology Park. This land use guidance dates to a period of time when this area was envisioned as a Technology Corridor. Technology Corridor zoning was removed from the site in 1999 as the land use concept never fully materialized.

2. *Whether the amendment is in the public interest and is not solely for the interest of a single property owner.*

   The proposed rezoning is not solely for the interest of the property owner. Rezoning the site to the C3A zoning district will allow for redevelopment of the property in a manner consistent with the comprehensive plan which calls for a mix of uses.

3. *Whether the existing uses of property and the zoning classification of property within the general area of the property in question are compatible with the proposed zoning classification, where the amendment is to change the zoning classification of particular property.*

   Properties in the immediate area are zoned R6 Multiple-family District, C3A Commercial Activity Center District, B4N Downtown Neighborhood District and I1 Light Industrial District. The site is surrounded by residential developments of varying densities, office and commercial establishments, cultural destinations, Gold Medal Park and the Mississippi River. Given the surrounding zoning classifications, the context and uses in the area, as well as adopted policy, rezoning the subject property to the C3A Commercial Activity Center District and removing the IL Industrial Living Overlay District would be appropriate and compatible in this location.

4. *Whether there are reasonable uses of the property in question permitted under the existing zoning classification, where the amendment is to change the zoning classification of particular property.*

   There are reasonable uses permitted in the I1 Light Industrial District. However, the future guidance in the comprehensive for this site is mixed use, not industrial. Rezoning the property to the C3A Commercial Activity Center District will allow for redevelopment of the site that is consistent with that guidance.

5. *Whether there has been a change in the character or trend of development in the general area of the property in question, which has taken place since such property was placed in its present zoning classification, where the amendment is to change the zoning classification of particular property.*

   Over the course of many years the surrounding area has been transitioning from an industrial area to a mixed use area with a variety of residential, office, commercial and cultural uses.

CONDITIONAL USE PERMIT - PLANNED UNIT DEVELOPMENT

The Department of Community Planning and Economic Development has analyzed the application to allow a Planned Unit Development containing 374 dwelling units based on the following findings:

1. *The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

   The establishment of a planned unit development that includes 374 dwelling units will not be detrimental to or endanger the public health, safety, comfort or general welfare. Redevelopment of the block will be beneficial to the neighborhood. The development will provide activity on the site at all hours of the day which will improve the overall area.

2. *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

   The establishment of a planned unit development that includes 374 dwelling units will not be injurious to the use and enjoyment of other property in the vicinity or impede the normal or orderly development and improvement of surrounding property. The site is surrounded by residential developments of varying densities, office and commercial establishments, cultural destinations, Gold Medal Park and the Mississippi River. The proposed development will complement the existing uses in the area.

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

   The Public Works Department has reviewed the preliminary plans and will review the final plans for compliance with standards related to access and circulation, drainage, and sewer/water connections. The applicant will be required to continue to work closely with the Public Works Department, the Construction Code Services Division of CPED related to building code review and inspections and the various utility companies during the duration of the development to ensure that all procedures are followed and that the development complies with all city and other applicable requirements. The applicant is aware that the final plans are expected to incorporate any applicable comments or modifications as required by the Public Works Department.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

   The minimum parking requirement for the development is one space per dwelling unit. However, the site is located within a half-mile of the US Bank Stadium Station and can take advantage of the 50 percent transit reduction. The DP Overlay sets a maximum parking allowance of 1.7 parking spaces per dwelling unit. The building also has to provide one guest parking space for every 50 dwelling units in the building. The maximum parking allowed is 644 spaces (includes eight guest parking spaces) and the applicant is proposing to provide 704 spaces.

   The site has been designed so all truck maneuvering and loading will occur on site. The off-street loading requirement for the development is two small spaces (10 feet by 25 feet) or one large space (12 feet by 50 feet). The applicant is proposing to have two small loading spaces on the site. They are proposed to be located in the porte cochere drop off area at the front of the building. The loading spaces can accommodate 30-foot box trucks (mail, UPS, Fed Ex, and straight body trucks). If residents use a semi truck to move into or out of the building they will be required to hood the parking meters on the street.

   A Travel Demand Management Plan (TDMP) has been prepared for the proposed development. The study concludes that, overall, the proposed development will have minimal traffic impacts on intersection operations, all intersections have adequate capacity to accommodate the number of trips that will be generated and, therefore, impacts on existing operations are minimal.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

   The proposed development would be consistent with the following general land use policies of *The Minneapolis Plan for Sustainable Growth*:

   Land Use Policy 1.1: Establish land use regulations to achieve the highest possible development standards, enhance the environment, protect public health, support a vital mix of land uses, and promote flexible approaches to carry out the comprehensive plan.

1.1.3 Encourage the use of flexible regulatory options that promote high quality development, such as the Planned Unit Development (PUD) tool.

Land Use Policy 1.2: Ensure appropriate transitions between uses with different size, scale, and intensity.

1.2.1 Promote quality design in new development, as well as building orientation, scale, massing, buffering, and setbacks that are appropriate with the context of the surrounding area.

Land Use Policy 1.3: Ensure that development plans incorporate appropriate transportation access and facilities, particularly for bicycle, pedestrian, and transit.

1.3.1 Require safe, convenient, and direct pedestrian connections between principal building entrances and the public right-of-way in all new development and, where practical, in conjunction with renovation and expansion of existing buildings.

1.3.2 Ensure the provision of high quality transit, bicycle, and pedestrian access to and within designated land use features.

Land Use Policy 1.15: Support development of Growth Centers as locations for concentration of jobs and housing, and supporting services.

1.15.3 Encourage the development of high- to very high-density housing within Growth Centers.

6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

If the requested land use applications are approved, the proposal will comply with all provisions of the C3A Commercial Activity Center District, the DP Downtown Parking Overlay District and the DH Downtown Height Overlay District.

Findings Required for Planned Unit Developments:

The planned unit development conforms to the applicable standards for alternatives and amenities. All planned unit developments shall provide at least one amenity or a combination of amenities that total at least 10 points, beyond those required for any alternatives. For each alternative requested, amenities shall total at least five points.

Alternatives requested:

Bulk regulations, building height.

In the C3A Commercial Activity Center District the height limitation is four stories or 56 feet. However, in the DH Downtown Height Overlay District a building on this site is allowed to be six stories or 84 feet. The height of the proposed building is 17 stories or 190 feet in height. The applicant is proposing to provide five points worth of amenities to allow an increase in the maximum height of the building.

In addition to the conditional use permit standards, the Planning Commission shall consider, but not be limited to, the following factors when determining the maximum height of principal structures in commercial districts:

1. *Access to light and air of surrounding properties.*

Increasing the height of the proposed building should have a minimal impact on the amount of light and air that surrounding properties receive. The overall height of the proposed building is 17

stories. However, it has been broken up into different volumes; a 17-story volume near South 2nd Street, an eight-story volume in the middle of the site and a 14-story volume closest to the Mississippi River. In addition, the proposed building has been oriented on the lot in such a way so as to create a significant amount of separation between it and the adjacent buildings. The proposed building will be located between 39 and 191 feet from the west property line and 30 feet from the north property line. All other surrounding development is separated from the site by public streets.

2. *Shadowing of residential properties, significant public spaces, or existing solar energy systems.*

   Shadow studies have been included as an attachment to the staff report and include shadowing impacts at three different times of the day during the summer, fall and winter. The shadow studies indicate that the proposed building will cast shadows on the residential properties to the west and on Gold Medal Park, the Mississippi River and the Grand Rounds Scenic Byway; however, the shadowing impacts do not appear to be significant given the context of the area. The shadow study indicates that due to the fact that the neighborhood is densely built-out, that during the periods of peak shadowing, much of the surrounding vicinity is currently shadowed by existing buildings. In addition, staff is not aware of any existing solar energy systems that would be affected by the proposed building.

3. *The scale and character of surrounding uses.*

   As noted above, the overall height of the proposed building is 17 stories. However, it has been broken up into different volumes; a 17-story volume near South 2nd Street, an eight-story volume in the middle of the site and a 14-story volume closest to the Mississippi River. The height of the proposed building will be taller, but compatible with buildings in the surrounding area which range between one story and 12 stories. The Stonebridge Condominiums, located just west of the site, is 12 stories in height. The Bridgewater Condominiums, located one block west of the site, ranges in height from seven to ten stories. The Encore Apartments, located two blocks west of the site, is 12 stories in heights. The Zenith Condominiums, also located two blocks west of the site, is nine stories in height. And lastly, the University of Minnesota building, located just east of the site, is seven stories in height.

   There has been a considerable amount of development in the surrounding area over the last decade. Many of the new buildings are contemporary in design and have a modern material pallet including brick, metal and glass.

4. *Preservation of views of landmark buildings, significant open spaces or water bodies.*

   The development site is not located in an historic district. However, the site is located within relatively close proximity to Gold Medal Park, the Mississippi River and the Grand Rounds Scenic Byway. While these significant open spaces are located close to the proposed building, there are existing buildings that block views of them from this location. It should be noted that the intent of the standard is to preserve public view corridors, not to preserve individual views from private developments.

Maximum off-street parking.

The maximum parking requirement for a residential development in the DP Downtown Parking Overlay District is 1.7 spaces per dwelling unit. There are 374 dwelling units proposed in the development which has a maximum parking requirement of 636 spaces. In addition, residential developments in the DP Downtown Parking Overlay District that provide parking are required to provide one guest parking space for every 50 dwelling units within the building. The guest parking requirement for the development is eight spaces. Overall, the maximum parking requirement for the development is 644 spaces. The applicant would be providing a total of 704 parking spaces (686 for the residential and 18 for

the guests) on the site; or 1.83 spaces per dwelling unit. The applicant is not required to provide amenity points for this exception to the zoning code. All of the proposed parking will be located within the building and most of the parking would be lined with active uses. Given this, regardless of the number of spaces provided within the building, visually the development will look the same from the exterior. The additional parking spaces will help alleviate pressure on the public streets. Further, the site is located near the perimeter of the Downtown Parking Overlay District.

Points required for alternatives:

- Establishment of the PUD – **10 points.**
- Bulk regulations, Height – **5 points.**
- Maximum off-street parking – **0 points.**

Total = 15 points.

Phasing plan.

This is the only phase of the Legacy planned unit development.

Amenities provided:

The applicant is proposing the following amenities from Table 527-1, Amenities:

Outdoor open space (5 points)

Contiguous ground level outdoor open space that is related to and proportional with the bulk of the building and landscaped with trees and shrubs. Rain gardens, where appropriate, are encouraged. Walkways and pathways shall be surfaced with pervious pavers, pervious concrete, decorative pavers, stamped concrete, colored concrete, brick or other decorative and durable materials. A minimum of thirty (30) percent of the site not occupied by buildings shall be landscaped outdoor open space. A minimum of fifty (50) percent of the provided open space shall be contiguous. The open space must be immediately accessible from the principal structure. Areas should be designed for winter use and relate to the built form with consideration given to elements such as providing shelter from wind, utilizing seasonally appropriate materials, maximizing access to sunlight and providing for snow and ice removal.

The applicant is proposing to provide an outdoor open space on the west side of the site. The space would contain approximately 10,000 square feet of open lawn, an outdoor children's play area and a pet exercise area. The space will be connected to the building and the public sidewalk on South 2nd Street via a series of walkways made out of colored concrete. There will be both bollard-style lights and pedestrian-style lights located along the walkways in the outdoor open space.

In total, 70 percent of the site, not occupied by the building will be landscaped. Besides the concentrated outdoor open space there is generous landscaping around the perimeter of the building. There are two raingardens proposed as part of the overall landscaping plan. One is located on the northwest corner of the site and the other is located on the northeast corner of the site.

The outdoor open space is landscaped with both canopy trees and coniferous trees, however, CPED is recommending that additional coniferous trees be planted on the north and west sides of the open space to help protect the area from wind. In addition, there are no shrubs or perennials proposed to be planted in the outdoor open space. CPED is recommending that shrubs and perennials be planted in the open space to provide seasonal interest.

The first floor dwelling units on the west side of the building are located either at grade with or approximately ten feet above the outdoor open space. Unlike the first floor dwelling units along 13th Avenue South, these dwelling units do not have staircases and walkways that would provide access to

the ground. CPED encourages the developer to provide staircases and walkways from the first floor dwelling units on the west side of the building to the outdoor open space.

Outdoor children's play area (5 points)

An active, outdoor children's play area with a minimum of fifty (50) square feet for each unit containing three or more bedrooms but not less than five hundred (500) square feet of play area to a maximum required area of five thousand (5,000) square feet. The play area shall be secure, shall be separated from parking and maneuvering areas, and shall be designed to facilitate adult supervision. The play area shall include play equipment, installed to the manufacturer's specifications, or natural features suitable for children in both preschool and elementary school. Play equipment shall not be located in a required yard and not more than twenty-five (25) percent of the required square footage of the play area may be located in a required yard. Play areas should be designed for winter use and relate to the built form with consideration given to elements such as providing shelter from wind, utilizing seasonally appropriate materials, maximizing access to sunlight and providing for snow and ice removal.

There will be 87 three-bedroom units in the building which requires a 4,350 square foot outdoor children's play area. The applicant is proposing a 4,350 square foot outdoor children's play area in the northwest corner of the site. This area of the site is located away from vehicular access points and is not located in any required yards. The applicant is proposing to have a main playground structure that includes slides and other imaginative accessories as well as a separate swing structure and a tire swing structure.

Around the perimeter of the play area the applicant is proposing to have four benches. The play area is not proposed to be enclosed with a fence. For security purposes, CPED is recommending that decorative fencing and additional bollard style lights be installed around the perimeter of the play area. CPED is also recommending that at least one trash and recycling receptacle be installed near the play area.

The landscaping around the perimeter of the play area includes canopy trees which will provide shade in the warmer months. However, during the colder months they will not help protect the play area from the wind. CPED is recommending that coniferous trees be planted around the perimeter of the play area.

Decorative or pervious surface for on-site parking and loading areas, drives, driveways, and walkways (3 points)

Provide decorative pavers, pervious pavers, stamped concrete, colored concrete, pervious concrete, brick or other decorative or durable materials for a minimum of seventy-five (75) percent of surface parking and/or loading areas, drives aisles, driveways and walkways that comply with the Americans with Disabilities Act accessibility requirements.

More than 75 percent of the hardscape would be composed of stamped, scored or colored concrete. The three main driveways, the walkway that leads to the principal entrance, the walkways that lead to the individual dwelling units along 13th Avenue South and the walkway that connects the outdoor open space and the outdoor children's play area to the public sidewalk and the building would be constructed out of colored concrete. There is a total of 10,200 square feet of paving on-site; 79 percent would be colored concrete and 21 percent would be standard concrete.

Pet exercise area (1 point)

A pet exercise area shall have a minimum dimension of twelve (12) feet by sixty (60) feet. It shall be enclosed with decorative fencing, include lighting in compliance with Chapter 535, Regulations of General Applicability and

provide accommodations for proper disposal of animal waste. The pet exercise area shall not be located in a required yard.

The applicant is proposing a 24-foot by 66-foot pet exercise area on the southwest side of the site. This area of the site is not located in any required yards. The applicant is proposing to enclose the pet exercise area with a four-foot high decorative metal fence. The applicant is also proposing to have two benches located within the pet exercise area. Three lights would be attached to the building wall adjacent to the exercise area. CPED is recommending that at least one trash receptacle be installed near the exercise area.

Recycling storage area (1 point)

Provide an easily accessible area that serves the entire building and is dedicated to the collection and storage of non-hazardous materials for recycling, including but not limited to paper, corrugated cardboard, glass, plastics and metals. The recycling storage area shall be located entirely below grade or entirely enclosed within the building.

There would be two recycling storage areas located in the building. The recycling chutes would lead to recycling dumpsters located on the first floor of the building for comingled materials.

Table 1. Amenity Points Summary

Amenity	Points Requested by Applicant	Points Recommended by Staff
Outdoor open space	5 points	5 points
Outdoor children's play area	5 points	5 points
Decorative or pervious surface for on-site parking and loading areas, drives, driveways, and walkways	3 points	3 points
Pet exercise area	1 point	1 point
Recycling Storage Area	1 point	1 point
Total	**15 points**	**15 points**

A. The city planning commission may authorize additional uses, subject to the following standards:

The applicant is not proposing to have any uses in the Planned Unit Development that are not otherwise allowed in the C3A Commercial Activity Center District.

B. The planned unit development conforms to the required findings for a planned unit development:

1. That the planned unit development complies with all of the requirements and the intent and purpose of this chapter. In making such determination, the following shall be given primary consideration:

a) The character of the uses in the proposed planned unit development, including in the case of residential uses, the variety of housing types and their relationship to other site elements and to surrounding development.

The proposed development includes 374 dwelling units ranging between two- and three-bedroom units. The site is surrounded by residential developments of varying densities, office and commercial establishments, cultural destinations, Gold Medal Park and the Mississippi River. The proposed development will complement the existing uses in the area.

b) *The traffic generation characteristics of the proposed planned unit development in relation to street capacity, provision of vehicle access, parking and loading areas, pedestrian access, bicycle facilities and availability of transit alternatives.*

The principal entrance to the building would face South 2nd Street. However, the entrance is set back from the street and separated from the public sidewalk by the porte cochere. There would be a walkway that leads from the public sidewalk to the principal entrance abutting the driving lane. The walkway would be differentiated from the driving lane through pavement changes. The first floor dwelling units along 13th Avenue South would also have individual entrances facing the street. Staircases and walkways would provide access to these units from the public sidewalk.

The site would also be designed with a walkway leading from the public sidewalk along South 2nd Street into the property. The walkway would lead to the outdoor open space, the pet exercise area and the building entrances on the west side of the building. For ADA purposes, CPED is recommending that a walkway also be constructed between the outdoor children's play area and the pet exercise area.

For the development, the minimum parking requirement is 187 spaces and the maximum parking requirement is 644 spaces. The applicant would be providing a total of 704 parking spaces (686 for the residential and 18 for the guests) on the site. All of the proposed parking is located below ground or enclosed within the building.

The site has been designed so all truck maneuvering and loading will occur on site. The off-street loading requirement for the development is two small spaces (10 feet by 25 feet) or one large space (12 feet by 50 feet). The applicant is proposing to have two small loading spaces on the site. They are proposed to be located in the porte cochere drop off area at the front of the building. The loading spaces can accommodate 30-foot box trucks (mail, UPS, Fed Ex, and straight body moving trucks).

A TDMP has been prepared for the proposed development. The study concludes that, overall, the proposed development will have minimal traffic impacts on intersection operations, all intersections have adequate capacity to accommodate the number of trips that will be generated and, therefore, impacts on existing operations are minimal.

The applicant will be providing a total of 378 bike parking spaces (374 for the residents and four for guests) on the site. There are designated on-street bike routes along South 2nd Street, 11th Avenue South and West River Parkway which tie into the larger bike trail system in the Twin Cities. In addition, there are a few Nice Ride Minnesota bike stations located within walking distance of the site.

The site is located within walking distance of several bus routes and the US Bank Stadium Station is located with a half-mile of the site. Both the METRO Blue Line and the METRO Green Line stop at this station.

c) *The site amenities of the proposed planned unit development, including the location and functions of open space, the preservation or restoration of the natural environment or historic features, sustainability and urban design.*

The site amenities include outdoor open space, an outdoor children's play area, decorative or pervious surface for on-site parking and loading areas, drives, driveways, and walkways, a pet exercise area and a recycling storage area.

d) *The appearance and compatibility of individual buildings and parking areas in the proposed planned unit development to other site elements and to surrounding development, including but not limited to building scale and massing, microclimate effects of the development, and protection of views and corridors.*

The building will have a strong presence on South 2nd Street and 13th Avenue South. The massing of the building will step down toward the Mississippi River from South 2nd Street. The material palate is simplified and refined and applied to break up the massing of the three building volumes. The new building will complement existing buildings in the surrounding area.

There will be an outdoor open space, an outdoor children's play area and a pet exercise area located on the west side of the site. All of these site amenities can be accessed from within the building and from a walkway that leads from the public sidewalk along South 2nd Street.

e) *An appropriate transition area shall be provided between the planned unit development and adjacent residential uses or residential zoning that considers landscaping, screening, access to light and air, building massing, and applicable policies of the comprehensive plan and adopted small area plans.*

The proposed building has been oriented on the lot in such a way so as to create a significant amount of separation between it and the adjacent buildings. The proposed building will be located between 39 and 191 feet from the west property line and 30 feet from the north property line. All other surrounding development is separated from the site by public streets. The area between the proposed building and the adjacent residential building to the west is landscaped.

f) *The relation of the proposed planned unit development to existing and proposed public facilities, including but not limited to provision for stormwater runoff and storage, and temporary and permanent erosion control.*

The applicant will be working closely with the Public Works Department, the Construction Code Services Division of CPED and the various utility companies during the duration of the development to ensure that all procedures are followed in order to comply with city and other applicable requirements. The City of Minneapolis will review and approve the applicant's stormwater management plan and erosion control plan

g) *The consideration, where possible, of sustainable building practices during the construction phases and the use of deconstruction services and recycling of materials for the demolition phase.*

Demolition debris will be recycled. In addition, waste generated during construction will also be recycled. The design team used LEED principals during the design phase of the building. Such principals include rain gardens, pervious pavement, green roofs, low maintenance landscaping, a high efficiency boiler system, tinted low-e glass with darker tine on the southern façade, tall windows for light, regional materials and additional insulation in the roof.

2. *That the planned unit development complies with all of the applicable requirements contained in Chapter 598, Land Subdivision Regulations.*

The applicant is proposing to plat the property into one lot. The proposed Registered Land Survey will meet the requirements of Chapter 598, Land Subdivision Regulations.

SITE PLAN REVIEW

The Department of Community Planning and Economic Development has analyzed the application based on the required findings and applicable standards in the site plan review chapter:

1. Conformance to all applicable standards of Chapter 530, Site Plan Review.

BUILDING PLACEMENT AND DESIGN

Building placement – *Requires alternative compliance*

- The building is located five feet from the front property line along South 2nd Street except where the building is set back to accommodate the porte cochere. This area of the building is set back 37 feet from the front property line. The building is located between seven and 30 feet from the corner side property line along 13th Avenue South. Alternative compliance is required.
- The placement of the building would reinforce the street walls, maximize natural surveillance and visibility, and facilitate pedestrian access and circulation. The building will be set close to the front and corner side property lines given its height and mass. There will be a principal entrance facing South 2nd Street and the first floor dwelling units along 13th Avenue South would also have individual entrances facing the street. There would be large windows and balconies on all sides of the building and two rooftop decks that maximize the opportunities for people to observe adjacent spaces. The site would also be designed with a walkway leading from the public sidewalk along South 2nd Street into the property. The walkway would lead to the outdoor open space, the pet exercise area and the building entrances on the west side of the building.
- The area between the building and the lot lines include amenities such as landscaping and rain gardens. In addition, to accommodate a green boulevard between the street and sidewalk along 13th Avenue South the applicant is proposing to construct the public sidewalk on the property and install a green boulevard where the sidewalk is currently located.
- All of the on-site accessory parking will enclosed within the building.

Principal entrances – *Meets requirements*

- The principal entrance to the building would face South 2nd Street. In addition, the first floor dwelling units along 13th Avenue South would also have individual entrances facing the street.
- The principal entrance is set back from the street and separated from the public sidewalk by the porte cochere. There would be a walkway that leads from the public sidewalk to the principal entrance abutting the driving lane. To articulate the principal entrance the walkway leading to it would be differentiated from the driving lane through pavement changes. Signage would also be used to emphasize the principal entrance.

Visual interest – *Requires alternative compliance*

- The building walls provide architectural detail and contain windows in order to create visual interest.
- The building is large. To break up the overall mass of the building it has been divided into three separate volumes using variated heights. In addition, unique architectural elements including building projections, projecting balconies, a simple mixture of materials and window patterns have been emphasized.
- The ground floor of the building on the north and west sides are blank. Alternative compliance is required.

Exterior materials – *Requires alternative compliance*

- The applicant is proposing stone, metal, phenolic panels (three colors) and fiber cement panel as the building's primary exterior materials. Each elevation would comply with the City's durability standards for exterior materials (see Table 2). Please note that exterior material changes at a later date may require review by the Planning Commission and an amendment to the site plan review.

- On the east, south and west sides of the building the applicant is proposing to have four exterior materials, excluding windows, doors, and foundation materials. CPED is recommending that there be no more than three exterior materials, excluding windows, doors, and foundation materials, on any side of the building.
- Plain face concrete block is not proposed along any public streets, sidewalks, or adjacent to a residence or office residence district.
- The exterior materials and appearance of the rear and side walls of the building are similar to and compatible with the front of the building.

Table 2. Percentage of Exterior Materials per Elevation

Material	Allowed Max	North	South	East	West
Glass	**100%**	49%	44%	48%	44%
Fiber Cement Panel	**100%**	28%	20%	25%	20%
Stone	**100%**	18%	5%	13%	8%
Phenolic Panel	**100%**	5%	12%	9%	12%
Metal	**75%**	0%	19%	5%	19%

Windows – *Requires alternative compliance*

- For residential uses, the zoning code requires that no less than 20 percent of the walls on the first floor, and no less than ten percent of the walls on each floor above the first that face a public street, public sidewalk, public pathway, or on-site parking lot, shall be windows. The project is in compliance with the minimum window requirement except on the first floor facing 13th Avenue South (see Table 3). Alternative compliance is required.
- All windows are vertical in proportion and are evenly distributed along the building walls.

Table 3. Percentage of Windows per Applicable Elevation

	Code Requirement		Proposed	
Residential Uses				
1st floor facing South 2nd Street	20% minimum	486 sq. ft.	36%	880 sq. ft.
2nd floor and above facing South 2nd Street	10% minimum	101 sq. ft.	Greater than 10%	
1st floor facing 13th Avenue South	20% minimum	765 sq. ft.	18%	704 sq. ft.
2nd floor and above facing 13th Avenue South	10% minimum	502 sq. ft.	Greater than 10%	

Ground floor active functions – *Requires alternative compliance*

- The ground floor facing South 2nd Street contains 100 percent active functions and the ground floor facing 13th Avenue South contains 39 percent active functions. Alternative compliance is required.

Roof line – *Meets requirements*

- The principal roof line of the building will be flat, which is similar to that of surrounding buildings.

Parking garages – *Meets requirements*

- All of the proposed parking is located below ground or enclosed within the building.

ACCESS AND CIRCULATION

Pedestrian access – *Meets requirements*

- There are clear and well-lit walkways at least four feet in width connecting the building entrances to the adjacent public sidewalks.

Transit access – *Not applicable*

- No transit shelters are proposed as part of this development.

Vehicular access – *Meets requirements*

- Vehicular access and circulation has been designed to minimize conflicts with pedestrian traffic and with surrounding residential uses. The porte cochere drop off is located along South 2nd Street. The two curb cuts for the porte cochere are spaced approximately 70 feet apart. There is an additional curb cut located along South 2nd Street that will be used to access the transformers on the site. This curb cut is located approximately 165 feet from the westernmost curb cut for the porte cochere. The enclosed parking area has been designed with two entrances given the grade change on the site. The southerly garage entrance accesses the visitor parking area and the northerly garage entrance accesses the resident parking area. The two curb cuts are spaced approximately 200 feet apart.
- There are no public alleys adjacent to the site.
- The applicant is proposing to have two small loading spaces on the site. They are proposed to be located in the porte cochere drop off area at the front of the building. The loading spaces can accommodate 30-foot box trucks (mail, UPS, Fed Ex, and straight body trucks). If residents use a semi truck to move into or out of the building they will be required to hood the parking meters on the street.
- There is no maximum impervious surface requirement in the C3A Commercial Activity Center District. According to the materials submitted by the applicant, 67percent of the site will be impervious, while 85 percent of the existing site is impervious.

LANDSCAPING AND SCREENING

General landscaping and screening – *Meets requirements with Conditions of Approval*

- The overall composition and location of landscaped areas complement the scale of development and its surroundings.
- At least 20 percent of the site not occupied by the building is landscaped. The applicant is proposing approximately 60,984 square feet of landscaping on site, or approximately 70 percent of the site not occupied by buildings (see Table 4).
- The applicant is proposing at least one canopy tree per 500 square feet of the required landscaped area, including all required landscaped yards. The canopy tree requirement for the site is 35 and the applicant is proposing a total of 36 canopy trees.
- The applicant is not proposing at least one shrub per 100 square feet of the required landscaped area, including all required landscaped yards. The shrub requirement for the site is 175 and the applicant is proposing 131 shrubs. CPED is recommending that the minimum shrub requirement be met on the site.
- The remainder of the required landscaped area is covered with turf grass, native grasses, perennial flowering plants, vines, shrubs and other trees.

Table 4. Landscaping and Screening Requirements

	Code Requirement	Proposed
Lot Area	--	186,121 sq. ft.
Building Footprint	--	98,857 sq. ft.
Remaining Lot Area	--	87,264 sq. ft.
Landscaping Required	17,453 sq. ft.	60,984 sq. ft.
Canopy Trees (1:500 sq. ft.)	35 trees	36 trees
Shrubs (1:100 sq. ft.)	175 shrubs	131 shrubs

Parking and loading landscaping and screening – *Not applicable*

- There is no surface parking proposed for the site, so the site in not subject to the screening and landscaping requirements for parking areas per section 530.170.

ADDITIONAL STANDARDS

Concrete curbs and wheel stops – *Not applicable*

- There are no surface parking spaces proposed on the site.

Site context – *Meets requirements*

- There are no important elements of the city such as parks, greenways, significant buildings, and water bodies near the site that will be obstructed by the proposed building. The site is located within relatively close proximity to Gold Medal Park, the Mississippi River and the Grand Rounds Scenic Byway. While these significant open spaces are located close to the proposed building, there are existing buildings that block views of them from this location.
- Shadow studies have been included as an attachment to the staff report and include shadowing impacts at three different times of the day during the summer, fall and winter. The shadow studies indicate that the proposed building will cast shadows on the residential properties to the west and on Gold Medal Park, the Mississippi River and the Grand Rounds Scenic Byway; however, the shadowing impacts do not appear to be significant given the context of the area. The shadow study indicates that due to the fact that the neighborhood is densely built-out, that during the periods of peak shadowing, much of the surrounding vicinity is currently shadowed by existing buildings. In addition, staff is not aware of any existing solar energy systems that would be affected by the proposed building.
- The building has been designed with stepbacks at different levels, recesses and projections and canopies over the entryways which should help minimize wind effects on the surrounding area.

Crime prevention through environmental design – *Meets requirements*

- The site plan complies with crime prevention design elements as the principal entrance and individual ground floor unit entrances will be oriented towards the public sidewalks, walkways direct people to and from the building entrances, there will be large windows and balconies on all sides of the building that maximize the opportunities for people to observe adjacent spaces and the public sidewalks and there will be lights located near all of the building entrances and througout the grounds.

Historic preservation – *Meets requirements*

- This site is neither historically designated nor is it located in an historic district.

2. Conformance with all applicable regulations of the zoning ordinance.

The proposed use is conditional in the C3A Commercial Activity Center District.

Off-street Parking and Loading – *Requires conditional use permit*

- The applicant is seeking an exception to the maximum parking requirement through the planned unit development (see Table 5).

Table 5. Vehicle Parking Requirements Per Use (Chapter 541)

Use	Minimum	Reductions	Total with Reductions	Maximum Allowed	Proposed
Residential Dwellings	374 residential 8 guests	Transit Incentives (187)	187 residential 8 guests	636 residential	686 residential 18 guests
	382	**(187)**	**195**	**636 residential**	**704**

Table 6. Bicycle Parking Requirements (Chapter 541)

Use	Minimum	Short-Term	Long-Term	Proposed
Residential Dwellings	187	--	Not less than 90%	374 residential 4 guest
	187	**--**	**168**	**378**

Table 7. Loading Requirements (Chapter 541)

Use	Loading Requirement	Minimum Requirement	Proposed
Residential Dwellings	2 small spaces or 1 large space	2 small spaces or 1 large space	2 small spaces

Building Bulk and Height – *Requires conditional use permit*

- The development qualifies for a 20 percent FAR bonus for enclosed parking. The maximum FAR in the DH Downtown Height Overlay District is 4.0. The 20 percent bonus allows for an additional .8 FAR for a total maximum FAR of 4.8 (see Table 8).
- The applicant is seeking an exception to the maximum height through the planned unit development (see Table 8)

Table 8. Building Bulk and Height Requirements

	Code Requirement	Proposed
Lot Area	--	186,121 sq. ft. / 4.27 acres
Gross Floor Area	--	747,804 sq. ft.
Floor Area Ratio (Minimum)	Not applicable	4.02
Floor Area Ratio (Maximum)	4.81	
Building Height (Maximum)	6 stories or 84 ft., whichever is less	17 stories or 190 ft.

Lot Requirements – *Meets requirements*

Table 9. Lot Requirements Summary

	Code Requirement	Proposed
Dwelling Units (DU)	--	374 DUs
Density (DU/acre)	--	87.6 DU/acre
Lot Area (Minimum)	Not less than 1 acre	4.27 acres
Impervious Surface Area (Maximum)	Not applicable	67%
Lot Coverage (Maximum)	Not applicable	53%
Lot Width (Maximum)	Not applicable.	370 ft.

Yard Requirements – *Meets requirements*

Table 10. Minimum Yard Requirements

Setback	Zoning District	Overriding Regulations	Total Requirement	Proposed
Front (South 2nd Street)	0 ft.	--	0 ft.	Between 5 and 37 ft.
Corner Side (13th Avenue South)	0 ft.	--	0 ft.	Between 7 and 30 ft.
Interior Side (West)	39 ft.	--	39 ft.	Between 39 and 191 ft.
Rear (North)	15 ft.	--	15ft.	30 ft.

Signs – *Meets requirements*

- Signs are subject to the requirements of Chapter 543, On-Premise Signs. In the C3A zoning district there can be one-and-a-half square feet of signage for every one linear foot of primary building wall. Wall signs are limited to 180 square feet in size. Projecting signs are limited to 48 square feet in size. There is no height limitation for either wall signs or projecting signs. Freestanding monument signs are limited to 80 square feet in size and can be no taller than 8 feet. However, a freestanding monument sign shall not be allowed if the amount of signage exceeds one square foot of signage for every one foot of primary building wall. The zoning code limits the number of freestanding signs on a zoning lot to one
- The applicant is proposing to have one wall sign located near the entrance to the building along South 2nd Street. The sign would be 108 square feet in size and would have an overall height of 12 feet.

Screening of Mechanical Equipment – *Meets requirements with Conditions of Approval*

- There will be mechanical equipment located on the roof of the building. Specifically, the equipment will be located next to the stair overruns on top of the 14-story and 17 story volumes of the building. CPED is recommending that the rooftop mechanical equipment be screened with either a

screen wall or painted to match the color of the stair overruns. If any other equipment is placed on the roof of the building, CPED is recommending that it be screened per the requirements of Chapter 535, Regulations of General Applicability.

- There will also be mechanical equipment, including cooling towers, generators and transformers located on the ground towards the southwest corner of the site. The applicant is proposing to screen the cooling towers with a 14-foot tall metal screen wall. However, to screen the generators and transformers the applicant is proposing to plant a row of coniferous trees along the west property line. CPED is recommending that the generators and transformers be screened with a metal screen wall or that additional landscape materials be planted along the west property line to fully screen the mechanical equipment from the public right-of-way.

Refuse Screening – *Meets requirements*

- All refuse and recycling storage containers are located within the building.

Lighting – *Meets requirements with Conditions of Approval*

- A lighting plan showing footcandles was not submitted as part of the application materials. CPED is recommending that the final lighting plan conform to the standards of Chapter 535, Regulations of General Applicability.

Fences – *Meets requirements*

- The applicant is proposing to install a four-foot high decorative metal fence around the perimeter of the pet exercise area and CPED is recommending that the same fence be installed around the perimeter of the outdoor children's play area.

Specific Development Standards – *Not applicable*

DH Downtown Height Overlay District Standards – *Requires conditional use permit*

- The applicant is seeking an exception to the maximum height through the planned unit development.

DP Downtown Parking Overlay District Standards – *Requires conditional use permit*

- The applicant is seeking an exception to the maximum parking requirement through the planned unit development.

3. Conformance with the applicable policies of The Minneapolis Plan for Sustainable Growth.

The Minneapolis Plan for Sustainable Growth identifies the site as Mixed Use on the future land use map. The proposed development is consistent with the following principles and policies outlined in the comprehensive plan:

Urban Design Policy 10.1: Promote building designs and heights that enhance and complement the image and form of the Downtown skyline, provide transition to the edges of Downtown and protect the scale and quality in areas of distinctive physical or historical character.

10.1.1 Concentrate the tallest buildings in the Downtown core.

10.1.2 Building placement should preserve and enhance public view corridors that focus attention on natural or built features, such as landmark buildings, significant open spaces or water bodies.

10.1.3 Building placement should allow light and air into the site and surrounding properties.

Urban Design Policy 10.5: Support the development of multi-family residential dwellings of appropriate form and scale.

10.5.3 Large-scale, high-rise, multi-family residential development is more appropriate in the Downtown Minneapolis Growth Center.

Urban Design Policy 10.6: New multi-family development or renovation should be designed in terms of traditional urban building form with pedestrian scale design features at the street level.

10.6.1 Design buildings to fulfill light, privacy, and view requirements for the subject building as well as for adjacent properties by building within required setbacks.

10.6.2 Promote the preservation and enhancement of view corridors that focus attention on natural or built features, such as the Downtown skyline, landmark buildings, significant open spaces or bodies of water.

10.6.3 Provide appropriate physical transition and separation using green space, setbacks or orientation, stepped down height, or ornamental fencing to improve the compatibility between higher density and lower density residential uses.

10.6.4 Orient buildings and building entrances to the street with pedestrian amenities like wider sidewalks and green spaces.

10.6.5 Street-level building walls should include an adequate distribution of windows and architectural features in order to create visual interest at the pedestrian level.

10.6.6 Integrate transit facilities and bicycle parking amenities into the site design.

Urban Design Policy 10.17: Provide sufficient lighting to reflect community character, provide a comfortable environment in a northern city and promote environmentally friendly lighting systems.

10.17.1 Provide high-quality lighting fixture designs that are appropriate to street types and land use, and that provide pedestrian friendly illumination, but minimize glare and dark sky conditions, and other unnecessary light pollution.

10.17.5 Integrate exterior building lighting design to attune with building designs and landscaping.

10.17.6 Provide sufficient lighting for better way-finding and safe circulation within and around a development.

Urban Design Policy 10.19: Landscaping is encouraged in order to complement the scale of the site and its surroundings, enhance the built environment, create and define public and private spaces, buffer and screen, incorporate crime prevention principles, and provide shade, aesthetic appeal, and environmental benefits.

10.19.1 In general, larger, well-placed, contiguous planting areas that create and define public and private spaces shall be preferred to smaller, disconnected areas.

10.19.2 Plant and tree types should complement the surrounding area and should include a variety of species throughout the site that include seasonal interest. Species should be indigenous or proven adaptable to the local climate and should not be invasive on native species.

10.19.3 Landscaped areas should include plant and tree types that address ecological function, including the interception and filtration of stormwater, reduction of the urban heat island effect, and preservation and restoration of natural amenities.

10.19.4 Landscaped areas should be maintained in accordance with Crime Prevention Through Environmental Design (CPTED) principles, to allow views into and out of the site, to preserve view corridors and to maintain sight lines at vehicular and pedestrian intersections.

10.19.5 Landscaping plans should be designed to facilitate future maintenance including the consideration of irrigation systems, drought and salt-resistant species, ongoing performance of storm water treatment practices, snow storage, access to sun, proximity to buildings, paved surfaces and overhead utilities.

10.19.6 Green roofs, living walls, and porous pavement are encouraged but are not meant to be a substitute for ground-level landscaping of sites as landscaping provides both a natural amenity and aesthetic beauty to the urban landscape.

10.19.7 Boulevard landscaping and improvements, in accordance with applicable city polices, are encouraged.

4. Conformance with applicable development plans or objectives adopted by the City Council.

The site is located within the boundaries of the *Historic Mills District Master Plan* adopted by the City Council in 1998. In the plan, the future land use for this property is Technology Park. This land use guidance dates to a period of time when this area was envisioned as a Technology Corridor. Technology Corridor zoning was removed from the site in 1999 as the land use concept never fully materialized.

5. Alternative compliance.

The Planning Commission or zoning administrator may approve alternatives to any site plan review requirement upon finding that the project meets one of three criteria required for alternative compliance. Alternative compliance is requested for the following requirements:

- **Building placement.** The building is located five feet from the front property line along South 2nd Street except where the building is set back to accommodate the porte cochere. This area of the building is set back 37 feet from the front property line. The porte cochere will be used as a drop off area for residents and guests as well as a loading for 30-foot box trucks (mail, UPS, Fed Ex, and straight body trucks). CPED is recommending that the City Planning Commission grant alternative compliance. However, to screen the porte cochere from the public sidewalk, CPED is recommending that the area between the driveways be landscaped.
- **Building placement.** The building is located between seven and 30 feet from the corner side property line along 13th Avenue South. Given the height and massing of the building, the increased setback from the corner side property line is appropriate. In addition, the greater setback allows the applicant to construct the public sidewalk on the property and install a green boulevard where the sidewalk is currently located. The applicant has indicated that the building wall along 13th Avenue South has been set back as there is a series of overhead electric lines that will remain in the public right-of-way. CEPD encourages the developer to work with Xcel to bury these lines so they are not a visual obstruction for those living on this side of the building. CPED is recommending that the City Planning Commission grant alternative compliance.
- **Blank walls.** The ground floor of the building on the north and west sides are blank. The applicant is proposing to install green screens along these two building walls to help break them up. CPED is recommending that the green screens be spaced no more than 25 feet from one another.
- **Windows.** The project is in compliance with the minimum window requirement except on the first floor facing 13th Avenue South where there are 18 percent windows. The topography of the site is such that there is an 18-foot grade change from south to north. Given the grade change, the

enclosed parking area becomes more exposed along 13th Avenue South. CPED is recommending that the City Planning Commission grant alternative compliance.

- **Active functions.** The ground floor facing 13th Avenue South contains 39 percent active functions. The topography of the site is such that there is an 18-foot grade change from south to north. Given the grade change, the enclosed parking area becomes more exposed along 13th Avenue South. CPED is recommending that the City Planning Commission grant alternative compliance.

PRELIMINARY PLAT

The Department of Community Planning and Economic Development has analyzed the application for a Registered Land Survey based on the following findings:

1. *The subdivision is in conformance with these land subdivision regulations, the applicable regulations of the zoning ordinance and policies of the comprehensive plan.*

   The applicant is proposing to replat several lots and formerly vacated right-of-way into one new lot. Individual lots within PUDs are exempt from the public street frontage requirement of section 598.230 and the design requirements of sections 598.240 and 598.250.

   Section 598.260 Planned unit developments and cluster design, requires the design of a subdivision for a planned unit development to implement the site plan as approved by the Planning Commission and shall include a deed restriction designating the following:

   a. *The relationship between all common spaces and each individual lot (rights in the common spaces and proportionate ownership accruing to the individual lot).*

      The applicant will own all of the land so this provision is not necessary.

   b. *Provision for access to each lot that does not have frontage on a public street.*

      The lot will have frontage on a public street.

   c. *A requirement that an owner's association be created. The duties and responsibilities of the owner's association shall include maintaining the elements of the planned unit development as authorized under the zoning ordinance or other applicable regulations.*

      The applicant will own all of the land so this provision is not necessary.

   d. *A provision that the taxes, special assessments, and other charges and fees that would normally be levied against the common spaces shall be levied against the individual lot occupied or to be occupied by buildings in direct proportion to the interest that is stated in the deed restriction and shall provide that such levies shall be a lein against the individual lots.*

      The applicant will own all of the land so this provision is not necessary.

   e. *A requirement that any disposition of any of the common property situated within the planned unit development shall not be made without the prior approval of the Planning Commission.*

      If an amendment is made to the planned unit development the proposed amendments shall be reviewed and approved by the City Planning Commission.

2. *The subdivision will not be injurious to the use and enjoyment of other property in the immediate vicinity, nor be detrimental to present and potential surrounding land uses, nor add substantially to congestion in the public streets.*

   The applicant is proposing to replat several lots and formerly vacated right-of-way into one new lot. The site is surrounded by residential developments of varying densities, office and commercial

establishments, cultural destinations, Gold Medal Park and the Mississippi River. A TDMP has been prepared for the proposed development. The study concludes that, overall, the proposed development will have minimal traffic impacts on intersection operations, all intersections have adequate capacity to accommodate the number of trips that will be generated and, therefore, impacts on existing operations are minimal.

3. *All land intended for building sites can be used safely without endangering the residents or users of the subdivision or the surrounding area because of flooding, erosion, high water table, soil conditions, improper drainage, steep slopes, rock formations, utility easements or other hazard.*

   The land intended for building can be used safely without endangering the residents or users of the subdivision or the surrounding area because of flooding, erosion, high water table, soil conditions, improper drainage, steep slopes, rock formations, utility easements or other hazard.

4. *The lot arrangement is such that there will be no foreseeable difficulties, for reasons of topography or other conditions, in securing building permits and in providing driveway access to buildings on such lots from an approved street. Each lot created through subdivision is suitable in its natural state for the proposed use with minimal alteration.*

   The parcel created by this application presents no foreseeable difficulties for this development. No significant alterations to the land appear necessary.

5. *The subdivision makes adequate provision for stormwater runoff, and temporary and permanent erosion control in accordance with the rules, regulations and standards of the city engineer and the requirements of these land subdivision regulations. To the extent practicable, the amount of stormwater runoff from the site after development will not exceed the amount occurring prior to development.*

   The Surface Water and Sewers Division of the Public Works Department will review and approve the drainage and sanitary system plans for this development prior to building permits being issued.

FOR REZONINGS ONLY

ZONING PLATE NUMBER. 15

LEGAL DESCRIPTION.

Parcel 1:

Lots 1, 2, 3, 4, 5, 6, 7, 8, 9 and 10, Block 115, Town of Minneapolis, according to the recorded plat thereof, Hennepin County, Minnesota.

That part of the Southerly one-half of vacated First Street South which lies between the extension Northeasterly of the Southeasterly lot line of Lot 6, Block 115, Town of Minneapolis, and the extension Northeasterly of the Northwesterly lot line of Lot 8 in said Block 115.

That part of the Southerly one-half of vacated First Street South which lies between the extension Northeasterly of the Northwesterly lot line of Lot 8, Block 115, Town of Minneapolis, and the extension Northeasterly of the center line of vacated 12th Avenue South.

That part of the Northerly one-half of vacated First Street South described as follows: Beginning at the Southwesterly corner of Lot 7, Block 133, Town of Minneapolis; thence Southerly at right angles to the North line of said First Street a distance of 40 feet; thence Westerly on a line parallel to the North line

of said First Street a distance of 172.4 feet; thence Northerly on an angle to the right of 90 degrees a distance of 24.34 feet; thence Easterly on an angle to the right of 72 degrees 22 minutes a distance of 51.7 feet to the North line of said First Street; thence Easterly along the said North line of First Street a distance of 123.13 feet to the point of beginning.

That part of the Easterly one-half of vacated 12th Avenue South lying between the extension Northwesterly of the Southerly lot line of Lot 10, Block 115, Town of Minneapolis, and the extension Northwesterly of the Northerly lot line of said Lot 10.

(Abstract property)

Parcel 2:

That part of Block 133 and those parts of the vacated parts of 12th Avenue South, which was platted as Huy Street, First Street South, which was platted as 1st Street, and abandoned Bluff Street all in the Town of Minneapolis, described as follows: Beginning at a point on a line drawn parallel with and 40 feet Southwesterly from the Southwesterly line of said Block 133, and its Northwesterly extension, distant 172.40 feet Northwesterly along said parallel line from its intersection with a line drawn Southwesterly, at a right angle to the Southwesterly line of said Block 133, from the most Westerly corner of Lot 7, in said Block 133, thence Southeasterly along said parallel line a distance of 370.40 feet to its intersection with the Southwesterly extension of the Southeasterly line of said Block 133; thence Northeasterly to the most Southerly corner of said Block 133; thence Southeasterly to the most Westerly corner of said Block 132; thence Southeasterly along the Southwesterly line of said Block 132 to the most Southerly corner of Lot 3 in said Block 132; thence Northeasterly along the Southeasterly line of said Lot 3 to the most Easterly corner of Lot 3 in said Block 132; thence Southeasterly along the Southwesterly line of Lot 7 in said Block 132 a distance of 99 feet; thence at right angles Northeasterly to the East line of Section 26, Township 29, Range 24; thence North along the East line of said Section 26 a distance of 117.28 feet; thence Westerly 248.92 feet along a curve concave to the South having a radius of 1445.17 feet, the chord of said curve bears North 85 degrees 04 minutes 50 seconds West (assuming the East line of the Northeast Quarter of Section 26 as bearing North); thence South 89 degrees 59 minutes West along a line which is tangent to said curve a distance of 500.53 feet, more or less, to its intersection with a line drawn Northeasterly, at a right angle to said parallel line from the point of beginning; thence Southwesterly to the point of beginning; except that part of the following described tract lying within the boundaries of the above described land: Beginning at the most Westerly corner of Lot 7 in said Block 133; thence Southwesterly at right angles to the Southwesterly line of said Block 133 a distance of 40 feet; thence Northwesterly along a line parallel with the Southwesterly line of said Block 133; and its Northwesterly extension, a distance of 172.40 feet; thence Northeasterly at right angles a distance of 24.34 feet; thence Easterly deflecting to the right 72 degrees 22 minutes, a distance of 51.70 feet to the Southwesterly line of said Block 133; thence Southeasterly along the Southwesterly line of said Block 133 to the point of beginning of said exception; which lies Westerly of the Southeasterly line of said Block 133 and its extensions.

RECOMMENDATIONS

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt staff findings for the applications by Riverdale Ventures, LLC for the properties located at 1220 South 1st Street and 121 12th Avenue South:

A. Rezoning.

Recommended motion: **Approve** the application to rezone the properties from the I1 Light Industrial District to the C3A Commercial Activity Center District and remove the IL Industrial Living Overlay District.

B. Conditional Use Permit.

Recommended motion: **Approve** the application for a planned unit development containing 374 dwelling units, subject to the following conditions:

1. The conditional use permit shall be recorded with Hennepin County as required by Minn. Stat. 462.3595, subd. 4 before building permits may be issued or before the use or activity requiring a conditional use permit may commence. Unless extended by the zoning administrator, the conditional use permit shall expire if it is not recorded within two years of approval.
2. As required by section 527.120 of the zoning code, the development shall comply with the standards for outdoor open space, an outdoor children's play area, decorative or pervious surface for on-site parking and loading areas, drives, driveways, and walkways, a pet exercise area and a recycling storage area totaling a minimum of 15 points
3. Additional coniferous trees shall be planted on the north and west sides of the outdoor open space to help protect the area from wind.
4. Shrubs and perennials shall be planted in the outdoor open space to provide seasonal interest.
5. The developer is encouraged to provide staircases and walkways from the first floor dwelling units on the west side of the building to the outdoor open space.
6. The outdoor children's play area shall be 4,350 square feet in size.
7. For security purposes, decorative fencing and additional bollard style lights shall be installed around the perimeter of the outdoor children's play area.
8. At least one trash and recycling receptacle shall be installed near the outdoor children's play area.
9. Coniferous trees shall be planted around the perimeter of the outdoor children's play area.
10. At least one trash receptacle shall be installed near the pet exercise area.
11. A walkway shall be constructed between the outdoor children's play area and the pet exercise area.

C. Site Plan Review.

Recommended motion: **Approve** the site plan review application for a planned unit development containing 374 dwelling units, subject to the following conditions:

1. All site improvements shall be completed by July 1, 2018, unless extended by the Zoning Administrator, or the permit may be revoked for non-compliance.
2. CPED staff shall review and approve the final site, elevation, landscaping, and lighting plans before building permits may be issued.

3. There shall be no more than three exterior materials used, excluding windows, doors, and foundation materials, on any side of the building.
4. The minimum number of shrubs required by section 530.160 (175 shrubs) shall be met on the site.
5. The rooftop mechanical equipment next to the stair overruns shall be screened with either a screen wall or painted to match the color of the stair overruns.
6. All other rooftop mechanical equipment shall be screened per the requirements of Chapter 535, Regulations of General Applicability.
7. The generators and transformers shall be screened with a metal screen wall or additional landscape materials shall be planted along the west property line to fully screen the mechanical equipment from the public right-of-way.
8. The final lighting plan shall conform to the standards of Chapter 535, Regulations of General Applicability.
9. Landscaping shall be provided between the driveways of the porte cochere.
10. The developer is encouraged to work with Xcel to bury the overhead electric lines on 13th Avenue South.
11. The green screens on the ground floor of the north and west sides of the building shall be spaced no more than 25 feet from one another.

D. Preliminary Plat.

Recommended motion: **Approve** the Preliminary Plat.

ATTACHMENTS

1. Project summaries
2. Written findings submitted by applicant
3. Shadow studies and context study
4. PUD information
5. Subdivision materials
6. TDMP
7. Zoning map
8. Architectural plans
9. Landscape plan
10. Site survey
11. Civil plans
12. Electrical plans
13. Mechanical equipment screen wall details
14. Balcony railing details
15. Garage door details
16. Renderings
17. Photos
18. Correspondence

OERTEL ARCHITECTS

Date: April 8, 2016

TO: Hilary Dvorak
Minneapolis Principal City Planner

FROM: Deb Brandwick
Oertel Architects

RE: Legacy

PROJECT SUMMARY

Developer: Riverdale Ventures, LLC Inc. is proposing to construct its tenth condominium project in downtown Minneapolis. The first six are located in the North Loop (Lindsay Lofts, Rock Island Lofts, 212 Lofts, Security Warehouse Lofts, 5th Avenue Lofts and Riverwalk), one in Downtown East (Portland Tower) and two previous in the Mill City District (Bridgewater and Stonebridge)

Location: The site is on the corner of 13th Avenue and 2nd Street S. The site is currently occupied by the now empty Cenveo building and a 148 stall surface parking lot. The site contains approximately 186,121 s.f. (4.273 acres). There are also power lines (12 total) along the 13th Avenue property line that may impact the development.

Zoning: The parcels are zoned I1 with DH, DP and IL overlays. The developer will be requesting to rezone to C3A

Building Description: The proposed project is a residential project including 374 condominium units in twelve (14) stories with the units in the building as follows:

First Floor:	24 Units
Second Floor:	26 Units
Third-eighth Floors:	33 Units/Floor
Ninth-11th Floors	22 Units/Floor
12th-14th Floors:	16 Units/Floor
15th-17th Floors	5 Units/Floor
Total Units:	**374 Units**

No commercial use is proposed. Although the unit floor plans have not been finalized the units will range in size from approximately 950 to 3,100 square feet. The exterior of the project is proposed to be stone, glass, metal panel, phenolic (wood grain) panel and fiber cement composite panels and will compliment both the Bridgewater and Stonebridge projects in scale and design.

Building Details: The allowable Floor Area Ratio is 4.0. The site contains 186,121 square feet and the proposed project includes 725,326 square feet of above grade square footage resulting in a FAR of the proposed project of 4.010.

Amenities: The proposed project includes a playground, pet exercise area, pet grooming area, pool and spa, bike racks, a community room, game room, green roof, lawn bowling and an exercise room.

Parking: The proposed project includes three (3) levels of underground parking with a total of 703 parking stalls (17 guest stalls (only 8 allowed in calculation), 14 handicap stalls and 681 residential stalls). As part of the application for this property, Shamrock Development will be requesting an increase in the allowable parking ratio from 1.7 to 1.82.

Applications Required:

Rezoning from I1 to C3A. Remove IL Overlay District

CUP to increase height from 84' to 187', requested within the PUD application

Parking allowance to increase from 1.7 to 1.82 parking ratio, requested within the PUD application

Increased setback along 13th Avenue, requested as alternative compliance with the PUD application

Site Plan Review

OERTEL ARCHITECTS

DATE: April 26, 2016
TO: Hilary Dvorak, Principal City Planner
City of Minneapolis
FROM: Deb Brandwick
RE: **Legacy**
13th Street & 2nd Avenue South
Minneapolis, MN 55415

The attached submittal is for the Legacy, a proposed condominium project at the intersection of 2nd Avenue and 13th Street. The project is proposed to include 374 condominium units with enclosed parking and no retail space. The existing site houses the Cenveo office/warehouse building and a surface parking lot.

Following are project details relating to zoning code requirements.

Zoning District (to be re-zoned from I1)– C3A
Overlay Districts - DH, DP, IL

F.A.R. Calculations

FAR per district = 4.0 downtown height overlay
20% increase for enclosed parking
FAR allowed = 4.8
Site Total = 186,121 s.f.
186,121* 4.8 = **893,380 s.f. total allowable**

Above Grade Proposed Square Footage (not including parking):

		Total	Parking	Condo
First Floor (Street Level)	99,930 s.f.	99,930	35,500	64,430
2nd Floor	56,452 s.f.	56,452		56,452
3rd-8th Floors	56,770 s.f.	340,620		340,620
9th-14th Floors	41,630 s.f.	249780		249,780
15th-17th Floors	12,174 s.f. / floor	36,522		36,522
Total Square Footage		**783,304 s.f.**	**35,500**	**747,804**

F.A.R. Proposed = 4.017 (747,804/186,121)

Below Grade Proposed Parking Square Footage

	Total	
P3	95,454	
P2	95,454	
P1	95,454	
	Total	**286,362 s.f.**

Height

Height Allowed – 6 Stories

Height Requested – 17 Stories (three sections, 17 stories, 8 stories, 14 stories)

Maximum Residential Units

186,124/ 400 = 465.3 units allowed

374 Units Proposed

Residential Unit Counts Proposed:

		Total
First Floor	24 units	21 units
Second Floor	26 units	26 units
Third-8th Floors	33 units / floor	198 units
9th-11th Floors	22units / floor	66 units
12th-14th Floors	16 units / floor	48 units
15th-17th Floors	5 units / floor	15 units
Total Residential Units Proposed		**374 Total**

Parking Requirements -

Residential

Min. one stall per unit = 374 stalls

Guest

One stall / 50 dwelling units

374/50 = 8 stalls

374 + 8 = 382 total minimum parking stalls required

Proposed Parking Stall Counts:

P3	218
P2	215
P1	191
L1 Resident Parking	59
L1 Guest parking	18
Total Parking	**704 Stalls**

701 total stalls - 8 guest stalls - 14 handicap stalls (2% of total), = 679 resident parking stalls
679/374 = 1.82 parking ratio.

Riverdale Ventures, LLC
3200 Main Street NW
Suite 300
Coon Rapids, MN 55448

April 26, 2016

Ms. Hillary Dvorak
Planner
City of Minneapolis
250 South 4th Street
Room 300
Minneapolis, MN 55415

RE: Conditional Use Permit for PUD and Height
The Legacy

Dear Ms. Dvorak:

Please find listed below the required findings for a Conditional Use Permit for The Legacy.

1. The establishment of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare. The project is an upscale residential project and will improve the safety of the area by adding additional residents.
2. The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district. Additional residential units in the area will enhance the neighborhood with more pedestrian activity and add additional customers to support the surrounding commercial businesses.
3. The Legacy is a new construction building that has adequate utilities, access roads, drainage and necessary facilities for the project.
4. The Legacy is a new construction building that took adequate measures to minimize traffic congestion in the public streets. In addition, all residential and guest parking will be included in enclosed parking within the building (Please see Traffic Demand Management Study).
5. The conditional use is consistent with the applicable policies of the comprehensive plan.
6. The conditional use shall, in all other respects, conform to the applicable regulations of the district.

In addition, please find listed below the additional findings regarding the increase in height.

1. Generally the properties that surround The Legacy are 1-2 story office buildings with the exception of Stonebridge which is a 12-story residential building. The Legacy will not have an impact on the light and air of other surrounding properties. (Please see Shadow Study).
2. The two residential properties that are in the area include Stonebridge and Bridgewater and the public space in the area is Gold Medal Park. The Legacy will only have shadowing on other residential properties and/or public spaces that is different than what would have occurred with a six (6) story building in two cases. The Legacy will have no additional shadowing from the increase of height from six (6) stories to seventeen (17) stories on Stonebridge. The Legacy will have minimal shadowing on the NE corner of Bridgewater during the early morning in the fall (Shadow Study – September AM). The Legacy will have additional shadowing of Gold Medal Park but only near sunset during the winter (Shadow Study – December PM).
3. The Legacy is comparable to both Bridgewater and Stonebridge which are located at 215 10th Avenue South and 1120 2nd Street South, respectively.
4. The increased height of The Legacy from six (6) stories to seventeen (17) will have no impact on the preservation of views of landmark buildings, significant open spaces or water bodies. Bridgewater and Stonebridge will retain all of the views of Gold Medal Park, the Guthrie, the Mississippi River and the Stone Arch Bridge. The views for all of the other buildings in the area would be blocked by the allowable six (6) stories.

Please feel free to contact me if you have any questions.

Sincerely yours,

Lynn Leegard

Lynn M. Leegard
Vice President

April 8, 2016

Ms. Hilary Dvorak
Planner
City of Minneapolis
250 South 4th Street
Room 300
Minneapolis, MN 55415

RE: Findings Required for PUD
The Legacy

Dear Ms. Dvorak:

Please find listed below the required findings for approval of a Planned Unit Development ("PUD") for The Legacy.

1. The PUD complies with all of the requirements and the intent and purpose of Chapter 527.
    a. The character of uses in the PUD is residential. The project will include a building with varied heights ranging from one (1) to seventeen (17) stories with 374 condominium units and one (1) level of at-grade enclosed parking and three (3) levels of underground parking. The project is similar in nature to Bridgewater and Stonebridge which are located 215 10th Avenue South and 1120 2nd Street South, respectively. The developer of The Legacy is the same developer as both Bridgewater and Stonebridge. In addition, the area has become very neighborhood-like with the addition of Gold Medal Park and all of the walking and biking paths in the area.
    b. The Legacy is a new construction project which provides vehicle access for both owners and guests to the at-grade enclosed parking and underground parking from 13th Avenue South. Pedestrian access to the building will be along 2nd Street South via a 6' sidewalk. In addition, the building will include 374 bike racks for owners that are located in the underground parking and a guest bike rack to accommodate four (4) bikes that is located in the guest parking area. The Legacy also includes a porte cochere off 2nd Street South for resident pick-up and drop-off and other deliveries to the building. For additional information, please see the Traffic Demand Management Study (TDM).
    c. The Legacy has been specifically designed to provide substantially more than required open space which includes two (2) rain gardens, playground

and dog park area. In addition, the building includes an enhanced landscape yard for the walk-up units located along 13th Avenue South.

d. The Legacy includes only one (1) building and all of the parking for both owners and guests is located inside the building in either the at-grade interior parking or the three (3) levels of underground parking and is all out of sight. The building footprint is equal to 98,600 square feet and sits on a site that has a total of 186,121 square feet resulting in 34% of the site proposed for green space. The tallest section of the building is located at the southerly corner of the property to provide for an anchor to the end of the Mill District. The design of the building includes multiple heights to break up the massing of the building on the site.
e. The Legacy is located next to Stonebridge and the design includes a fully landscaped green space that separates the two buildings.
f. The Legacy is located approximately one (1) block east of Gold Medal Park and approximately one (1) block from the Mississippi River. The site will include two (2) rain gardens, sufficient landscaping and adequate design and development to properly address stormwater runoff. For additional information, please see Civil Engineering Plans.
g. The design team used LEED principals in the design of The Legacy as a basis for design however due to the expense of high application fees The Legacy will not be officially LEED certified. Examples of such design principals used are rain gardens, pervious pavement, green roof, low maintenance landscaping, high efficiency boiler system, tinted low-e glass with darker tint at the southern façade, tall windows to harvest light deep into the units, regional materials such as concrete and stone and average roof insulation at non-green roof is 50% more than the code minimum.

2. The Planned Unit Development complies with all of the applicable requirements contained in Chapter 598, Land Subdivision Regulations.

Please feel free to contact me if you have any questions

Sincerely,

Lynn M. Leegard
Vice President

RED CROSS
OFFICE BUILDING
UNIVERSITY OF MINNESOTA OFFICE BUILDING
GOLD MEDAL PARK
STONEBRIDGE
BRIDGEWATER
OFFICE BUILDING
OFFICE BUILDINGS
13th Ave S
14th Ave S
9th Ave S
10th Ave S
2nd St S
S 12th Ave
Washington Avenue South
the LEGACY
SUN SHADOW STUDY
JUNE 21 - 6:26 AM

the LEGACY
SUN SHADOW STUDY
JUNE 21 - NOON

the LEGACY
SUN SHADOW STUDY
JUNE 21 - 8:03 PM

the LEGACY
SUN SHADOW STUDY
SEPTEMBER 20 - 758 AM

the LEGACY
SUN SHADOW STUDY
SEPTEMBER 20 - NOON

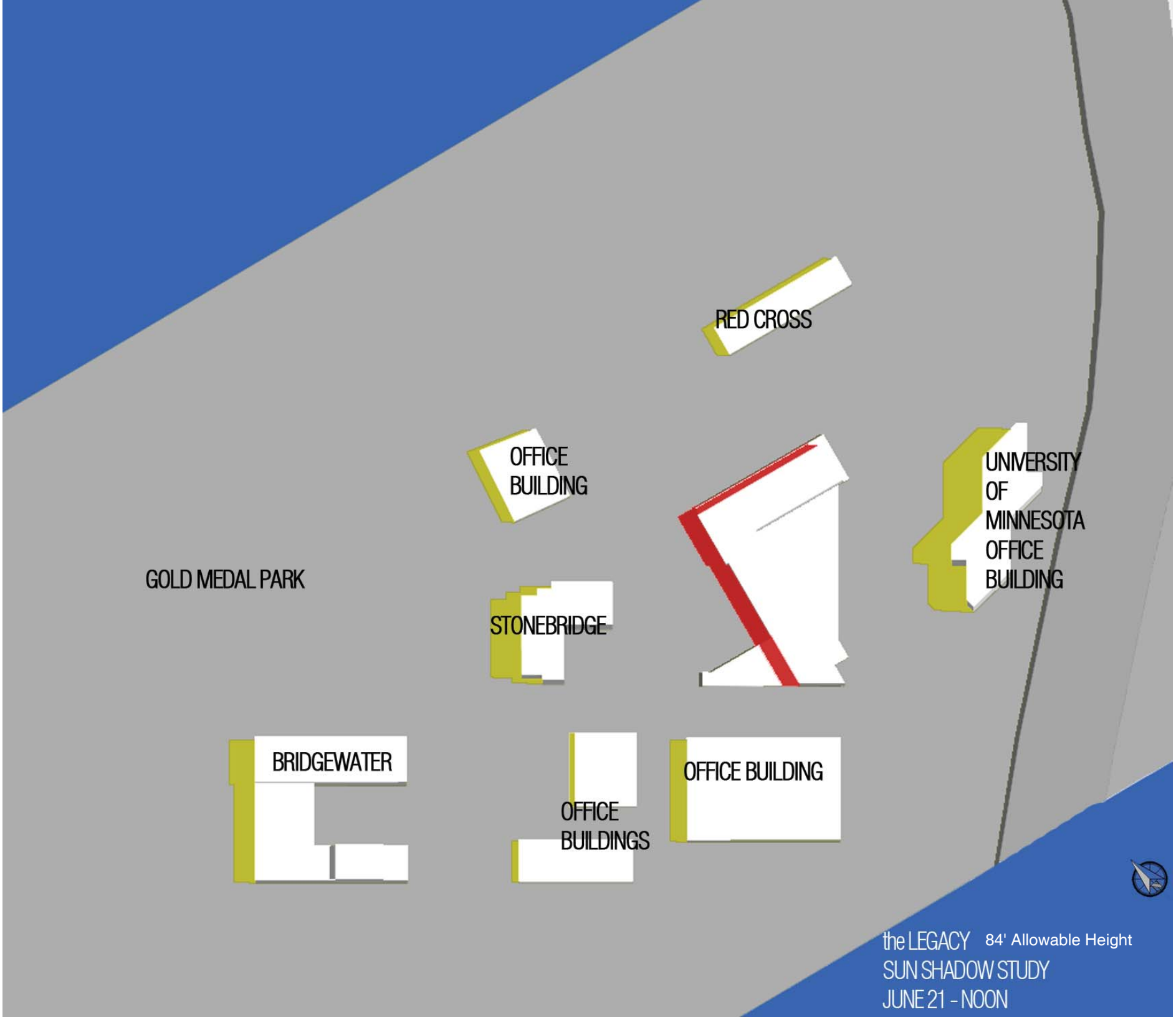
the LEGACY
SUN SHADOW STUDY
SEPTEMBER 20 - 614PM

RED CROSS
OFFICE BUILDING
UNIVERSITY OF MINNESOTA OFFICE BUILDING
GOLD MEDAL PARK
STONEBRIDGE
BRIDGEWATER
OFFICE BUILDINGS
OFFICE BUILDING
2nd St S
Washington Avenue South
13th Ave S
14th Ave S
9th Ave S
S 12th Ave
the LEGACY
SUN SHADOW STUDY
DECEMBER 22 - 848 PM

the LEGACY
SUN SHADOW STUDY
DECEMBER 22 - NOON

the LEGACY
SUN SHADOW STUDY
DECEMBER 22 - 334PM

RED CROSS
OFFICE BUILDING
UNIVERSITY OF MINNESOTA OFFICE BUILDING
GOLD MEDAL PARK
STONEBRIDGE
BRIDGEWATER
OFFICE BUILDING
OFFICE BUILDINGS
the LEGACY
84' Allowable Height
SUN SHADOW STUDY
JUNE 21 - 6:26 AM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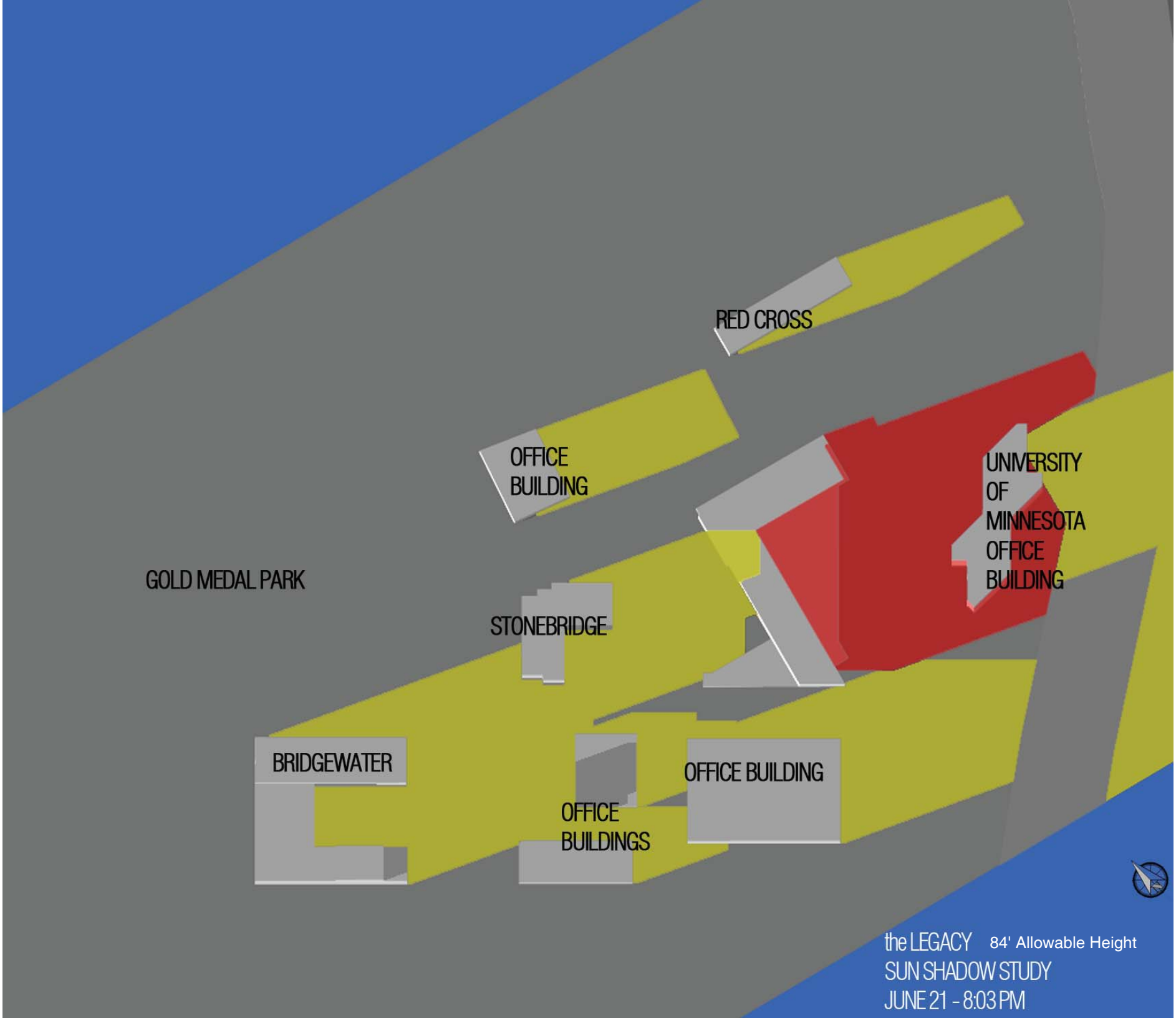
SUN SHADOW STUDY
JUNE 21 - NOON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JUNE 21 - 8:03 PM

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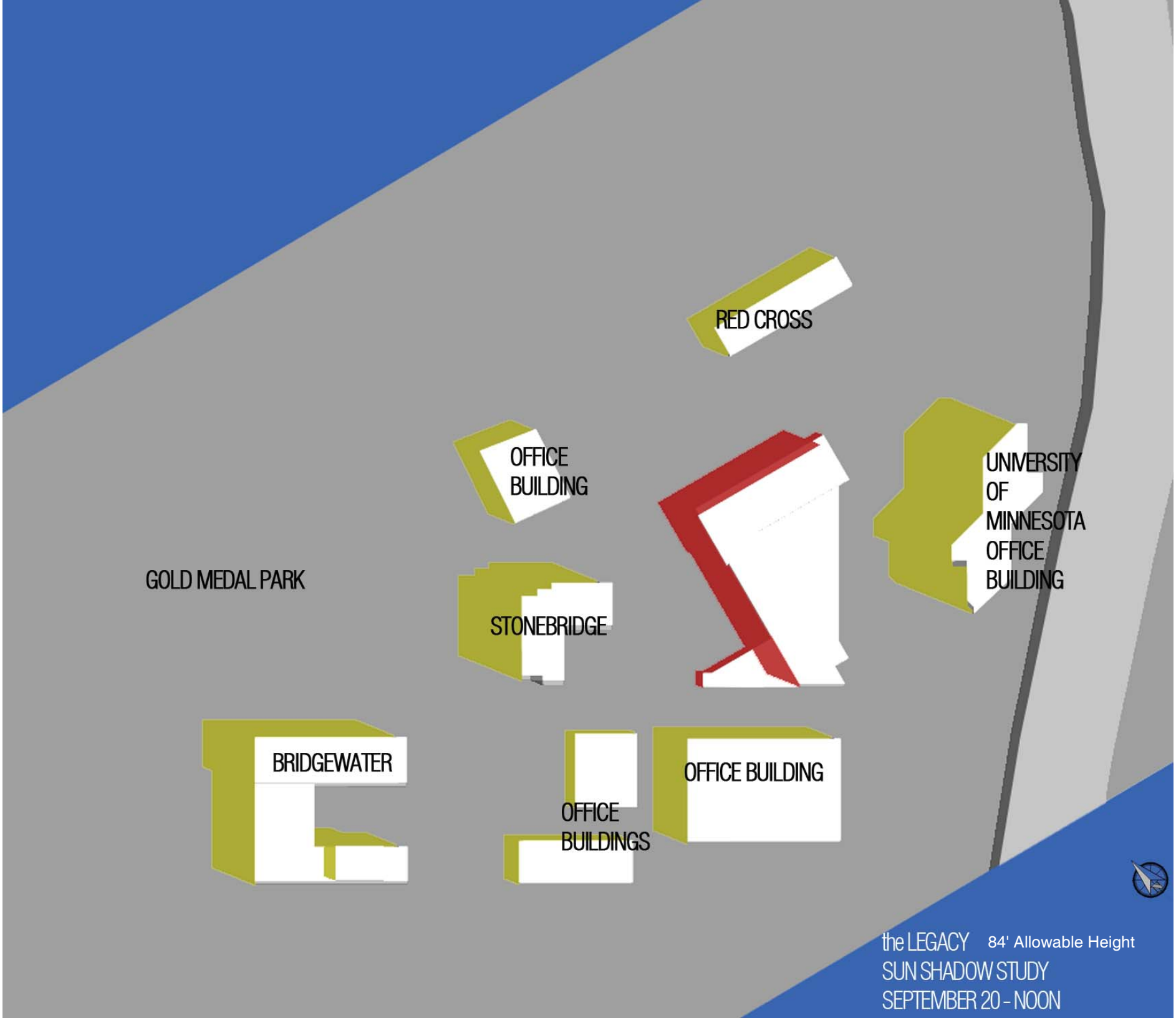
SUN SHADOW STUDY
SEPTEMBER 20 - 758 AM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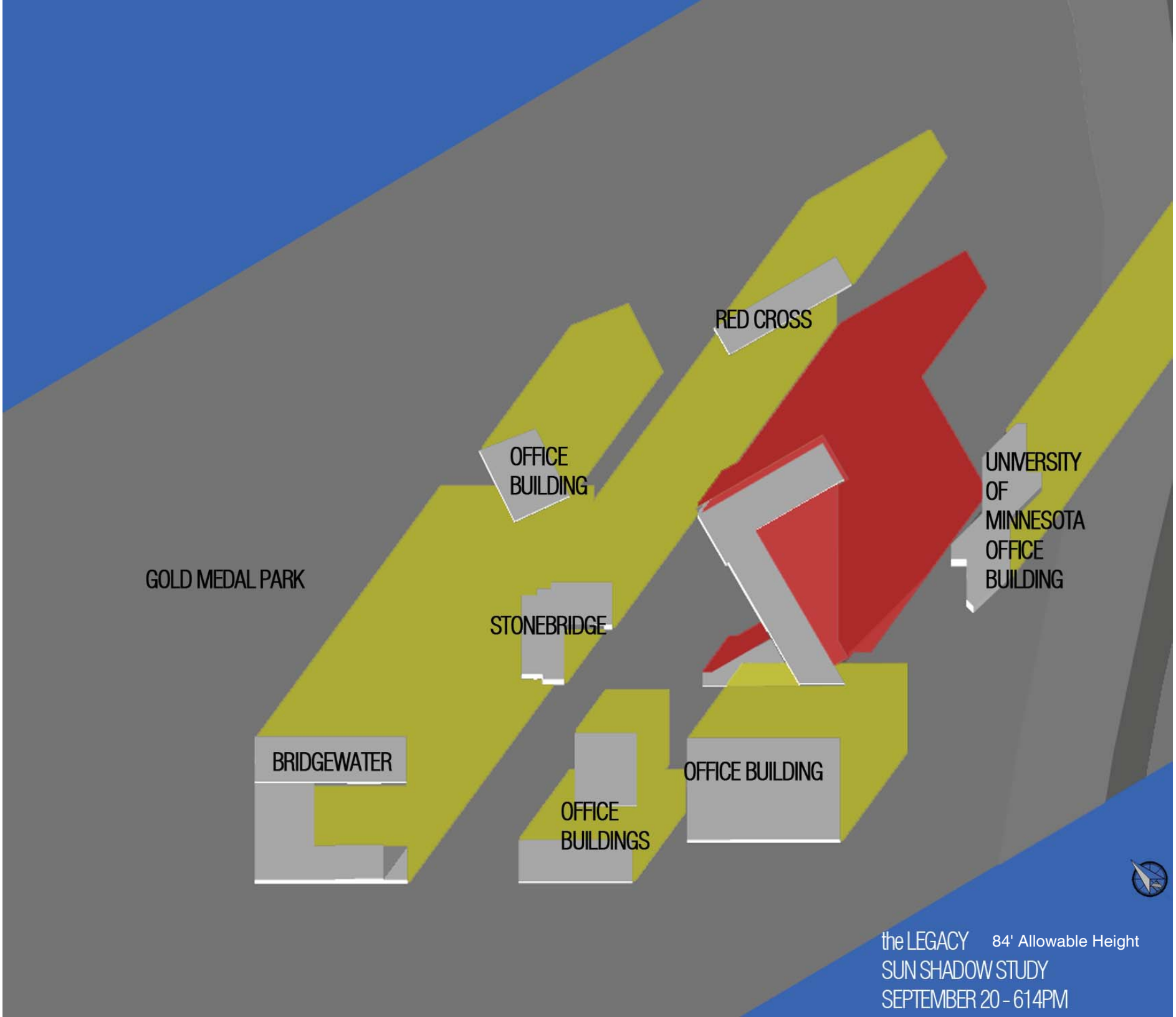
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SEPTEMBER 20 - 614PM

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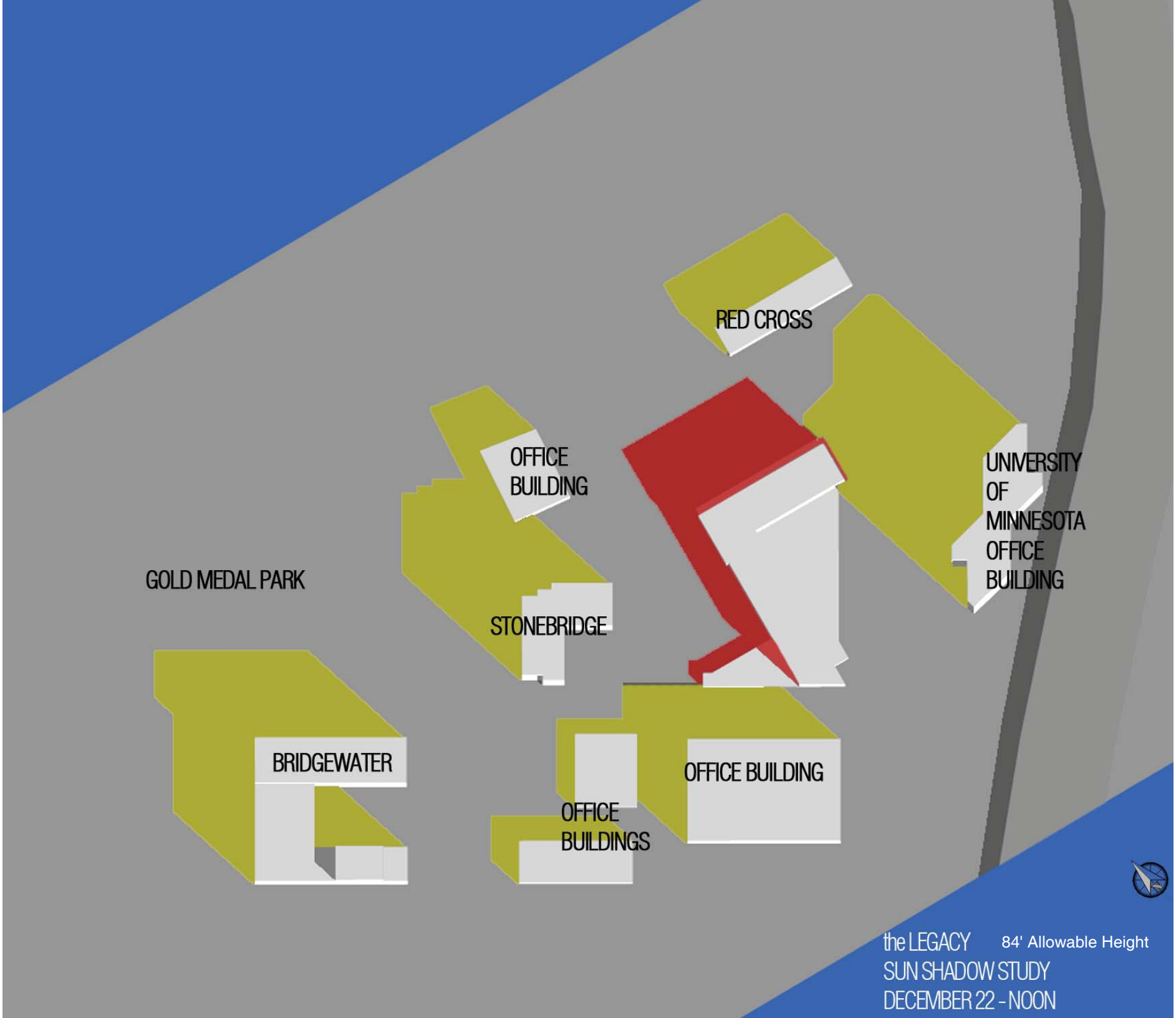
DECEMBER 22 - 848 PM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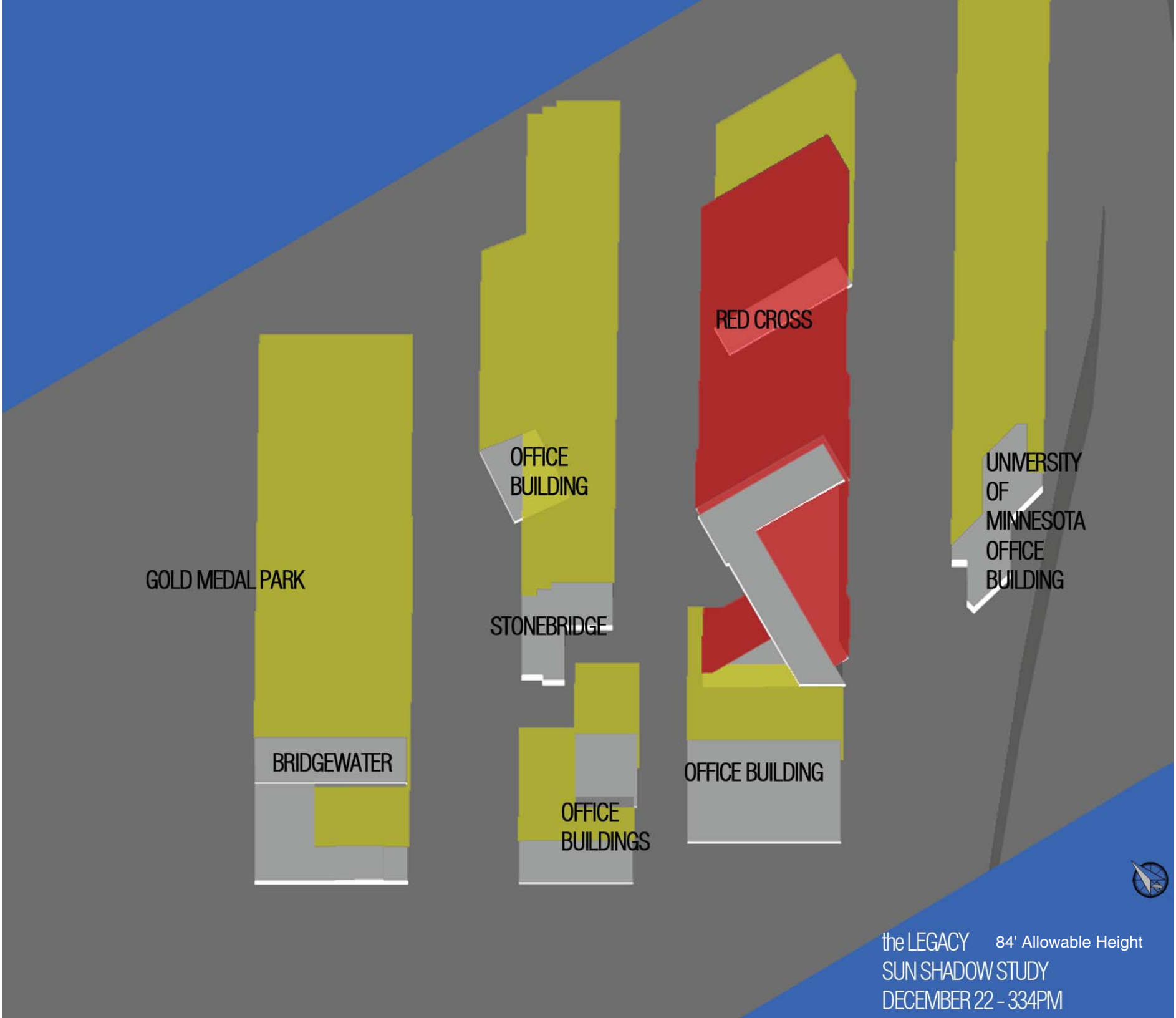
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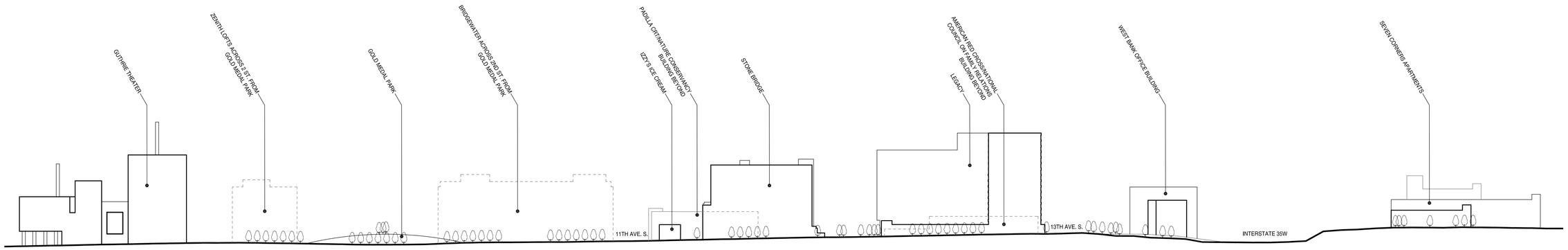


1 SITE SECTION
FROM 2ND AVE, LOOKING NORTH
1" = 80'-0"

OERTEL ARCHITECTS
1795 Saint Clair Avenue
St. Paul, Minnesota 55105
(651) 696-5186 Tel
(651) 696-5188 Fax
www.oertelarchitects.com

PROJECT NAME:

LEGACY

2nd St. & 13th Ave.
Mpls, MN 55415

PLANNING COMMISSION
4.8.16

PDR REVIEW
4.18.16

PROJECT NUMBER: 15–03

DATE OF ISSUE: xxx, 2016

DRAWN BY: DLB

CHECKED BY: JLO

REVISIONS:

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA

JEFFREY L. OERTEL

SIGNATURE

DATE xxx, 2016

LICENSE NUMBER 15840

SHEET NAME:

SITE SECTION

SHEET NO:

OERTEL ARCHITECTS

Date: 5.13.16
To: Hillary Dvorak
From: Deb Brandwick
RE: Legacy - PUD Requirements

PUD Requirements

Base, 10 points

Outdoor Childrens Play Area (50 s.f. per three bedroom unit, min 500 s.f., max. 5000 s.f.)
-- 5 points

87 three bedroom units X 50 s.f. = 4,350 s.f. minimum play area.
The play area is located to the NW of the building, away from streets and drive lanes. Added deciduous trees will provide shade in the summer, while coniferous trees will provide a wind break in the winter months. Benches will be provided around the play area for adult supervision. There is a restroom provided on the first floor for use by residents and guests.

Outdoor Open Space (186,121 – 98,857*30% = 26,180 s.f. min. 39,000 contiguous s.f. provided,
– 5 points

The green space is located to the NW of the building, away from streets and drive lanes. The area will include a playground, dog run and rain garden area. A sidewalk area is provided to connect the spaces. Paving within the site is to be colored concrete – see site plan.
Additionally, a rain garden is proposed to the NE of the building and landscaped yards provided to the front of the walk-up units along 13th. A sculpture (design t.b.d.) is proposed in front of the porte cochere area. As we are not yet certain of the value of the sculpture, we are not pursuing the Art Feature credit. These spaces are not included in the square footage calculation.

Request for additional height, 5 points

Pervious/decorative surface drive lanes (75% of paved surfaces required)
-- 3 points

The three main drives and associated sidewalks to be colored concrete, two colors to be used. See site plan for locations.
8,065 s.f. colored concrete paving
10,200 s.f. total paving.
8,065/10,200 = 79%

Pet Exercise Area, (minimum 12'x60', 25'x68' provided) - 1 point

An aluminum fenced pet exercise/relief area is proposed immediately outside of the north exit door. The one story section of building will screen the southern sun. Trees to the north will provide a wind break. Benches will be provided for pet owner convenience.

Recycling Area - 1 point

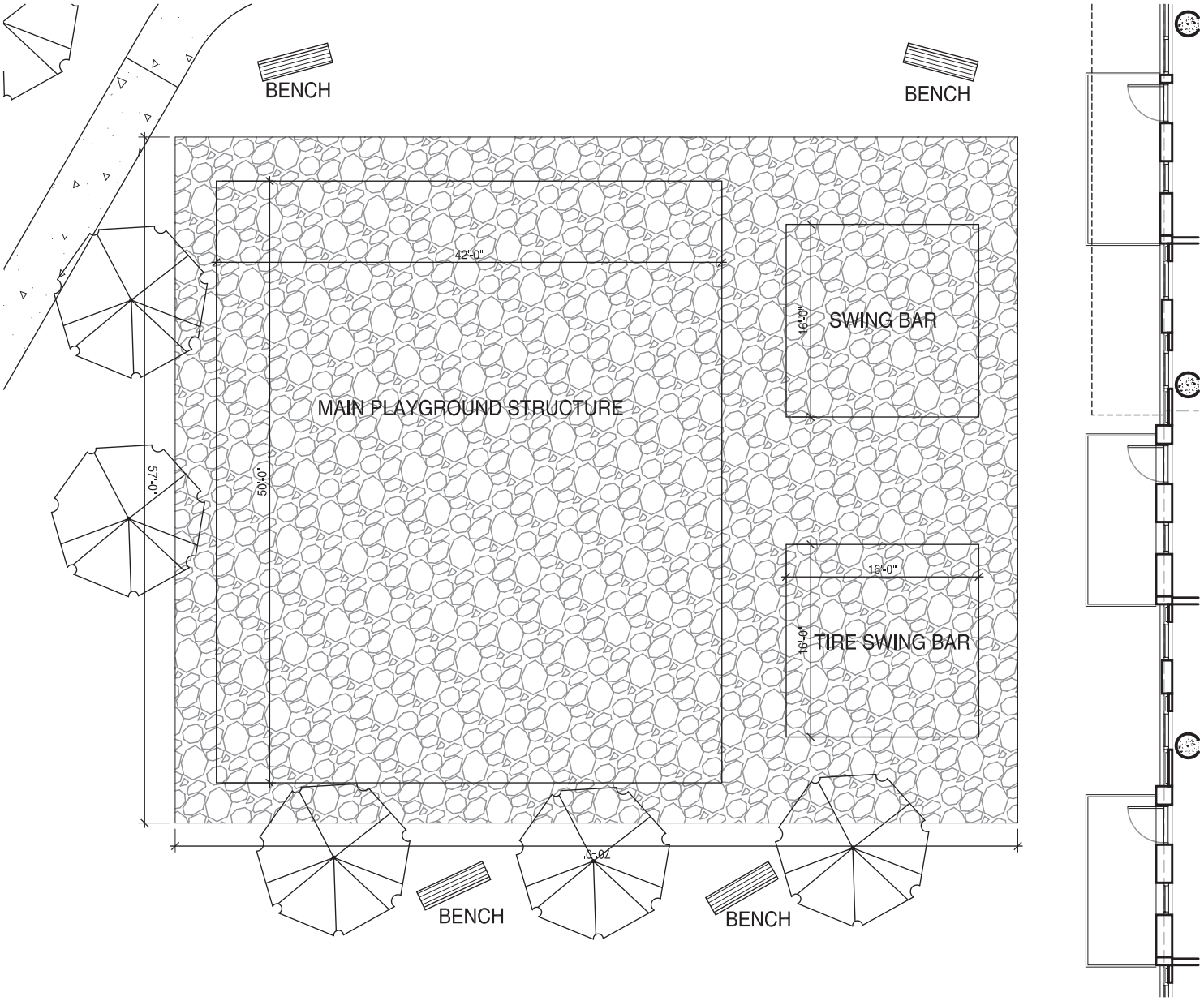
Two chutes are provided at each trash room, one for trash and one for recycling. All dumpsters, including recycling, will be kept within the building at all times.

Additional items provided, though not to the full extent of the credit requirement:

Green Roof - provided at the pool deck, and larger east roof deck, though less than 50% of total roof area.
Art Feature – a sculpture at the south corner will be provided, along with art in the corridor along 2nd street, visible from the public sidewalk. Values have not yet been determined to allow pursuing this credit.
Energy Efficiency – we are working with the Weidt Group to provide energy design assistance. We do not yet know if we will achieve a 30% reduction in energy use.
Living Wall System – sections of living wall will be provided at the north wall of the first parking level, though greatly less than 60% of the total wall area of the elevation.
Enhanced Stormwater Management – Two rain garden area are proposed, though not to the scale to accommodate all rainwater from the site. Below ground storage tanks will still be required.

Thank you,

Deb Brandwick
Oertel Architects
651.696.5186, ext. 315
dbrandwick@oertelarchitects.com


BENCH
BENCH
42'-0"
SWING BAR
16'-0"
MAIN PLAYGROUND STRUCTURE
57'-0"
50'-0"
16'-0"
16'-0"
TIRE SWING BAR
70'-0"
BENCH
BENCH

ZOOM + | START SLIDESHOW

Number of Play Activities:	34
Ages:	2-12
Child Capacity:	44-50 kids
Actual Unit Size:	38 ft L x 30.5 ft W
Actual Unit Height:	13.5 ft
Minimum Play Area:	50 ft L x 42.5 ft W
Overall Unit Weight:	6512 LBS
ADA Accessible	Yes

Playgrounds should be supervised at all times when children are present. Installation over a hard surface such as concrete, asphalt, or packed earth may result in serious injury from falls. Minimum use zones (minimum play area) require impact absorbing materials that meet CPSC guidelines and ASTM standards.

Rainbow Systems Swing Beam

Rainbow Systems Tire Swing Beam

NOTES:
STREET LIGHTING INSTALLED AS PART OF THE PROJECT SHALL BE INSPECTED BY THE CITY. CONTRACTORS SHALL ARRANGE FOR INSPECTIONS WITH THE TRAFFIC DEPARTMENT. PLEASE CONTACT DAVE PREHALL AT 612.673.5759 FOR FURTHER INFORMATION. ANY LIGHTING INSTALLATIONS NOT MEETING CITY SPECIFICATIONS WILL BE REQUIRED TO BE REINSTALLED AT OWNER EXPENSE.
AN OBSTRUCTION PERMIT IS REQUIRED ANYTIME CONSTRUCTION WORK IS PERFORMED IN THE PUBLIC RIGHT-OF-WAY. PLEASE CONTACT SCOTT KRAMER AT 612.673.2383 REGARDING DETAILS OF SIDEWALK AND LANE CLOSURES. LOG ON TO HTTP://MINNEAPOLIS.MN.ROWAY.NET/ FOR A PERMIT.
CONTACT ALLAN KLUGMAN AT 612.673.2743 PRIOR TO CONSTRUCTION FOR THE TEMPORARY REMOVAL/TEMPORARY RELOCATION OF ANY CITY OF MINNEAPOLIS SIGNAL SYSTEM THAT MAY BE IN THE WAY OF CONSTRUCTION.
ALL COSTS FOR RELOCATION AND/OR REPAIR OF CITY TRAFFIC FACILITIES SHALL BE BORNE BY THE CONTRACTOR AND/OR PROPERTY OWNER.
CONTACT DOUG MADAY AT 612.673.5755 PRIOR TO CONSTRUCTION FOR THE REMOVAL OF ANY CITY OF MINNEAPOLIS RIGHT OF WAY SIGNS THAT MAY BE IN THE WAY OF CONSTRUCTION.
THE CONTRACTOR, PROPERTY OWNER OR RESPONSIBLE PARTY SHALL CONTACT MINNEAPOLIS SURFACE WATERS AND SEWER 48 HOURS PRIOR TO ANY EXCAVATION OR CONSTUCTION RELATED TO OR IN THE LOCATION OF THE PROPOSED STORMWATER MANAGEMENT MBP.

UPON THE PROJECT'S COMPLETION, THE GENERAL CONTRACTOR, PROPERTY OWNER OR RESPONSIBLE PARTY SHALL PROVIDE TO THE DEPARTMENT OF PUBLIC WORKS A FINAL STORMWATER MANAGEMENT REPORT INCLUDING RECORD DRAWINGS. THIS REPORT WILL SERVE AS A MEANS OF VERIFICATION THAT THE INTENT OF THE APPROVED STORMWATER MANAGEMENT DESIGN HAS BEEN MET. THIS FINAL REPORT SHALL SUBSTANTIATE THAT ALL ASPECTS OF THE ORIGINAL DESIGN HAVE BEEN ADEQUATELY PROVIDED FOR BY THE CONSTRUCTION OF THE PROJECT.

1 SITE PLAN
1" = 20'-0"

OERTEL ARCHITECTS
1795 Saint Clair Avenue
St. Paul, Minnesota 55105
(651) 696-5186 Tel
(651) 696-5188 Fax
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PROJECT NAME:
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PLANNING COMMISSION
4.8.16

PDR REVIEW
4.18.16

PROJECT NUMBER: 15-03
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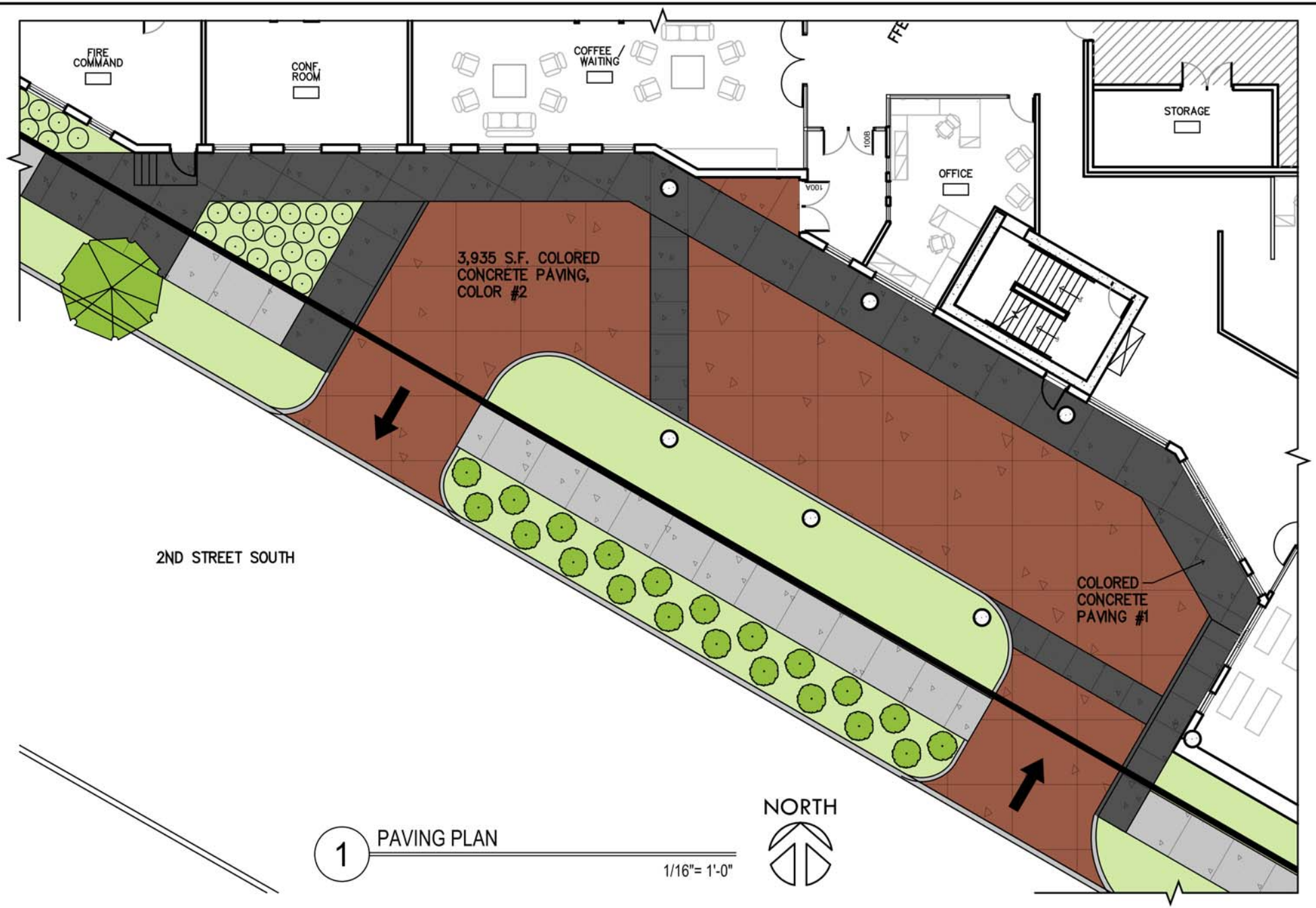
JEFFREY L. OERTEL
SIGNATURE
DATE xxx, 2016
LICENSE NUMBER 15840

SHEET NAME:
SITE PLAN

SHEET NO:
A0.0



1 PAVING PLAN

1/16"= 1'-0"

OERTEL ARCHITECTS
1795 SAINT CLAIR AVE. / ST. PAUL, MN 55105
(651) 696-5186 / (651) 696-5188 FAX
www.oertelarchitects.com

REVISED:

I HEREBY CERTIFY THAT THIS DOCUMENT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED ARCHITECT IN THE STATE OF MINNESOTA.

SIGNATURE _____

DATE _____

REGISTRATION _____

LEGACY

2ND STREET & 13TH AVENUE SOUTH
MINNEAPOLIS, MN

SHEET NO.

A1.12

5059 Sorrento Red

C-24 Charcoal

Decorative Pavement, style as shown in photo immediately above, colors as shown in swatches further above

April 8, 2016

Ms. Hilary Dvorak
Planner
City of Minneapolis
250 South 4th Street
Room 300
Minneapolis, MN 55415

RE: Alternative Compliance for increased Setback along 13th Avenue
The Legacy

Dear Ms. Dvorak:

Please find listed below the required findings for a alternative compliance for the increased setback along 13th Avenue at The Legacy.

1. To address any adverse effects of the increased setback along 13th Avenue, we have added five walk-up units each with a front patio area that will provide activity at the street level. The public sidewalk has been shifted inside the property line and widened to 6' with a lawn boulevard between the sidewalk and curb. We also added landscaping in the remaining setback area between the sidewalk and building.
2. The powerlines along the property line (15 lines +/-) make construction close to the property line impractical, and potentially dangerous for the roof deck area to become in proximity to the power lines. The proposed design described above meets the code intent of providing an active street life with eyes on the street.
3. The proposed alternative is consistent with applicable development plans or development objectives adopted by the city council and meets the intent of this chapter.

Please feel free to contact me if you have any questions.

Sincerely yours,

Lynn M. Leegard
Vice President

Riverdale Ventures, LLC
3200 Main Street NW
Suite 300
Coon Rapids, MN 55448
763.421.3500

April 8, 2016

Ms. Hilary Dvorak
Planner
City of Minneapolis
250 South 4th Street
Room 300
Minneapolis, MN 55415

RE: Subdivision Application (Plat)
The Legacy
121 12th Avenue South, Minneapolis
1220 1st Street South, Minneapolis

Dear Ms. Dvorak:

Please find listed below the required findings for a subdivision for the property located as referenced above.

1. The subdivision is in conformance with the land subdivision regulations, the applicable regulations of the zoning ordinance and policies of the comprehensive plan.

2. The subdivision will not be injurious to the use and enjoyment of other property in the immediate vicinity, nor be detrimental to present and potential surrounding land uses, nor add substantially to congestion in the public streets.

3. All land intended for building sites can be used safely without endangering the residents or users of the subdivision or the surrounding area because of flooding, erosion, high water table, soil conditions, improper drainage, steep slopes, rock formations, utility easements or other hazard.

4. The lot arrangement is such that there will be no foreseeable difficulties, for reasons of topography or other conditions, in securing building permits and in providing driveway access to building on such lots from an approved street. Each lot created through subdivision is suitable in its natural state for the proposed use with minimal alteration.

5. The subdivision makes adequate provision for stormwater runoff, and temporary and permanent erosion control in accordance with the rules, regulations and standards of the city engineer and the requirements of these land subdivision

regulations. To the extent practicable, the amount of stormwater runoff from the site after development will not exceed the amount occurring prior to development.

Please feel free to contact me if you have any questions.

Sincerely yours,

Lynn M. Leegard

Lynn M. Leegard
Vice President

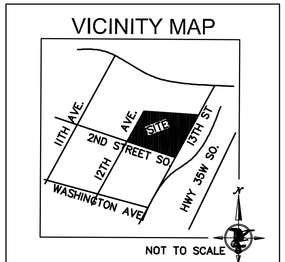
FILE: P:\2016\16-016 - The Legacy\Dwg\16-016 - The Legacy - Pre-Plat.dwg LAYOUT: 22x34-Surv
SCALE: 0 DATE/TIME: Mar 21, 2016 - 3:43pm
USER: Rory XREF(s): 16-016 - The Legacy.dwg HSJohnson Convert.dwg HC-Section1.DWG Hennepin County GIS Parcels-9-14-15.dwg

Linetype & Symbol Legend

- FIBER OPTIC
- GASMAIN
- WATERMAIN
- SANITARY SEWER
- STORM SEWER
- OVERHEAD UTILITIES
- TELEPHONE LINE
- ELECTRIC LINE
- CABLE LINE
- GUARDRAIL
- WOODEN FENCELINE
- CHAINLINK FENCELINE
- BARBED WIRE FENCE
- BLOCK RET WALL
- WATER MANHOLE
- WATER VALVE
- SET IRON MONUMENT
- UTILITY MANHOLE
- SIGN
- SANITARY MANHOLE
- STORM MANHOLE
- TELEPHONE BOX
- TELEPHONE MANHOLE
- ELECTRIC TRANSFORMER
- TRAFFIC SIGNAL
- TRAFFIC LIGHT
- CABLE TV BOX
- ELECTRICAL METER
- GAS METER
- WATER METER
- FOUND IRON MONUMENT
- POWER POLE
- AIR CONDITIONER
- BOLLARD
- SOIL BORING
- CATCH BASIN
- CB BEEHIVE
- CLEAN OUT
- ELECTRIC BOX
- ELECTRIC MANHOLE
- FLAG POLE
- FLARED END SECTION
- GAS VALVE
- HANDICAP SYMBOL
- HANDHOLE
- HYDRANT
- CAST IRON MONUMENT

40 20 0 20 40 80
SCALE IN FEET

VICINITY MAP



NOT TO SCALE

PRELIMINARY PLAT GENERAL NOTES

SURVEYOR:
HTPO
7510 Marketplace Drive
Eden Prairie, MN 55344
952-829-0700

OWNER/DEVELOPER:
Shamrock Development, Inc.
3200 Main Street NW, Suite 300
Coon Rapics, MN 55448
763.421.3500

LEGAL DESCRIPTION:

The real property situated in the City of Minneapolis, County of Hennepin, State of Minnesota, described as follows:

Parcel 1:

Lots 1, 2, 3, 4, 5, 6, 7, 8, 9 and 10, Block 115, Town of Minneapolis, according to the recorded plat thereof, Hennepin County, Minnesota.

That part of the Southerly one-half of vacated First Street South which lies between the extension Northeasterly of the Southeasterly lot line of Lot 6, Block 115, Town of Minneapolis, and the extension Northeasterly of the Northwesterly lot line of Lot 8 in said Block 115.

That part of the Southerly one-half of vacated First Street South which lies between the extension Northeasterly of the Northwesterly lot line of Lot 8, Block 115, Town of Minneapolis, and the extension Northeasterly of the center line of vacated 12th Avenue South.

That part of the Northerly one-half of vacated First Street South described as follows: Beginning at the Southwesterly corner of Lot 7, Block 133, Town of Minneapolis; thence Southerly at right angles to the North line of said First Street a distance of 40 feet; thence Westerly on a line parallel to the North line of said First Street a distance of 172.4 feet; thence Northerly on an angle to the right of 90 degrees a distance of 24.34 feet; thence Easterly on an angle to the right of 72 degrees 22 minutes a distance of 51.7 feet to the North line of said First Street; thence Easterly along the said North line of First Street a distance of 123.13 feet to the point of beginning.

That part of the Easterly one-half of vacated 12th Avenue South lying between the extension Northwesterly of the Southerly lot line of Lot 10, Block 115, Town of Minneapolis, and the extension Northwesterly of the Northerly lot line of said Lot 10.

(Abstract property)

Parcel 2:

That part of Block 133 and those parts of the vacated parts of 12th Avenue South, which was platted as Huy Street, First Street South, which was platted as 1st Street, and abandoned Bluff Street all in the Town of Minneapolis, described as follows: Beginning at a point on a line drawn parallel with and 40 feet Southwesterly from the Southwesterly line of said Block 133, and its Northwesterly extension, distant 172.40 feet Northwesterly along said parallel line from its intersection with a line drawn Southwesterly, at a right angle to the Southwesterly line of said Block 133, from the most Westerly corner of Lot 7, in said Block 133, thence Southeasterly along said parallel line a distance of 370.40 feet to its intersection with the Southwesterly extension of the Southeasterly line of said Block 133; thence Northeasterly to the most Southerly corner of said Block 133; thence Southeasterly to the most Westerly corner of said Block 132; thence Southeasterly along the Southwesterly line of said Block 132 to the most Southerly corner of Lot 3 in said Block 132; thence Northeasterly along the Southeasterly line of said Lot 3 to the most Easterly corner of Lot 3 in said Block 132; thence Southeasterly along the Southwesterly line of Lot 7 in said Block 132 a distance of 99 feet; thence at right angles Northeasterly to the East line of Section 26, Township 29, Range 24; thence North along the East line of said Section 26 a distance of 117.28 feet; thence Westerly 248.92 feet along a curve concave to the South having a radius of 1445.17 feet, the chord of said curve bears North 85 degrees 04 minutes 50 seconds West (assuming the East line of the Northeast Quarter of Section 26 as bearing North); thence South 89 degrees 59 minutes West along a line which is tangent to said curve a distance of 500.53 feet, more or less, to its intersection with a line drawn Northeasterly, at a right angle to said parallel line from the point of beginning; thence Southwesterly to the point of beginning; except that part of the following described tract lying within the boundaries of the above described land: Beginning at the most Westerly corner of Lot 7 in said Block 133; thence Southwesterly at right angles to the Southwesterly line of said Block 133 a distance of 40 feet; thence Northwesterly along a line parallel with the Southwesterly line of said Block 133; and its Northwesterly extension, a distance of 172.40 feet; thence Northeasterly at right angles a distance of 24.34 feet; thence Easterly deflecting to the right 72 degrees 22 minutes, a distance of 51.70 feet to the Southwesterly line of said Block 133; thence Southeasterly along the Southwesterly line of said Block 133 to the point of beginning of said exception; which lies Westerly of the Southeasterly line of said Block 133 and its extensions.

(Torrens property, Certificate of Title No. 1315861)

DATE OF PREPARATION:
3-18-2016

BENCHMARK:
Elevations are based on the NAVD88 datum.

FLOOD ZONE CLASSIFICATION:
This property is contained in Zone X (area determined to be outside the 0.2% annual chance floodplain) per Flood Insurance Rate Map, Community Panel No. 27053C0357E, effective date of September 2, 2004.

EXISTING ZONING:
The current Zoning for the subject property is I1 per the City of Minneapolis's zoning database found on their website on March 17, 2016.

AREA:
Total Property Area = 186,928 Sq.Ft. or 4.291 Acres

NOTE:
Please note that the existing conditions as shown hereon we provided by the client and were obtained from a survey prepared by Harry S. Johnson dated December 18, 2018

Preliminary Plat

The Legacy

HT PO Engineering • Surveying Landscape Architecture
HANSEN THORP PELLINEN OLSON, Inc.
7510 Market Place Drive • Eden Prairie, MN 55344
952-829-0700 • 952-829-7806 fax

Revision

Project No.	16-016
Drawn by	DRB
Checked by	RLS
Book/Page	
Date:	March 21, 2016
Client:	

Legacy Development Travel Demand Management Plan

Minneapolis, MN

Prepared By:
Stephen Smith
Alliant Engineering, Inc.
233 Park Avenue South, Suite 300
Minneapolis, MN 55415

Prepared For:

Riverdale Ventures, LLC.
3200 Main Street NW, Suite 300
Coon Rapids, MN 55448

ALLIANT
ENGINEERING

Final Report
May 13th, 2016

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1.0 Introduction

The Legacy development project (i.e. proposed project) is a residential condominium development located in the Downtown East Neighborhood of Minneapolis. The project is scheduled to be completed by late 2017. Upon full completion, the development will consist of 374 condominium units and three levels of enclosed parking with a total of 704 parking stalls.

The site is 186,121 s.f. in size and the building will be 748,000 s.f. resulting in a 17-story luxury condominium complex divided in three sections: 17 stories, 8 stories, and 14 stories. There will be amenity space within the building for the residents including a pet grooming area, community rooms, exercise space, a pool and roof terrace areas. The site also includes a dog run enclosure and a playground. The residents and guests will be provided with 704 total parking stalls. Of the 704 parking stalls, 18 parking stalls will be provided for guests and 686 will be for the residents.

As part of the proposed project, the City of Minneapolis has required a Travel Demand Management (TDM) Plan. The goal of the TDM plan is to accomplish the following:

- Document the existing and proposed conditions with respect to transit, parking, bicycle and pedestrian facilities;
- Document the parking impacts with the proposed project;
- Identify any loading operations issues for the different uses of the site;
- Identify any traffic operations issues; and
- Develop and implement measures to encourage non-single occupancy vehicle modes of transportation.

1.1 Site and Development Characteristics

Site Location

The proposed project site is located in the Downtown East Neighborhood, immediately adjacent to the Minneapolis downtown Central Business District (CBD). The proposed development is located north of South 2nd Street and south of West River Parkway, between 12th Avenue South and 13th Avenue South. **Figure 1** illustrates the project location.

Existing Site Characteristics

The site of the proposed project is on the corner of South 2nd Street and 13th Avenue South. Currently, the Cenveo building occupies the site. The existing building has a surface parking lot that holds 148 parking stalls. The property is currently zoned as I1 (Light Industrial District) with DH (Downtown Height Overlay District), DP (Downtown Parking Overlay District), and IL (Industrial Living Overlay District) overlays. The zoning will need to be changed to C3A, Community Activity Center District, to accommodate the proposed land development purpose.

Proposed Site Characteristics

As submitted for site plan approval, the enclosed parking area will be served by two main driveways along 13th Avenue South. A one-way drive-lane serving the front door drop-off/pick-up area is proposed on South 2nd Street. The proposed site plan is illustrated in **Figure 2**.

The proposed development will add 374 residential dwellings to the area. Off-Street parking stalls for the residential units will be provided at a 1.83 ratio (off-street parking stalls per dwelling unit) resulting in 686 designated residential parking stalls. It should be noted that this parking ratio does not include the 18 designated guest parking spaces. This site is located in the DP Downtown Parking Overlay District where the maximum parking ratio is 1.7 stalls per unit. The developer is seeking a variance to allow a 1.83 parking ratio. Any changes to these parking considerations will require City approval and changes to this TDM plan. Additionally, one bike space per unit will be provided within the resident parking areas and four bike spaces for the guest parking area.

Table 1 summarizes the proposed land use characteristics.

Table 1. Proposed Land Use Characteristics Summary

Estimated Schedule	Land Use Changes	Parking Changes
Project Completion: 2017	• Change Zoning from I1 to C3A. • 12 Power lines will be impacted. • Construct a new 374-unit residential building	• Remove 148 parking stalls within the existing surface lot. • Add a 704 space enclosed parking garage.

1.2 Proposed Parking Characteristics

As described previously, the enclosed parking area will be served by two main driveways along 13th Avenue South. A one-way drive-lane serving the front door drop-off/pick-up area is proposed on South 2nd Street.

The proposed project intends to provide adequate parking for its residents. However, the site location being adjacent to the CBD enables residents and guests to access the project site and nearby commercial districts by other modes. Refer to Section 1.3, below, and Section 2.0 (Parking Impact Analysis) for further discussion.

1.3 Pedestrian, Bicycle, And Transit Characteristics

The proposed development is located six blocks from the Minneapolis CBD. The location proximity to a major destination provides residents with easy access to other modes of transportation, such as walking, bicycling, and transit. The project site is approximately eight blocks from the nearest skyway entrance which is located in the Gateway Ramp, on South 3rd Street between 4th Avenue South and 5th Avenue South. The proposed development is located adjacent to designated bike routes on South 2nd Street and 11th Avenue South, which provide access to the Stone Arch Bridge, Mill Ruins Trails, LRT trail and the Park Avenue and Portland Avenue Bikeways. These bikeways provide convenient access to all downtown and Mississippi River bicycle trails. **Figure 3** illustrates designated bicycle trails within the Minneapolis downtown area. However, in an urban downtown setting, bicycles are allowed on all city streets, except where otherwise posted. The proposed project will provide 374 bike spaces around the perimeter of the parking areas.

The proposed development is also ideally suited for transit access. Currently, two bus routes maintain frequent service along Washington Avenue – Metro Transit Route 7 and Metro Transit Route 22. These two bus routes connect the project site to the Minneapolis CBD, South

Minneapolis, North Minneapolis, and the near North Suburbs. At each of these destinations numerous bus routes are available for transfer.

Numerous express and local service bus routes can be accessed at the Gateway Transit Center, located in the Gateway Ramp, seven blocks west of the proposed project site. Additional bus routes accessed via the Gateway Transit Center include: 133, 135, 146, 156, 460, 464, 465, 467, 470, 472, 476, 477, 478, 479, 490, 491, 554, 568, 587, 665, 670, 677, 684, 698, 755, 756, 758 and 764.

In addition to bus service, the METRO Blue line, with destinations between Target Field, Nicollet Mall, Minneapolis CBD, Mall of America, and the International Airport can be accessed at the US Bank Station (former Downtown East/Metrodome Station), which is located at the Chicago Avenue/4th Street intersection, five blocks away. With the addition of the METRO Green line, downtown Minneapolis and downtown St. Paul via Washington Avenue and University Avenue can be accessed by residents. The METRO Green line can also be accessed at the US Bank Station (former Downtown East/Metrodome Station). **Figure 4** illustrates the downtown transit system.

There is additional opportunity around the area to use Peer-to-peer Car Share or Round-Trip Car Share. The City of Minneapolis offers may different car sharing opportunities in the Downtown CBD area. A point-point floating car, car2go, located near Park Avenue and South 5th Street is available for use. Vehicles are generally parked on-street and each vehicle displays a permit sticker identifying it as a Minneapolis Car Sharing Program participant's vehicle. A round-trip car is available through HOURCAR, located near South 3rd Street and 2nd Avenue South, cars will be parked at reserved on-street spaces for exclusive use of HOURCAR vehicles.

1.4 City Of Minneapolis Transportation Goals

The following policies on transportation are included in Chapter 2 (Transportation) of the Minneapolis Plan for Sustainable Growth, adopted by the Minneapolis City Council, October 2, 2009 (http://www.minneapolismn.gov/cped/planning/plans/cped_comp_plan_2030):

2.1 Encourage growth and reinvestment by sustaining the development of a multi-modal transportation system.

2.2 Support successful streets and communities by balancing the needs of all modes of transportation with land use policy.

2.3 Encourage walking throughout the city by ensuring that routes are safe, comfortable, pleasant, and accessible.

2.4 Make transit a more attractive option for both new and existing riders.

2.5 Ensure that bicycling throughout the city is safe, comfortable and pleasant.

2.6 Manage the role and impact of automobiles in a multi-modal transportation system.

2.7 Ensure that freight movement and facilities throughout the city meet the needs of the local and regional economy while remaining sensitive to impacts on surrounding land uses.

2.8 Balance the demand for parking with objectives for improving the environment for transit, walking and bicycling, while supporting the city's business community.

2.9 Promote reliable funding and pricing strategies to manage transportation demand and improve alternative modes.

2.10 Support the development of a multi-modal Downtown transportation system that encourages an increasingly dense and vibrant regional center.

2.11 Minneapolis recognizes the economic value of Minneapolis-St. Paul International Airport and encourages its healthy competition to reach global markets in an environmentally responsible manner.

Table 2 presents the proposed mode split goals for the site based on similar development's mode splits in the area. The mode-split percentages listed in **Table 2** serve as a benchmark to make the management of the developments cognizant of transportation management issues that accompany a diverse and growing neighborhood. Therefore, the purpose of the TDM plan is to identify workable strategies that the Developer can implement in support of the City of Minneapolis transportation goals.

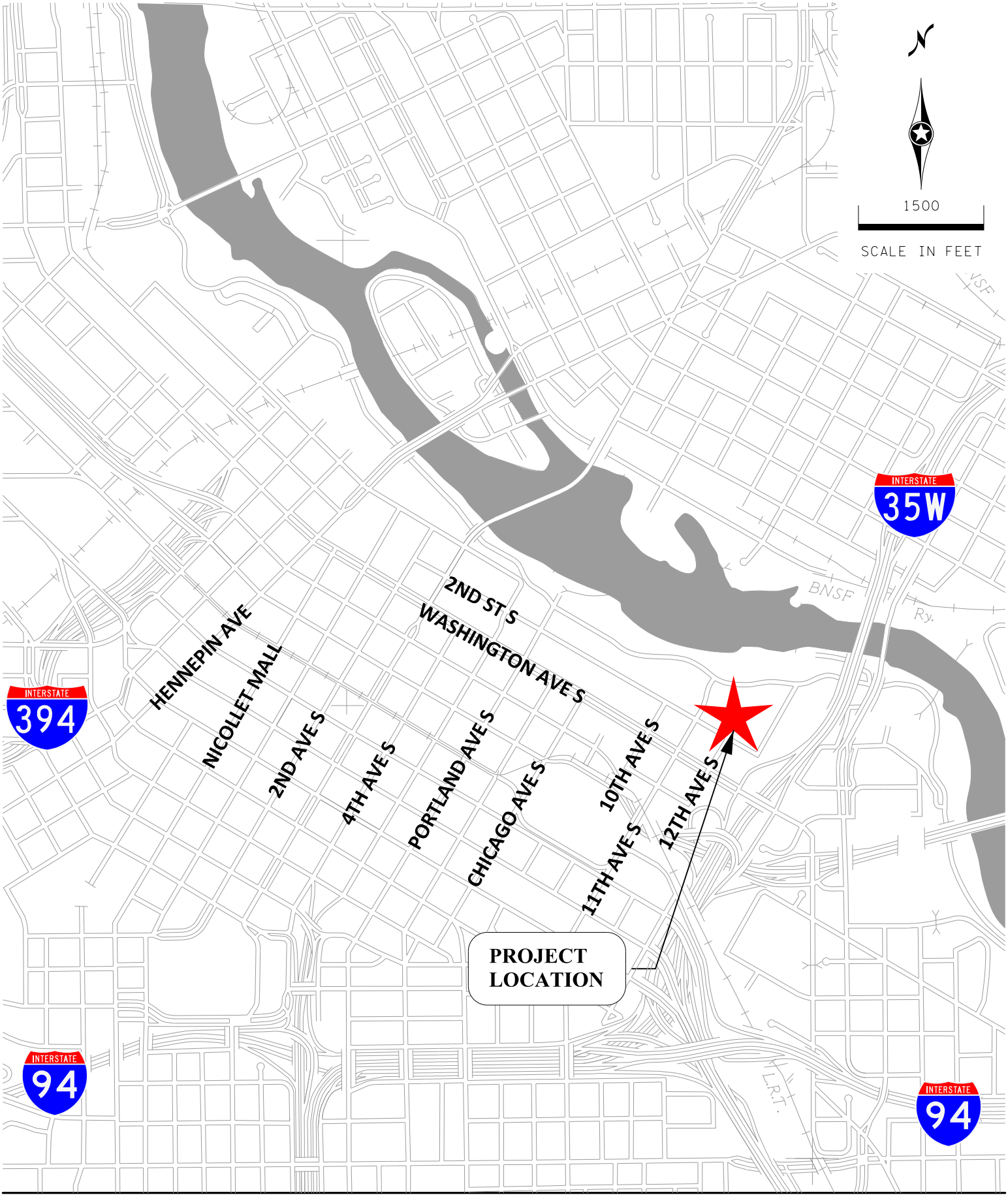
Table 2. Proposed Mode Split Goals

Mode	Goal
Single Occupant Vehicle (SOV)	30%
Transit (Bus & LRT)	20%
Carpool and Drop-off	15%
Pedestrian/Bicycle/Other	35%

1.5 Goals of the Travel Demand Management Plan

Ultimately, the responsibility to use other forms of transportation lies with the individuals choosing to visit, work or live within the proposed development. However, the Developer understands and acknowledges the overall transportation goals of the City. Therefore, the goal of this TDM plan is to support the City of Minneapolis in achieving its transportation goals for Minneapolis and to work towards or improve on the mode split targets for site-generated trips.

This TDM plan identifies actions to be taken by the proposed management in order to manage site-generated traffic and minimize peak period trip generation. The addition of a condominium complex near major transit lines captures many people who would have otherwise commuted via motorized vehicle. Nevertheless, the proposed development makes available to its staff and residents information on a variety of programs and services that include: bus service, park-and-ride lots, and bicycle and car sharing facility information. The TDM plan will be presented in Section 4.0.



LEGACY DEVELOPMENT TDMP

Figure 1
Project Location

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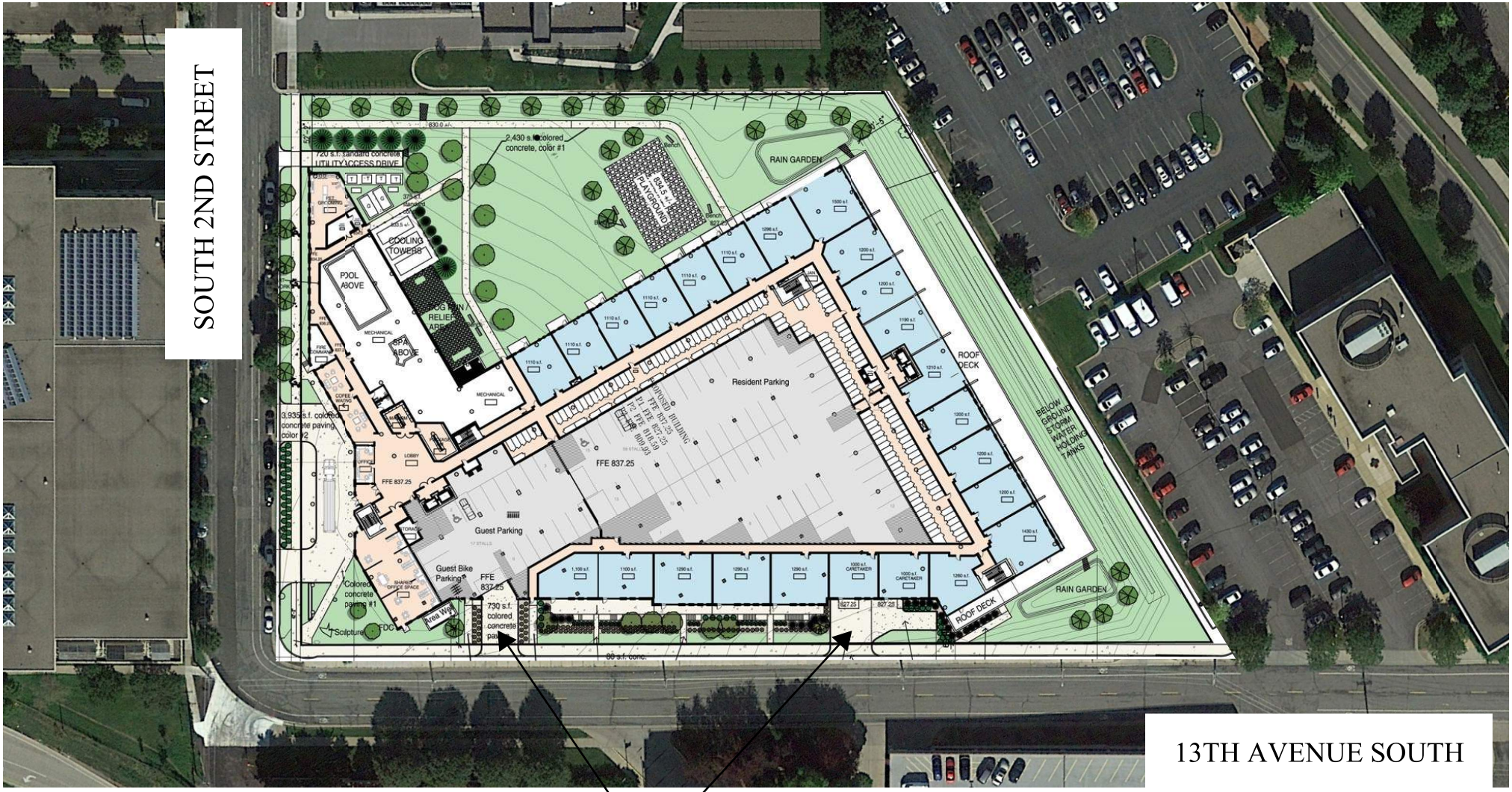
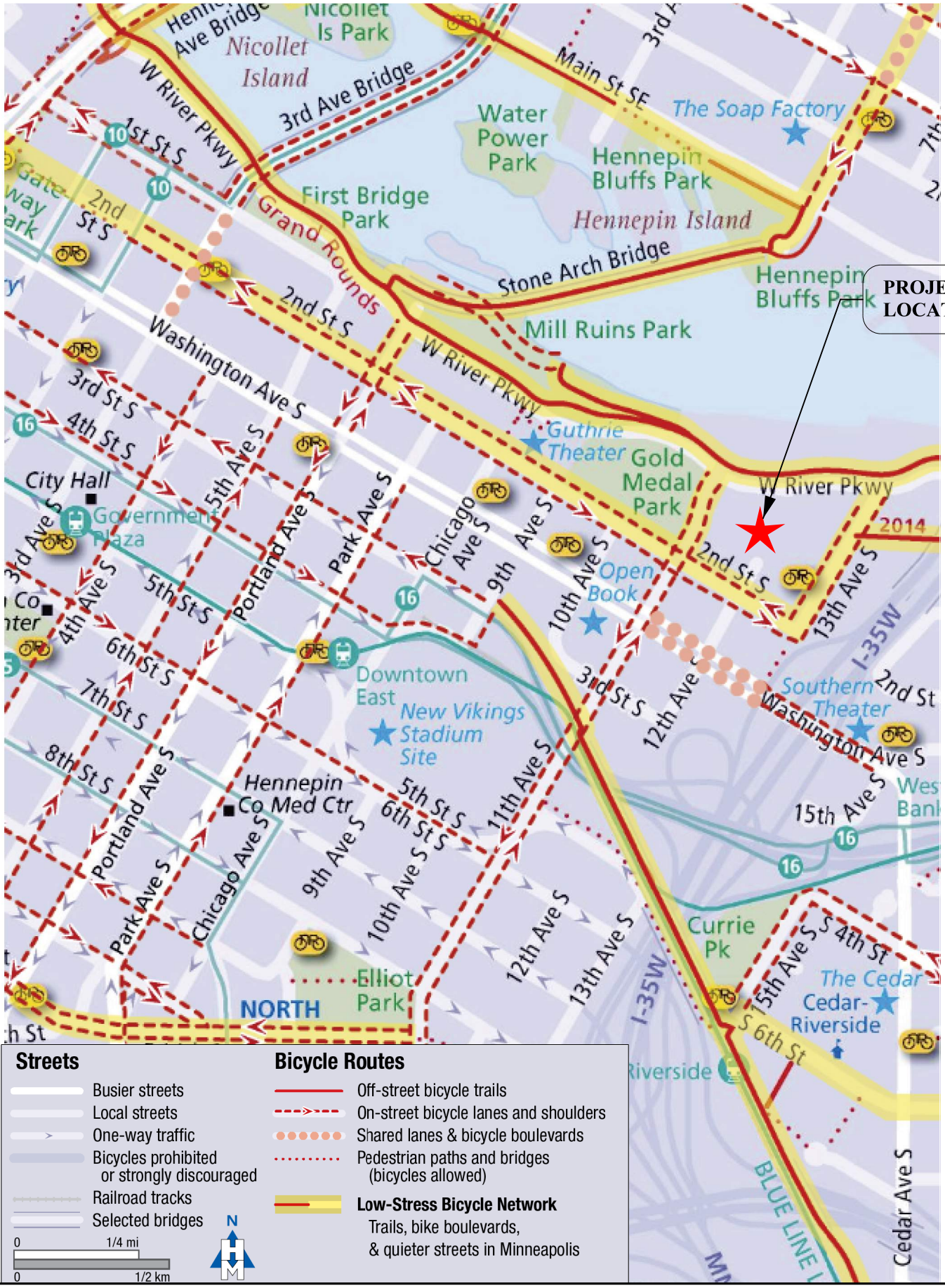


ACCESS 1 & 2
- **ACCESS TO GUEST AND TENANT PARKING**

ALLIANT ENGINEERING

Figure 2
Site Plan



LEGACY DEVELOPMENT TDMP

Figure 3
Bicycle Facilities

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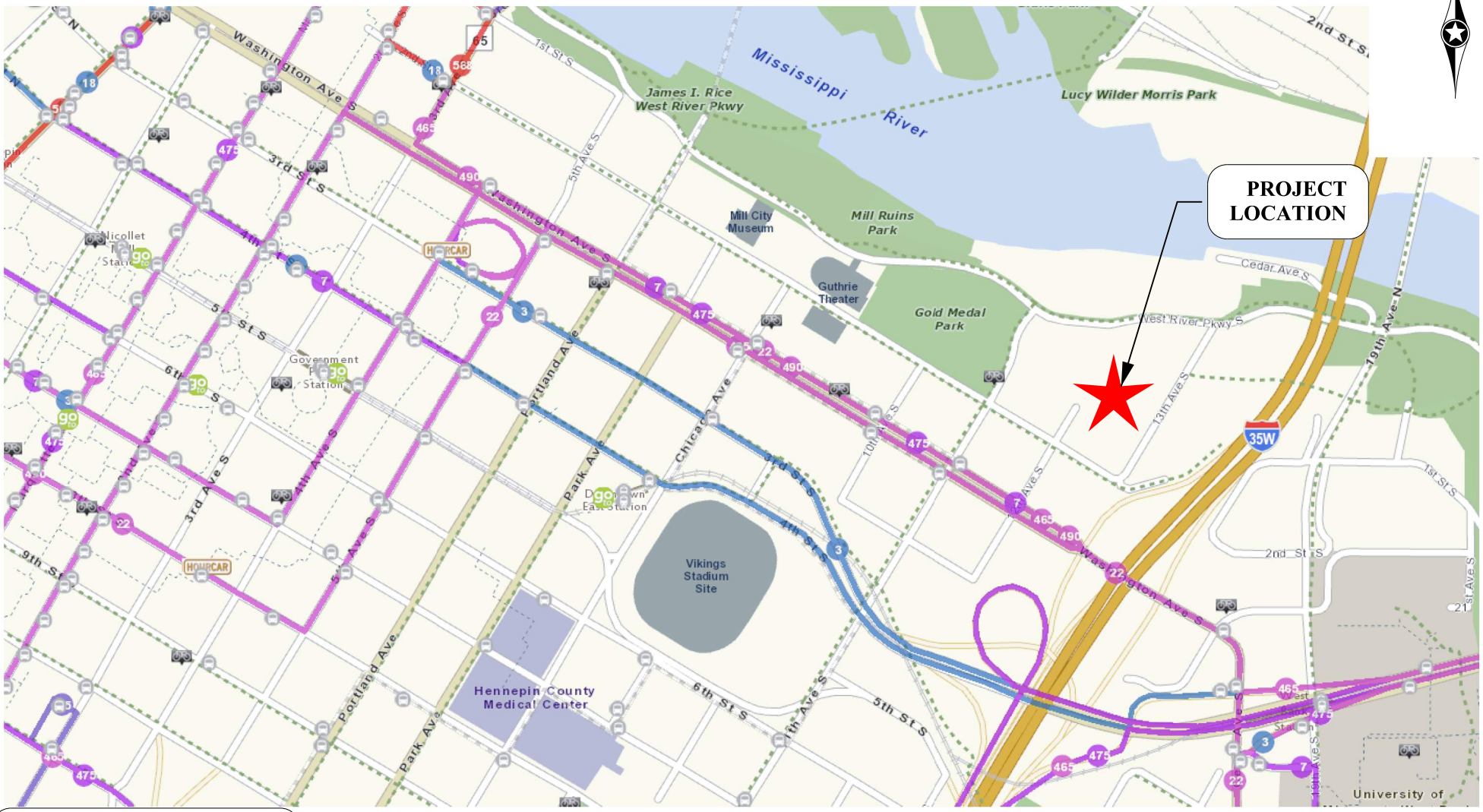


BUS ROUTES
7,22,465,475, and 490

ALLIANT ENGINEERING

Figure 4
Transit Routes

2.0 Parking Impact Analysis

As documented previously, the existing vacant site consists of 148 parking stalls. With the proposed project, the site will remove all 148 parking stalls and will construct a 704-space enclosed parking garage for use by residents and guests.

The parking characteristics of the proposed site consist mainly of off-street parking, but visitors may occasionally park on-street if the allocated guest parking stalls in the above ground lot are full. The existing on-street parking services the surrounding land uses by providing metered short duration parking for visitors and patrons of the area's commercial uses. Off-street parking provides for long duration parking associated with residential owners and guests. This section summarizes the parking impact analysis for the proposed development. **Table 3** summarizes the proposed parking details for the site.

Table 3. Proposed Parking Characteristics

Use	Size	# of Spaces	Location	Access
Condominium	374 Units	704	Enclosed Parking Garage	Entering and Exiting: 13th Avenue South
Total	**374 Units**	**704**		

Residential Parking

An access controlled enclosed parking garage is proposed for the residents and guests. The garage is proposed to be three levels consisting of a total of 704 stalls. Access to the parking garage will be secure and controlled with an electronic key. There will also be a call device at the garage door for visitors to call a resident and get buzzed in.

2.1 Parking Demand Calculations

The parking demand expected with the Project was estimated based on two methodologies – the City of Minneapolis Zoning Code and the Institute of Transportation Engineers Parking Generation Manual.

Method 1 – Minneapolis Zoning Code

The first method involves the application of the computation for off-street parking found in Article III of Chapter 541 of the Minneapolis Zoning Code. The Minneapolis Zoning Code states that a minimum of one off-street parking stall and maximum of 1.7 off-street parking stalls (based on DP Downtown Parking Overlay District requirements) per residential unit is required. In accordance with the Minneapolis Zoning Code Section 541.200, the required number of parking stalls may be reduced by 50 percent due to the proposed sites proximity to the US Bank Station. Table 4 documents the parking provided verses the minimum and maximum parking required by the Minneapolis Zoning Code. As shown in Table 4, the proposed project will exceed the maximum number of allowed parking spaces for residential development in the DP Downtown Parking Overlay District. The developer is requesting a variance to allow for the additional parking stalls.

Table 4. Maximum Parking Stalls Required – Chapter 541

Parking Use	Dwellings	Reductions	Rate		Min.	Required Spaces	
			Min	Max[1]		Min. Total with Reductions	Max
Proposed Land Uses							
Residential	374	Parking reduction of 50%	1 space/dwelling unit	1.7 spaces/dwelling unit	374	187	636
Guest			1 space/50 dwelling units			8	
Grand Total						**195**	**644**

[1]Max residential parking ratio for the DP Downtown Parking Overlay District.

Method 2 – Parking Generation Computation

The second method involves the application of the parking generation rate from the *Institute of Transportation Engineers (ITE) Parking Generation Manual, Fourth Edition*. This resource documents numerical trends in parking demand for a variety of land uses based upon the day of the week, time of day, and size of the development, and whether the site is urban or suburban. Furthermore, these rates are developed upon research of similar locations and experiences of transportation engineering professionals and would be considered a more accurate depiction of the actual parking demand expected with the proposed project.

Based on the Parking Generation Manual, a parking demand of 1.4 stalls per dwelling unit[1] is expected for a development located in an urban area. **Table 5** illustrates the parking supply and demand for the development.

Table 5. Minimum Parking Stalls Required – Parking Generation Method

Parking Use	Dwellings	Rate	Required Spaces	Provided Spaces
Residential	374	1.4 spaces/dwelling unit	524	704

<u>Bicycle Parking</u>
The City of Minneapolis requires a minimum of 1 bicycle parking space for every 2 dwelling units. Riverdale Ventures will provide one bike space per unit or 374 spaces within the resident parking areas and four bike spaces for the guest parking area. **Table 6** details the minimum parking requirements for the proposed land use.

[1] *ITE Parking Generation Manual, 4th Edition, Land Use Code 230*

Table 6. Minimum Required Bicycle Parking Spaces

Parking Use	Units/SF	Rate	Required Spaces
Residential	374	0.5 space/dwelling unit	187

Parking Conclusions

The proposed project will provide adequate parking. The developer is requesting a variance to increase the maximum amount of parking from 636 spaces to 704 spaces, which results in a 1.83 parking ratio. In addition to off-street parking, ample metered on-street parking is available within a one-block radius of the site. The Minneapolis bicycle parking requirement of 187 spaces is satisfied with the 374 proposed bike spaces.

3.0 Transportation Impact Analysis

This section of the TDM plan describes impacts to the transportation system resulting from the development of this property. The purpose of the traffic analysis is to evaluate the impact of traffic generated by the proposed project on the operations and safety of the adjacent roadway network and immediate site/parking access. The study will detail changes in roadway conditions in the study area due to the development. Recommendations regarding geometric and/or traffic control needs to accommodate additional background traffic, site-generated traffic, and improve safety are included, as required. The following intersections were evaluated:

- Washington Avenue and 11th Avenue South
- 11th Avenue South and West River Parkway
- Washington Avenue and 12th Avenue South

AM and PM peak hour turning movement counts were collected by Alliant Engineering on Tuesday, February 9, 2016 for the three intersections mentioned above. The AM peak hour is from 8:00 a.m. to 9:00 a.m. The PM Peak hour is from 5:00 p.m. to 6:00 p.m. Peak-hour turning movement counts were taken to represent the 2017 No-Build scenario, or the existing traffic system. The existing No-Build peak-hour volumes are shown in **Figure 6**.

3.1 Site Generated Traffic

To establish the 2017 Build traffic volumes, the number of trips generated by the proposed project were estimated. Trip generation rates for the proposed development were obtained from the *Trip Generation Manual*[2]. **Table 7** presents the new external AM peak hour, PM peak hour and daily vehicle trips expected.

Table 7. Trip Generation

Land Use (ITE Code)	Units	Size	AM Peak Hour Trips				PM Peak Hour Trips				Daily Trips	
			Rate	Trips In	Trips Out	Total Trips	Rate	Trips In	Trips Out	Total Trips	Rate	Total Trips
Proposed Land Use - Legacy Condominiums												
Residential Condominium / Townhouse (230)	Units	374	0.44	32	134	166	0.52	125	71	196	5.81	2,173
Net Total Trips				**32**	**134**	**166**		**125**	**71**	**196**		**2,173**

1. Per the Institute of Transportation Engineers (ITE) Trip Generation Manual, 9th Edition.

Based on the proposed land uses for the project, 166 new AM peak hour, 196 new PM peak hour, and 2,173 new total daily trips are expected in addition to existing trips. It should be noted that the estimated number of external trips generated is expected to be conservative. The actual number of trips should be less based upon the proximity of the proposed project in relation to the CBD and achievement of the anticipated modal split goals. In addition, the existing trips from existing land uses were not subtracted out of the network. These new vehicle trips were distributed throughout the system and evaluated in the traffic operations analysis using system

[2] *Trip Generation Manual, Institute of Transportation Engineers (ITE), 9th Edition*

generation assumptions, as shown in **Figure 7.** The distributed generated trip volumes, in addition to existing volume data, are depicted in the Build scenario in **Figure 8**.

3.2 Traffic Operations Analysis

A traffic operations analysis was performed for the two volume scenarios (2017 No-Build and 2017 Build conditions) utilizing the existing street network. The analysis was completed using Synchro 8.0 and Simtraffic 8.0 and assumed existing traffic signal timing.

The traffic operations analysis followed methods set forth in the Highway Capacity Manual (HCM). The Level of Service (LOS) was documented for the key intersections. LOS is a measure used to quantify the traffic operations at an intersection or roadway section. LOS ranges from A to F, with A being non-congested, free-flowing conditions, and F being very congested and near capacity conditions. An LOS of D or E is generally considered the indicator of congestion in an urban area. The results of the No-Build and Build traffic operations analysis are shown below in **Tables 8 and 9**.

Table 8. No-Build Operational Analysis Results

Node	Control	Intersection	AM Peak		PM Peak	
			Int. Delay (s/v)	LOS	Int. Delay (s/v)	LOS
701	Signal	Washington Ave and 11th Street	22.7	C	25.3	C
702	Thru-Stop	11th Street and West River Parkway	2.3	A	3.0	A
703	Thru-Stop	Washington Ave and 12th Street	1.8	A	2.0	A

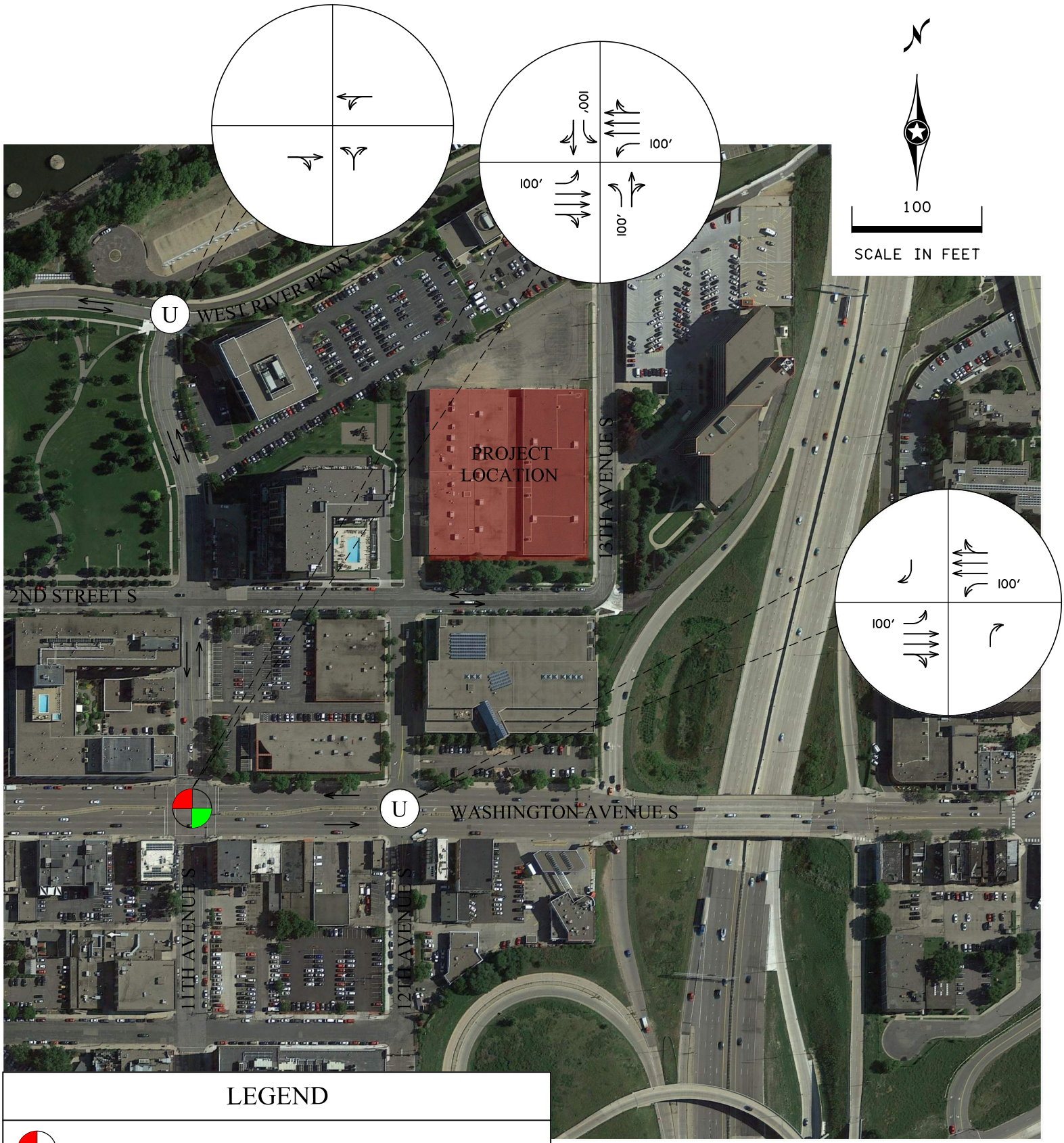
1. AM Peak and PM Peak delays computed using 5 random seeds in SimTraffic

Table 9. Build Operational Analysis Results

Node	Control	Intersection	AM Peak		PM Peak	
			Int. Delay (s/v)	LOS	Int. Delay (s/v)	LOS
701	Signal	Washington Ave and 11th Street	24.4	C	27.4	C
702	Thru-Stop	11th Street and West River Parkway	2.3	A	3.0	A
703	Thru-Stop	Washington Ave and 12th Street	1.9	A	2.0	A

1. AM Peak and PM Peak delays computed using 5 random seeds in SimTraffic

For both No-Build and Build conditions, all intersections and access points operate acceptably with an LOS of C or better during the AM and PM Peak periods. The addition of the site-generated traffic had a negligible effect on the operations of the traffic system. Based on the analysis there are no extra traffic control or geometric improvements expected to be necessary.



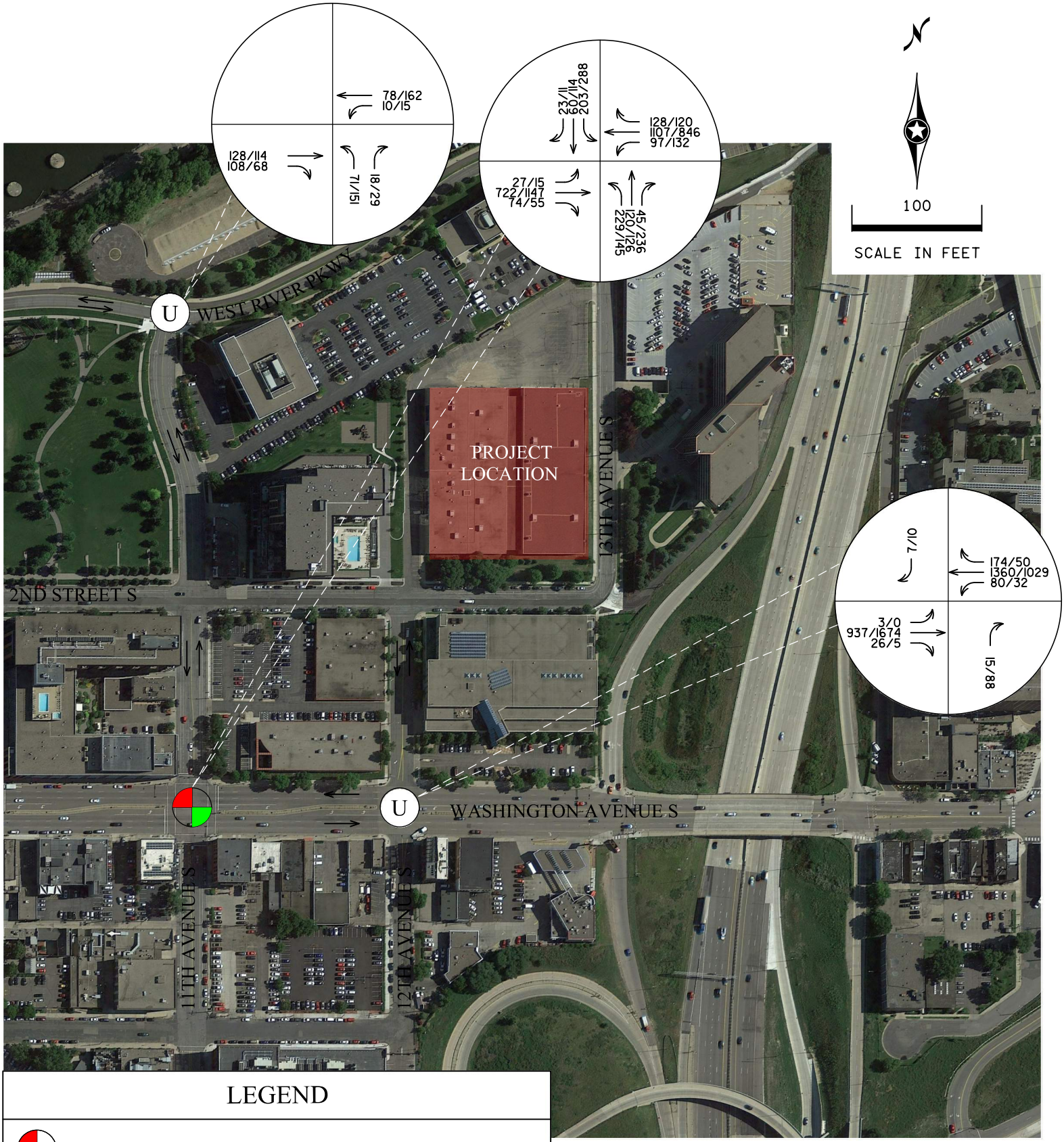
LEGEND

	TRAFFIC SIGNAL	→	TRAVEL LANES
U	UNSIGNALIZED INTERSECTION	XX'	TURN BAY LENGTH

LEGACY DEVELOPMENT TDMP

Figure 5
Existing Geometrics

ALLIANT ENGINEERING



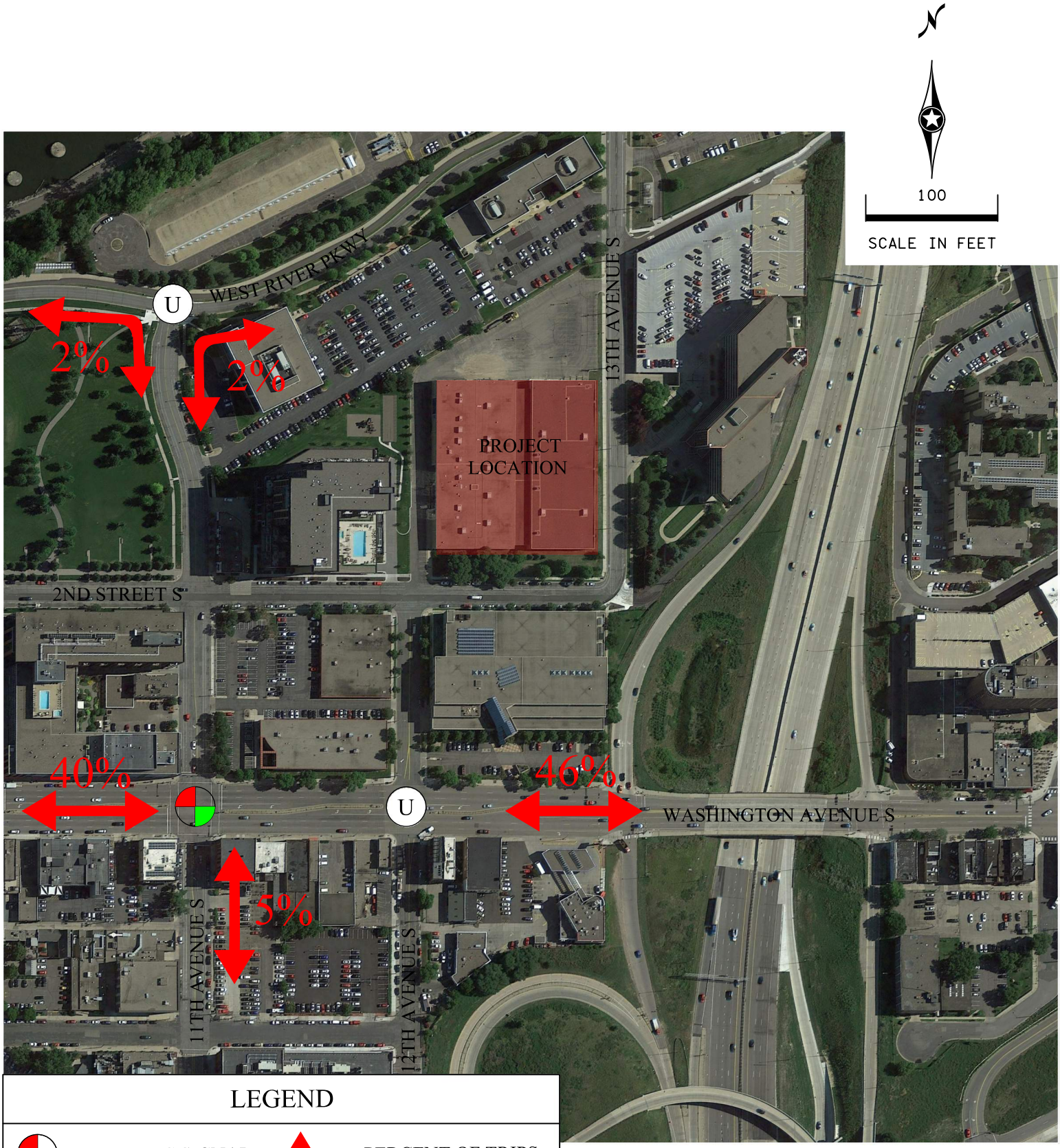
LEGEND

	TRAFFIC SIGNAL	⟶	TRAVEL LANES
U	UNSIGNALIZED INTERSECTION	XX/XX	AM VOLUMES /PM VOLUMES

LEGACY DEVELOPMENT TDMP

Figure 6
2017 No-Build Peak Hour Volumes

ALLIANT ENGINEERING



LEGEND

TRAFFIC SIGNAL

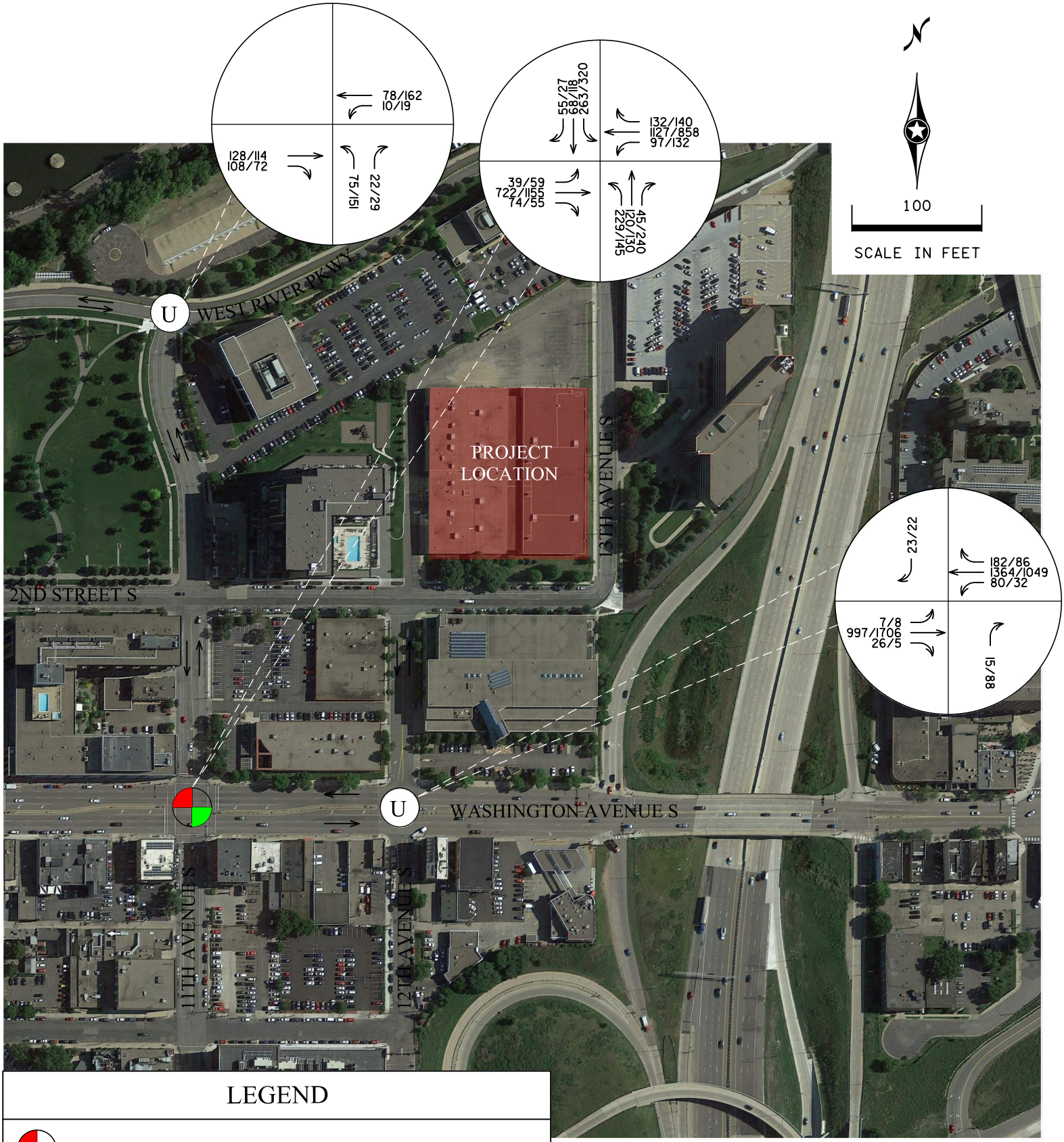
U — UNSIGNALIZED INTERSECTION

X% — PERCENT OF TRIPS GENERATED AND DIRECTION

LEGACY DEVELOPMENT TDMP

Figure 7
Generated Trip Distribution

ALLIANT ENGINEERING



LEGEND

	TRAFFIC SIGNAL	→	TRAVEL LANES
U	UNSIGNALIZED INTERSECTION	XX/XX	AM VOLUMES /PM VOLUMES

LEGACY DEVELOPMENT TDMP

Figure 8
2017 Build Peak Hour Volumes

ALLIANT ENGINEERING

4.0 Travel Demand Management Plan

The following TDM plan for the proposed project identifies the program, responsibilities, and detailed implementation of the TDM plan by Riverdale Ventures, LLC. or their successor for the parking and transportation issues summarized above.

Riverdale Ventures, LLC. recognizes the desire for safe and convenient travel alternatives to single-occupant vehicle commuting that reflects the Twin Cities region's economic, environmental, and social needs. Riverdale Ventures, LLC. understands and supports the objective to minimize travel demand, especially during the morning and afternoon peak hours.

Riverdale Ventures, LLC. or their successors, the building managers, or the Homeowners Association agree to the following measures to help meet the City of Minneapolis Transportation Goals:

1. Level of Participation with Transportation Management Organization (TMO)

- ***Resident Transportation Coordinator:*** Riverdale Ventures, LLC. or their successors will designate a Transportation Coordinator to work with residents on the implementation of this TDM plan. The Transportation Coordinator will work closely with the Metro Transit TDM Consultant, Suzan Forsberg, to disseminate commuting information and materials to residents and visitors; participate in regional training or informational sessions about TDM programs; be available to meet once a year with the Transportation Coordinator to review available regional programs and services; and monitor progress on fulfilling the TDM commitments.

- ***Downtown Minneapolis TMO:*** The Downtown TMO is a private partnership of the City of Minneapolis and downtown business community. The Downtown TMO website (http://www.commuter-connection.org/) is an excellent resource for Developers that encourage commuting.

2. Communication and Information for Residents

- ***Dissemination of information:*** Riverdale Ventures, LLC. or their successors will disseminate information on transit and biking alternatives to its residents. Bus route maps and schedules will be available in the Business Office. Information will include transit schedules, local bicycle commuting facilities and maps, car-pooling programs, flex-car programs and other similar programs.

- ***Commuter information program:*** With assistance from the Downtown Minneapolis TMO, Metro Transit, and Metro Commuter Services, Riverdale Ventures, LLC. or their successors will develop a commuter information program to familiarize residents with the variety of available transportation resources.

- ***Move-in packages***: Riverdale Ventures, LLC. or their successors will create and implement a "move-in" transportation alternatives package for new resident. The move-in package will provide each resident with all information and resources available to inform and encourage alternative modes of transportation.

- ***Transit:*** Riverdale Ventures, LLC. or their successors will include a real time transit information screen in the lobby for guests.

3. Encourage Car and Vanpooling

- Riverdale Ventures, LLC. or their successor will actively promote car and vanpooling as alternative means of commuting among the residents and visitors, primarily through information dissemination.

4. Support and Promote Bicycling and Walking as Alternatives Facilities

- Riverdale Ventures, LLC. or their successors will actively promote bicycling and walking as alternative means of commuting among residents and visitors of the site, primarily through information dissemination and the provision of bicycle storage facilities.

5. Off-peak Truck and Service Scheduling

- ***Off-peak scheduling:*** Riverdale Ventures, LLC. or their successors will encourage truck and service vehicles, not including small package deliveries, to access the site outside of the weekday AM and PM peak traffic periods.

  The proposed project will accommodate the loading/unloading of deliveries, service vehicles and garbage trucks on site. No on-street space will be allocated for delivery and service vehicles.

6. Monitoring of Travel Behavior

With the assistance of the Downtown Minneapolis TMO, Riverdale Ventures, LLC or their successors commit to the following:

- ***Original survey:*** Riverdale Ventures, LLC or their successors will develop a methodology for surveying the travel behavior of a sample of its residents, once 70% of the units are occupied.

- ***Ongoing surveys:*** Riverdale Ventures, LLC, their successors or the property manager will be requested to conduct a similar sampling every two years after the original baseline survey with assistance from the Downtown Minneapolis TMO and continuing for a time period that will be determined jointly with the City of Minneapolis.

- ***Status reports:*** After each round of biennial sampling, the association will be requested to review the TDM Plan with the Downtown Minneapolis TMO to assess the effectiveness of the Plan in encouraging the use of alternative modes. A status report will be provided to the Downtown Minneapolis TMO and the City of Minneapolis Planning and Public Works Departments for review and comment with recommendations for possible revisions to the TDM Plan.

Revisions: Based upon this review, identification of measures of effectiveness, and changes in transportation and parking in the vicinity of the proposed project, the TDM plan may be revised to emphasize effective measures and to reflect changes.

TRAVEL DEMAND MANAGEMENT PLAN
Legacy Development
Minneapolis, MN

PLAN APPROVAL

Riverdale Ventures, LLC.

By_____ Dated:_____

James M. Stanton - President
Riverdale Ventures, LLC.
3200 Main Street NW, Suite 300
Coon Rapids, MN 55448

MINNEAPOLIS COMMUNITY AND ECONOMIC DEVELOPMENT DEPARTMENT

By_____ Dated:_____

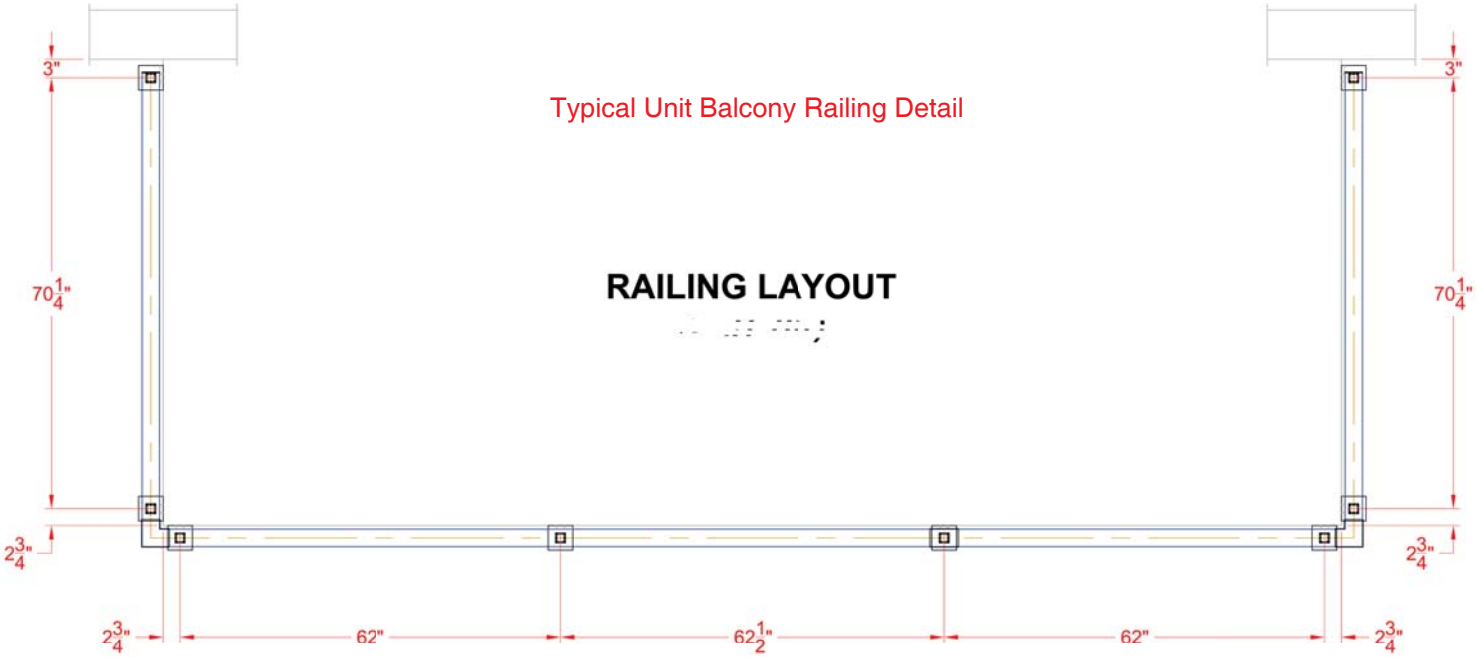
CPED Planning Director

MINNEAPOLIS PUBLIC WORKS DEPARTMENT

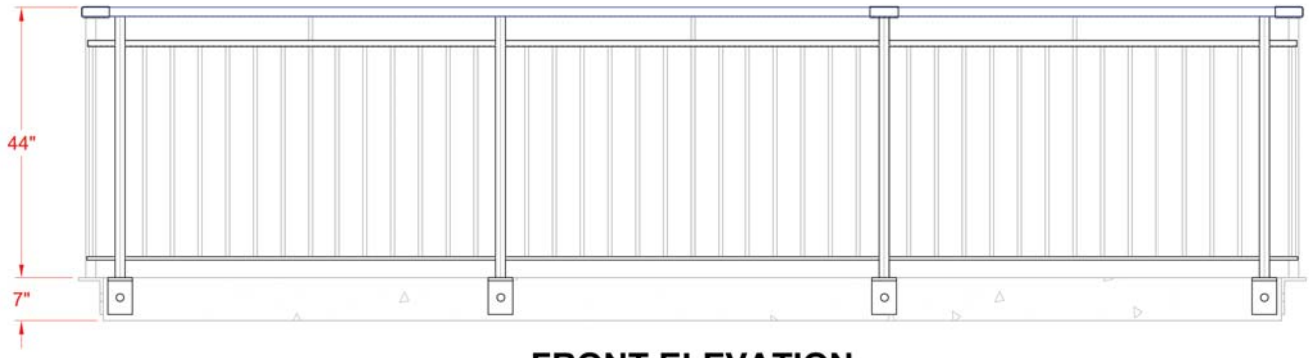
By_____ Dated:_____

Steve Mosing, PE, PTOE,
Traffic Operations Engineer

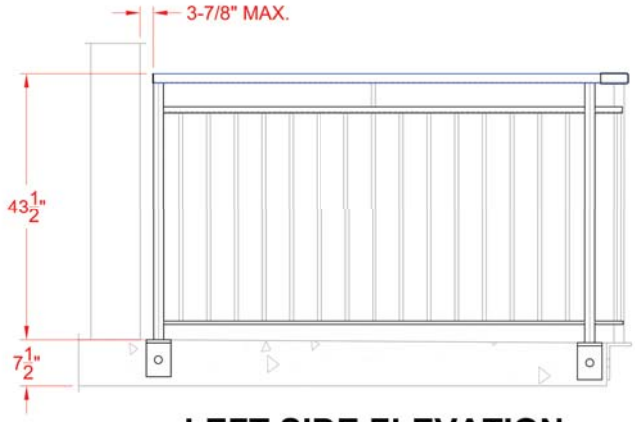
Typical Unit Balcony Railing Detail



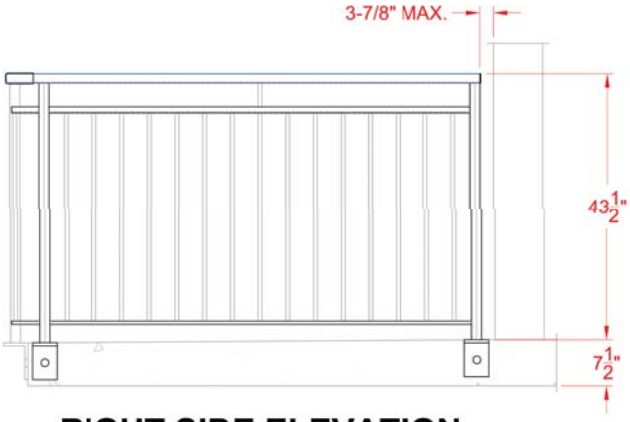
RAILING LAYOUT



FRONT ELEVATION



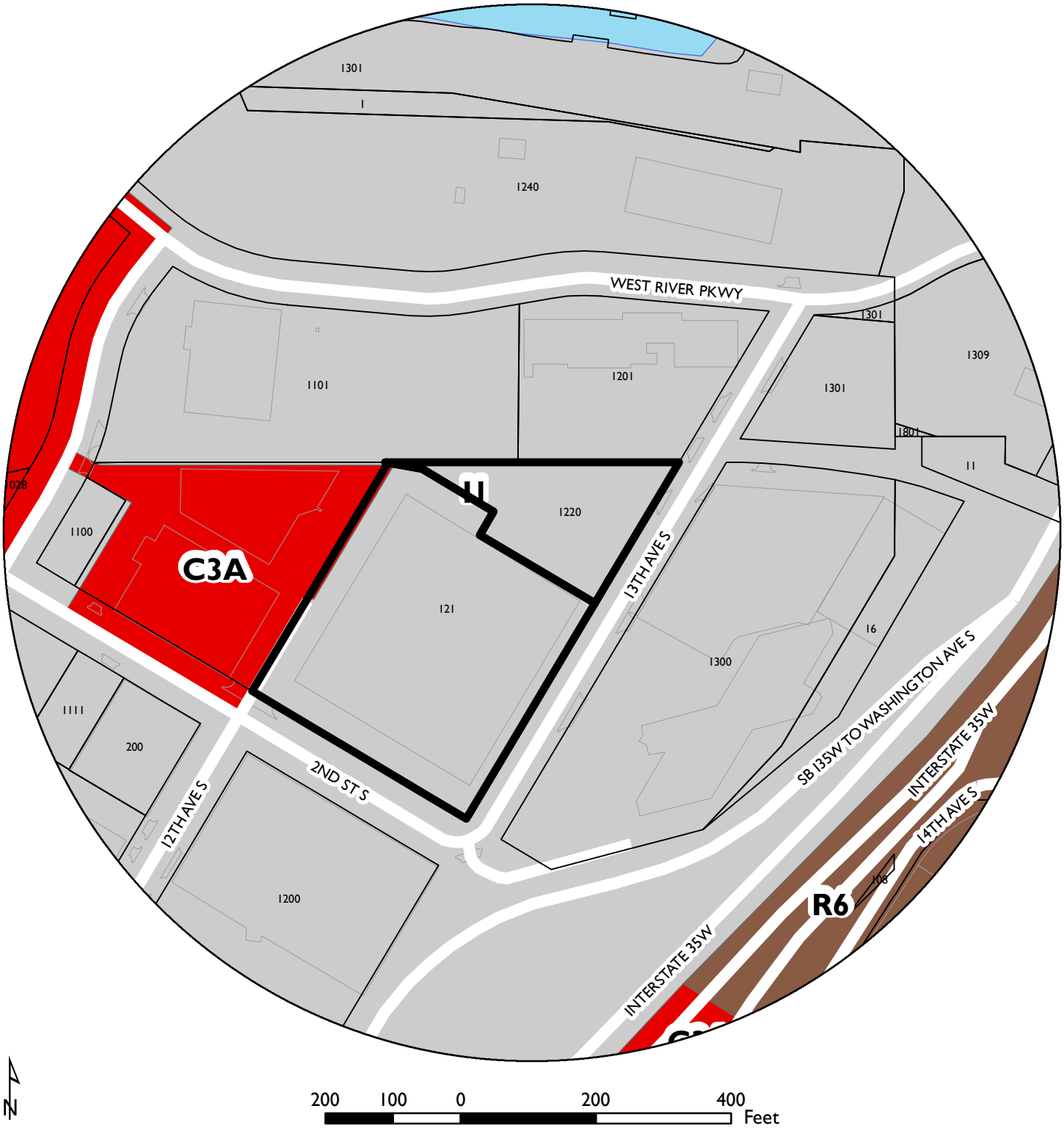
LEFT SIDE ELEVATION



RIGHT SIDE ELEVATION

Riverdale Ventrues, LLC **3rd**

NAME OF APPLICANT WARD



PROPERTY ADDRESS

1220 South 1st Street and 121 12th Avenue South

FILE NUMBER

BZZ-7676

1 SITE PLAN

1" = 20'-0"

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Mpls, MN 55415

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DATE OF ISSUE:	xxx, 2016
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CHECKED BY:	JLO
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JEFFREY L. OERTEL

SIGNATURE

DATE xxx, 2016

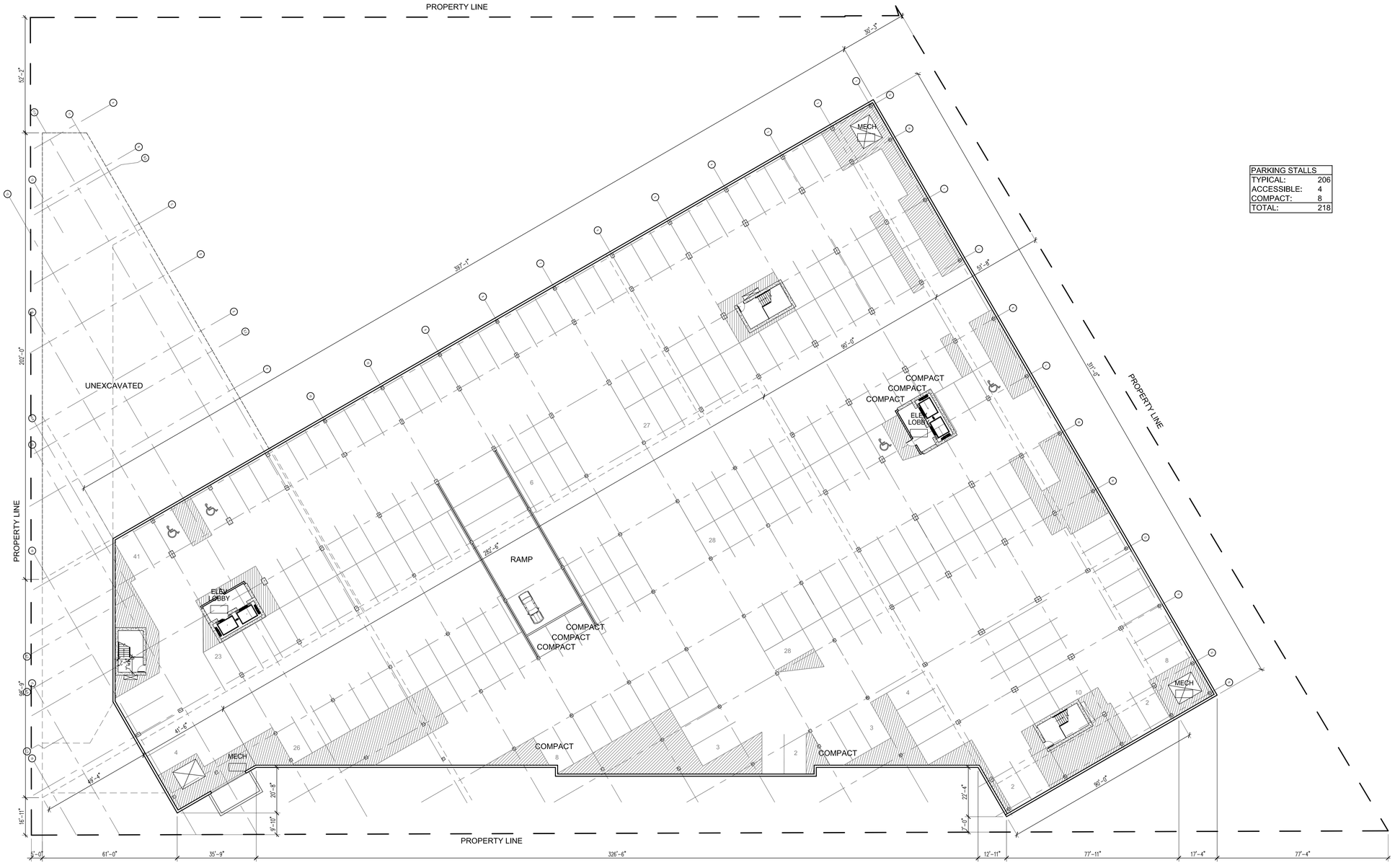
LICENSE NUMBER 15840

SHEET NAME:

SITE PLAN

SHEET NO:

A0.0



PARKING STALLS	
TYPICAL:	206
ACCESSIBLE:	4
COMPACT:	8
TOTAL:	218

North

1 P3 Floor Plan

1/16" = 1'-0"

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SIGNATURE

DATE xxx, 2016

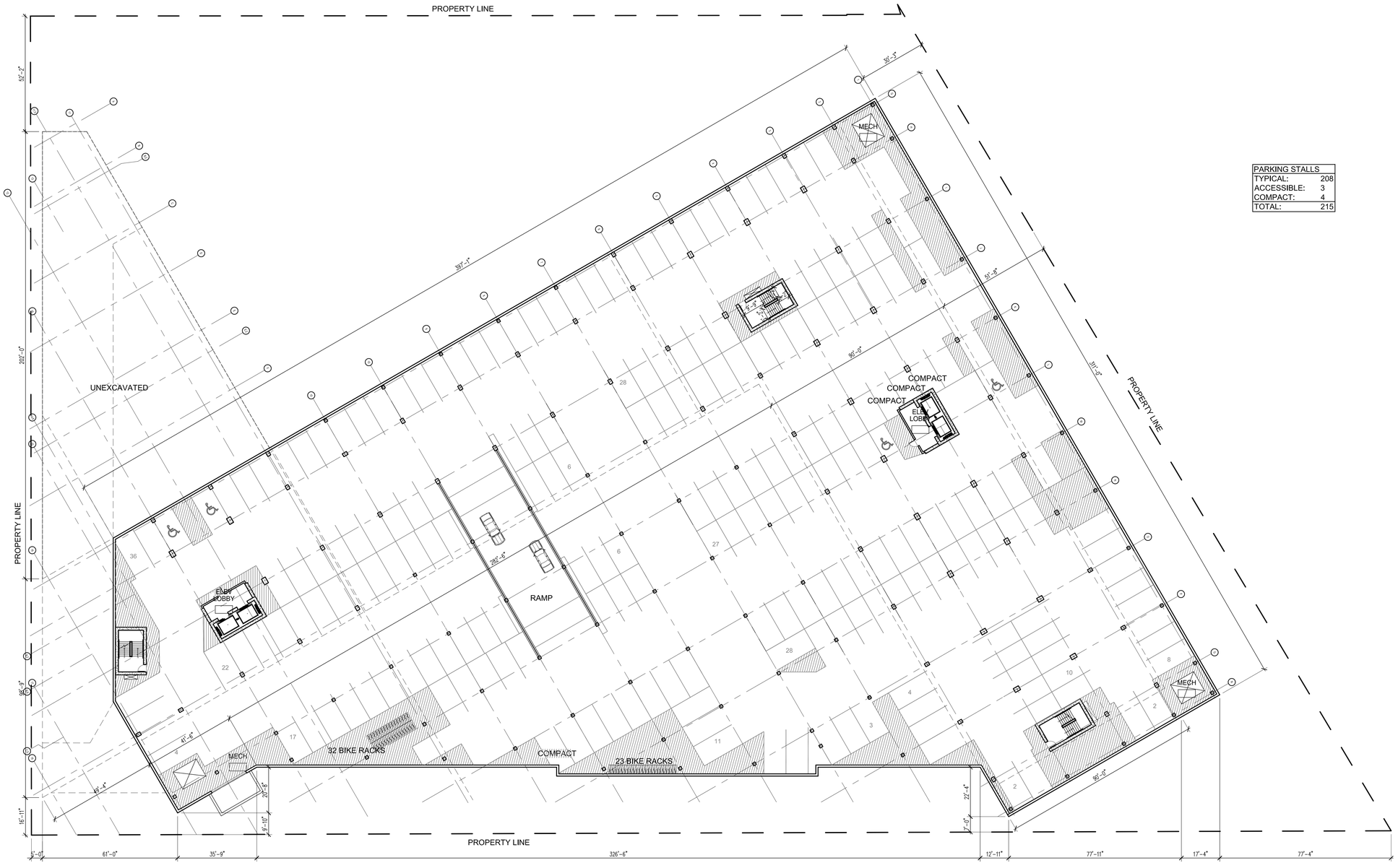
LICENSE NUMBER 15840

SHEET NAME:

P3 Parking

SHEET NO:

A0.3



PARKING STALLS	
TYPICAL:	208
ACCESSIBLE:	3
COMPACT:	4
TOTAL:	215

North

1 P2 Floor Plan

1/16" = 1'-0"

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SIGNATURE

DATE xxx, 2016

LICENSE NUMBER 15840

SHEET NAME:

P2 Parking

SHEET NO:

A0.2

North

1 P1 Floor Plan

1/16" = 1'-0"

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4.8.16

PDR REVIEW
4.18.16

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DATE OF ISSUE: APRIL 27, 2016

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SIGNATURE

DATE: APRIL 27, 2016

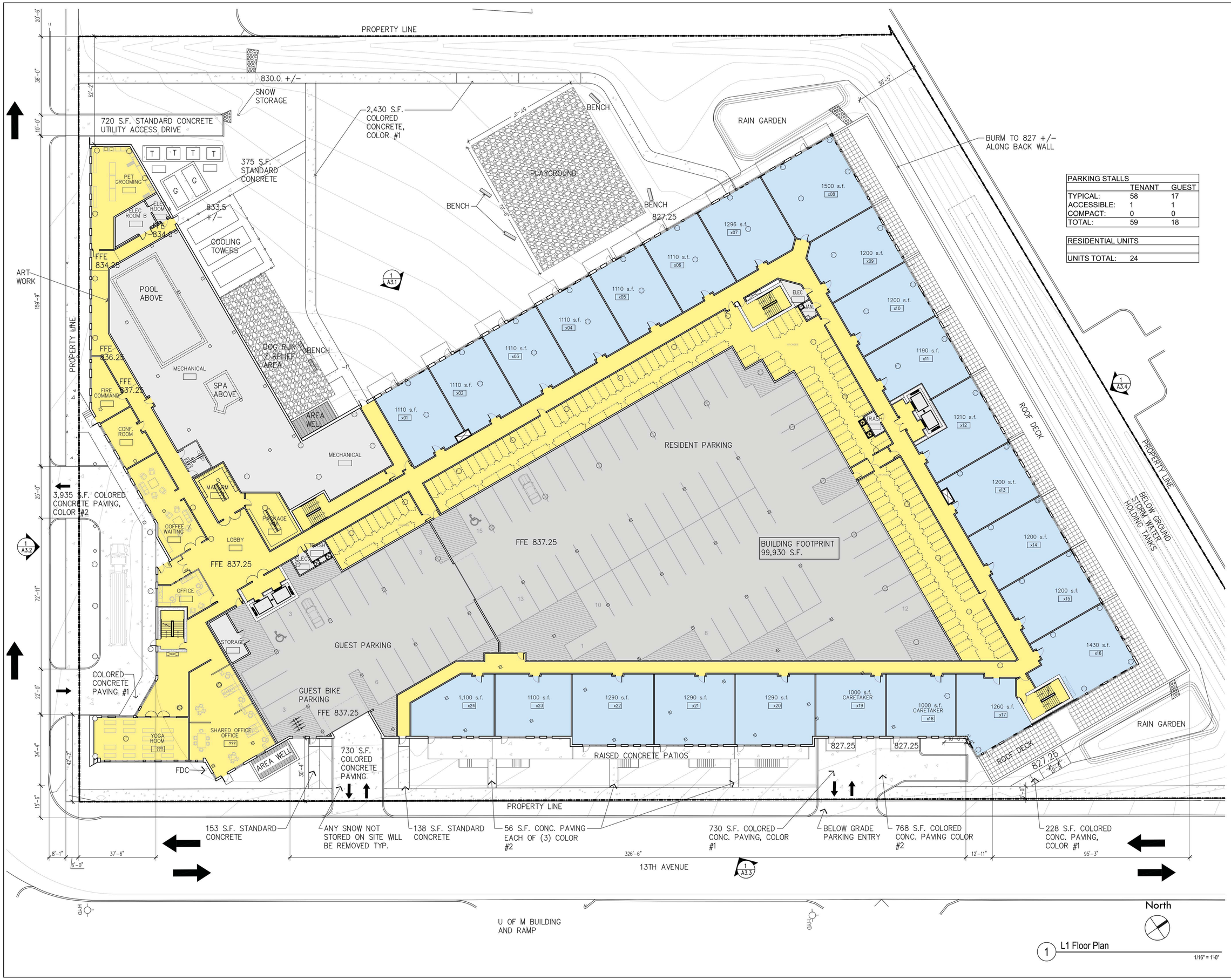
LICENSE NUMBER: 15840

SHEET NAME:

P1
Parking

SHEET NO:

A0.1



PARKING STALLS		
	TENANT	GUEST
TYPICAL:	58	17
ACCESSIBLE:	1	1
COMPACT:	0	0
TOTAL:	59	18

RESIDENTIAL UNITS	
UNITS TOTAL:	24

1 L1 Floor Plan

1/16" = 1'-0"

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St. Paul, Minnesota 55105

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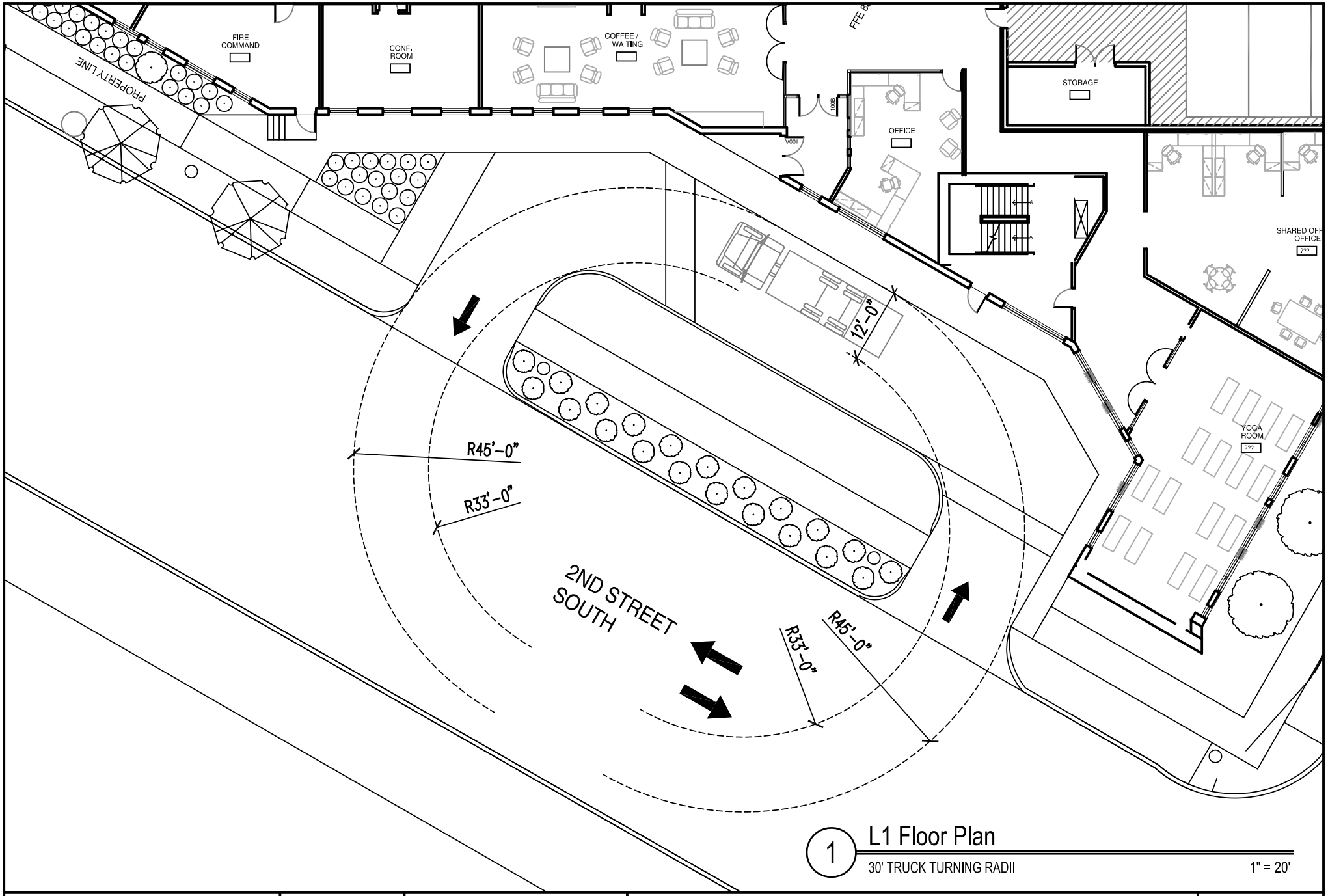
LICENSE NUMBER 15840

SHEET NAME:

L1
Entry Level

SHEET NO: A1.1



1 L1 Floor Plan

30' TRUCK TURNING RADII 1" = 20'

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DATE _____

REGISTRATION _____

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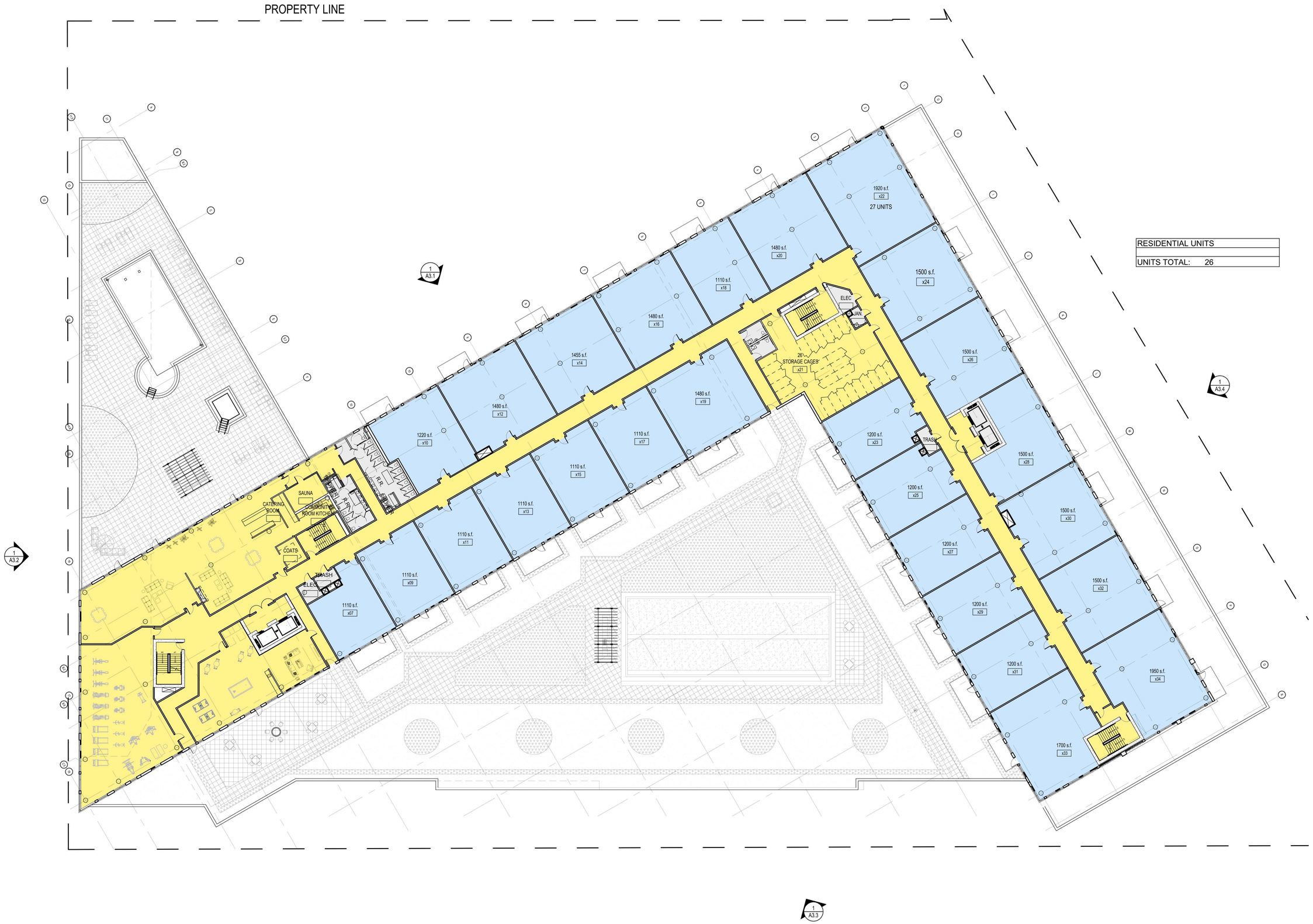
MINNEAPOLIS, MN

SHEET NO. A1.1.3



RESIDENTIAL UNITS
UNITS TOTAL: 26

North

1 L2 FLOOR PLAN

1/16"=1'-0"

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SIGNATURE

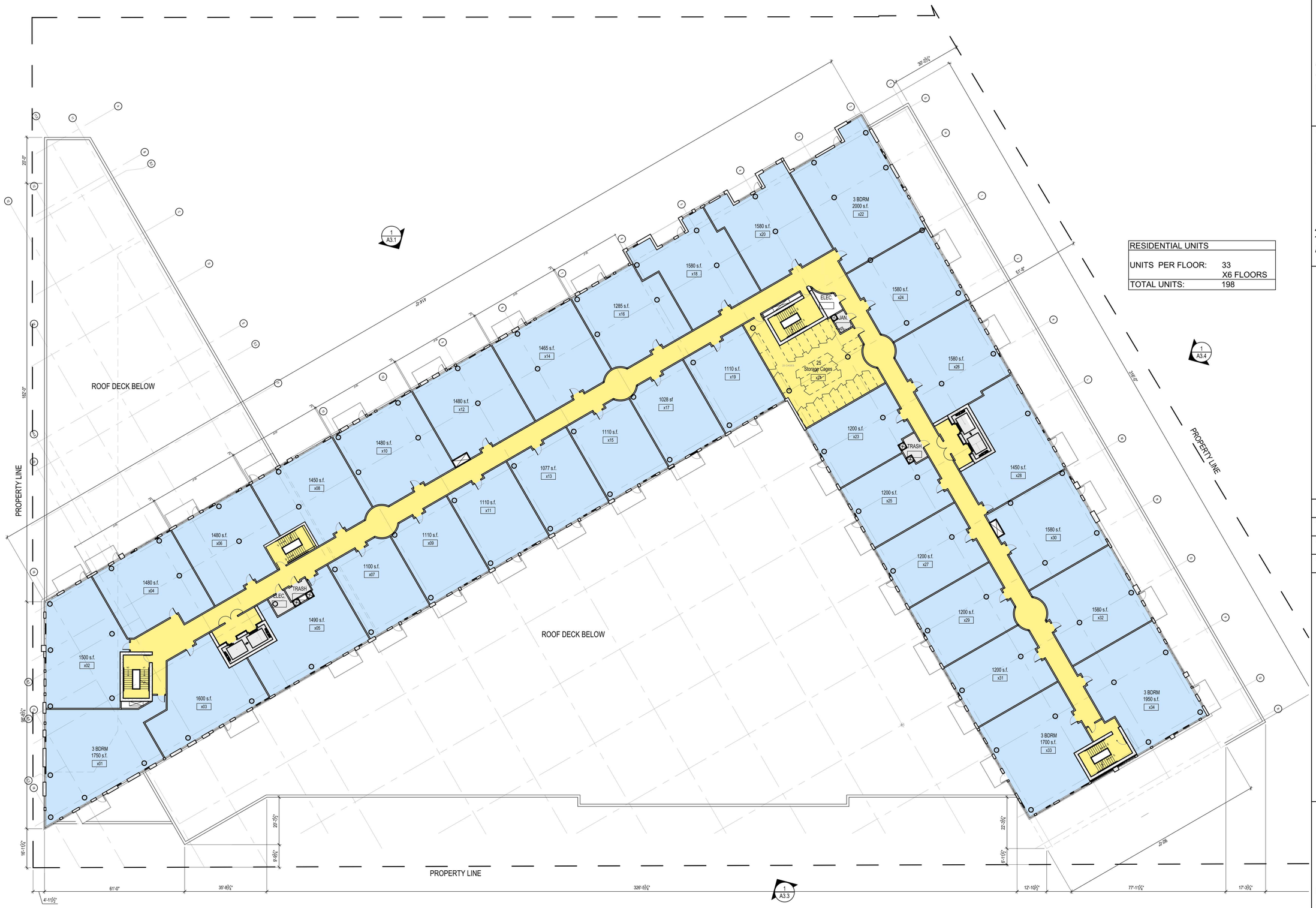
DATE xxx, 2016

LICENSE NUMBER 15840

SHEET NAME:

L2
Units

SHEET NO: A1.2



RESIDENTIAL UNITS	
UNITS PER FLOOR:	33 X6 FLOORS
TOTAL UNITS:	198

North

1 L3-L8 FLOOR PLAN

1/16"=1'-0"

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SIGNATURE

DATE xxx, 2016

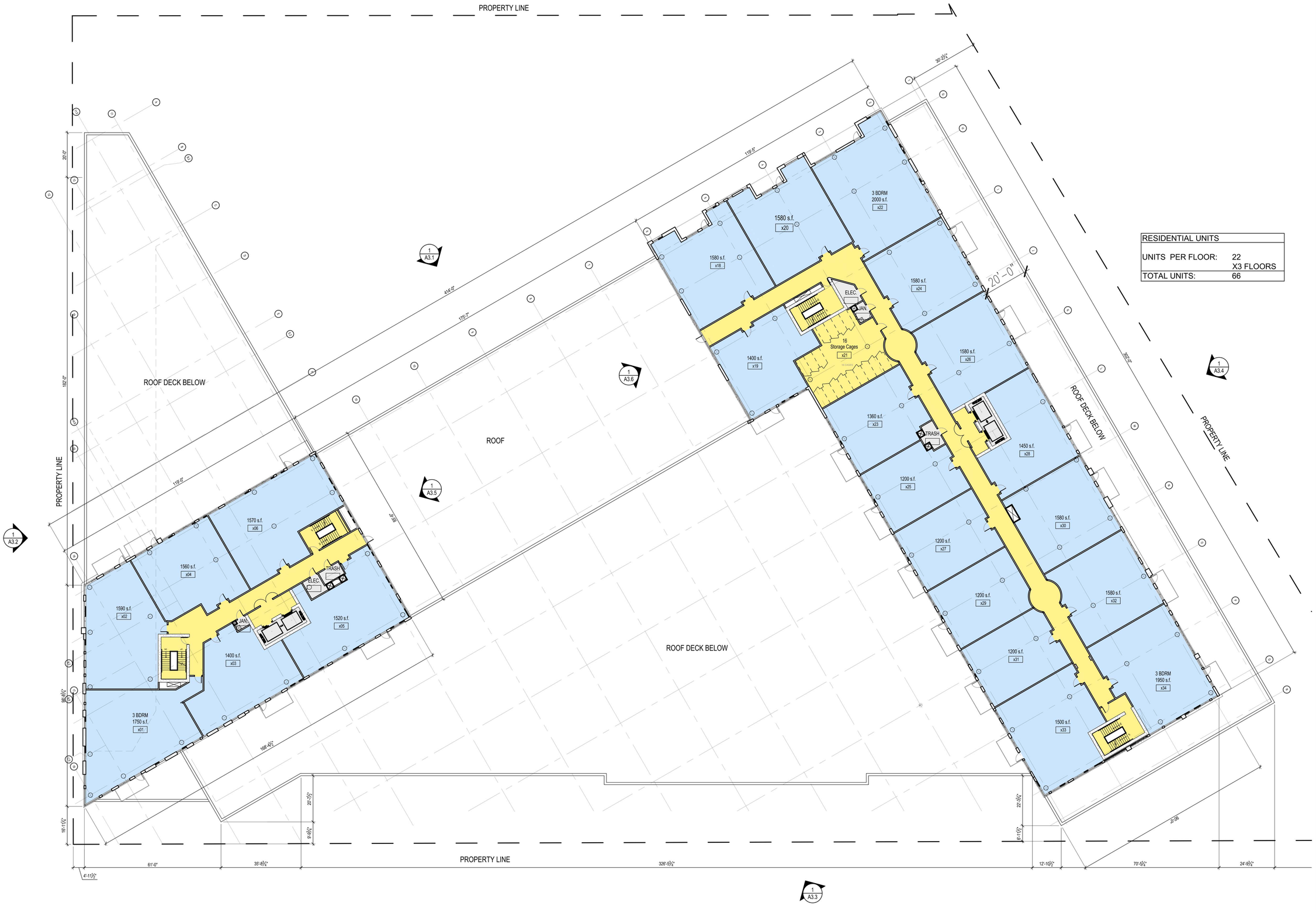
LICENSE NUMBER 15840

SHEET NAME:

L3-L8
Units

SHEET NO:

A1.3



RESIDENTIAL UNITS	
UNITS PER FLOOR:	22 X3 FLOORS
TOTAL UNITS:	66

North

1 L9-L11 FLOOR PLAN

1/16" = 1'-0"

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SIGNATURE

DATE xxx, 2016

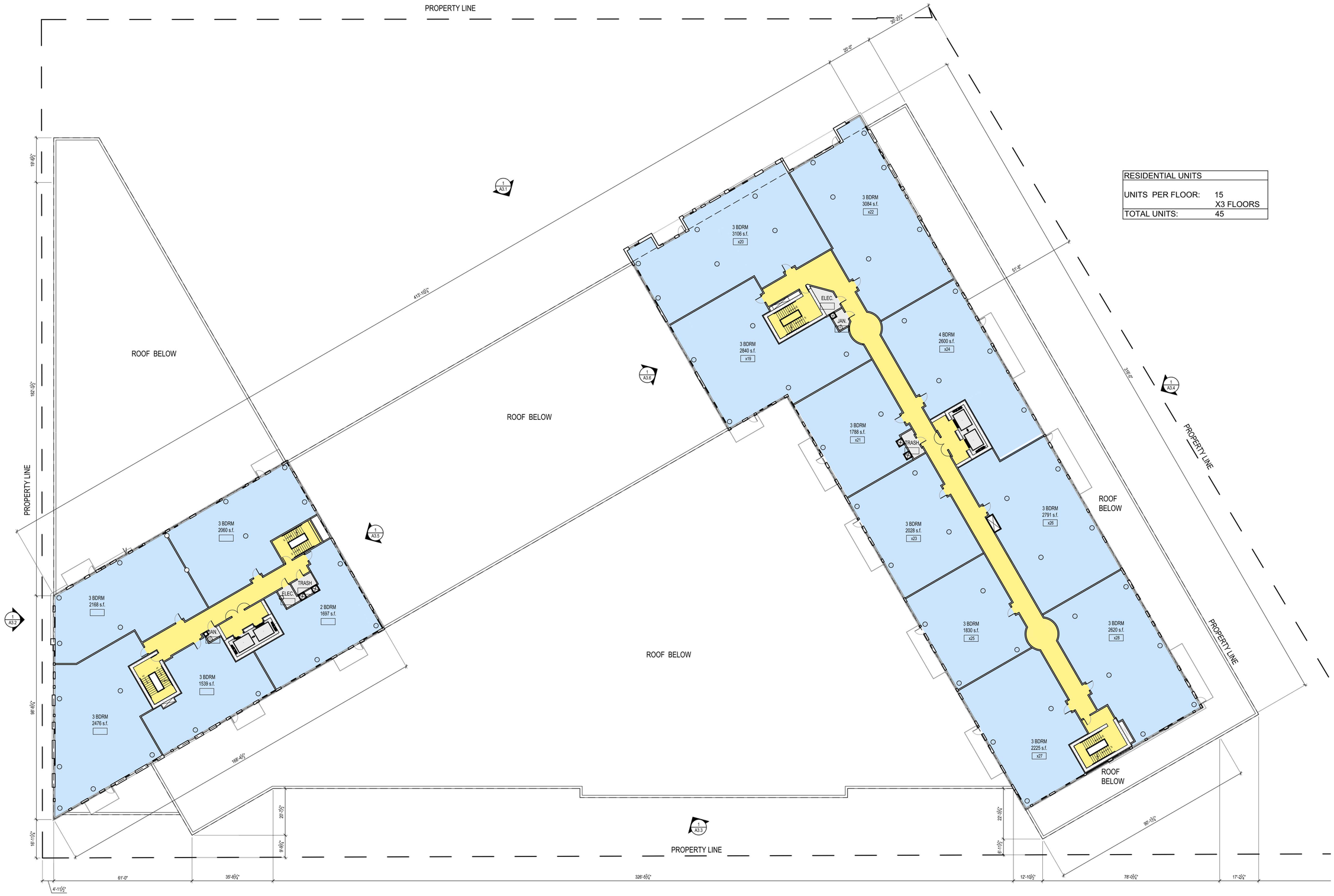
LICENSE NUMBER 15840

SHEET NAME:

L9-L11
Units

SHEET NO:

A1.9



RESIDENTIAL UNITS	
UNITS PER FLOOR:	15 X3 FLOORS
TOTAL UNITS:	45

North

1 L12-L14 FLOOR PLAN

1" = 20'-0"

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PDR REVIEW
4.18.16

MECH RFP REISSUE - 5.11.16

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JEFFREY L. OERTEL

SIGNATURE

DATE xxx, 2016

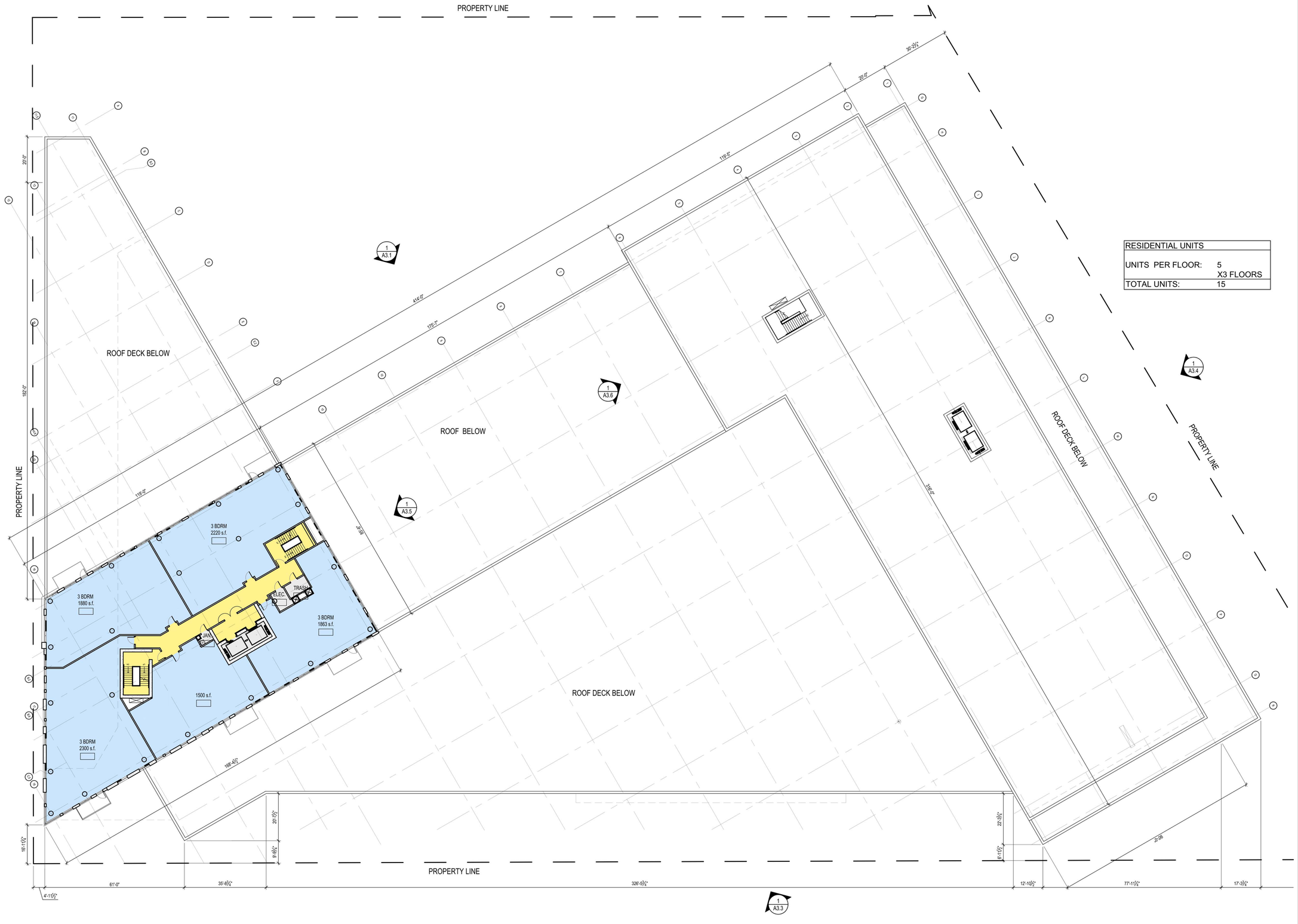
LICENSE NUMBER 15840

SHEET NAME:

L12-L14
Units

SHEET NO:

A1.12



RESIDENTIAL UNITS	
UNITS PER FLOOR:	5 X3 FLOORS
TOTAL UNITS:	15

North

1 L15-L17 FLOOR PLAN

1/16"=1'-0"

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PROJECT NUMBER: 15-03

DATE OF ISSUE: xxx, 2016

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CHECKED BY: JLO

REVISIONS:

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JEFFREY L. OERTEL

SIGNATURE

DATE xxx, 2016

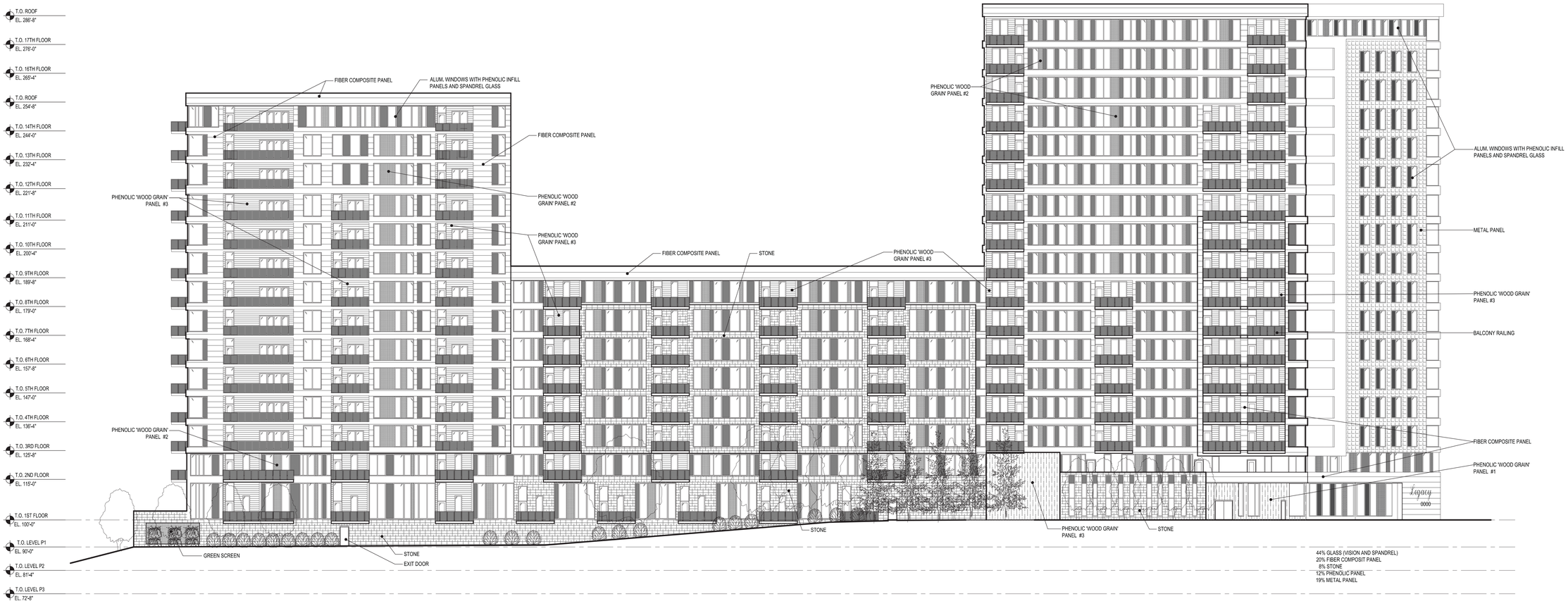
LICENSE NUMBER 15840

SHEET NAME:

L15-L17
Units

SHEET NO:

A1.15



1 WEST ELEVATION

1/16" = 1'-0"

OERTEL ARCHITECTS

1795 Saint Clair Avenue
St. Paul, Minnesota 55105

(651) 696-5186 Tel
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PROJECT NAME:

LEGACY

2nd St. & 13th Ave.
Mpls, MN 55415

PLANNING COMMISSION
4.8.16

PROJECT NUMBER: 15–03

DATE OF ISSUE: xxx, 2016

DRAWN BY: DLB

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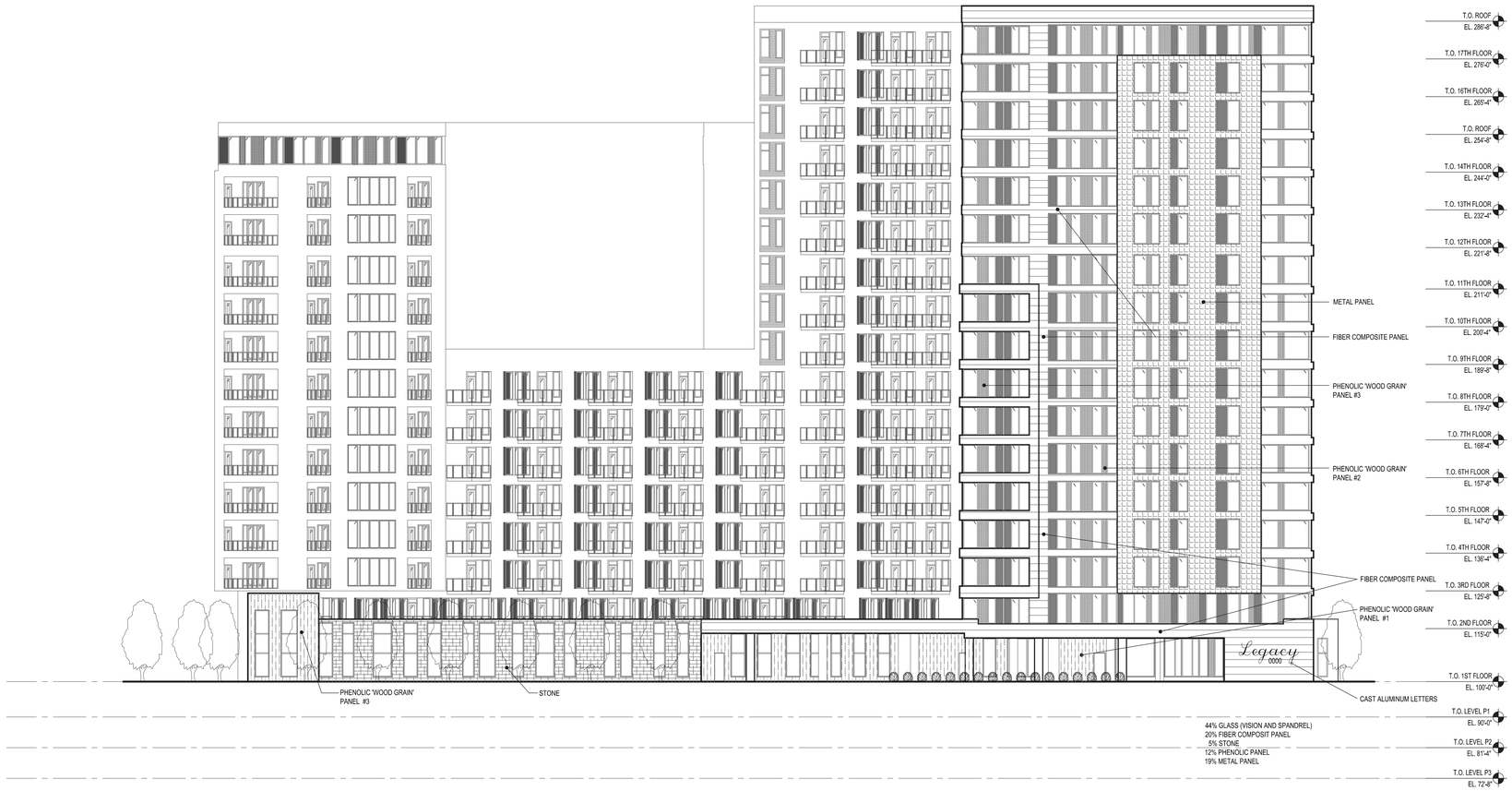
SHEET NAME:

WEST ELEVATION

SHEET NO:

A3.1



1 SOUTH/WEST ELEVATION

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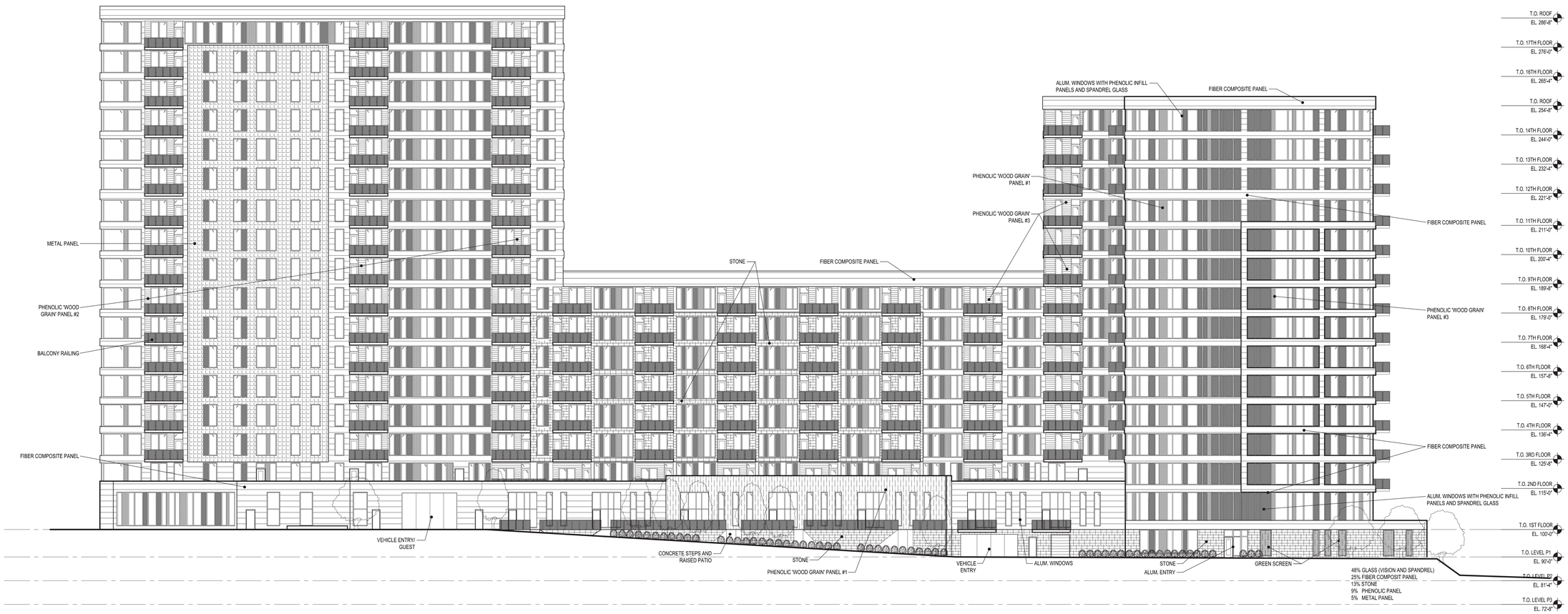
SHEET NAME:

SOUTH/WEST ELEVATION

SHEET NO:

A3.2



1 EAST ELEVATION

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DATE xxx, 2016

LICENSE NUMBER 15840

SHEET NAME:

EAST ELEVATION

SHEET NO:

A3.3

1 NORTH ELEVATION

1/16" = 1'-0"

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SIGNATURE

xxx, 2016
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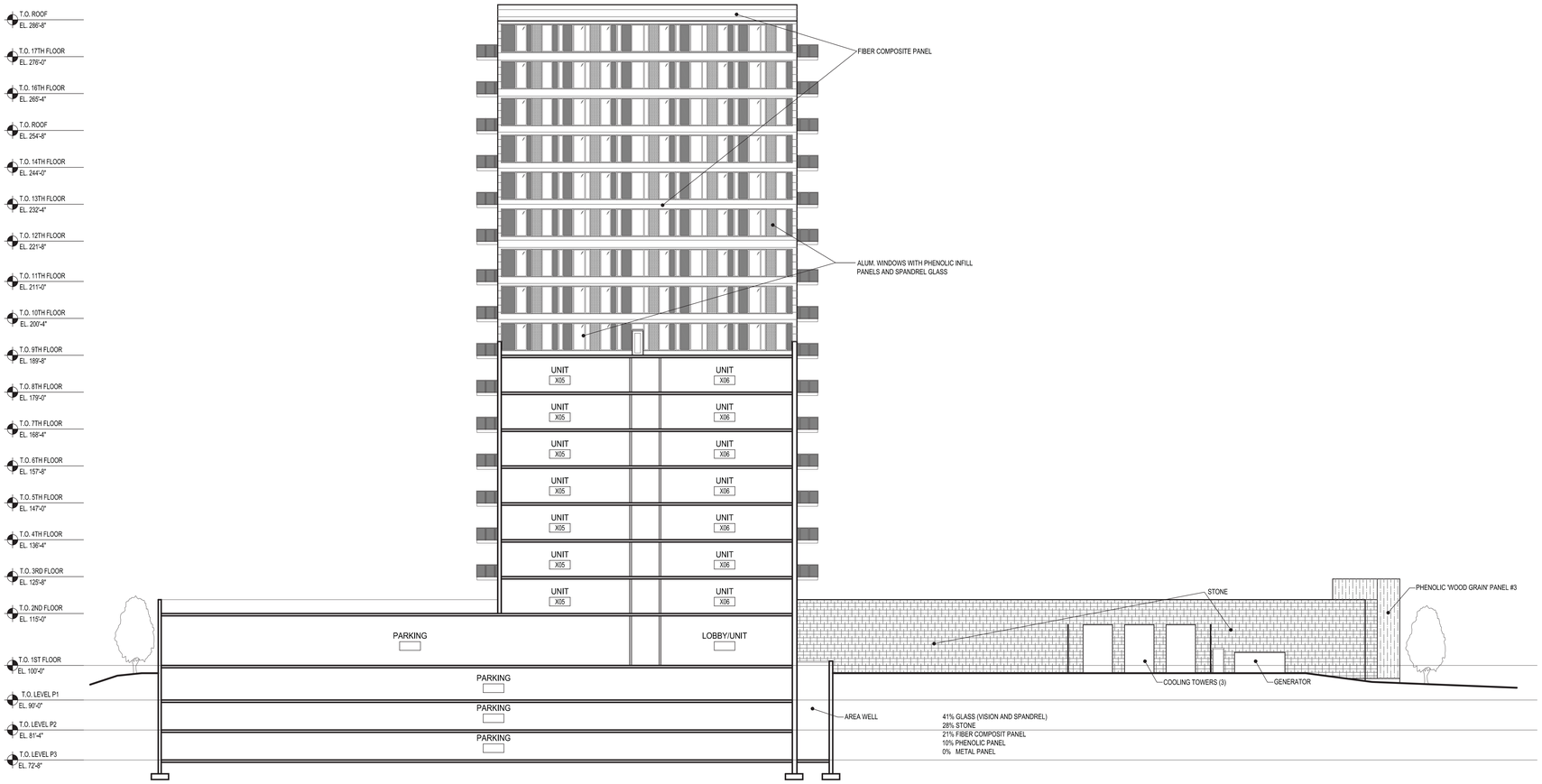
15840
LICENSE NUMBER

SHEET NAME:

NORTH ELEVATION

SHEET NO:

A3.4



1 NORTH ELEVATION/SECTION

1/16" = 1'-0"

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SHEET NAME:

NORTH ELEVATION

SHEET NO:

A3.5

1 SOUTH ELEVATION/SECTION

1/16" = 1'-0"

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SHEET NAME:

SOUTH ELEVATION

SHEET NO:

A3.6

1 WEST ELEVATION

1/16" = 1'-0"

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PDR REVIEW
4.18.16

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SOUTH/WEST ELEVATION

SHEET NO.

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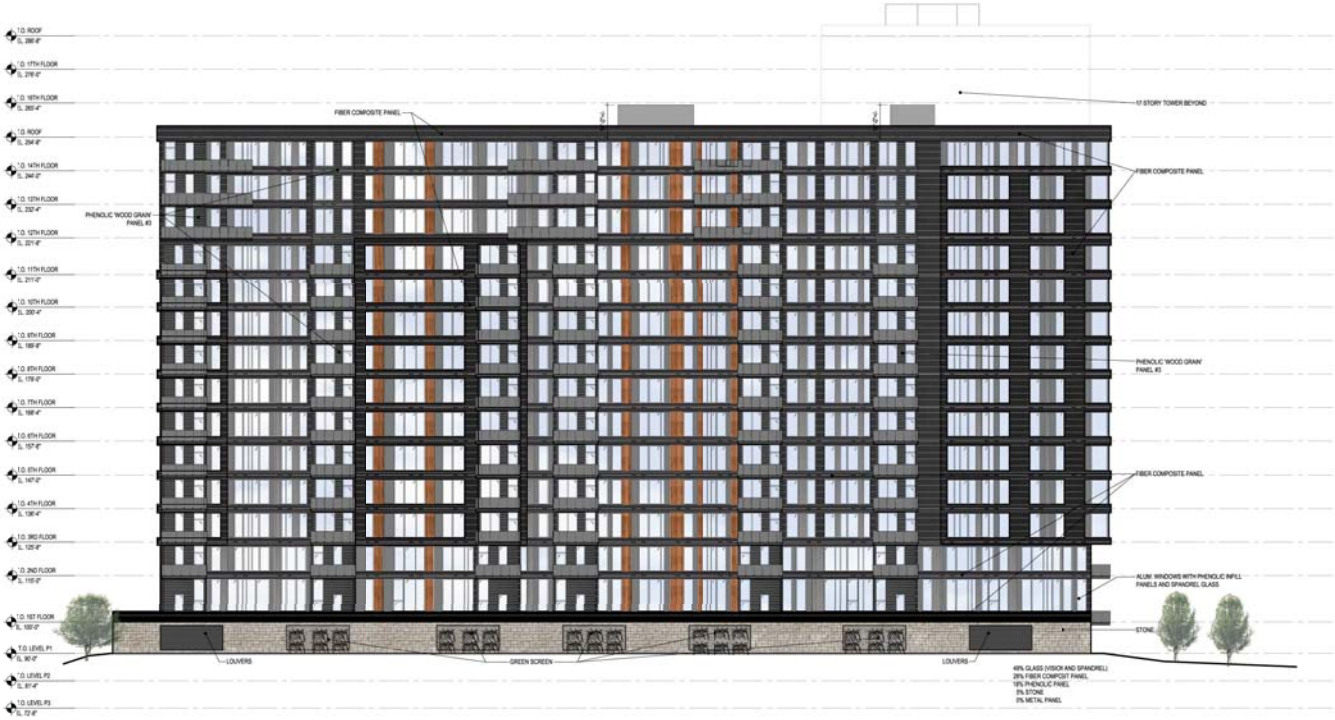

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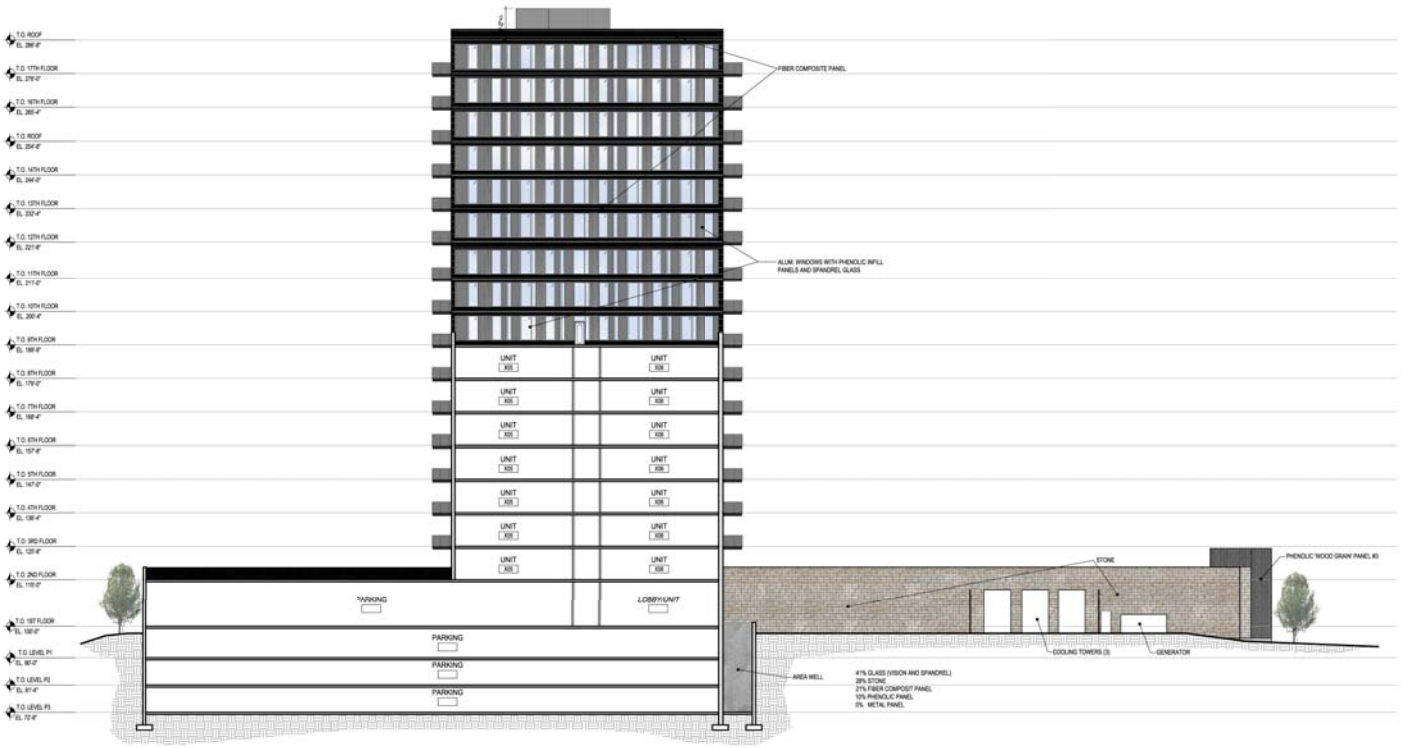

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SHEET NO.

A3.5

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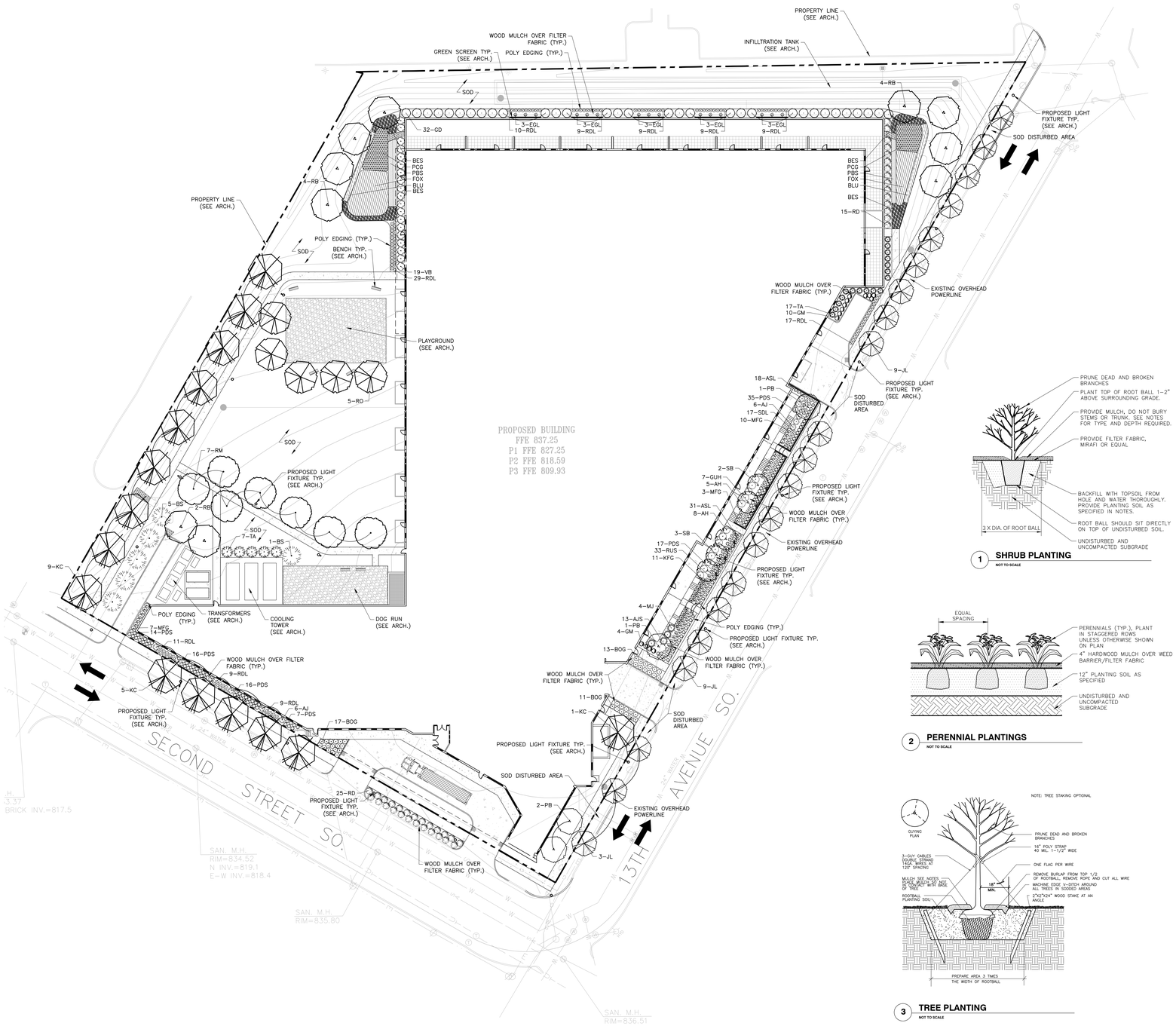
SHEET NO:

A3.6



1 **SHRUB PLANTING**
NOT TO SCALE

2 **PERENNIAL PLANTINGS**
NOT TO SCALE

3 **TREE PLANTING**
NOT TO SCALE

LEGEND

- DECIDUOUS OVERSTORY TREES
- ORNAMENTAL TREES
- CONIFEROUS TREES
- FLOWERING & EVERGREEN SHRUBS
- PERENNIALS
- POLY EDGING

PLANTING NOTES:

1. INSTALL 6" MIN. TOPSOIL TO ALL SOD AND SHRUB AREAS. FINE GRADE ALL SOD AREAS.
2. STAKE OR MARK ALL PLANT MATERIAL LOCATIONS PRIOR TO INSTALLATION. HAVE OWNERS REPRESENTATIVE APPROVE ALL STAKING PRIOR TO INSTALLATION.
3. ALL SHRUB AREAS TO BE BED MULCHED WITH 4" DEPTH OF SHREDDED HARDWOOD MULCH OVER FILTER FABRIC, UNLESS SPECIFIED AS OTHER. POLY–EDGER TO BE VALLEY VIEW BLACK DIAMOND OR APPROVED EQUAL.
4. INSTALL 4–6" DEPTH SHREDDED HARDWOOD MULCH AROUND ROOT SAUCER OF ALL TREES ISOLATED FROM PLANT BEDS.
5. TOPSOIL SHALL CONSIST OF 1:1:1 SELECT LOAMY TOPSOIL, PEAT MOSS, PIT RUN SAND.
6. COMPLETELY GUARANTEE ALL WORK FOR A PERIOD OF ONE YEAR BEGINNING AT THE DATE OF ACCEPTANCE. MAKE ALL REPLACEMENTS PROMPTLY (AS PER DIRECTION OF OWNER).
7. ALL MATERIAL SHALL COMPLY WITH THE LATEST EDITION OF THE AMERICAN STANDARD FOR NURSERY STOCK, AMERICAN ASSOCIATION OF NURSERYMEN.
8. ALL TREE TRUNKS SHALL BE WRAPPED WITH BROWN CREPE TREE WRAP. APPLY WRAP IN NOVEMBER AND REMOVE IN APRIL.
9. CALL GOPHER STATE ONE CALL AT 651–454–0002 FOR LOCATING ALL UNDERGROUND UTILITIES AND AVOID DAMAGE TO UTILITIES DURING THE COURSE OF THE WORK.
10. MAINTAIN ALL PLANT MATERIALS, INCLUDING WATERING, UNTIL THE TIME OF ACCEPTANCE.
11. COORDINATE INSTALLATION WITH GENERAL CONTRACTOR.
12. STAKING AND GUYING OF TREES IS OPTIONAL; MAINTAIN PLUMBNESS OF TREES FOR DURATION OF WARRANTY PERIOD.
13. SWEEP AND WASH ALL PAVED SURFACES AND REMOVE ALL DEBRIS RESULTING FROM LANDSCAPE OPERATIONS.
14. SUPPLY DESIGN AND INSTALLATION OF AN IRRIGATION PLAN WITH 100% COVERAGE FOR SOD AND SHRUB AREAS TO CONNECT TO EXISTING SYSTEM. SOD AND SHRUB AREAS TO BE ON SEPARATE ZONES. USE RAINBIRD OR APPROVED EQUAL COORDINATE WITH G.C.

RAINGARDEN NOTES

1. EXCAVATION EQUIPMENT IS PROHIBITED WITHIN THE RAIN GARDEN BASIN AS ROUGH EXCAVATION BECOMES WITHIN 2" OF FINISHED GRADE.
2. INFILTRATION TEST SHALL BE PERFORMED IN THE RAINGARDEN LOCATIONS. AFTER OVER–EXCAVATION, BUT BEFORE BACKFILLING OF PLANTING SOIL, CONDUCT AN INFILTRATION TEST DIRECTLY ON SUB–SOILS. CONDUCT A MODIFIED PHILIP–DUNNE INFILTROMETER TEST OR A DOUBLE–RING INFILTROMETER TEST TO ENSURE THAT 1 INCH/HR OR GREATER INFILTRATION RATES INTO THE SUBSOIL WILL BE PROVIDED. PROVIDE FOR SUBSOIL DEEP LOOSENING, IF NEEDED, TO REACH MINIMUM 1 INCH/HR INFILTRATION RATE.
3. RAINGARDEN PERENNIAL AREAS TO BE BED MULCHED WITH 2–3" DEPTH SHREDDED HARDWOOD MULCH AND 4" DEPTH AROUND TREES AND SHRUBS. PROVIDE FILTER FABRIC AROUND SHRUB MATERIAL.

LANDSCAPE SCHEDULE

R.O.W QTY	PROPERTY QTY	KEY	COMMON NAME / SCIENTIFIC NAME	SIZE	REMARKS
OVERSTORY TREES					
	5	RO	Red Oak / Quercus rubra	2.5" Cal. B&B	Straight Trunk, No V-Crotch
5	10	KC	Espresso Kentucky Coffee Tree / Gymnocladus dioicus 'Esspresso-JFS'	2.5" Cal. B&B	Straight Trunk, No V-Crotch
	4	PB	Paper Birch / Betula papyrifera	10' ht. B&B	Clump Form
	10	RB	River Birch / Betula nigra	10' ht. B&B	Clump Form
	7	RM	Northwoods Red Maple / Acer rubrum 'Northwood'	2.5" Cal. B&B	Straight Trunk, No V-Crotch
CONIFERS					
	6	BS	Blackhills Spruce / Picea glauca densata	6' ht. B&B	Full Form
ORNAMENTAL TREES					
	5	SB	Autumn Brilliance Serviceberry / Amerlanchier x grandiflora 'Autumn Brilliance'	6' ht. B&B	Clump Form
21		JL	Ivory Silk Lilac Tree / Syringa reticulata 'Ivory Silk'	6' ht. B&B	Straight Trunk, No V-Crotch
SHRUBS					
	14	GM	Goldmound Spirea / Spirea x 'Goldmound'	18" ht. cont.	Min. 5 canes at ht. specified.
	13	AH	Annabell Hydrangea / Hydrangea paniculata 'Annabell'	24" ht. cont.	Min. 5 canes at ht. specified.
	4	MN	Mint Julep Juniper / Juniperus chinensis 'Monlep'	30" dia. cont.	Min. 5 canes at dia. specified.
	12	AJ	Blue Arrow Juniper / Juniperus virginiana 'Blue Arrow'	4' B&B	Min. 5 canes at dia. specified.
25	15	RD	Alleman's Compact Red Dogwood / Cornus alba 'Alleman's Compact'	24" ht. cont.	Min. 5 canes at ht. specified.
	32	GD	Huron Gray Dogwood / Cornus racemosa 'Hurzam'	24" ht. cont.	Min. 5 canes at ht. specified.
	5	TA	Techny Arborvitae / Thuja occidentalis 'Techny'	6' ht. B&B	Min. 5 canes at ht. specified.
	17	TN	Technito Arborvitae / Thuja occidentalis 'Bailjohn'	36" ht. cont.	Min. 5 canes at ht. specified.
	19	VB	Bailey Compact Cranberry Bush / Viburnum trilobum 'Bailey Compact'	30" ht. cont.	Min. 5 canes at ht. specified.
PERENNIALS					
	41	BOG	Blue Oat Grass / Helictotrichon sempervirens 'Sapphire'	3 gal. cont.	
	11	KFG	Karl Foerster Feather Reed Grass / Calamagrostis x acutiflora 'Karl Foerster'	3 gal. cont.	
	21	MFG	Miscanthus Flame Grass / Miscanthus sinensus 'Purpurascens'	3 gal. cont.	
	105	PDS	Prairie Dropseed / Sporobolus Heterolepis	1 gal. cont.	
	74	RDL	Ruby Stella Daylilly / Hemerocallis 'Ruby Stella'	1 gal. cont.	
	23	SDL	Stella Supreme Daylily / Hemerocallis 'Stella Supreme'	1 gal. cont.	
	7	GUH	Guacamole Hosta / Hosta 'Guacamole'	1 gal. cont.	
	13	AJS	Autumn Joy Sedum / Sedum spectabile 'Autumn Joy'	1 gal. cont.	
	33	RUS	Little Spire Russian Sage / Perovskia atriplicifolia 'Little Spire'	1 gal. cont.	
	49	ASL	Astilbe x arendsii 'Beauty of Ernst'	1 gal. cont.	
	15	EGL	Englemann Creeper / Parthenocissus quinquefolia "Engelmannii"	1 gal. cont.	
RAINGARDEN PLANT LIST					
	137	BLA	Black Eyed Susan / Rudbekia hirta	1 gal. cont.	24" O.C.
	154	PCG	Prairie Cord Grass / Spartina pectinata	4" pot	24" O.C.
	246	FOX	Fox Sedge / Carex vulpinoidea	4" pot	24" O.C.
	93	STR	Prairie Blazing Star / Liatris pycnostachya	1 gal. cont.	24" O.C.
	78	BLU	Blue Lobelia / Lobelia siphilitica	1 gal. cont.	24" O.C.

ALLIANT ENGINEERING
233 Park Ave S, Ste 300
Minneapolis, MN 55415
612.758.3080 MAIN
612.758.3099 FAX
www.alliant-inc.com

LEGACY
2ND STREET & 13TH AVENUE SOUTH
CITY PLANNING SUBMITTAL
LANDSCAPE PLAN

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed LANDSCAPE ARCHITECT under the laws of the State of MINNESOTA

MARK KRONBECK, PLA, ASLA
4–5–16
Date License No.

QUALITY ASSURANCE/CONTROL

BY	DATE

DATE	ISSUE
4–5–16	CLIENT REVIEW
4–22–16	1ST CITY COMMENT REV.

PROJECT TEAM DATA
DESIGNED: JG
DRAWN: JG

L-1.0

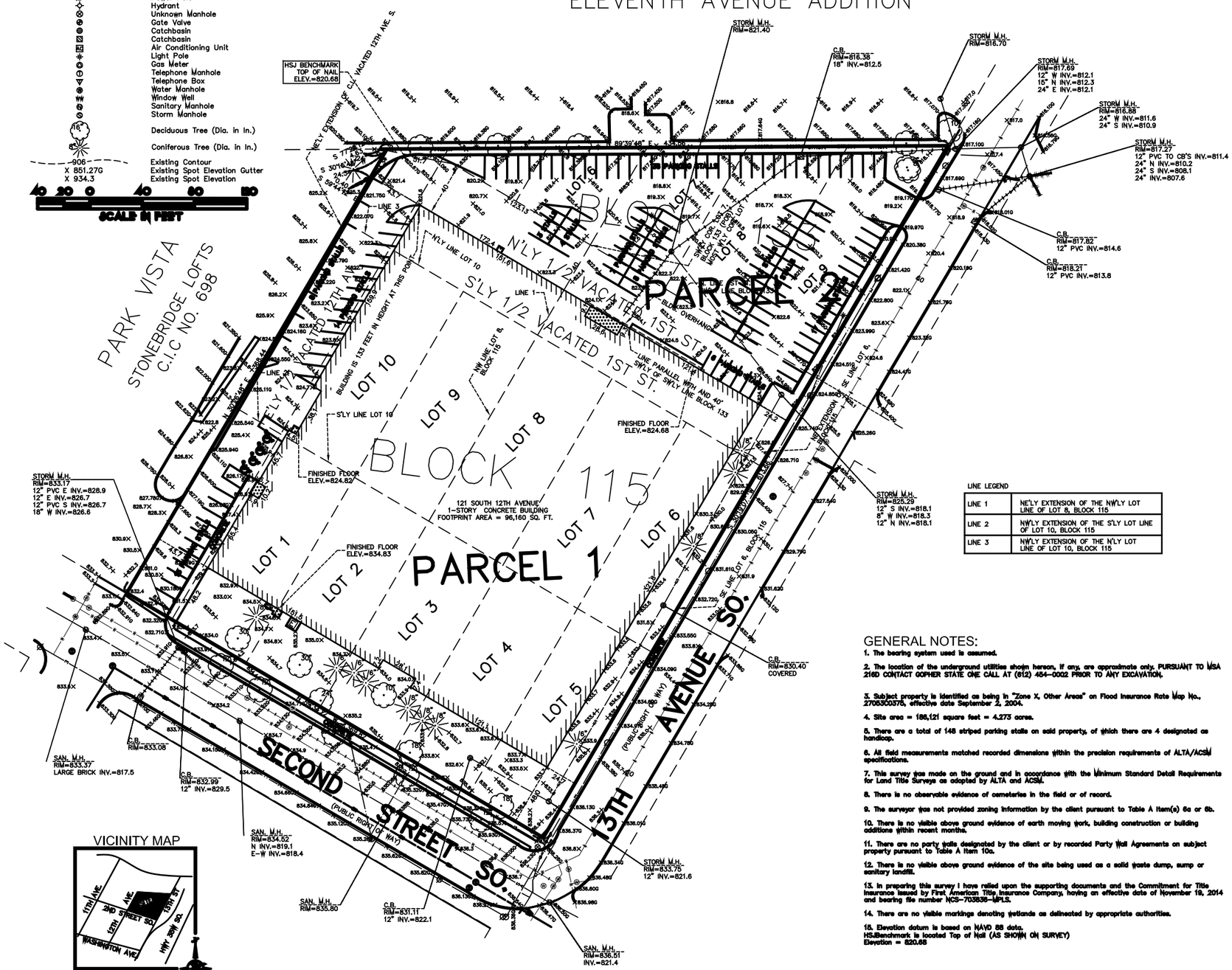
Drawing name: X:\2016\160012\plan sheets\City Planning Submittal\160012land.dwg Apr 22, 2016 3:24pm

LEGEND

- Property Monument
- Concrete
- Concrete Curb
- Fence
- Overhead Electric
- Underground Electric
- Underground Telephone
- Water
- Gas
- Sanitary Sewer
- Storm Sewer
- Electric Meter
- Electric Box
- Electric Manhole
- Power Pole
- Hydrant
- Unknown Manhole
- Gate Valve
- Catchbasin
- Catchbasin
- Air Conditioning Unit
- Light Pole
- Gas Meter
- Telephone Manhole
- Telephone Box
- Water Manhole
- Window Well
- Sanitary Manhole
- Storm Manhole
- 16" Deciduous Tree (Dia. in in.)
- 8" Coniferous Tree (Dia. in in.)
- 906 Existing Contour
- X 851.27G Existing Spot Elevation Gutter
- X 934.3 Existing Spot Elevation

40 20 0 40 80 120
SCALE IN FEET

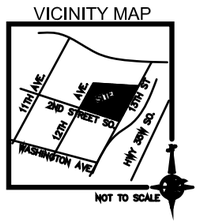
ELEVENTH AVENUE ADDITION



LINE LEGEND

LINE 1	NE'LY EXTENSION OF THE NW'LY LOT LINE OF LOT 8, BLOCK 115
LINE 2	NW'LY EXTENSION OF THE S'LY LOT LINE OF LOT 10, BLOCK 115
LINE 3	NW'LY EXTENSION OF THE N'LY LOT LINE OF LOT 10, BLOCK 115

VICINITY MAP



GENERAL NOTES:

1. The bearing system used is assumed.
2. The location of the underground utilities shown hereon, if any, are approximate only. PURSUANT TO MSA 216D CONTACT GOPHER STATE ONE CALL AT (612) 454-0002 PRIOR TO ANY EXCAVATION.
3. Subject property is identified as being in "Zone X, Other Areas" on Flood Insurance Rate Map No., 27053C0376, effective date September 2, 2004.
4. Site area = 186,121 square feet = 4.273 acres.
5. There are a total of 148 striped parking stalls on said property, of which there are 4 designated as handicap.
6. All field measurements matched recorded dimensions within the precision requirements of ALTA/ACSM specifications.
7. This survey was made on the ground and in accordance with the Minimum Standard Detail Requirements for Land Title Surveys as adopted by ALTA and ACSM.
8. There is no observable evidence of cemeteries in the field or of record.
9. The surveyor was not provided zoning information by the client pursuant to Table A Item(s) 6a or 6b.
10. There is no visible above ground evidence of earth moving work, building construction or building additions within recent months.
11. There are no party walls designated by the client or by recorded Party Wall Agreements on subject property pursuant to Table A Item 10a.
12. There is no visible above ground evidence of the site being used as a solid waste dump, sump or sanitary landfill.
13. In preparing this survey I have relied upon the supporting documents and the Commitment for Title Insurance issued by First American Title Insurance Company, having an effective date of November 19, 2014 and bearing file number NCS-703836-MPLS.
14. There are no visible markings denoting wetlands as delineated by appropriate authorities.
15. Elevation datum is based on NAVD 88 data.
HSJBenchmark is located Top of Nail (AS SHOWN ON SURVEY)
Elevation = 820.68

STATEMENT OF POTENTIAL ENCROACHMENTS:

There are no visible above ground encroachments over or across any property lines of subject property.

BOUNDARY DESCRIPTION

Parcel 1:

Lots 1, 2, 3, 4, 5, 6, 7, 8, 9 and 10, Block 115, Town of Minneapolis, according to the recorded plat thereof, Hennepin County, Minnesota.

That part of the Southerly one-half of vacated First Street South which lies between the extension Northeasterly of the Southeasterly lot line of Lot 6, Block 115, Town of Minneapolis, and the extension Northeasterly of the Northwesterly lot line of Lot 8 in said Block 115.

That part of the Southerly one-half of vacated First Street South which lies between the extension Northeasterly of the Northwesterly lot line of Lot 8, Block 115, Town of Minneapolis, and the extension Northeasterly of the center line of vacated 12th Avenue South.

That part of the Northerly one-half of vacated First Street South described as follows: Beginning at the Southwesterly corner of Lot 7, Block 133, Town of Minneapolis; thence Southerly at right angles to the North line of said First Street a distance of 40 feet; thence Westerly on a line parallel to the North line of said First Street a distance of 172.4 feet; thence Northerly on an angle to the right of 90 degrees a distance of 24.34 feet; thence Easterly on an angle to the right of 72 degrees 22 minutes a distance of 51.7 feet to the North line of said First Street; thence Easterly along the said North line of First Street a distance of 123.13 feet to the point of beginning.

That part of the Easterly one-half of vacated 12th Avenue South lying between the extension Northeasterly of the Southerly lot line of Lot 10, Block 115, Town of Minneapolis, and the extension Northwesterly of the Northerly lot line of said Lot 10.

(Abstract property)

Parcel 2:

That part of Block 133 and those parts of the vacated parts of 12th Avenue South, which was platted as Hay Street, First Street South, which was platted as 1st Street, and abandoned Bluff Street all in the Town of Minneapolis, described as follows: Beginning at a point on a line drawn parallel with and 40 feet Southwesterly from the Southwesterly line of said Block 133, and its Northwesterly extension, distant 172.40 feet Northwesterly along said parallel line from its intersection with a line drawn Southwesterly, at a right angle to the Southwesterly line of said Block 133, from the most Westerly corner of Lot 7, in said Block 133; thence Southeasterly along said parallel line a distance of 370.40 feet to its intersection with the Southwesterly extension of the Southeasterly line of said Block 133; thence Northeasterly to the most Southerly corner of said Block 133; thence Southeasterly to the most Westerly corner of said Block 132; thence Southeasterly along the Southwesterly line of said Block 132 to the most Southerly corner of Lot 3 in said Block 132; thence Northeasterly along the Southeasterly line of said Lot 3 to the most Easterly corner of Lot 3 in said Block 132; thence Southeasterly along the Southwesterly line of Lot 7 in said Block 132 a distance of 99 feet; thence at right angles Northeasterly to the East line of Section 28, Township 29, Range 24; thence North along the East line of said Section 28 a distance of 117.38 feet; thence Westerly 248.92 feet along a curve concave to the South having a radius of 1445.17 feet, the chord of said curve bears North 85 degrees 04 minutes 50 seconds West (assuming the East line of the Northeast Quarter of Section 28 as bearing North); thence South 88 degrees 59 minutes West along a line which is tangent to said curve a distance of 500.53 feet, more or less, to its intersection with a line drawn Northeasterly, at a right angle to said parallel line from the point of beginning; thence Southwesterly to the point of beginning; except that part of the following described tract lying within the boundaries of the above described land: Beginning at the most Westerly corner of Lot 7 in said Block 133; thence Southwesterly at right angles to the Southwesterly line of said Block 133 a distance of 40 feet; thence Northwesterly along a line parallel with the Southwesterly line of said Block 133, and its Northwesterly extension, a distance of 172.40 feet; thence Northeasterly at right angles a distance of 24.34 feet; thence Easterly deflecting to the right 72 degrees 22 minutes, a distance of 51.70 feet to the Southwesterly line of said Block 133; thence Southeasterly along the Southwesterly line of said Block 133 to the point of beginning of said exception; which lies Westerly of the Southeasterly line of said Block 133 and its extensions.

(Torrens property, Certificate of Title No. 1315861)

NOTES CORRESPONDING TO SCHEDULE B:

10. Reservations by the State of Minnesota of minerals and mineral rights. (As to Parcel 1) (NON-SURVEY MATTER)

11. Resolution vacating a portion of a certain street and avenues in the Original Town of Minneapolis approved November 4, 1903, recorded December 16, 1949, in Book 590 of Misc., Page 32, as Document No. 2800899. (As to Parcel 1) (VACATED 1ST AND 12TH AVENUE DEPICTED ON SURVEY)

12. Resolution vacating a part of 1st Street South adopted August 11, 1967, recorded September 20, 1967, as Document No. 3676615. (As to Parcel 1) (VACATED PORTION OF 1ST STREET SOUTH DEPICTED ON SURVEY)

13. Resolution vacating a part of 1st Street South adopted August 11, 1967, recorded March 21, 1968, as Document No. 3706316. (As to Parcel 1) (VACATED PORTION OF 1ST STREET SOUTH DEPICTED ON SURVEY)

14. Resolution of the City of Minneapolis vacating all of 12th Avenue South north of 2nd Street South adopted August 20, 2010, recorded September 10, 2010, as Document No. 9557320. (As to Parcel 1) (VACATED PORTION OF 12TH AVENUE SOUTH DEPICTED ON SURVEY)

15. Notice of Adverse Claim dated December 10, 1984, recorded December 18, 1984, as Document No. 1617569. (As to Parcel 2)

CERTIFICATION:

To National Life Insurance Company and Life Insurance Company of the Southwest, First American Title Insurance Company:

This is to certify that this map or plat and the survey on which it is based were made in accordance with the 2011 Minimum Standard Detail Requirements for ALTA/ACSM Land Title Surveys, jointly established and adopted by ALTA and NSPS, and includes Items 1, 2, 3, 4, 5, 6(b), 7(a), 7(b1), 8, 9, 10(a), 11(a), 16, 18, 19, 20 and 21 of Table A thereof.

The field work was completed on December 18, 2015.

Date of Plat or Map: December 18, 2015

Thomas E. Hoderff, L.S.
Minn. Reg. No. 23677

SCALE: 1 INCH = 40 FEET

REVISIONS

Date:	

I hereby certify that this survey, plan or report was prepared by me or under my direct supervision and that I am a duly Registered Land Surveyor under the laws of the State of Minnesota

Thomas E. Hoderff, L.S.
Minnesota Reg. No. 23677

Date: _____

ALTA LAND TITLE SURVEY WITH TOPOGRAPHY

For:

SHAMROCK DEVELOPMENT

SITE:

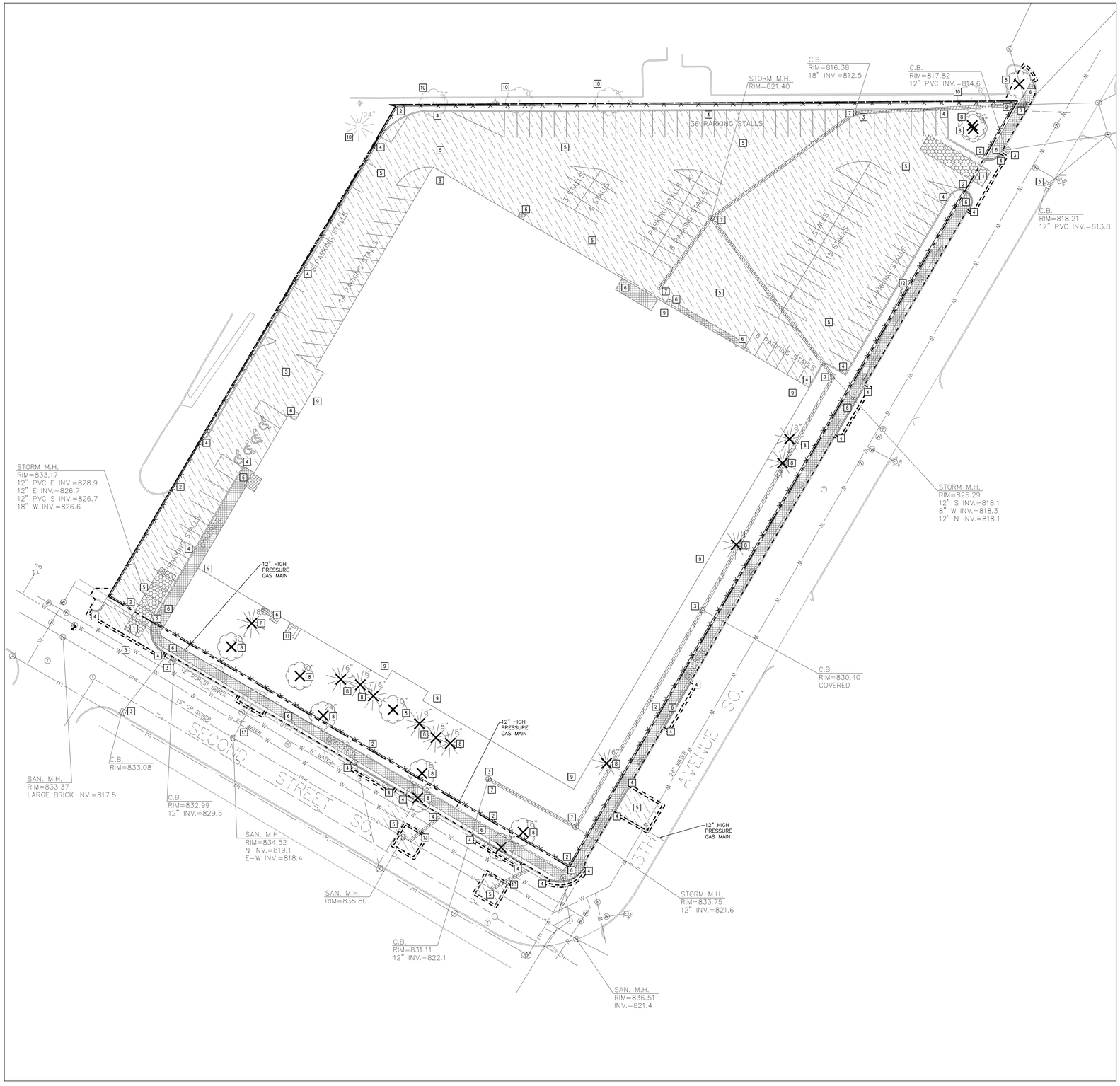
121 S. 12TH

MINNEAPOLIS, MINNESOTA

HENNEPIN COUNTY

HARRY S. JOHNSON CO., INC.
LAND SURVEYORS
9063 Lyndale Avenue South
Bloomington, MN. 55437
Tele. 952-884-5341 Fax 952-884-5344
www.hsjsurveyors.com

Book: 647	File No. 1-3-9001T
Page: 8	Job Number 2015507
Crew Technician: CT	
Sheet No.	1 OF 1



1 C1.0 SELECTIVE SITE DEMOLITION AND EROSION CONTROL PLAN
1"=30'

PROPOSED PLAN SYMBOLS

- CONSTRUCTION LIMITS
- SILTATION FENCE
- PROPERTY LINE
- SAWCUT LINE (APPROX.)
- ROCK CONSTRUCTION ENTRANCE
- BITUMINOUS REMOVAL
- CONCRETE REMOVAL
- PIPE REMOVAL
- CURB REMOVAL
- TREE REMOVAL

ABBREVIATIONS

BLDG	Building
BM	Bench Mark
CB	Catch Basin
CONC	Concrete
DIP	Ductile Iron Pipe
ELEV	Elevation
EX	Existing
FFE	Finished Floor Elevation
INV	Invert
MAX	Maximum
MH	Manhole
MIN	Minimum
PVC	Polyvinyl Chloride
RCP	Reinforced Concrete Pipe

APPROXIMATE DISTURBED AREA IS 4.39 ACRES

NOTE:
STORM SEWER INLETS NOT SHOWN ON PLAN MAY RECEIVE RUNOFF FROM CONSTRUCTION ACTIVITIES. INSTALL INLET SEDIMENT PROTECTION PER DETAIL 3/C5.0 ON ALL STORM INLETS THAT MAY RECEIVE RUNOFF.

N

30' 15' 0 30' 60'
SCALE 1" = 30'

KEYED NOTES

KEYED NOTES ARE DENOTED BY NO ON PLAN.

1. INSTALL ROCK CONSTRUCTION ENTRANCE. REFER TO DETAIL 2/C5.0.
2. INSTALL SILT FENCE. REFER TO DETAIL 1/C5.0.
3. INSTALL INLET SEDIMENT PROTECTION. REFER TO DETAIL 3/C5.0.
4. REMOVE CURB AND GUTTER IN ITS ENTIRETY TO THE EXTENT SHOWN. SAWCUT AND REMOVE AT NEAREST JOINT. THE CURB AND GUTTER WITHIN THE PROPERTY LINE SHALL BE REMOVED IN ITS ENTIRETY.
5. REMOVE BITUMINOUS PAVEMENT IN ITS ENTIRETY TO THE EXTENTS SHOWN.
6. REMOVE CONCRETE PAVEMENT IN ITS ENTIRETY TO THE EXTENTS SHOWN.
7. REMOVE STORM SEWER IN ITS ENTIRETY TO THE EXTENTS SHOWN.
8. REMOVE TREE IN ITS ENTIRETY INCLUDING STUMP.
9. REMOVE BUILDING IN ITS ENTIRETY, INCLUDING EXTERIOR STAIRS, OVERHANGS, FOOTINGS, AND SMALL UTILITIES. COORDINATE REMOVAL OF SMALL UTILITIES WITH LOCAL UTILITY COMPANIES.
10. EXISTING TREE TO REMAIN. PROTECT AT ALL TIMES.
11. REMOVE ELECTRICAL MANHOLE. COORDINATE RELOCATION OF MANHOLE WITH ELECTRICAL UTILITY COMPANY.
12. REMOVE LIGHT POLE IN ITS ENTIRETY, INCLUDING FOUNDATION.
13. REUSE EXISTING SEWER SERVICE.

DEMOLITION AND REMOVAL NOTES:

1. PRIOR TO START OF DEMOLITION, ALL EROSION AND SEDIMENT CONTROL MEASURES ARE TO BE INSTALLED BY THE CONTRACTOR AND INSPECTED BY THE CITY OF MINNEAPOLIS. ALL SILT FENCES SHALL BE INSTALLED AND INSPECTED PRIOR TO ANY CONSTRUCTION ACTIVITY. SILT FENCES SHALL BE INSTALLED ALONG THE CONTOUR.
2. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN THE LOCATION OF ALL EXISTING UTILITIES. THE CONTRACTOR SHALL VERIFY THE LOCATION, ELEVATION AND MARK ALL EXISTING UTILITIES 48 HOURS BEFORE CONSTRUCTION STARTS. THE ENGINEER, ARCHITECT OR OWNER DOES NOT GUARANTEE THAT ALL THE UTILITIES ARE MAPPED, OR IF MAPPED, ARE SHOWN CORRECTLY. CONTACT GOPHER STATE ONE CALL AT 651-454-0002 FOR FIELD LOCATING EXISTING UTILITIES. CONTACT UTILITY OWNER IF DAMAGE OCCURS DUE TO CONSTRUCTION.
3. THERE MAY BE MISCELLANEOUS ITEMS TO BE REMOVED THAT ARE NOT IDENTIFIED ON THESE PLANS. THE CONTRACTOR SHALL VISIT THE SITE AND REVIEW THE DOCUMENTS TO OBTAIN A CLEAR UNDERSTANDING OF THE INTENDED SCOPE OF WORK.
4. PRIOR TO START OF CONSTRUCTION, DISCONNECT ALL GAS AND ELECTRIC SERVICES. COORDINATE DISCONNECTION OF EACH UTILITY WITH THE UTILITY OWNER. REMOVE ALL GAS AND ELECTRIC LINES UNDER PROPOSED BUILDING FOOTPRINT.
5. ANY UTILITIES NOT INDICATED FOR REMOVAL OR ABANDONMENT, ARE TO BE PROTECTED AT ALL TIMES.
6. ALL EXISTING CURB AND GUTTER IS TO BE REMOVED WITHIN THE SCOPE OF THE PROJECT FROM THE SAW CUT LINES TO THE NEAREST JOINT.
7. THE BACKGROUND INFORMATION WAS PREPARED BY HARRY S JOHNSON. (952) 884-5341.
8. ALL WORK IN THE PUBLIC RIGHT OF WAY IS TO BE COORDINATED WITH THE CITY OF MINNEAPOLIS. ROADWAY REPAIRS, BOULEVARD REPAIRS, AND TRAFFIC CONTROL ARE TO BE PER CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS.

GENERAL NOTES:

1. CONCRETE CURB AND GUTTER REMOVAL, PAVEMENT REMOVAL, AND UTILITY REMOVAL LIMITS ARE TO BE COORDINATED THE CITY OF MINNEAPOLIS AND UTILITY OWNER. REFER TO ALL CONSTRUCTION DOCUMENTS.
2. THE CONTRACTOR SHALL DEVELOP AND IMPLEMENT A TRAFFIC CONTROL PLAN WHILE WORKING WITHIN THE RIGHT-OF-WAY. THE TRAFFIC CONTROL PLAN SHALL BE APPROVED BY THE CITY ENGINEERING DEPARTMENT PRIOR TO STREET ENCROACHMENT.
3. CONTRACTOR SHALL VISIT THE SITE PRIOR TO BIDDING AND REVIEW ALL CONSTRUCTION DOCUMENTS AND GEOTECHNICAL REPORTS. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR ITEMS THAT SHOULD HAVE BEEN ANTICIPATED BY PERFORMING THE ABOVE.
4. THE ROCK CONSTRUCTION ENTRANCE INDICATED ON THE PLAN IS SHOWN IN AN APPROXIMATE LOCATION. PRIOR TO START OF CONSTRUCTION, THE CONTRACTOR IS TO COORDINATE WITH THE CITY OF MINNEAPOLIS FOR THE EXACT ROCK CONSTRUCTION ENTRANCE LOCATION.

MINNEAPOLIS STANDARD EROSION CONTROL NOTES – (JANUARY 7, 2011)

1. **CONTRACTOR MUST CALL A CONSTRUCTION START 48 HOURS PRIOR TO ANY LAND DISTURBANCES 612-673-3867. FAILURE TO DO SO MAY RESULT IN FINES, THE REVOCATION OF PERMIT AND A STOP WORK ORDER BEING ISSUED.**
2. INSTALL PERIMETER EROSION CONTROL AT THE LOCATIONS SHOWN ON THE PLANS PRIOR TO THE COMMENCEMENT OF ANY LAND DISTURBANCE OR CONSTRUCTION ACTIVITIES.
3. BEFORE BEGINNING CONSTRUCTION, INSTALL A TEMPORARY ROCK CONSTRUCTION ENTRANCE AT EACH POINT WHERE VEHICLES EXIT THE CONSTRUCTION SITE. USE 2 INCH OR GREATER DIAMETER ROCK IN A LAYER AT LEAST 6 INCHES THICK ACROSS THE ENTIRE WIDTH OF THE ENTRANCE. EXTEND THE ROCK ENTRANCE AT LEAST 50 FEET INTO THE CONSTRUCTION ZONE USING A GEO-TEXTILE FABRIC BENEATH THE AGGREGATE TO PREVENT MIGRATION OF SOIL INTO THE ROCK FROM BELOW.
4. REMOVE ALL SOILS AND SEDIMENTS TRACKED OR OTHERWISE DEPOSITED ONTO PUBLIC AND PRIVATE PAVEMENT AREAS. REMOVAL SHALL BE ON A DAILY BASIS WHEN TRACKING OCCURS AND MAY BE ORDERED BY MINNEAPOLIS INSPECTORS AT ANY TIME IF CONDITIONS WARRANT. SWEEPING SHALL BE MAINTAINED THROUGHOUT THE DURATION OF THE CONSTRUCTION AND DONE IN A MANNER TO PREVENT DUST BEING BLOWN TO ADJACENT PROPERTIES.
5. INSTALL INLET PROTECTION AT ALL PUBLIC AND PRIVATE CATCH BASIN INLETS, WHICH RECEIVE RUNOFF FROM THE DISTURBED AREAS. CONTRACTOR SHALL CLEAN, REMOVE SEDIMENT OR REPLACE STORM DRAIN INLET PROTECTION DEVICES ON A ROUTINE BASIS SUCH THAT THE DEVICES ARE FULLY FUNCTIONAL FOR THE NEXT RAIN EVENT. SEDIMENT DEPOSITED IN AND/OR PLUGGING DRAINAGE SYSTEMS IS THE RESPONSIBILITY OF THE CONTRACTOR. **HAY BALES OR FILTER FABRIC WRAPPED GRATES ARE NOT ALLOWED FOR INLET PROTECTION.**
6. LOCATE SOIL OR DIRT STOCKPILES NO LESS THAN 25 FEET FROM ANY PUBLIC OR PRIVATE ROADWAY OR DRAINAGE CHANNEL. IF REMAINING FOR MORE THAN SEVEN DAYS, STABILIZE THE STOCKPILES BY MULCHING, VEGETATIVE COVER, TARPS, OR OTHER MEANS. CONTROL EROSION FROM ALL STOCKPILES BY PLACING SILT BARRIERS AROUND THE PILES. TEMPORARY STOCKPILES LOCATED ON PAVED SURFACES MUST BE NO LESS THAN TWO FEET FROM THE DRAINAGE/GUTTER LINE AND SHALL BE COVERED IF LEFT MORE THAN 24 HOURS.
7. MAINTAIN ALL TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES IN PLACE UNTIL THE CONTRIBUTING DRAINAGE AREA HAS BEEN STABILIZED. INSPECT TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES ON A DAILY BASIS AND REPLACE DETERIORATED, DAMAGED, OR ROTTED EROSION CONTROL DEVICES IMMEDIATELY.
8. TEMPORARILY OR PERMANENTLY STABILIZE ALL CONSTRUCTION AREAS WHICH HAVE UNDERGONE FINAL GRADING, AND ALL AREAS IN WHICH GRADING OR SITE BUILDING CONSTRUCTION OPERATIONS ARE NOT ACTIVELY UNDERWAY AGAINST EROSION DUE TO RAIN, WIND AND RUNNING WATER WITHIN 7-14 DAYS. USE SEED AND MULCH, EROSION CONTROL MATTING, AND/OR SODDING AND STAKING IN GREEN SPACE AREAS. REMOVE ALL TEMPORARY SYNTHETIC, STRUCTURAL, NON-BIODEGRADABLE EROSION AND SEDIMENT CONTROL DEVICES AFTER THE SITE HAS UNDERGONE FINAL STABILIZATION WITH PERMANENT VEGETATION ESTABLISHMENT. FINAL STABILIZATION FOR PURPOSES OF THIS REMOVAL IS 70% ESTABLISHED COVER OVER DENUDED AREA.
9. READY MIXED CONCRETE AND CONCRETE BATCH/MIX PLANTS ARE PROHIBITED WITHIN THE PUBLIC RIGHT OF WAY. ALL CONCRETE RELATED PRODUCTION, CLEANING AND MIXING ACTIVITIES SHALL BE DONE IN THE DESIGNATED CONCRETE MIXING/WASHOUT LOCATIONS AS SHOWN IN THE EROSION CONTROL PLAN. UNDER NO CIRCUMSTANCE MAY WASHOUT WATER DRAIN ONTO THE PUBLIC RIGHT OF WAY OR INTO ANY PUBLIC OR PRIVATE STORM DRAIN CONVEYANCE.
10. CHANGES TO APPROVED EROSION CONTROL PLAN MUST BE APPROVED BY THE EROSION CONTROL INSPECTOR PRIOR TO IMPLEMENTATION. CONTRACTOR TO PROVIDE INSTALLATION AND DETAILS FOR ALL PROPOSED ALTERNATE TYPE DEVICES.
11. IF DEWATERING OR PUMPING OF WATER IS NECESSARY, THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ANY NECESSARY PERMITS AND/OR APPROVALS PRIOR TO DISCHARGE OF ANY WATER FROM THE SITE. IF THE DISCHARGE FROM THE DEWATERING OR PUMPING PROCESS IS TURBID OR CONTAINS SEDIMENT LADEN WATER, IT MUST BE TREATED THROUGH THE USE OF SEDIMENT TRAPS, VEGETATIVE FILTER STRIPS, OR OTHER SEDIMENT REDUCING MEASURES SUCH THAT THE DISCHARGE IS NOT VISIBLY DIFFERENT FROM THE RECEIVING WATER. ADDITIONAL EROSION CONTROL MEASURES MAY BE REQUIRED AT THE DISCHARGE POINT TO PREVENT SCOUR EROSION. THE CONTRACTOR SHALL PROVIDE A DEWATERING/PUMPING PLAN TO THE EROSION CONTROL INSPECTOR PRIOR TO INITIATING DEWATERING ACTIVITIES.

OERTEL ARCHITECTS
1795 SAINT CLAIR AVENUE
ST. PAUL, MINNESOTA 55105
(651) 696-5186 TEL
(651) 696-5188 FAX
WWW.OERTELARCHITECTS.COM

PROJECT NAME:
LEGACY

2nd St. & 13th Ave.
Mpls, MN 55415

PROJECT NUMBER: 14-18
DATE OF ISSUE: April 8, 2016
DRAWN BY: NPA
CHECKED BY: JWM
REVISIONS:

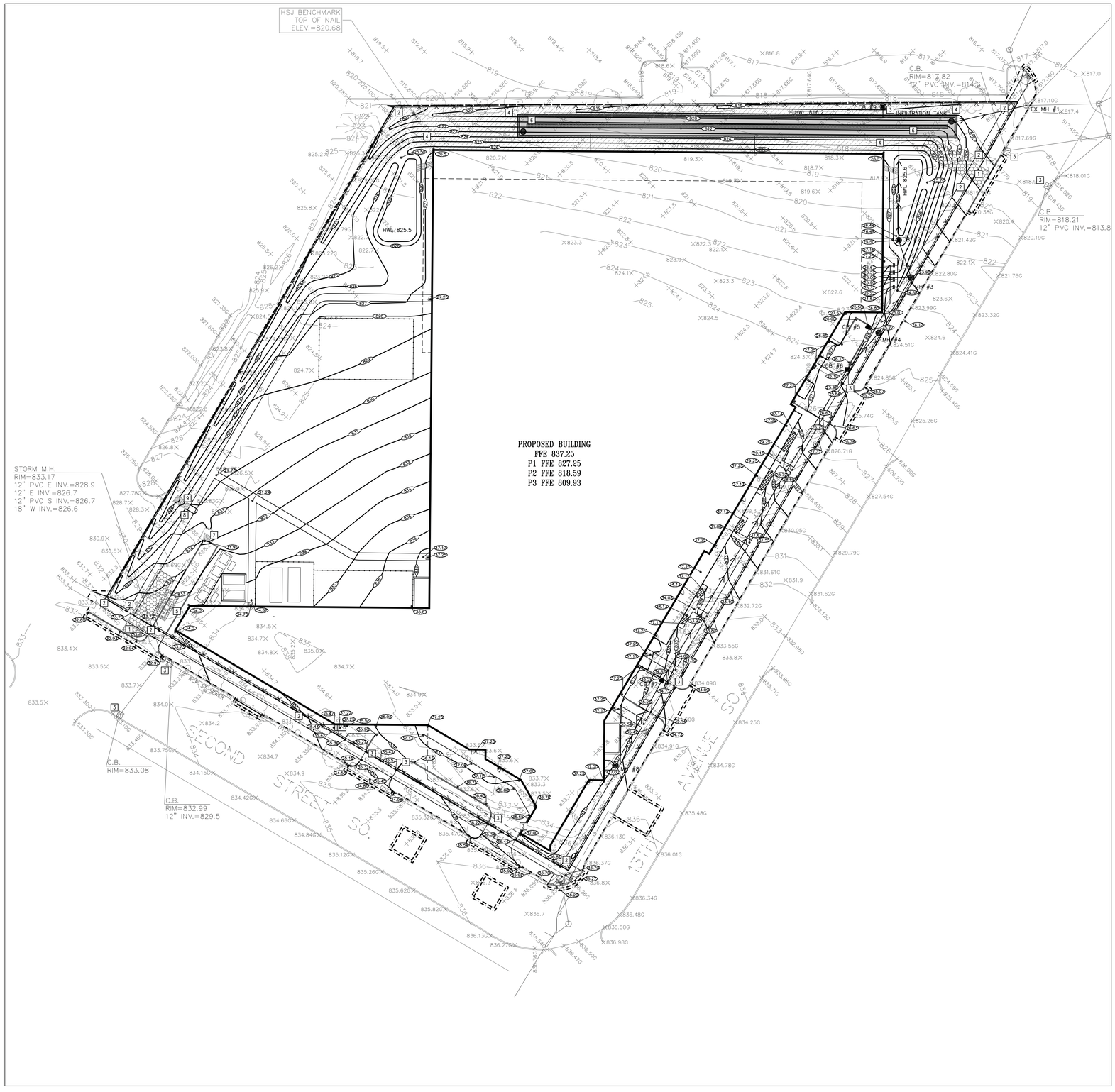
I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA

Joel W. Maier

SIGNATURE Joel W. Maier
DATE April 8, 2016
LICENSE NUMBER 19181

SHEET NAME:
SELECTIVE SITE DEMOLITION AND EROSION CONTROL PLAN

SHEET NO:
C1.0



1 / C2.0 GRADING, DRAINAGE, AND EROSION CONTROL PLAN
1"=30'

PROPOSED PLAN SYMBOLS

- CONSTRUCTION LIMITS
- SILTATION FENCE
- PROPERTY LINE
- SAWCUT LINE (APPROX.)
- PROPOSED CONTOUR (834)
- STORM SEWER
- CATCH BASIN
- MANHOLE
- EROSION CONTROL BLANKET (TEMPORARY)
- ROCK CONSTRUCTION ENTRANCE
- DRAINAGE FLOW ARROW
- SPOT ELEVATION (47.25)
- SOIL BORING (SB-1)
- CONCRETE WASHOUT AREA (CONCRETE W.O. AREA #)

ABBREVIATIONS

BLDG	Building
BM	Bench Mark
CB	Catch Basin
CONC	Concrete
ELEV	Elevation
EX	Existing
FFE	Finished Floor Elevation
HWL	High Water Level
INV	Invert
MAX	Maximum
MH	Manhole
MIN	Minimum
PVC	Polyvinyl Chloride
RCP	Reinforced Concrete Pipe
W.O.	Washout

APPROXIMATE DISTURBED AREA IS 4.39 ACRES

NOTE:
STORM SEWER INLETS NOT SHOWN ON PLAN MAY RECEIVE RUNOFF FROM CONSTRUCTION ACTIVITIES. INSTALL INLET SEDIMENT PROTECTION PER DETAIL 3/C5.0 ON ALL STORM INLETS THAT MAY RECEIVE RUNOFF.

N

30' 15' 0 30' 60'
SCALE 1" = 30'

NOTE:
CONTRACTOR, PROPERTY OWNER OR RESPONSIBLE PARTY SHALL CONTACT MINNEAPOLIS SURFACE WATERS AND SEWERS 48 HOURS PRIOR TO AND EXCAVATION OR CONSTRUCTION RELATED TO OR IN THE LOCATION OF THE PROPOSED STORMWATER MANAGEMENT BMP. CONTACT PAUL CHELLSEN, 612-673-2406 OR PAUL.CHELLSEN@MINNEAPOLISMN.GOV.

UPON THE PROJECT'S COMPLETION THE GENERAL CONTRACTOR, PROPERTY OWNER OR RESPONSIBLE PARTY SHALL PROVIDE TO THE DEPARTMENT OF PUBLIC WORKS A FINAL STORMWATER MANAGEMENT REPORT INCLUDING RECORD DRAWINGS. THIS REPORT WILL SERVE AS A MEANS OF VERIFICATION THAT THE INTENT OF THE APPROVED STORMWATER MANAGEMENT DESIGN HAS BEEN MET. THIS FINAL REPORT SHALL SUBSTANTIATE THAT ALL ASPECTS OF THE ORIGINAL DESIGN HAVE BEEN ADEQUATELY PROVIDED FOR BY THE CONSTRUCTION OF THE PROJECT.

NOTE: ALL SIDEWALK, CURB AND GUTTER, DRIVEWAY ENTRANCES, AND ALLEYS ARE TO CONSTRUCTED PER:

STANDARD SUPPLEMENTAL SPECIFICATIONS FOR CONSTRUCTION OF PUBLIC INFRASTRUCTURE IN THE CITY OF MINNEAPOLIS 2011 EDITION

BY

DEPARTMENT OF PUBLIC WORKS
MINNEAPOLIS, MINNESOTA
FEBRUARY 1, 2011

KEYED NOTES

KEYED NOTES ARE DENOTED BY NO ON PLAN.

1. INSTALL ROCK CONSTRUCTION ENTRANCE. REFER TO DETAIL 2/C5.0.
2. INSTALL SILT FENCE. REFER TO DETAIL 1/C5.0.
3. INSTALL INLET SEDIMENT PROTECTION. REFER TO DETAIL 3/C5.0.
4. INSTALL MN/DOT 3885 CATEGORY 2 TEMPORARY STRAW FIBER EROSION CONTROL BLANKET.
5. APPROXIMATE LOCATION OF TEMPORARY CONTAINED CONCRETE WASH OUT BIN. REFER TO THE MINNESOTA'S NPDES/SDS GENERAL STORMWATER PERMIT FOR CONSTRUCTION ACTIVITY FOR MORE DETAILS. SELF CONTAINED CONCRETE WASHOUTS ON CONCRETE DELIVERY TRUCKS IS AN ACCEPTABLE ALTERNATIVE TO ON-SITE CONTAINMENT.
6. 60-INCH DIAMETER UNDERGROUND INFILTRATION TANK. REFER TO C3.0 UTILITY PLAN AND DETAIL 4/C5.1.
7. INSTALL ½ YARDS OF MN/DOT 3601 CLASS II RIPRAP.
8. CONSTRUCT SIDEWALK CHASE. REFER TO DETAIL 11/C5.0.
9. INSTALL 2.0 YARDS OF MN/DOT 3601 CLASS II RIPRAP BETWEEN SIDEWALK CHASE AND BOTTOM OF DRAINAGE SWALE.

MINNEAPOLIS STANDARD EROSION CONTROL NOTES - (JANUARY 7, 2011)

1. **CONTRACTOR MUST CALL A CONSTRUCTION START 48 HOURS PRIOR TO ANY LAND DISTURBANCES 612-673-3867. FAILURE TO DO SO MAY RESULT IN FINES, THE REVOCATION OF PERMIT AND A STOP WORK ORDER BEING ISSUED.**
2. INSTALL PERIMETER EROSION CONTROL AT THE LOCATIONS SHOWN ON THE PLANS PRIOR TO THE COMMENCEMENT OF ANY LAND DISTURBANCE OR CONSTRUCTION ACTIVITIES.
3. BEFORE BEGINNING CONSTRUCTION, INSTALL A TEMPORARY ROCK CONSTRUCTION ENTRANCE AT EACH POINT WHERE VEHICLES EXIT THE CONSTRUCTION SITE. USE 2 INCH OR GREATER DIAMETER ROCK IN A LAYER AT LEAST 6 INCHES THICK ACROSS THE ENTIRE WIDTH OF THE ENTRANCE. EXTEND THE ROCK ENTRANCE AT LEAST 50 FEET INTO THE CONSTRUCTION ZONE USING A GEO-TEXTILE FABRIC BENEATH THE AGGREGATE TO PREVENT MIGRATION OF SOIL INTO THE ROCK FROM BELOW.
4. REMOVE ALL SOILS AND SEDIMENTS TRACKED OR OTHERWISE DEPOSITED ONTO PUBLIC AND PRIVATE PAVEMENT AREAS. REMOVAL SHALL BE ON A DAILY BASIS WHEN TRACKING OCCURS AND MAY BE ORDERED BY MINNEAPOLIS INSPECTORS AT ANY TIME IF CONDITIONS WARRANT. SWEEPING SHALL BE MAINTAINED THROUGHOUT THE DURATION OF THE CONSTRUCTION AND DONE IN A MANNER TO PREVENT DUST BEING BLOWN TO ADJACENT PROPERTIES.
5. INSTALL INLET PROTECTION AT ALL PUBLIC AND PRIVATE CATCH BASIN INLETS, WHICH RECEIVE RUNOFF FROM THE DISTURBED AREAS. CONTRACTOR SHALL CLEAN, REMOVE SEDIMENT OR REPLACE STORM DRAIN INLET PROTECTION DEVICES ON A ROUTINE BASIS SUCH THAT THE DEVICES ARE FULLY FUNCTIONAL FOR THE NEXT RAIN EVENT. SEDIMENT DEPOSITED IN AND/OR PLUGGING DRAINAGE SYSTEMS IS THE RESPONSIBILITY OF THE CONTRACTOR. **HAY BALES OR FILTER FABRIC WRAPPED GRATES ARE NOT ALLOWED FOR INLET PROTECTION.**
6. LOCATE SOIL OR DIRT STOCKPILES NO LESS THAN 25 FEET FROM ANY PUBLIC OR PRIVATE ROADWAY OR DRAINAGE CHANNEL. IF REMAINING FOR MORE THAN SEVEN DAYS, STABILIZE THE STOCKPILES BY MULCHING, VEGETATIVE COVER, TARPS, OR OTHER MEANS. CONTROL EROSION FROM ALL STOCKPILES BY PLACING SILT BARRIERS AROUND THE PILES. TEMPORARY STOCKPILES LOCATED ON PAVED SURFACES MUST BE NO LESS THAN TWO FEET FROM THE DRAINAGE/GUTTER LINE AND SHALL BE COVERED IF LEFT MORE THAN 24 HOURS.
7. MAINTAIN ALL TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES IN PLACE UNTIL THE CONTRIBUTING DRAINAGE AREA HAS BEEN STABILIZED. INSPECT TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES ON A DAILY BASIS AND REPLACE DETERIORATED, DAMAGED, OR ROTTED EROSION CONTROL DEVICES IMMEDIATELY.
8. TEMPORARILY OR PERMANENTLY STABILIZE ALL CONSTRUCTION AREAS WHICH HAVE UNDERGONE FINAL GRADING, AND ALL AREAS IN WHICH GRADING OR SITE BUILDING CONSTRUCTION OPERATIONS ARE NOT ACTIVELY UNDERWAY AGAINST EROSION DUE TO RAIN, WIND AND RUNNING WATER WITHIN 7-14 DAYS. USE SEED AND MULCH, EROSION CONTROL MATTING, AND/OR SODDING AND STAKING IN GREEN SPACE AREAS. REMOVE ALL TEMPORARY SYNTHETIC, STRUCTURAL, NON-BIODEGRADABLE EROSION AND SEDIMENT CONTROL DEVICES AFTER THE SITE HAS UNDERGONE FINAL STABILIZATION WITH PERMANENT VEGETATION ESTABLISHMENT. FINAL STABILIZATION FOR PURPOSES OF THIS REMOVAL IS 70% ESTABLISHED COVER OVER DENUDED AREA.
9. READY MIXED CONCRETE AND CONCRETE BATCH/MIX PLANTS ARE PROHIBITED WITHIN THE PUBLIC RIGHT OF WAY. ALL CONCRETE RELATED PRODUCTION, CLEANING AND MIXING ACTIVITIES SHALL BE DONE IN THE DESIGNATED CONCRETE MIXING/WASHOUT LOCATIONS AS SHOWN IN THE EROSION CONTROL PLAN. UNDER NO CIRCUMSTANCE MAY WASHOUT WATER DRAIN ONTO THE PUBLIC RIGHT OF WAY OR INTO ANY PUBLIC OR PRIVATE STORM DRAIN CONVEYANCE.
10. CHANGES TO APPROVED EROSION CONTROL PLAN MUST BE APPROVED BY THE EROSION CONTROL INSPECTOR PRIOR TO IMPLEMENTATION. CONTRACTOR TO PROVIDE INSTALLATION AND DETAILS FOR ALL PROPOSED ALTERNATE TYPE DEVICES.
11. IF DEWATERING OR PUMPING OF WATER IS NECESSARY, THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ANY NECESSARY PERMITS AND/OR APPROVALS PRIOR TO DISCHARGE OF ANY WATER FROM THE SITE. IF THE DISCHARGE FROM THE DEWATERING OR PUMPING PROCESS IS TURBID OR CONTAINS SEDIMENT LADEN WATER, IT MUST BE TREATED THROUGH THE USE OF SEDIMENT TRAPS, VEGETATIVE FILTER STRIPS, OR OTHER SEDIMENT REDUCING MEASURES SUCH THAT THE DISCHARGE IS NOT VISIBLY DIFFERENT FROM THE RECEIVING WATER. ADDITIONAL EROSION CONTROL MEASURES MAY BE REQUIRED AT THE DISCHARGE POINT TO PREVENT SCOUR EROSION. THE CONTRACTOR SHALL PROVIDE A DEWATERING/PUMPING PLAN TO THE EROSION CONTROL INSPECTOR PRIOR TO INITIATING DEWATERING ACTIVITIES.

GRADING NOTES:

1. THE CONTRACTOR SHALL VISIT THE SITE, REVIEW ALL CONSTRUCTION DOCUMENTS AND FIELD VERIFY THE EXISTING CONDITIONS PRIOR TO BIDDING. NO ADDITIONAL COMPENSATION WILL BE GIVEN FOR WORK THAT COULD HAVE BEEN IDENTIFIED BY A SITE VISIT OR CONSTRUCTION DOCUMENT REVIEW.
2. THE BACKGROUND INFORMATION WAS PREPARED BY HARRY S JOHNSON (952) 884-5341.
3. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN THE LOCATION OF ALL EXISTING UTILITIES. THE CONTRACTOR SHALL VERIFY THE LOCATION, ELEVATION AND MARK ALL EXISTING UTILITIES 48 HOURS BEFORE CONSTRUCTION STARTS. THE ENGINEER, ARCHITECT OR OWNER DOES NOT GUARANTEE THAT ALL THE UTILITIES ARE MAPPED, OR IF MAPPED, ARE SHOWN CORRECTLY. CONTACT GOPHER ONE AT 651-454-0002 FOR FIELD LOCATING EXISTING UTILITIES. CONTACT UTILITY OWNER IF DAMAGE OCCURS DUE TO CONSTRUCTION.
4. PROTECT ALL EXISTING STRUCTURES AND UTILITIES WHICH ARE NOT SCHEDULED FOR REMOVAL.
5. NOTIFY CITY BUILDING INSPECTOR BEFORE TRENCHING AND EXCAVATION WORK COMMENCES. THE CONTRACTOR SHALL OBTAIN ALL APPLICABLE PERMITS PRIOR TO START OF CONSTRUCTION.
6. ALL SPOT ELEVATIONS SHOWN AS 36.48, FOR EXAMPLE, ARE TO BE UNDERSTOOD TO MEAN 936.48.
7. ALL SPOT ELEVATIONS ALONG THE CURB-LINE INDICATE THE ELEVATION OF THE GUTTER, UNLESS NOTED OTHERWISE.
8. NO LANDSCAPED SLOPES ARE TO EXCEED 3:1 (3 FEET HORIZONTAL TO 1 FOOT VERTICAL) UNLESS NOTED OTHERWISE.
9. ACCESSIBLE PARKING AREAS SHALL NOT HAVE SLOPES IN ANY DIRECTION THAT EXCEED 2%.
10. PROVIDE POSITIVE DRAINAGE FROM BUILDINGS AT ALL TIMES.
11. UPON COMPLETION OF THE GRADING AND UTILITY WORK, THE CONTRACTOR SHALL CERTIFY THAT ALL GRADING AND UTILITY WORK WAS PERFORMED IN ACCORDANCE WITH THE APPROVED GRADING AND UTILITY PERMITS. AN AS-BUILT GRADING AND UTILITY PLAN SHALL BE SUBMITTED TO THE CITY FOR REVIEW AND DISTRIBUTION.
12. PRIOR TO ISSUANCE OF BUILDING PERMITS, ALL NECESSARY EROSION CONTROL DEVICES MUST BE IN PLACE AND FUNCTIONING. THE CITY OF MINNEAPOLIS WILL INSPECT THE SITE TO DETERMINE ITS SUITABILITY FOR BUILDING ACTIVITIES. IF THE PUBLIC UTILITIES HAVE NOT BEEN INSTALLED AT THIS POINT, IT MAY BE NECESSARY TO WITHHOLD BUILDING PERMITS FOR VARIOUS LOTS TO ALLOW THE CONTRACTOR ADEQUATE SPACE TO PERFORM THIS WORK.
13. ALL DEBRIS CREATED IN THE PROCESS OF CLEARING AND GRADING THE SITE SHALL BE REMOVED FROM THE SITE. THIS INCLUDES TREES AND SHRUBS. UNDER NO CIRCUMSTANCES SHALL THIS TYPE OF MATERIAL BE BURIED OR BURNED ON THE SITE.
14. THE INTENT IS TO STRIP AND SALVAGED TOPSOIL FOR POTENTIAL RE-SPREADING ON THE SITE. SIX INCHES OF TOPSOIL - AFTER COMPACTION - SHALL BE RE-SPREAD PRIOR TO SEEDING AND MULCHING. EXCESS TOPSOIL MAY BE REMOVED FROM THE SITE PROVIDING THERE IS ADEQUATE TOPSOIL REMAINING TO PROPERLY FINISH THE SITE AS NOTED ABOVE. THE TOPSOIL STRIPPING, STOCKPILING AND RE-SPREADING SHALL BE DONE IN ACCORDANCE TO, AND NOTED ON, THE APPROVED GRADING PLAN AND SPECIFICATIONS. THE CONTRACTOR SHALL REFER TO THE LANDSCAPE DRAWINGS AND SPECIFICATIONS FOR ANY SPECIAL TOPSOIL OR PLANTING REQUIREMENTS.
15. ALL GRADING OPERATIONS SHALL BE CONDUCTED IN A MANNER TO MINIMIZE THE POTENTIAL FOR SITE EROSION. EROSION CONTROL MEASURES SHALL BE INSTALLED TO PREVENT SEDIMENT FROM RUNNING OFF ONTO ADJACENT PROPERTIES. ANY DAMAGE TO ADJACENT PROPERTIES MUST BE CORRECTED AND RESTORED AS SOON AS PERMISSION IS GRANTED FROM THE ADJACENT PROPERTY OWNER(S).
16. IF CONSTRUCTION OF THE SITE WORK PROCEEDS THROUGH THE WINTER MONTHS, ANY DISTURBED AREAS OUTSIDE THE BUILDING FOOTPRINTS ARE TO BE MINIMALLY STABILIZED PRIOR TO MARCH 1, AS FOLLOWS: AREAS PLANNED TO RECEIVE PAVEMENTS ARE TO HAVE CLASS 5 BASE INSTALLED; ALL OTHER DISTURBED AREAS ARE TO BE SEEDED, STRAW MULCH PLACED, AND DISC-ANCHORED.
17. WINTER MULCHING:
    - 17.A. SNOW MULCHING SHALL BE DEFINED AS MULCH MATERIAL SPREAD OVER THE TOP OF SNOW SO THAT THE MULCH MELTS THROUGH THE SNOW AND STICKS TO THE EXPOSED SOILS.
    - 17.B. FROZEN GROUND MULCHING SHALL BE DEFINED AS MULCH MATERIAL SPREAD OVER FROZEN GROUND. MULCH MATERIALS THAT DO NOT REQUIRE DISC-ANCHORING INTO THE SOIL MAY BE PLACED WITHOUT MODIFICATION. MULCH MATERIALS THAT REQUIRE DISC-ANCHORING MAYBE ANCHORED WITH HYDRAULIC SOIL STABILIZERS OR MAY BE FROZEN TO THE SOIL BY APPLYING WATER, AT A RATE OF 2000 GALLONS PER ACRE, OVER THE MULCH AS A SUBSTITUTION FOR DISC-ANCHORING.
18. RETAINING WALLS AND APPROPRIATE SAFETY FENCING ALONG THE TOP OF WALLS ARE TO BE DESIGNED AND CERTIFIED BY A REGISTERED PROFESSIONAL ENGINEER. SUBMIT RETAINING WALL SHOP DRAWINGS TO PROJECT TEAM PRIOR TO CONSTRUCTION.
19. THE CONTRACTOR SHALL LIMIT THE DISTURBED AREA AS MUCH AS POSSIBLE.

WALKWAY NOTES:

1. GRADING CONTRACTOR IS TO COORDINATE WITH PAVING CONTRACTOR SO THAT ALL STEPS AND LANDINGS ARE SLOPED PER CODE.
2. ALL SIDEWALK LONGITUDINAL AND TRANSVERSE SLOPES ARE TO BE PER CODE.

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PROJECT NAME:
LEGACY

2nd St. & 13th Ave.
Mpls, MN 55415

PROJECT NUMBER: 14-18
DATE OF ISSUE: April 8, 2016
DRAWN BY: NPA
CHECKED BY: JWM
REVISIONS:

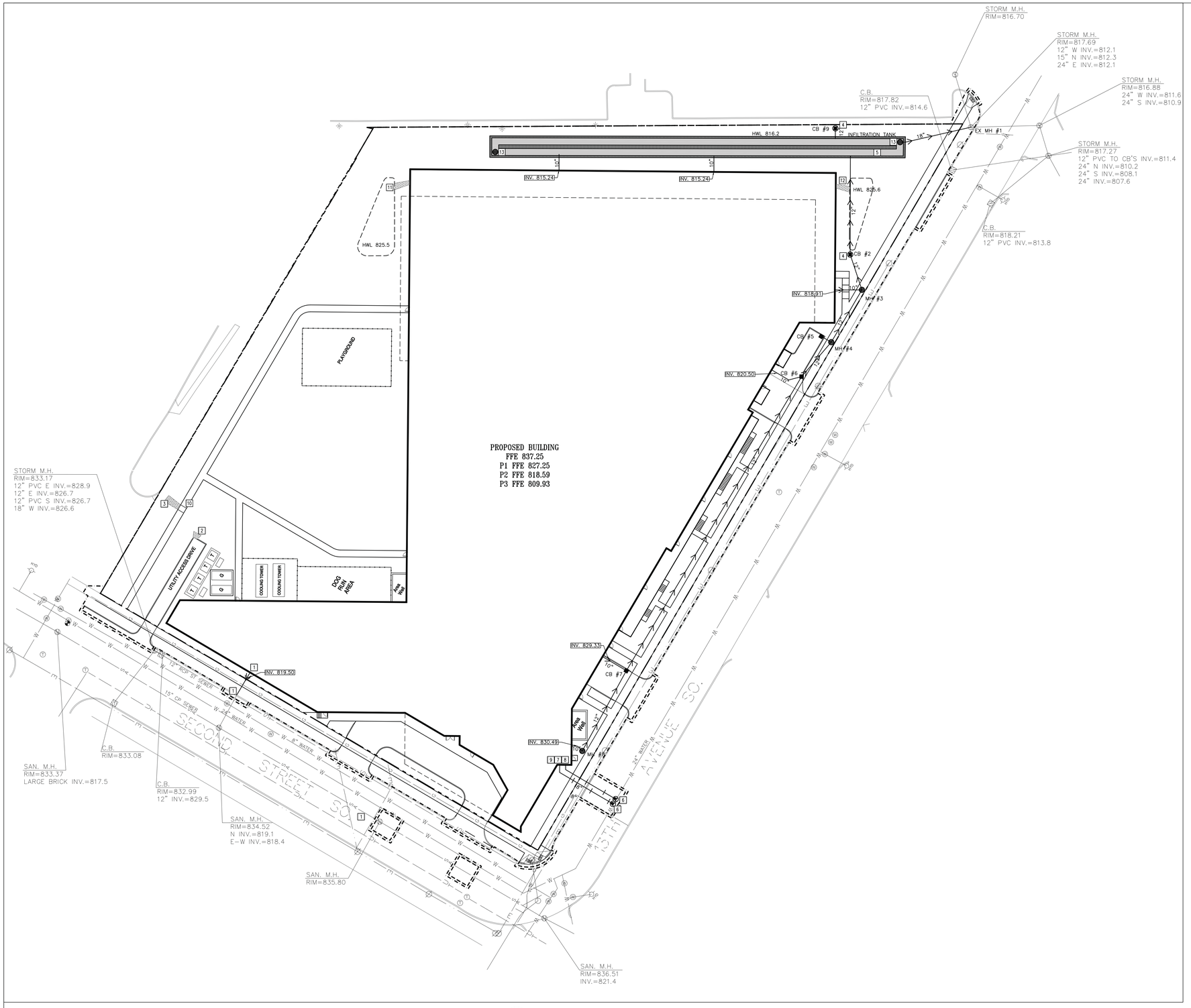
I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA

Joel W. Maier

SIGNATURE Joel W. Maier
DATE April 8, 2016
LICENSE NUMBER 19181

SHEET NAME:
GRADING, DRAINAGE, AND EROSION CONTROL PLAN

SHEET NO:
C2.0



1 / C3.0 UTILITY PLAN
1"=30'

PROPOSED PLAN SYMBOLS

- CONSTRUCTION LIMITS
- PROPERTY LINE
- SAWCUT LINE (APPROX.)
- SANITARY SEWER
- WATER PIPE
- STORM SEWER
- GATE VALVE
- CATCH BASIN
- MANHOLE
- SEWER INVERT ELEVATION — INV. 827.50

ABBREVIATIONS

BLDG	Building
BM	Bench Mark
BOT	Bottom
CB	Catch Basin
CONC	Concrete
DIP	Ductile Iron Pipe
ELEV	Elevation
EX	Existing
FFE	Finished Floor Elevation
HDPE	High Density Polyethylene
HWL	High Water Level
INV	Invert
MAX	Maximum
MH	Manhole
MIN	Minimum
PVC	Polyvinyl Chloride
RCP	Reinforced Concrete Pipe

N

30' 15' 0 30' 60'
SCALE 1" = 30'

KEYED NOTES

KEYED NOTES ARE DENOTED BY NO. ON PLAN.

1. CONNECT TO EXISTING PIPE STUB AND EXTEND NEW 8-INCH SANITARY SEWER SERVICE TO WITHIN 5-FEET OF BUILDING. **COORDINATE EXACT LOCATION AND INVERT ELEVATION WITH MECHANICAL CONTRACTOR PRIOR TO THE START OF CONSTRUCTION.**
2. INSTALL 3/4 CUBIC YARDS OF CLASS 2 RIPRAP AT CURB CUT OPENING.
3. INSTALL 2.0 CUBIC YARDS OF MN/DOT 3601 CLASS II RIPRAP BETWEEN SIDEWALK CHASE AND BOTTOM OF DRAINAGE SWALE.
4. INSTALL SUMP CATCH BASIN ON SITE STORM SEWER SYSTEM. REFER TO DETAIL 5/C5.0.
5. INSTALL INFILTRATION SYSTEM PER DETAIL 4/C5.1. INFILTRATION SYSTEM SHALL HAVE AN INFILTRATION VOLUME OF 9,932 CUBIC FEET, A TOTAL VOLUME OF 24,662 CUBIC FEET, AND MEET THE RATE CONTROL REQUIREMENTS INDICATED ON SHEET C6.0. ANY ALTERNATE DESIGNS MUST BE APPROVED BY CIVIL ENGINEER PRIOR TO CONSTRUCTION.
6. CONNECT TO EXISTING 24-INCH WATER MAIN WITH NEW 8-INCH FIRE SERVICE AND NEW 8-INCH DOMESTIC SERVICE. THE CITY OF MINNEAPOLIS WATER DEPARTMENT WILL PROVIDE AND INSTALL NEW 24"X8" TEES AND VALVE AS PART OF THE PERMIT FEE. THE CONTRACTOR IS TO EXCAVATE FOR THE CONNECTION, CONSTRUCT THE WATER VALVE MANHOLE, CONNECT TO THE VALVE AND BACKFILL THE TRENCH PER CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS. REFER TO DETAIL 4/C5.0. COORDINATE ALL WORK WITH CITY OF MINNEAPOLIS.
7. STUB 8-INCH FIRE PROTECTION LINE TO WITHIN 5-FEET OF PROPOSED BUILDING. FOLLOW ALL CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS.
8. STUB 8-INCH DOMESTIC WATER SERVICE TO WITHIN 5-FEET OF PROPOSED BUILDING. FOLLOW ALL CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS.
9. LOCATION OF NEW FIRE DEPARTMENT CONNECTION.
10. INSTALL CONCRETE SIDEWALK CHASE. REFER TO DETAIL 11/C5.0.
11. RAIN GARDEN AREA WEST. INSTALL 1.5 CUBIC YARDS OF CLASS 2 RIPRAP AT ROOF DOWNSPOUT.
12. RAIN GARDEN AREA EAST. INSTALL 1.0 CUBIC YARDS OF CLASS 2 RIPRAP AT ROOF DOWNSPOUT.
13. 30-INCH DIAMETER STORMWATER TANK ACCESS RISER.

CITY OF MINNEAPOLIS UTILITY/RIGHT-OF-WAY NOTES:

1. ALL WORK DONE OUTSIDE OF THE LOT LINE ON PUBLIC RIGHT-OF-WAY MUST BE DONE BY A CONTRACTOR LICENSED TO WORK IN THE CITY OF MINNEAPOLIS WHO HAS THE $100,000.00 SIDEWALK CONTRACTOR'S BOND ON FILE WITH THE SIDEWALK DIVISION.
2. ALL UTILITIES MUST BE INSTALLED BY A MINNEAPOLIS LICENSED CONTRACTOR.
2. FOLLOW ALL CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS.
3. PRIOR TO CONSTRUCTION, CONTRACTORS ARE TO COORDINATE ALL WORK WITHIN RIGHT OF WAY AND OBTAIN ALL APPLICABLE PERMITS.
4. THE CONTRACTOR, PROPERTY OWNER OR RESPONSIBLE PARTY SHALL CONTACT MINNEAPOLIS SURFACE WATERS AND SEWERS 48 HOURS PRIOR TO ANY EXCAVATION OR CONSTRUCTION RELATED TO OR IN THE LOCATION OF THE PROPOSED STORMWATER MANAGEMENT BMP (CONTACT PAUL CHELLSEN, 673-2406 OR PAUL.CHELLSEN@MINNEAPOLISMN.GOV.
5. UPON THE PROJECT'S COMPLETION, THE GENERAL CONTRACTOR SHALL PROVIDE TO THE DEPARTMENT OF PUBLIC WORKS A FINAL STORMWATER MANAGEMENT REPORT INCLUDING RECORD DRAWINGS. THIS REPORT WILL SERVE AS A MEANS OF VERIFICATION THAT THE INTENT OF THE APPROVED STORM WATER MANAGEMENT DESIGN HAS BEEN MET. THIS FINAL REPORT SHALL SUBSTANTIATE THAT ALL ASPECTS OF THE ORIGINAL DESIGN HAVE BEEN ADEQUATELY PROVIDED FOR BY THE CONSTRUCTION OF THE PROJECT.
6. STREET LIGHTING INSTALLED AS PART OF THE PROJECT SHALL BE INSPECTED BY THE CITY. CONTRACTORS SHALL ARRANGE FOR INSPECTIONS WITH THE TRAFFIC DEPARTMENT, PLEASE CONTACT DAVE PREHALL AT (612) 673-5759 FOR FURTHER INFORMATION. ANY LIGHTING INSTALLATIONS NOT MEETING CITY SPECIFICATIONS WILL BE REQUIRED TO BE REINSTALLED AT OWNER EXPENSE.
7. AN OBSTRUCTION PERMIT IS REQUIRED ANYTIME CONSTRUCTION WORK IS PERFORMED IN THE PUBLIC RIGHT-OF-WAY. PLEASE CONTACT SCOTT KRAMER AT (612) 673-2383 REGARDING DETAILS OF SIDEWALK AND LANE CLOSURES. LOG ON TO HTTP://MINNEAPOLIS.MN.ROWAY.NET/. FOR A PERMIT.
8. CONTACT ALLAN KLUGMAN AT (612) 673-2743 PRIOR TO CONSTRUCTION FOR THE TEMPORARY REMOVAL/TEMPORARY RELOCATION OF ANY CITY OF MINNEAPOLIS SIGNAL SYSTEM THAT MAY BE IN THE WAY OF CONSTRUCTION.
9. ALL COSTS FOR RELOCATION AND/OR REPAIR OF CITY TRAFFIC FACILITIES SHALL BE BORNE BY THE CONTRACTOR AND/OR PROPERTY OWNER.
10. CONTACT DOUG MADAY AT (612) 673-5755 PRIOR TO CONSTRUCTION FOR THE REMOVAL OF ANY CITY OF MINNEAPOLIS RIGHT OF WAY SIGNS THAT MAY BE IN THE WAY OF CONSTRUCTION.

UTILITY NOTES:

1. COORDINATE SERVICE CONNECTION LOCATIONS AT THE BUILDING WITH THE MECHANICAL CONTRACTOR PRIOR TO CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR UNCOORDINATED WORK.
2. ALL SEWER SERVICE CONNECTIONS WITH LESS THEN 5 FEET OF COVER OVER THE TOP OF PIPE ARE TO BE INSULATED. INSULATION SHALL BE INSTALLED FROM THE CONNECTION OF THE SERVICE AT THE BUILDING TO THE POINT WHICH THE SERVICE ATTAINS 5 FEET OF COVER. CONTRACTOR SHALL OBTAIN WRITTEN PERMISSION FROM ARCHITECT OR ENGINEER PRIOR TO INSTALLATION OF INSULATION.
3. PROTECT ALL EXISTING STRUCTURES AND UTILITIES WHICH ARE NOT SCHEDULED TO BE REMOVED.
4. ALL SEWER AND WATER CROSSINGS SHALL HAVE A MINIMUM VERTICAL SEPARATION OF 1.5 FEET AND HORIZONTAL SEPARATION OF 10 FEET. FOLLOW ALL HEALTH DEPARTMENT AND CITY OF MINNEAPOLIS STANDARDS.
5. ALL WATER MAINS SHALL BE DUCTILE IRON PIPE, CLASS 52, UNLESS NOTED OTHERWISE.
6. ALL WATER MAIN SHALL HAVE A MINIMUM DEPTH OF COVER OF 8.0 FEET OVER TOP OF WATER MAIN.
7. PROVIDE THRUST BLOCKING ON ALL WATER MAIN PER CITY OF MINNEAPOLIS. PROVIDE MECHANICAL JOINT RESTRAINTS ON ALL BENDS, VALVES, TEES, PLUGS AND HYDRANT LEADS.
8. SANITARY SEWER PIPING SHALL BE SDR 26 PVC UNLESS NOTED OTHERWISE.
9. STORM SEWER PIPING SHALL BE SMOOTH INTERIOR AND ANNULAR EXTERIOR CORRUGATED HIGH DENSITY POLYETHYLENE (HDPE), UNLESS NOTED OTHERWISE. HDPE PIPE SHALL CONFORM TO ASTM F2306.
10. ALL STORM SEWER PIPE THAT IS EXTENDED TO THE BUILDING FOR ROOF DRAIN SERVICES IS TO BE SCHEDULE 40 PVC. PVC PIPE AND FITTINGS SHALL CONFORM TO THE REQUIREMENTS OF ASTM D2665 FOR SCHEDULE 40.
11. CONTRACTORS ARE TO COORDINATE ALL WORK WITH GAS, ELECTRIC, TELEVISION AND TELEPHONE COMPANIES PRIOR TO START OF CONSTRUCTION.
12. ALL PORTIONS OF THE STORM SEWER SYSTEM LOCATED WITHIN 10-FEET OF THE BUILDING OR WATER SERVICE LINE SHALL BE TESTED IN ACCORDANCE WITH MN PLUMBING CODE.
13. ALL JOINTS AND CONNECTIONS IN THE STORM SEWER SYSTEM SHALL BE GAS TIGHT OR WATER TIGHT IN ACCORDANCE TO MN PLUMBING CODE. APPROVED RESILIENT RUBBER JOINTS MUST BE USED TO MAKE WATER TIGHT CONNECTIONS TO MANHOLES, CATCH BASINS, AND OTHER STRUCTURES. GROUT RINGS ARE AN ACCEPTABLE ALTERNATIVE. CEMENT MORTAR JOINTS ARE PERMITTED ONLY FOR REPAIRS AND CONNECTIONS OF EXISTING LINES CONSTRUCTED WITH SUCH JOINTS.

UTILITY NOTES FOR WORK IN PUBLIC RIGHT-OF-WAY:

1. FOLLOW ALL CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS.
2. PRIOR TO CONSTRUCTION, CONTRACTORS ARE TO COORDINATE ALL WORK WITHIN RIGHT OF WAY AND OBTAIN ALL APPLICABLE PERMITS.

STORM SEWER TABLE

STRUCTURE IDENTIFICATION	STRUCTURE DIMENSION (INCHES)	NEENAH CASTING	TOP ELEVATION	INVERT ELEVATION	PIPE LENGTH, DIAMETER, SLOPE & NEXT UPSTREAM STRUCTURE
EX MH #1	NA	NA	817.69	E. 812.10 N. 812.30 W. 812.10	49 L.F. OF 18" HDPE PIPE @ 2.24%, TANK
INFILTRATION TANK	60"	NA	NA	E. 813.20 S. 813.75 N. 813.92 TANK BOT. 811.00 S. 815.00 S. 815.00	72 L.F. OF 12" HDPE PIPE @ 4.69%, CB #2 8 L.F. OF 12" HDPE PIPE @ 1.00%, CB #9 12 L.F. OF 10" PVC PIPE @ 2.00%, ROOF 12 L.F. OF 10" PVC PIPE @ 2.00%, ROOF
CB #2	60"	R-4342	825.50	N. 817.13 SE. 817.13 SUMP 813.13	26 L.F. OF 12" HDPE PIPE @ 4.69%, MH #3
MH #3	48"	R-1733	824.60	N. 818.35 SW. 818.35 W. 818.77	44 L.F. OF 12" HDPE PIPE @ 1.51%, MH #4 14 L.F. OF 10" PVC PIPE @ 1.00%, ROOF
MH #4	48"	R-1733	825.14	NE. 819.01 SW. 819.01 NW. 821.64	33 L.F. OF 12" HDPE PIPE @ 3.42%, CB #6 8 L.F. OF 10" PVC PIPE @ 4.50%, CB 5
CB #5	24X36	R-3067	825.50	SE. 822.00	----------
CB #6	48"	R-3067	825.90	NE. 820.14 SW. 820.14 NW. 820.31	250 L.F. OF 12" HDPE PIPE @ 3.02%, CB #7 19 L.F. OF 10" PVC PIPE @ 1.00%, ROOF
CB #7	48"	R-3067	835.18	NE. 827.70 SW. 827.70 NW. 829.18	67 L.F. OF 12" HDPE PIPE @ 3.43%, MH #8 15 L.F. OF 10" PVC PIPE @ 1.00%, ROOF
MH #8	48"	R-1733	836.20	NE. 830.00 SW. 830.00 NW. 830.45	4 L.F. OF 10" PVC PIPE @ 1.00%, ROOF
CB #9	48"	R-4342	817.50	S. 814.00 SUMP 810.00	----------

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PROJECT NAME:

LEGACY

2nd St. & 13th Ave.
Mpls, MN 55415

PROJECT NUMBER: 14-18

DATE OF ISSUE: April 8, 2016

DRAWN BY: NPA

CHECKED BY: JWM

REVISIONS:

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA

Joel W. Maier

SIGNATURE: Joel W. Maier

DATE: April 8, 2016

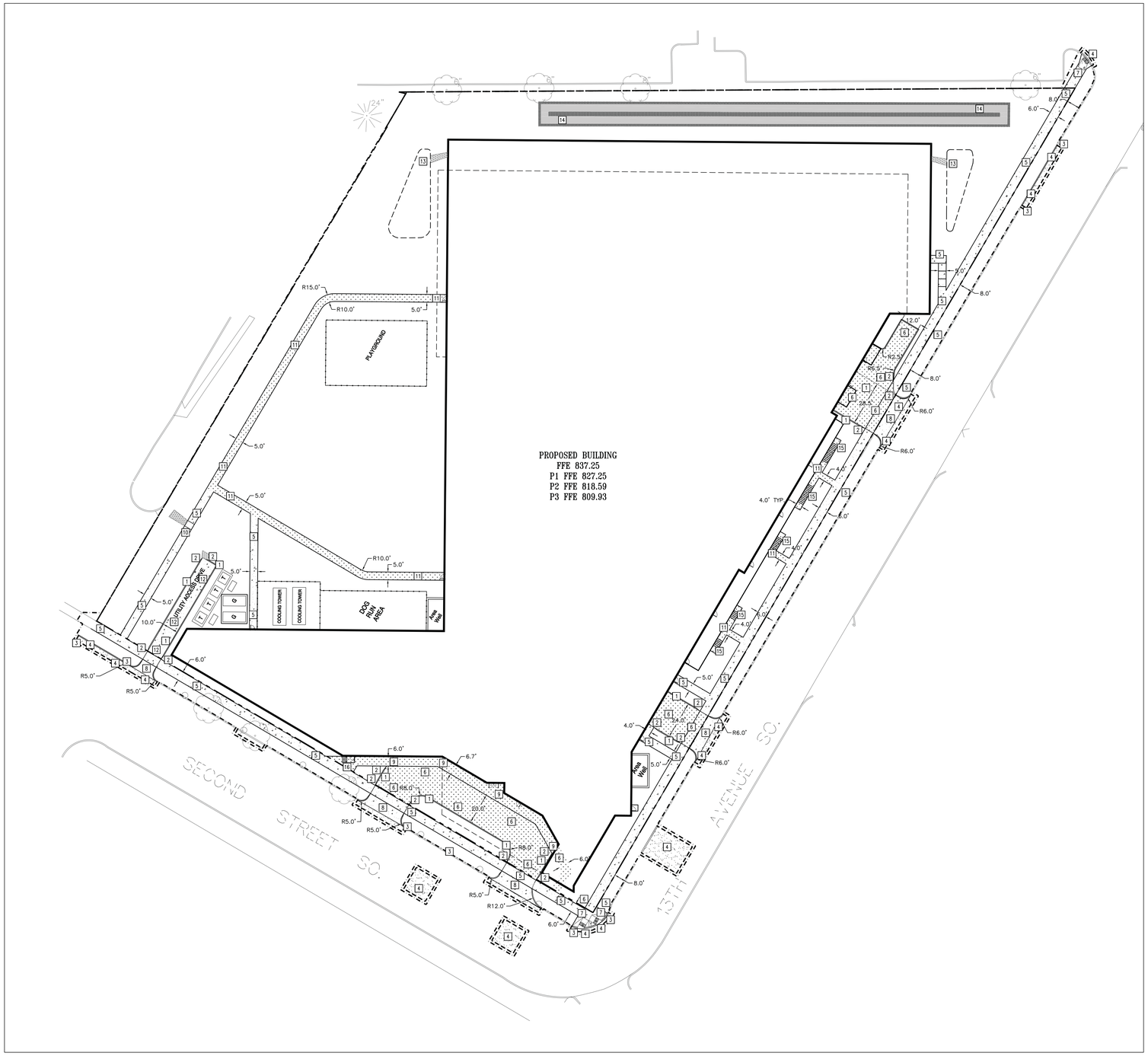
LICENSE NUMBER: 19181

SHEET NAME:

UTILITY PLAN

SHEET NO:

C3.0



1 C4.0 PAVING AND GEOMETRIC PLAN
1"=30'

PROPOSED PLAN SYMBOLS	
BITUMINOUS PAVEMENT	
CONCRETE PAVEMENT/SIDEWALK	
COLORED CONCRETE PAVING (REFER TO ARCHITECTURAL PLANS)	
PEDESTRIAN CURB RAMP	

ABBREVIATIONS	
BLDG	Building
BM	Bench Mark
CONC	Concrete
ELEV	Elevation
EX	Existing
FFE	Finished Floor Elevation
LFE	Lower Floor Elevation
MAX	Maximum
MIN	Minimum
R	Radius
TYP.	Typical
MNMUTCD	Minnesota Manual on Uniform Traffic Control Devices

N

30' 15' 0 30' 60'
SCALE 1" = 30'

KEYED NOTES

KEYED NOTES ARE DENOTED BY [NO] ON PLAN.

1. INSTALL B612 CONCRETE CURB AND GUTTER. REFER TO DETAIL 2/C5.1.
2. INSTALL NOSEDOWN CURB SECTION. REFER TO DETAIL 7/C5.1.
3. INSTALL B624 CONCRETE CURB AND GUTTER. REFER TO DETAIL 3/C5.1.
4. INSTALL BITUMINOUS PAVEMENT. CONTRACTOR SHALL MATCH EXISTING PAVEMENT SECTION PER MINNEAPOLIS STANDARDS AND SPECIFICATIONS.
5. INSTALL STANDARD COLOR CONCRETE WALK. REFER TO DETAIL 1/C5.1 FOR SIDEWALKS WITHIN PUBLIC RIGHT-OF-WAY AND DETAIL 6/C5.1 FOR SIDEWALKS WITHIN SITE.
6. INSTALL INTEGRAL COLORED CONCRETE PAVEMENT. REFER TO ARCHITECTURAL PLANS FOR COLOR. REFER TO DETAIL 5/C5.1 FOR CONCRETE PAVEMENT SECTION.
7. INSTALL PEDESTRIAN CURB RAMP. REFER TO DETAIL 8/C5.1. REFER TO MN/DOT STANDARD PLAN 5-297.250 AND MN/DOT STANDARD PLATE 7038.
8. INSTALL CONCRETE DRIVE ENTRANCE. REFER TO DETAIL 1/C5.1. FOLLOW ALL CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS.
9. INSTALL INTEGRAL COLORED THICKENED EDGE SIDEWALK . REFER TO ARCHITECTURAL PLANS FOR COLOR. REFER TO DETAIL 9/C5.1 FOR THICKENED EDGE SIDEWALK.
10. INSTALL CONCRETE SIDEWALK CHASE. REFER TO DETAIL 11/C5.0.
11. INSTALL INTEGRAL COLORED CONCRETE SIDEWALK. REFER TO ARCHITECTURAL PLANS FOR COLOR. REFER TO DETAIL 6/C5.1 FOR CONCRETE SIDEWALK SECTION.
12. INSTALL STANDARD COLOR CONCRETE PAVEMENT. REFER TO DETAIL 5/C5.1 FOR CONCRETE PAVEMENT SECTION.
13. RAIN GARDEN AREAS.
14. INFILTRATION TANK.
15. STAIRS TO ELEVATED PATIO. REFER TO ARCHITECTURAL DRAWINGS.
16. STEPS TO FIRE COMMAND DOOR. REFER TO ARCHITECTURAL DRAWINGS.

CITY OF MINNEAPOLIS RIGHT-OF-WAY NOTES:

1. STREET LIGHTING INSTALLED AS PART OF THE PROJECT SHALL BE INSPECTED BY THE CITY. CONTRACTORS SHALL ARRANGE FOR INSPECTIONS WITH THE TRAFFIC DEPARTMENT. PLEASE CONTACT DAVE PREHALL AT (612) 673-5759 FOR FURTHER INFORMATION. ANY LIGHTING INSTALLATIONS NOT MEETING CITY SPECIFICATIONS WILL BE REQUIRED TO BE REINSTALLED AT OWNER EXPENSE.
2. AN OBSTRUCTION PERMIT IS REQUIRED ANYTIME CONSTRUCTION WORK IS PERFORMED IN THE PUBLIC RIGHT-OF-WAY. PLEASE CONTACT SCOTT KRAMER AT (612) 673-2383 REGARDING DETAILS OF SIDEWALK AND LANE CLOSURES. LOG ON TO HTTP://MINNEAPOLIS.MN.ROWAY.NET/. FOR A PERMIT.
3. CONTACT ALLAN KLUGMAN AT (612) 673-2743 PRIOR TO CONSTRUCTION FOR THE TEMPORARY REMOVAL/TEMPORARY RELOCATION OF ANY CITY OF MINNEAPOLIS SIGNAL SYSTEM THAT MAY BE IN THE WAY OF CONSTRUCTION.
4. ALL COSTS FOR RELOCATION AND/OR REPAIR OF CITY TRAFFIC FACILITIES SHALL BE BORNE BY THE CONTRACTOR AND/OR PROPERTY OWNER.
5. CONTACT DOUG MADAY AT (612) 673-5755 PRIOR TO CONSTRUCTION FOR THE REMOVAL OF ANY CITY OF MINNEAPOLIS RIGHT OF WAY SIGNS THAT MAY BE IN THE WAY OF CONSTRUCTION.

NOTE: ALL SIDEWALK, CURB AND GUTTER, DRIVEWAY ENTRANCES, AND ALLEYS ARE TO CONSTRUCTED PER:

STANDARD SUPPLEMENTAL SPECIFICATIONS FOR CONSTRUCTION OF PUBLIC INFRASTRUCTURE IN THE CITY OF MINNEAPOLIS 2011 EDITION

BY

DEPARTMENT OF PUBLIC WORKS MINNEAPOLIS, MINNESOTA FEBRUARY 1, 2011

GENERAL NOTES:

1. ALL DIMENSIONS ARE TO FACE OF CURB UNLESS NOTED OTHERWISE.
2. ALL SITE CURB AND GUTTER IS TO BE B612 CONCRETE CURB AND GUTTER UNLESS NOTED OTHERWISE. ALL STREET CURB AND GUTTER IS TO BE B624 CONCRETE CURG AND GUTTER.
3. NO SIDEWALK IS TO HAVE MORE THAN A 2% CROSS SLOPE OR MORE THAN A 5% LONGITUDINAL SLOPE.
4. REFER TO ARCHITECTURAL PLANS FOR PROPOSED BUILDING LAYOUT.
5. FOLLOW ALL CITY OF MINNEAPOLIS RULES, REGULATIONS AND SPECIFICATIONS WHEN WORKING IN PUBLIC RIGHT OF WAY.

PAVING NOTES:

1. REFER TO STRUCTURAL PLANS FOR STOOP DETAILS. ALL WALKS ARE TO BE CENTERED ON THE DOORS.
2. INSTALL APPROPRIATE EXPANSION MATERIAL WHERE CONCRETE IS ADJACENT TO BUILDING FACE.
3. SAWCUT AND MATCH NEW BITUMINOUS PAVEMENT INTO EXISTING PAVEMENT. NO ABRUPT GRADE TRANSITIONS OR PONDING OF WATER WILL BE ALLOWED.
4. MATCH NEW CONCRETE CURB AND GUTTER INTO EXISTING. FOLLOW ALL CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS FOR CURB TYPE, MATERIAL AND INSTALLATION METHODS.
5. SAWCUT EXISTING BITUMINOUS PAVEMENT AND CURB AND GUTTER TO NEAREST JOINT. COORDINATE REMOVAL LIMITS WITH SITE DEMOLITION CONTRACTOR AND CONSTRUCTION MANAGER. INSTALL DRIVE ENTRANCE PER CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS. FOLLOW ALL CITY OF MINNEAPOLIS REQUIREMENTS FOR TRAFFIC CONTROL.

OERTEL ARCHITECTS

1795 SAINT CLAIR AVENUE
ST. PAUL, MINNESOTA 55105

(651) 696-5186 TEL
(651) 696-5188 FAX

WWW.OERTELARCHITECTS.COM

PROJECT NAME:

LEGACY

2nd St. & 13th Ave.
Mpls, MN 55415

PROJECT NUMBER: 14-18

DATE OF ISSUE: April 8, 2016

DRAWN BY: NPA

CHECKED BY: JWM

REVISIONS:

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA

Joel W. Maier

SIGNATURE Joel W. Maier

DATE April 8, 2016

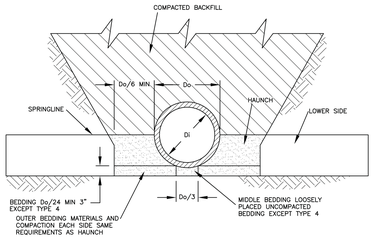
LICENSE NUMBER 19181

SHEET NAME:

PAVING AND GEOMETRIC PLAN

SHEET NO:

C4.0

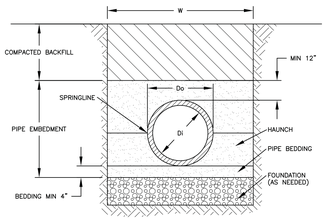


NOTES:
1. COMPACTION AND SOIL SYMBOLS–I.E. "95% CATEGORY I" REFERS TO CATEGORY I SOIL MATERIAL WITH MINIMUM STANDARD PROCTOR COMPACTION OF 95%.
2. SOIL IN BEDDING AND HAUNCH ZONES SHALL BE COMPACTED TO AT LEAST THE SAME COMPACTION AS SPECIFIED FOR THE MAJORITY OF SOIL IN THE BACKFILL ZONE.
3. THE TRENCH WIDTH SHALL BE WIDER THAN SHOWN IF REQUIRED FOR ADEQUATE SPACE TO ATTAIN SPECIFIED COMPACTION IN THE HAUNCH AND BEDDING ZONES.
4. FOR TRENCH WALLS WITH GREATER THAN 10 DEGREE SLOPES THAT CONSIST OF EMBANKMENT, THE LOWER SIDE SHALL BE COMPACTED TO AT LEAST THE SAME COMPACTION AS SPECIFIED FOR THE SOIL IN THE BACKFILL ZONE.
5. NO BEDDING IS REQUIRED FOR TYPE 4 STANDARD INSTALLATION.
6. REFER TO ASTM C1479–07 FOR DETAILS.
7. TYPE III BEDDING SHALL BE USED UNLESS NOTED OTHERWISE.

STANDARD TRENCH INSTALLATION SOIL AND MINIMUM STANDARD PROCTOR COMPACTION REQUIREMENTS		
INSTALLATION TYPE	HAUNCH AND OUTER BEDDING	LOWER SIDE
TYPE 1	95% CATEGORY I	90% CATEGORY I 95% CATEGORY II 100% CATEGORY III
TYPE 2	90% CATEGORY I 95% CATEGORY II	85% CATEGORY I 90% CATEGORY II 95% CATEGORY III
TYPE 3	85% CATEGORY I 90% CATEGORY II 95% CATEGORY III	85% CATEGORY I 90% CATEGORY II 95% CATEGORY III
TYPE 4	NO COMPACTION REQUIRED, EXCEPT IF CATEGORY III USE 85% CATEGORY III	NO COMPACTION REQUIRED, EXCEPT IF CATEGORY III USE 85% CATEGORY III

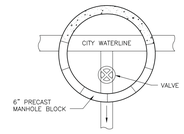
EQUIVALENT SOIL CLASSIFICATIONS FOR SOIL DESIGNATIONS		
SOIL	UNIFIED SOIL CLASSIFICATION SYSTEM (USCS)	MN/DOT SPECIFICATION
CATEGORY I	CLEAN COURSE GRAINED SOILS: SW, SP, GW, GP, OR ANY SOIL BEGINNING WITH ONE OF THESE SYMBOLS WITH 12% OR LESS PASSING A #200 SIEVE.	COARSE FILTER AGGREGATE MN/DOT 3149.2H
CATEGORY II	COURSE GRAINED SOILS WITH FINES: GM, GC, SM, SC, OR ANY SOIL BEGINNING WITH ONE OF THESE SYMBOLS CONTAINING MORE THAN 12% PASSING A #200 SIEVE	AGGREGATE BEDDING MN/DOT 3149.2G
CATEGORY III	FINE GRAINED SOILS: CL, ML, (OR CL–ML, CL,ML, ML/CL) WITH LESS THAN 30% RETAINED ON A #200 SIEVE.	NOT APPLICABLE

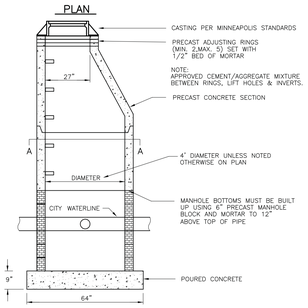
10 C5.0 TYPICAL PIPE BEDDING FOR DIP
NOT TO SCALE



NOTES:
1. THE MINIMUM TRENCH WIDTH "W" SHALL BE W=Do+16" OR 1.25*Do+12 WHICHEVER IS GREATER.
2. PIPE EMBEDMENT MATERIAL SHALL BE CLASS I OR CLASS II MATERIAL. REFER TO SPECIFICATIONS FOR DETAILS.
3. REFER TO ASTM D2321–05 "UNDERGROUND INSTALLATION OF THERMOPLASTIC PIPE FOR SEWERS AND OTHER GRAVITY–FLOW APPLICATIONS" FOR DETAILS.

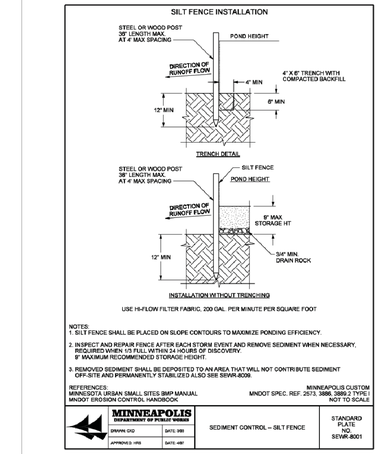
7 C5.0 TYPICAL PIPE BEDDING FOR PVC
NOT TO SCALE



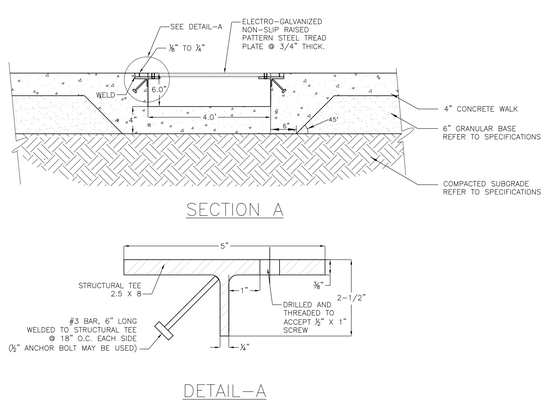


NOTE:
THE CONTRACTOR IS TO EXCAVATE FOR WATER VALVE, MANHOLE AND PIPING. CONTRACTOR IS TO PROVIDE ALL MATERIAL THE CITY WILL INSTALL WETTAP AND TEE. CONTRACTOR IS TO BACKFILL AND INSTALL WATER VALVE MANHOLE. FOLLOW ALL CITY OF MINNEAPOLIS STANDARDS AND SPECIFICATIONS.

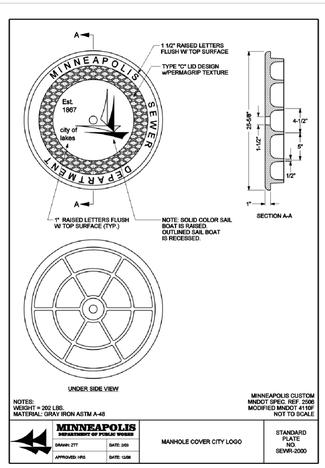
4 C5.0 WATER VALVE MANHOLE
NOT TO SCALE



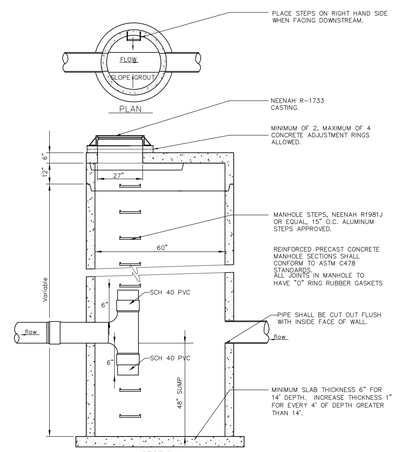
1 C5.0 SILT FENCE
NOT TO SCALE



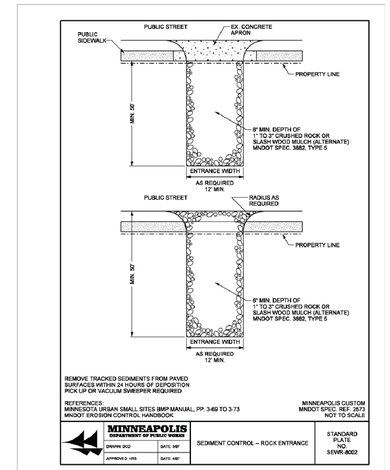
11 C5.0 CONCRETE SIDEWALK CHASE
NOT TO SCALE



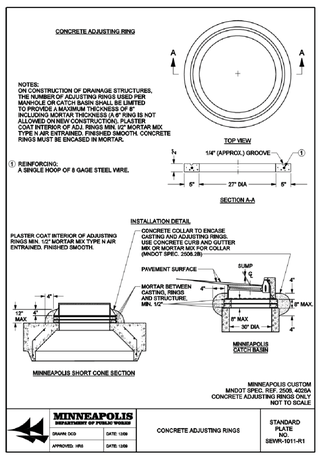
8 C5.0 STANDARD MPLS CITY LOGO COVER
NOT TO SCALE



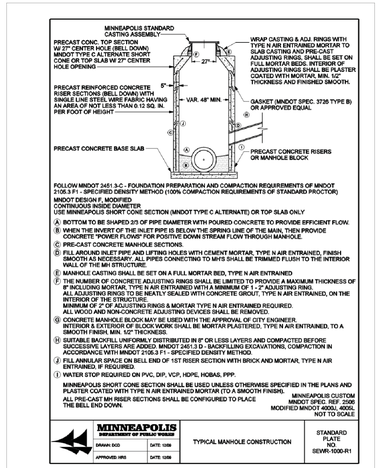
5 C5.0 SUMP MANHOLE #2
NOT TO SCALE



2 C5.0 ROCK CONSTRUCTION ENTRANCE
NOT TO SCALE



9 C5.0 CONCRETE ADJUSTMENT RINGS
NOT TO SCALE



6 C5.0 STANDARD PRECAST MANHOLE
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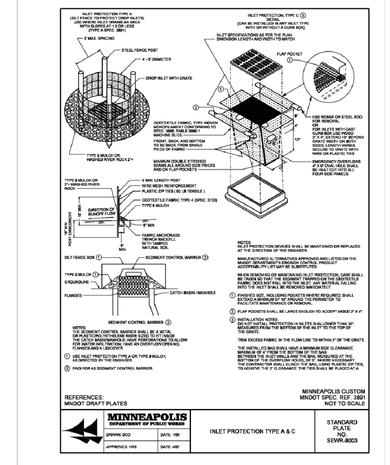

3 C5.0 INLET SEDIMENT PROTECTION
NOT TO SCALE

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PROJECT NAME:
LEGACY
2nd St. & 13th Ave.
Mpls, MN 55415

PROJECT NUMBER: 14-18
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I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA
Joel W. Maier
SIGNATURE: Joel W. Maier
DATE: April 8, 2016
LICENSE NUMBER: 19181

SHEET NAME:
CIVIL DETAILS
SHEET NO:
C5.0

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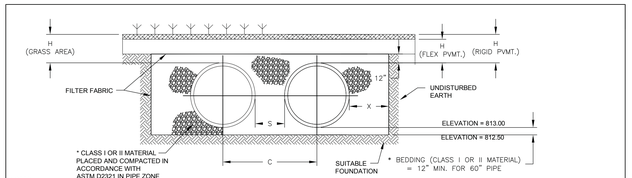

NOTES:

1. ALL REFERENCES TO CLASS I OR II MATERIAL ARE PER ASTM D2321 "STANDARD PRACTICE FOR UNDERGROUND INSTALLATION OF THERMOPLASTIC PIPE FOR SEWERS AND OTHER GRAVITY FLOW APPLICATIONS", LATEST EDITION.

2. ALL RETENTION AND DETENTION SYSTEMS SHALL BE INSTALLED IN ACCORDANCE WITH ASTM D2321, LATEST EDITION AND THE MANUFACTURER'S PUBLISHED INSTALLATION GUIDELINES.

3. MEASURES SHOULD BE TAKEN TO PREVENT THE MIGRATION OF NATIVE FINES INTO THE BACKFILL MATERIAL, WHEN REQUIRED. SEE ASTM D2321.

4. FILTER FABRIC: A GEOTEXTILE FABRIC MAY BE USED AS SPECIFIED BY THE ENGINEER TO PREVENT THE MIGRATION OF FINES FROM THE NATIVE SOIL INTO THE SELECT BACKFILL MATERIAL.

5. FOUNDATION: WHERE THE TRENCH BOTTOM IS UNSTABLE, THE CONTRACTOR SHALL EXCAVATE TO A DEPTH REQUIRED BY THE ENGINEER AND REPLACE WITH SUITABLE MATERIAL, AS SPECIFIED BY THE ENGINEER, AS AN ALTERNATIVE AND AT THE DISCRETION OF THE DESIGN ENGINEER. THE TRENCH BOTTOM MAY BE STABILIZED USING A GEOTEXTILE MATERIAL.

6. BEDDING: SUITABLE MATERIAL SHALL BE CLASS I OR II. THE CONTRACTOR SHALL PROVIDE DOCUMENTATION FOR MATERIAL SPECIFICATION TO ENGINEER. UNLESS OTHERWISE NOTED BY THE ENGINEER, MINIMUM BEDDING THICKNESS SHALL BE 4" (100mm) FOR 4"-24" (100mm-600mm); 6" (150mm) FOR 30"-60" (750mm-900mm).

7. INITIAL BACKFILL: SUITABLE MATERIAL SHALL BE CLASS I OR II IN THE PIPE ZONE EXTENDING NOT LESS THAN 6" ABOVE OVER OF PIPE. THE CONTRACTOR SHALL PROVIDE DOCUMENTATION FOR MATERIAL SPECIFICATION TO ENGINEER. MATERIAL SHALL BE INSTALLED AS REQUIRED IN ASTM D2321, LATEST EDITION.

8. MINIMUM COVER: MINIMUM COVER OVER ALL RETNETION/DETENTION SYSTEMS IN NON-TRAFFIC APPLICATIONS (GRASS OR LANDSCAPE AREAS) IS 12" FROM TOP OF PIPE TO GROUND SURFACE. ADDITIONAL COVER MAY BE REQUIRED TO PREVENT FLOATATION. FOR TRAFFIC APPLICATIONS, MINIMUM COVER IS 12" UP TO 36" DIAMETER PIPE AND 24" OF COVER FOR 42" - 60" DIAMETER PIPE, MEASURED FROM TOP OF PIPE TO BOTTOM OF FLEXIBLE PAVEMENT OR TO TOP OF RIGID PAVEMENT.

NOMINAL DIAMETER	NOMINAL O.D.	TYPICAL SPACING "S"	TYPICAL SPACING "C"	TYPICAL SIDE WALL "X"	H (NON-TRAFFIC)	H (TRAFFIC)
30" (750 MM)	36" (914 MM)	18" (457 MM)	53.1" (1347 MM)	18" (457 MM)	12" (292 MM)	12" (292 MM)
36" (900 MM)	42" (1067 MM)	22" (559 MM)	63" (1600 MM)	18" (457 MM)	12" (292 MM)	12" (292 MM)
42" (1050 MM)	48" (1219 MM)	24" (610 MM)	71.9" (1826 MM)	18" (457 MM)	12" (292 MM)	24" (610 MM)
48" (1200 MM)	54" (1372 MM)	25" (635 MM)	78.5" (1994 MM)	18" (457 MM)	12" (292 MM)	24" (610 MM)
60" (1500 MM)	67" (1702 MM)	30" (610 MM)	90" (2286 MM)	18" (457 MM)	12" (292 MM)	24" (610 MM)

* CLASS I BACKFILL REQUIRED AROUND 60" DIAMETER FITTINGS.

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ADVANCED DRAINAGE SYSTEMS, INC. ("ADS") HAS PREPARED THIS DETAIL BASED ON INFORMATION PROVIDED TO ADS. THIS DRAWING IS INTENDED TO DEPICT THE COMPONENTS AS REQUESTED. ADS HAS NOT PERFORMED ANY ENGINEERING OR DESIGN SERVICES FOR THIS PROJECT, NOR HAS ADS INDEPENDENTLY VERIFIED THE INFORMATION SUPPLIED. THE INSTALLATION DETAILS PROVIDED HEREIN MEETS OR EXCEEDS THE APPLICABLE NATIONAL, STATE, OR LOCAL REQUIREMENTS AND TO ENSURE THAT THE DETAILS PROVIDED HEREIN ARE ACCEPTABLE FOR THIS PROJECT. IT IS THE DESIGN ENGINEERS RESPONSIBILITY TO ENSURE THE DETAILS PROVIDED HEREIN MEETS OR EXCEEDS THE APPLICABLE NATIONAL, STATE, OR LOCAL REQUIREMENTS AND TO ENSURE THAT THE DETAILS PROVIDED HEREIN ARE ACCEPTABLE FOR THIS PROJECT.

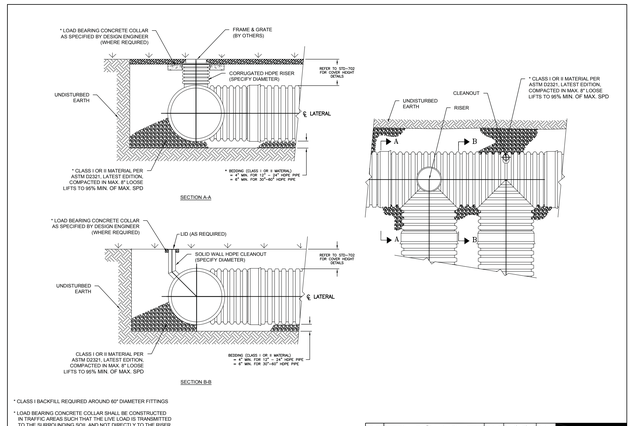
REV.	DESCRIPTION	BY	MM/DD/YY	CHK'D
3	REVISED 18" SPACING	TJR	01/05/09	CKS

TYPICAL RET/DET CROSS SECTION DETAIL — DRAWING NUMBER: STD-702

ADS — 4640 TRUEMAN BLVD, HILLIARD, OHIO 43026 — ADVANCED DRAINAGE SYSTEMS, INC.

DRAWN BY: AMM — DATE: 03.25.08 — REVISED: — SCALE: NTS — SHEET OF

4 / C5.1 INFILTRATION/RATE CONTROL TANK
NOT TO SCALE



* CLASS I BACKFILL REQUIRED AROUND 60" DIAMETER FITTINGS

* LOAD BEARING CONCRETE COLLAR SHALL BE CONSTRUCTED IN TRAFFIC AREAS SUCH THAT THE LIVE LOAD IS TRANSMITTED TO THE SURROUNDING SOIL AND NOT DIRECTLY TO THE RISER.

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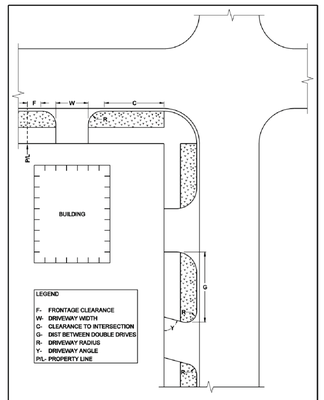
REV.	DESCRIPTION	BY	MM/DD/YY	CHK'D
2	ADDED 60" FITTINGS NOTE	TJR	07/25/08	CKS

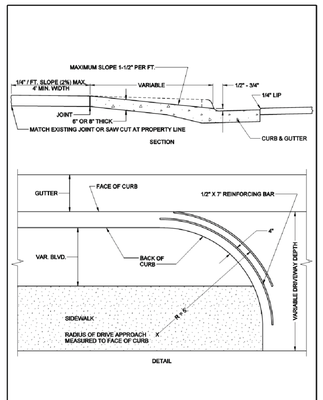
TYPICAL RET/DET RISER/CLEAN-OUT DETAIL — DRAWING NUMBER: STD-703

ADS — 4640 TRUEMAN BLVD, HILLIARD, OHIO 43026 — ADVANCED DRAINAGE SYSTEMS, INC.

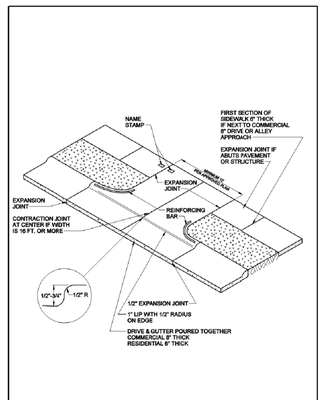
DRAWN BY: CRM — DATE: 07.23.08 — REVISED: — SCALE: NTS — SHEET OF



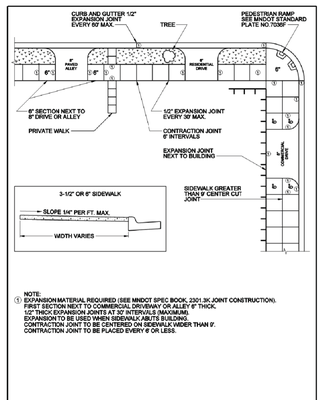
MINNEAPOLIS DEPARTMENT OF PUBLIC WORKS — DRAWN: JFC — DATE: 9/10/07 — APPROVED: DAS — DATE: 9/10/08 — DRIVEWAY REFERENCE — STANDARD PLATE NO. ROAD-2000



MINNEAPOLIS DEPARTMENT OF PUBLIC WORKS — DRAWN: JFC — DATE: 9/10/07 — APPROVED: DAS — DATE: 9/10/08 — TYPICAL DRIVEWAY CONSTRUCTION — STANDARD PLATE NO. ROAD-2001

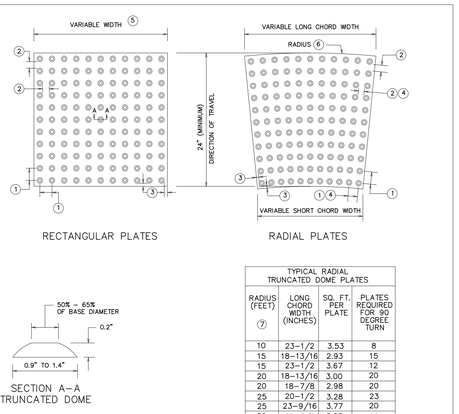


MINNEAPOLIS DEPARTMENT OF PUBLIC WORKS — DRAWN: JFC — DATE: 9/10/07 — APPROVED: DAS — DATE: 9/10/08 — TYPICAL DRIVEWAY — STANDARD PLATE NO. ROAD-2002



MINNEAPOLIS DEPARTMENT OF PUBLIC WORKS — DRAWN: JFC — DATE: 9/10/07 — APPROVED: DAS — DATE: 9/10/08 — TYPICAL SIDEWALK AND DRIVEWAY CONSTRUCTION — STANDARD PLATE NO. ROAD-2003

1 / C5.1 DRIVEWAY & SIDEWALK DETAILS
NOT TO SCALE



TYPICAL RADIAL TRUNCATED DOME PLATES			
RADIUS (FEET) ⑦	LONG CHORD WIDTH (INCHES)	SQ. FT. PER PLATE	PLATES REQUIRED FOR 90 DEGREE TURN
10	23-1/2	3.53	8
15	18-13/16	2.93	15
15	23-1/2	3.67	12
20	18-13/16	3.00	20
20	18-7/8	2.98	20
25	20-1/2	3.28	23
25	23-9/16	3.77	20
30	22-5/8	3.65	25
35	22	3.56	30

NOTES:

DETECTABLE WARNING SURFACES SHALL FOLLOW THE PUBLIC RIGHTS-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG).

DETECTABLE WARNINGS CONSIST OF TRUNCATED DOMES ALIGNED IN A SQUARE OR RADIAL GRID PATTERN.

DETECTABLE WARNINGS ARE REQUIRED:
- WHERE RAMPS, LANDINGS, OR BLENDED TRANSITIONS PROVIDE A FLUSH PEDESTRIAN CONNECTION TO THE ROADWAY.
- AT PEDESTRIAN ACCESS ROUTES CROSS COMMERCIAL DRIVEWAYS THAT ARE PROVIDED WITH TRAFFIC CONTROL DEVICES OR OTHERWISE PERMITTED TO OPERATE LIKE A PUBLIC ROADWAY.
- AT PEDESTRIAN RAILWAY CROSSINGS.
- ON RAIL PLATFORMS WHERE BOARDING EDGES ARE NOT PROTECTED.

DETECTABLE WARNINGS SHALL EXTEND:
- A MINIMUM OF 24" IN THE DIRECTION OF TRAVEL.
- THE FULL WIDTH OF THE RAMP, LANDING, OR BLENDED TRANSITION, WITHIN 3" OF FULL WIDTH ON EITHER END.
- THE FULL LENGTH OF THE PUBLIC USE AREA OF A RAIL PLATFORM.

DETECTABLE WARNING SURFACES SHALL CONTRAST VISUALLY WITH ADJACENT GUTTER, ROADWAY, OR WALKWAY, EITHER A LIGHT-ON-DARK OR DARK-ON-LIGHT. CONTRAST MAY BE PROVIDED ON THE FULL RAMP SURFACE, EXCLUDING THE FLARED SIDES.

FOR MN/DOT PROJECTS, SEE MN/DOT'S APPROVED/QUALIFIED PRODUCT LISTS.

DETECTABLE WARNING SURFACE SHALL BE PAID FOR AS TRUNCATED DOMES BY THE SQUARE FOOT.

ALL TRUNCATED DOME SYSTEMS SHALL BE PLACED IN STRICT ACCORDANCE WITH THE RECOMMENDATIONS OF THE MANUFACTURER.

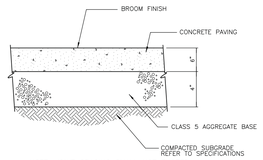
① CENTER TO CENTER DOME SPACING: 1.6" MINIMUM, 2.4" MAXIMUM.
② BASE TO BASE DOME SPACING: 0.65" MINIMUM.
③ DOME BASE TO PLATE EDGE SPACING: 0.35" MINIMUM, 0.75" MAXIMUM.
④ SPACING VARIES ON RADIAL PLATES.
⑤ TYPICAL WIDTHS AVAILABLE: 12", 18", 24", 30", 36". CHECK WITH MANUFACTURERS FOR AVAILABLE WIDTHS.
⑥ ON RADIAL PLATE, RADIUS DEFINED AT BACK OF CURB.
⑦ TYPICAL RADII. CHECK WITH MANUFACTURERS FOR AVAILABLE RADII.

STATE OF MINNESOTA DEPARTMENT OF TRANSPORTATION	SPECIFICATION REFERENCE	STANDARD PLATE NO.
DETECTABLE WARNING SURFACE TRUNCATED DOMES	2531	7038A

NOTE:
FOR ADA COMPLIANT PEDESTRIAN RAMPS, REFER TO MN/DOT STANDARD PLAN 5-297.250 PEDESTRIAN CURB RAMP DETAILS AT THE FOLLOWING WEB ADDRESS:
HTTP://STANDARDPLATES.DOT.STATE.MN.US/STDPLATE.ASPX

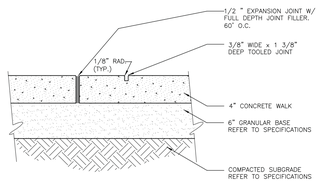
CONTRACTOR SHALL COORDINATE FINAL CURB RAMP LOCATION AND PEDESTRIAN CURB RAMP DETAIL WITH THE CITY AND OWNER PRIOR TO INSTALLATION.

8 / C5.1 PEDESTRIAN CURB RAMP – DETECTABLE WARNING
NOT TO SCALE

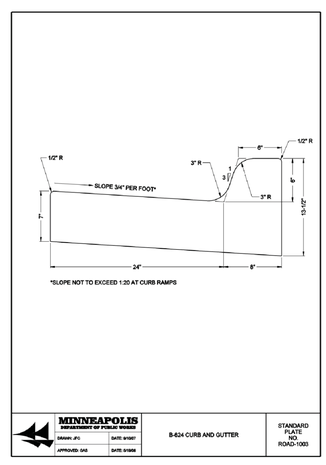


NOTE: JOINT SPACING SHALL NOT EXCEED 24 TO 30 TIMES THE PAVEMENT THICKNESS WITH A MAXIMUM OF 15 FEET.

5 / C5.1 CONCRETE PAVEMENT
NOT TO SCALE

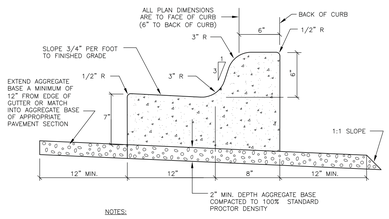


6 / C5.1 CONCRETE SIDEWALK CONSTRUCTION (TYP.)
NOT TO SCALE



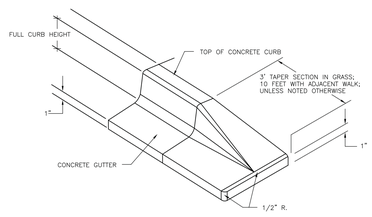
MINNEAPOLIS DEPARTMENT OF PUBLIC WORKS — DRAWN: JFC — DATE: 9/10/07 — APPROVED: DAS — DATE: 9/10/08 — B-624 CURB AND GUTTER — STANDARD PLATE NO. ROAD-1003

3 / C5.1 PUBLIC STREET CURB DETAIL
NOT TO SCALE

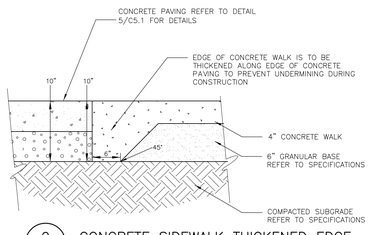


NOTES:
1. DIRECTION OF TRANSVERSE GUTTER SLOPE TO MATCH DIRECTION OF ADJACENT PAVEMENT SLOPE.
2. CONSTRUCT CURB AND GUTTER IN ACCORDANCE WITH MNDOT SPECIFICATION 2531.

2 / C5.1 B612 (SITE) CONCRETE CURB AND GUTTER
NOT TO SCALE



7 / C5.1 NOSE-DOWN CURB SECTION
NOT TO SCALE



9 / C5.1 CONCRETE SIDEWALK THICKENED EDGE
NOT TO SCALE

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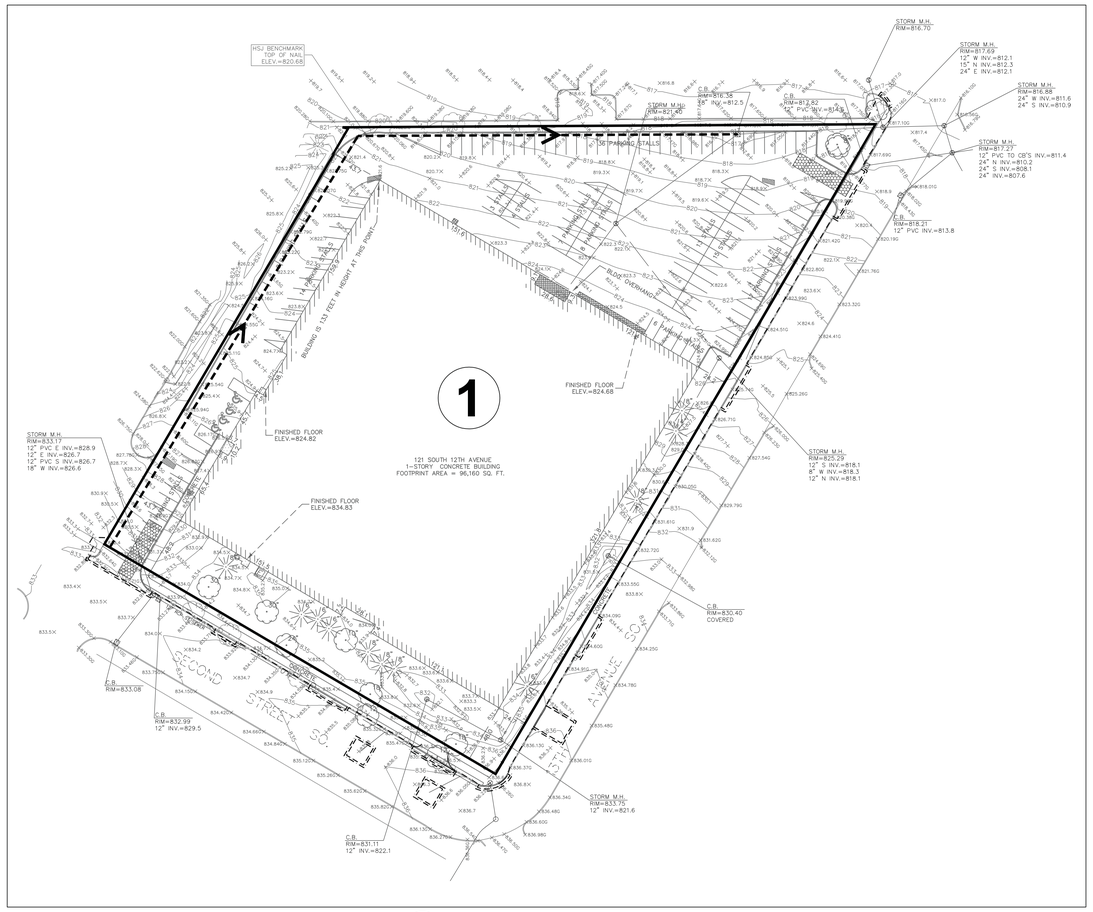
PROJECT NAME:
LEGACY
2nd St. & 13th Ave.
Mpls, MN 55415

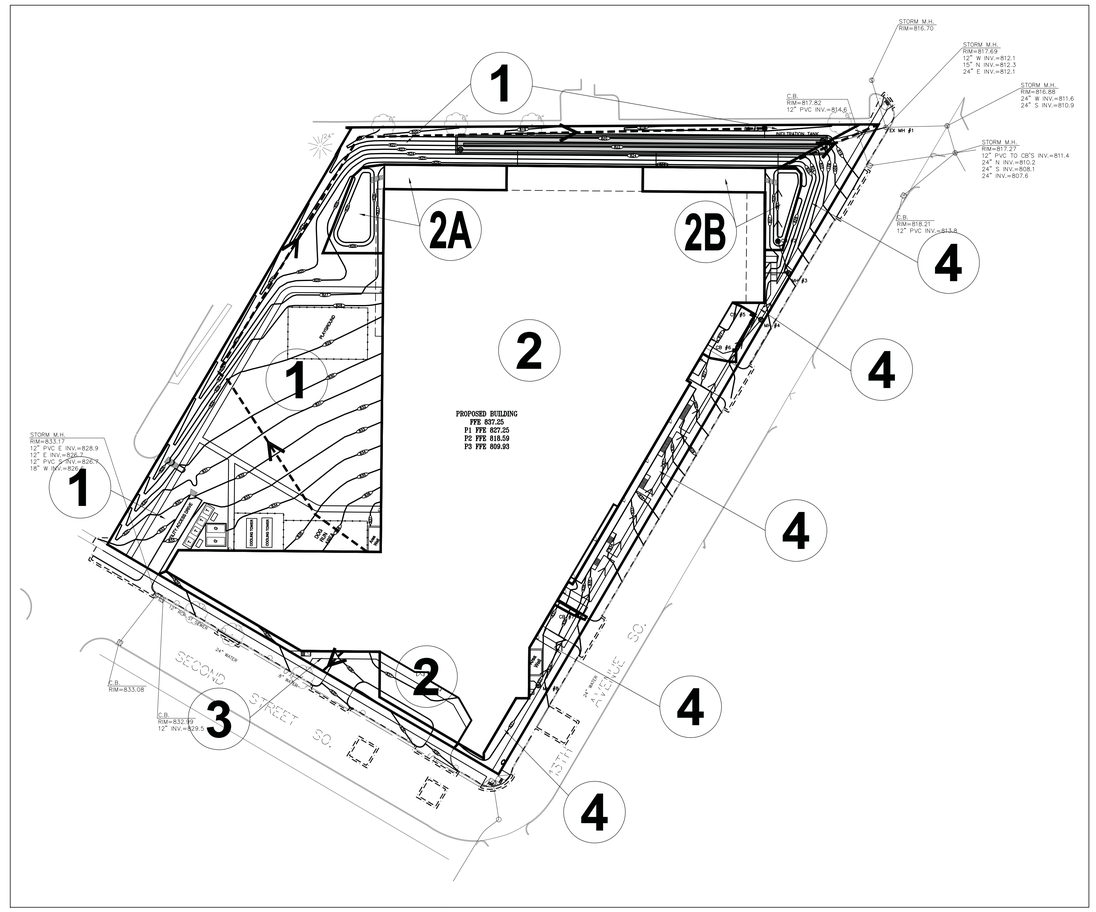
PROJECT NUMBER: 14-18
DATE OF ISSUE: April 8, 2016
DRAWN BY: NPA
CHECKED BY: JWM
REVISIONS:

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA
Joel W. Maier
SIGNATURE: Joel W. Maier
DATE: April 8, 2016
LICENSE NUMBER: 19181

SHEET NAME:
CIVIL DETAILS
SHEET NO:
C5.1



1 C6.0 EXISTING CONDITIONS
1"=50'



2 C6.0 PROPOSED CONDITIONS
1"=50'

N

50' 25' 0 50' 100'

SCALE 1" = 50'

EXISTING DRAINAGE AREAS

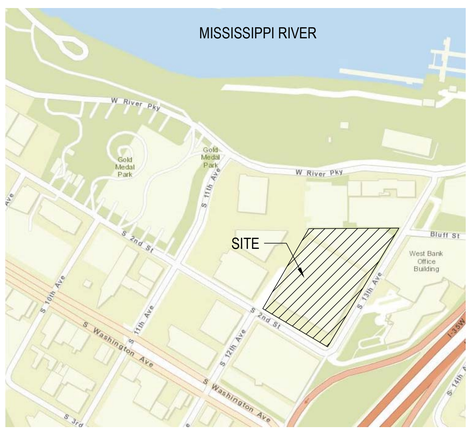
DRAINAGE AREA	IMPERVIOUS AREA (ACRES)	PERVIOUS AREA (ACRES)	TOTAL AREA (ACRES)	TIME OF CONCENTRATION	Q OUT (CFS) STORM EVENT				ROUTING
					2-YEAR (2.75")	10-YEAR (4.15")	100-YEAR (5.90")		
1	3.62	0.65	4.27	5.0	14.64	22.99	33.72		STORM SEWER FLOWS NE
TOTAL	3.62	0.65	4.27		14.64	22.99	33.72		

STORMWATER RUNOFF SUMMARY

	2-YR STORM (2.75") RUNOFF (CFS)	10-YR STORM (4.15") RUNOFF (CFS)	100-YR STORM (5.90") RUNOFF (CFS)
EXISTING SITE	14.64	22.99	33.72
PROPOSED SITE	4.08	10.23	15.10
PREDEVELOPMENT CONDITIONS	0.29	2.43	6.79

PROPOSED DRAINAGE AREAS

DRAINAGE AREA	IMPERVIOUS AREA (ACRES)	PERVIOUS AREA (ACRES)	TOTAL AREA (ACRES)	TIME OF CONCENTRATION	Q OUT (CFS) STORM EVENT				ROUTING
					1.25" (1.25")	2-YEAR (2.75")	10-YEAR (4.15")	100-YEAR (5.90")	
1	0.11	1.11	1.22	16.2	0.14	0.49	1.53	3.25	TO INFILTRATON TANK
2	2.47	0.00	2.47	5.0	4.29	9.89	15.06	21.49	TO INFILTRATION TANK
2A	0.05	0.06	0.11	2.0	0.09	0.23	0.43	0.71	RAIN GARDEN WEST
RAIN GARDEN WEST	NA	NA	NA	--	0.00	0.00	0.00	0.01	TO INFILTRATION TANK
2B	0.05	0.04	0.09	2.0	0.09	0.23	0.39	0.61	RAIN GARDEN EAST
RAIN GARDEN EAST	NA	NA	NA	--	0.00	0.00	0.00	0.32	TO INFILTRATION TANK
INFILTRATION TANK	NA	NA	NA	--	0.00	3.26	8.76	12.74	TO STORM NE
3	0.03	0.03	0.06	0.3	0.06	0.15	0.27	0.43	SHEET FLOWS TO 2ND ST
4	0.16	0.16	0.32	6.1	0.27	0.67	1.20	1.93	SHEET FLOWS TO 13TH AVE
TOTAL	2.87	1.40	4.27		0.33	4.08	10.23	15.10	



MINNEAPOLIS VICINITY MAP

PROJECT NARRATIVE

THE EXISTING 4.27 ACRE SITE CONSISTS OF A 122,000 SQUARE FOOT BUILDING AND ASSOCIATED SURFACE PARKING THAT WILL BE RAZED IN ITS ENTIRETY. CURRENTLY THE SITE DRAINS VIA ON-SITE STORM SEWER TO A STORM SEWER MAIN LOCATED IN THE NE CORNER OF THE SITE.

THE PROPOSED PROJECT SITE INCLUDES A 14-STORY, 374-UNIT CONDOMINIUM. THE ROOF AND A MAJORITY OF THE SURFACE RUNOFF WILL BE ROUTED TO AN UNDERGROUND INFILTRATION TANK LOCATED ON THE NORTH SIDE OF THE SITE THAT WILL DRAIN INTO THE STORM SEWER MAIN LOCATED IN THE NE CORNER OF THE SITE.

CONSTRUCTION ACTIVITY EROSION PREVENTION PRACTICES

CONTRACTOR SHALL STABILIZE ALL EXPOSED SOIL AREAS (INCLUDING STOCKPILES). STABILIZATION MUST BE INITIATED IMMEDIATELY TO LIMIT SOIL EROSION WHENEVER ANY CONSTRUCTION ACTIVITY HAS PERMANENTLY OR TEMPORARILY CEASED ON ANY PORTION OF THE SITE AND WILL NOT RESUME FOR A PERIOD EXCEEDING 14 CALENDAR DAYS. STABILIZATION MUST BE COMPLETED NO LATER THAN 14 CALENDAR DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

FOR PUBLIC WATER THAT THE DNR HAS PROMULGATED "WORK IN WATER RESTRICTIONS" DURING SPECIFIED FISH SPAWNING TIME FRAMES, ALL EXPOSED SOIL AREAS THAT ARE WITHIN 200 FEET OF THE WATER'S EDGE, AND DRAIN TO THESE WATERS MUST COMPLETE THE STABILIZATION ACTIVES WITHIN 24 HOURS DURING THE RESTRICTION PERIOD.

PIPE OUTLETS MUST BE PROVIDED WITH TEMPORARY OR PERMANENT ENERGY DISSIPATION WITHIN 24-HOURS AFTER CONNECTION TO A SURFACE WATER.

SEDIMENT CONTROL MEASURES MUST BE INSTALLED ON ALL DOWN GRADIENT PERIMETERS BEFORE ANY UPGRADIENT LAND DISTURBING ACTIVITIES BEGIN.

SPECIAL AND IMPAIRED WATERS

THE MISSISSIPPI RIVER IS LOCATED WITHIN ONE MILE (AERIAL RADIUS) OF THE PROJECT LIMITS AND RECEIVES RUNOFF FROM THE PROJECT SITE. THE PROJECT LIMITS ARE NOT LOCATED WITHIN ONE MILE (AERIAL RADIUS) OF ANY IMPAIRED WATERS.

WATERBODY	IMPAIRMENT(S)
MISSISSIPPI RIVER	NONE

MINIMUM ESTIMATED QUANTITIES FOR EROSION CONTROL

ITEM DESCRIPTION	ESTIMATED QUANTITY	
DRAINAGE STRUCT. INLET FILTER	7	EACH
ROCK CONSTRUCTION ENTRANCE	2	EACH
CONCRETE WASHOUT	1	EACH
SILT FENCE	1730	LF
EROSION CONTROL BLANKET	1387	SY

NOTE: QUANTITEIS SHOWN ARE THE MINIMUM REQUIRED, ADDITIONAL QUANTITIES MAY BE NEEDED IF REQUIRED BY THE MPCA, WATERSHED DISTRICT, OR CITY. CONTRACTOR IS RESPONSIBLE FOR FINAL DETERMINATION OF QUANTITIES PRIOR TO CONSTRUCTION.

SEDIMENT AND EROSION CONTROL MAINTENANCE

PERIMETER SEDIMENT CONTROL PRACTICES: WHEN SEDIMENT REACHES 1/3 THE HEIGHT OF THE BMP, THE SEDIMENT MUST BE REMOVED WITHIN 24 HOURS. IF PERIMETER SEDIMENT CONTROL HAS BEEN DAMAGED OR IS NOT FUNCTIONING PROPERLY, IT MUST BE REPAIRED AND/OR REPLACED WITHIN 24 HOURS. PERIMETER BMP MEASURES MAY INCLUDE SILT FENCING.

CONSTRUCTION SITE VEHICLE EXIT LOCATIONS: ALL TRACKED SEDIMENT ONTO PAVED SURFACES MUST BE REMOVED WITHIN 24 HOURS OF DISCOVERY OR MORE FREQUENTLY IF REQUIRED BY CITY OR WATERSHED.

CONSTRUCTION SITE DEWATERING: THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL DEWATERING PERMITS. DISCHARGE FROM ALL DEWATERING OPERATIONS SHALL BE DIRECTED TO ON-SITE DEPRESSIONS. NO DISCHARGE FROM DEWATERING OPERATIONS SHALL BE DIRECTED OFF-SITE TOWARDS A WATER OF THE STATE.

HANDLING AND STORAGE OF HAZARDOUS MATERIALS:

IF THE CONTRACTOR INTENDS TO USE POLYMERS, FLOCCULANTS, OR OTHER SEDIMENTATION TREATMENT CHEMICALS ON THE PROJECT SITE, THE CONTRACTOR MUST COMPLY WITH THE FOLLOWING MINIMUM REQUIREMENTS:

1. THE CONTRACTOR MUST USE CONVENTIONAL EROSION AND SEDIMENT CONTROLS PRIOR TO CHEMICAL ADDITION TO ENSURE EFFECTIVE TREATMENT. CHEMICALS MAY ONLY BE APPLIED WHERE TREATED STORMWATER IS DIRECTED TO A SEDIMENT CONTROL SYSTEM WHICH ALLOWS FOR FILTRATION OR SETTLEMENT OF THE FLOC PRIOR TO DISCHARGE.
2. CHEMICALS MUST BE SELECTED THAT ARE APPROPRIATELY SUITED TO THE TYPES OF SOILS LIKELY TO BE EXPOSED DURING CONSTRUCTION, AND TO THE EXPECTED TURBIDITY, PH AND FLOW RATE OF STORMWATER FLOWING INTO THE CHEMICAL TREATMENT SYSTEM OR AREA.
3. CHEMICALS MUST BE USED IN ACCORDANCE WITH ACCEPTED ENGINEERING PRACTICES, AND WITH DOSING SPECIFICATIONS AND SEDIMENT REMOVAL DESIGN SPECIFICATIONS PROVIDED BY THE MANUFACTURER OR PROVIDER/SUPPLIER OF THE APPLICABLE CHEMICALS.

ON-SITE FUEL TANKS REQUIRE SECONDARY CONTAINMENT AS REQUIRED BY THE PERMIT. PORTABLE FUEL TRUCKS SHALL HAVE THEIR SPILL KITS AVAILABLE DURING FUELING. SPILLS GREATER THAN 5 GALLONS MUST BE REPORTED TO THE PROPER AUTHORITIES.

INSPECTIONS

EXPOSED SOIL AREAS: ONCE EVERY 7 DAYS AND WITHIN 24 HOURS FOLLOWING A 1/2 INCH OVER 24 HOURS RAIN EVENT.

STABILIZED AREAS: ONCE EVERY 30 DAYS.

FROZEN GROUND: AS SOON AS RUNOFF OCCURS OR PRIOR TO RESUMING CONSTRUCTION.

RECORDS: A COPY OF THE GRADING, DRAINAGE EROSION CONTROL PLAN AND WATERSHED DATA & SWPPP PLANS AS WELL AS THE INSPECTIONS/MAINTENANCE LOGS ARE TO BE KEPT EITHER IN THE FIELD OFFICE, INSPECTOR'S VEHICLE OR CONTRACTOR'S VEHICLE.

AGENCY CONTACTS

CITY OF MINNEAPOLIS
PUBLIC WORKS
309 2ND AVENUE S.,
RM 300 CITY HALL
MINNEAPOLIS, MN 55401
PHONE: (612) 673-2157

MINNESOTA POLLUTION CONTROL AGENCY
520 LAFAYETTE ROAD
SAINT PAUL, MN 55155
PHONE: (651) 296-6300

NOTE:
THE STORM WATER POLLUTION PREVENTION PLAN FOR THIS PROJECT INCLUDES THE CIVIL ENGINEERING PLANS AND THE PROJECT MANUAL.

CONTRACTOR TO SUPPLY CONSTRUCTION PHASING NARRATIVE, ESTIMATED PRELIMINARY QUANTITIES OF ALL EROSION PREVENTION AND SEDIMENT CONTROL BMP'S ANTICIPATED AT THE START OF THE PROJECT AND FOR THE LIFE OF THE PROJECT, AND LOCATION OF AREAS WHERE CONSTRUCTION WILL BE PHASED TO MINIMIZE DURATION OF EXPOSED SOIL AREAS. CONTRACTOR IS TO REVIEW MINNESOTA POLLUTION CONTROL AGENCY'S INSTRUCTIONS FOR THE APPLICATION FOR MINNESOTA'S NPDES/SDS GENERAL STORMWATER PERMIT FOR CONSTRUCTION ACTIVITY PRIOR TO SUBMITTING APPLICATION.

GRADING & SOILS

BASED ON SOIL BORING(S) PROVIDED BY BRAUN INTEREC FOR THE ADJACENT PROJECT, SOILS ARE BELIEVED TO BE SP AND SM SOILS

REFER TO THE GEOTECHNICAL REPORT FOR ADDITIONAL INFORMATION.

POLLUTION PREVENTION MANAGEMENT MEASURES

SOLID WASTE DISPOSED PROPERLY; COMPLY WITH MPCA REQUIREMENTS.

HAZARDOUS WASTE STORED (SECONDARY CONTAINMENT, RESTRICTED ACCESS) AND DISPOSED IN COMPLIANCE WITH MPCA REQUIREMENTS.

EXTERNAL WASHING OF TRUCKS AND OTHER CONSTRUCTION VEHICLES MUST BE LIMITED TO A DEFINED AREA OF THE SITE. RUNOFF MUST BE CONTAINED AND WASTE PROPERLY DISPOSED.

NO ENGINE DEGREASING ALLOWED ON-SITE.

CONCRETE WASHOUT ON-SITE: ALL LIQUID AND SOLID WASTES GENERATED BY CONCRETE WASHOUT OPERATIONS MUST BE CONTAINED IN A LEAK-PROOF CONTAINMENT FACILITY OR IMPERMEABLE LINER. A COMPACTED CLAY LINER THAT DOES NOT ALLOW LIQUIDS TO ENTER GROUND WATER IS CONSIDERED AN IMPERMEABLE LINER. THE LIQUID AND SOLID WASTES MUST NOT CONTACT THE GROUND, AND THERE MUST NOT BE RUNOFF FROM THE CONCRETE WASHOUT OPERATIONS OR AREAS. LIQUID AND SOLID WASTES MUST BE DISPOSED OF PROPERLY AND IN COMPLIANCE WITH MPCA REGULATIONS. A SIGN MUST BE INSTALLED ADJACENT TO EACH WASHOUT FACILITY TO INFORM CONCRETE EQUIPMENT OPERATORS TO UTILIZE THE PROPER FACILITIES. THE CONCRETE WASHOUT AREA INDICATED ON THE PLANS IS SHOWN IN AN APPROXIMATE LOCATION. PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION IN ACCORDANCE WITH MPCA REQUIREMENTS.

FINAL STABILIZATION

STABILIZATION BY UNIFORM PERENNIAL VEGETATIVE COVER (70% DENSITY)

DRAINAGE DITCHES STABILIZED.

ALL TEMPORARY SYNTHETIC AND STRUCTURAL BMP'S REMOVED.

CLEAN OUT SEDIMENT FROM CONVEYANCES AND SEDIMENTATION BASINS (RETURN TO DESIGN CAPACITY).

MECHANICAL AND NON STORMWATER DISCHARGES, EXISTING AND PROPOSED

1. WATER LINE FLUSHING
2. LANDSCAPE IRRIGATION
3. DISCHARGE FROM POTABLE WATER SOURCES
4. FOUNDATION DRAINS
5. AIR CONDITIONING CONDENSATION

STOCKPILES:

ON-SITE STOCKPILES OF SOIL SHALL HAVE PERIMETER SEDIMENT CONTROL. STOCKPILES SHALL BE STABILIZED WITH BLANKETS, TARPS, OR HYDRO MULCH IF LEFT ON-SITE FOR MORE THAN 14 DAYS.

TEMPORARY SEDIMENT BASINS:

TEMPORARY SEDIMENT BASINS SHALL BE PROVIDED PER APPENDIX A, SECTION C.1.B OF THE MPCA GENERAL STORMWATER PERMIT.

ENGINEER ANTICIPATES THAT, PRIOR TO INSTALLATION OF FILTRATION MEDIA AND DRAIN TILE, CONTRACTOR WILL USE PROPOSED FILTRATION BASINS AS TEMPORARY SEDIMENT BASINS. CONTRACTOR SHALL EXCAVATE TEMPORARY BASINS AND CLAY LINE PRIOR TO USE. SURFACE WATER SHALL BE REMOVED BY SKIMMER DEVICE SUCH AS FAIRCLOTH SKIMMER OR THIRST DUCK, OR USING A PUMP WITH A FILTER. ALTERNATIVE TEMPORARY SEDIMENT BASINS SHALL BE APPROVED BY ENGINEER PRIOR TO USE.

SEDIMENT AND EROSION CONTROL MAINTENANCE

PERIMETER SEDIMENT CONTROL PRACTICES: WHEN SEDIMENT REACHES 1/3 THE HEIGHT OF THE BMP, THE SEDIMENT MUST BE REMOVED WITHIN 24 HOURS. IF PERIMETER SEDIMENT CONTROL HAS BEEN DAMAGED OR IS NOT FUNCTIONING PROPERLY, IT MUST BE REPAIRED AND/OR REPLACED WITHIN 24 HOURS. PERIMETER BMP MEASURES MAY INCLUDE SILT FENCING.

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PORTABLE TOILET NOTES:

1. PORTABLE TOILETS POSE AN ENVIRONMENTAL HAZARD WHEN PLACED IN THE VICINITY OF STORM DRAINS OR BODIES OF WATER. PORTABLE TOILET CLEANING ACTIVITIES CAN ALSO GENERATE POLLUTANTS THAT CAN DEGRADE WATER QUALITY.
2. PORTABLE TOILET PLACEMENT:
   - 2.1. PLACE PORTABLE TOILETS ON FLAT STABLE GROUND WITH CLEAR ACCESS TO THE UNITS.
   - 2.2. LOCATE TOILETS A MINIMUM OF 20 FEET FROM ANY WATER BODY AND 10 FEET FROM ANY CURB AND GUTTER. IF UNFEASIBLE, AN EARTHERN BERM OR SAND BAG BERM SHALL BE PLACED AROUND THE UNIT FOR SPILL AND LEAK CONTAINMENT.
   - 2.3. AVOID PLACING TOILETS ON IMPERVIOUS SURFACES THAT WILL QUICKLY DRAIN TO STORM SEWERS.
   - 2.4. LOCATE TOILETS SO THAT EXPOSURE TO TRAFFIC AND MOVING EQUIPMENT IS MINIMIZED.
   - 2.5. SECURE TOILETS TO THE GROUND WITH STAKES OR CABLES.
   - 2.6. RINSE WATER FROM CLEANING ACTIVITIES SHALL NOT BE DISPOSED ON SITE.
3. REGULARLY CHECK TOILETS FOR DAMAGE, LEAKS AND SPILLS AS PART OF THE WEEKLY STORMWATER SITE INSPECTION.
4. OWNER IDENTIFICATION AND CONTACT INFORMATION SHALL BE DISPLAYED IN A PROMINENT LOCATION ON EACH UNIT.

NOTE:

THE CONTRACTOR MUST COMPLETE, SIGN, OBTAIN OWNERS SIGNATURE, PAY FEE, AND SEND IN THE NPDES PERMIT APPLICATION. CONTRACTOR SHALL PROVIDE A CERTIFIED EROSION CONTROL SUPERVISOR. SWPPP DOCUMENTATION, INCLUDING INSPECTION REPORTS SHALL BE RETAINED FOR A PERIOD OF THREE (3) YEARS. DESIGN CALCULATIONS ARE ON FILE AT BKBM.

THE OWNER AND CONTRACTOR ARE RESPONSIBLE FOR IMPLEMENTATION OF THE SWPPP AND INSTALLATION, INSPECTION, AND MAINTENANCE OF THE EROSION PREVENTION AND SEDIMENT CONTROL BMPS, BEFORE, DURING, AND AFTER CONSTRUCTION UNTIL THE NOTICE OF TERMINATION HAS BEEN FILED.

SWPPP IMPLEMENTATION, INSTALLATION, INSPECTION, AND BMP MAINTENANCE BY FRATTALONE COMPANIES.

NAME: _____

CERTIFICATION #:_____

DATE: _____

OTHER NOTES:

1. LONG TERM MAINTENANCE OF THE SITE WILL BE PERFORMED BY THE HOME OWNERS ASSOCIATION. INCLUDED MAINTENANCE FOR STORMWATER DEVISES SHALL BE:
   - 1.1. INSPECT SUMP CATCH BASINS ON A BIANNUAL BASIS, ONCE IN THE SPRING AND ONCE IN THE FALL
   - 1.2. CLEAN SUMP CATCH BASINS OF SEDIMENT AND DEBRIS ANNUALLY OR WHEN SEDIMENTS FILL ½ OF THE STORAGE VOLUME.
   - 1.3. ADDITIONAL LONG TERM MAINTENANCE MEASURES INCLUDE YEARLY INSPECTION OF TANK AND PERIODIC REMOVAL OF SEDIMENT ACCUMULATION IN TANK BY JET SPAY AND VAC TRUCK.
2. THIS SWPPP WAS PREPARED BY PERSONNEL THAT ARE CERTIFIED IN THE DESIGN OF CONSTRUCTION SWPPPS. COPIES OF THE CERTIFICATIONS ARE ON FILE WITH BKBM AND ARE AVAILABLE UPON REQUEST.
3. THIS SWPPP DOCUMENT MUST BE AMENDED AS NECESSARY DURING CONSTRUCTION IN ORDER TO KEEP IT CURRENT WITH THE POLLUTANT CONTROL MEASURES UTILIZED AS THE SITE. THE SITE MAP SHOWING LOCATIONS OF ALL STORM WATER CONTROLS MUST BE POSTED ON THE SITE AND UPDATED TO REFLECT THE PROGRESS OF CONSTRUCTION.

OERTEL ARCHITECTS
1795 Saint Clair Avenue
St. Paul, Minnesota 55105
(651) 696-5186 Tel
(651) 696-5188 Fax
www.oertelarchitects.com

PROJECT NAME:

LEGACY

2nd St. & 13th Ave.
Mpls, MN 55415

PROJECT NUMBER:	14-18
DATE OF ISSUE:	April 8, 2016
DRAWN BY:	NPA
CHECKED BY:	JWM
REVISIONS:	

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA

Joel W. Maier

SIGNATURE Joel W. Maier

DATE April 8, 2016

LICENSE NUMBER 19181

SHEET NAME:

STORM WATER POLLUTION PREVENTION PLAN

SHEET NO:

C6.0

P:\14018\Drawings\Civil\C6.0 SWPP 14018.dwg, 4/7/2016 4:07:48 PM

GENERAL ELECTRICAL NOTES:

A. REFER TO ARCHITECTURAL REFLECTED CEILING PLANS FOR PLACEMENT OF LUMINAIRES, AND FOR COORDINATION OF OTHER SYSTEMS IN CEILING.

B. REFER TO ARCHITECTURAL DETAIL SHEETS FOR LOCATION INTENT OF LUMINAIRES IN LIGHT COVES, SOFFITS, ETC.

C. HOME RUNS LONGER THAN 100' FOR 20 AMP, 120 VOLT, OR HOME RUNS LONGER THAN 200' FOR 20 AMP, 277 VOLT SHALL BE #10 AWG MINIMUM.

D. MAINTAIN FIRE RATING OF ALL WALLS OR SLABS. FIRE SEAL ALL PENETRATIONS AS REQUIRED.

E. PROVIDE SEPARATE NEUTRALS FOR BRANCH CIRCUITS.

SPECIFIC ELECTRICAL NOTES:

1. EXTERIOR LIGHTING CONTROLS. REFER TO DETAIL **/E### FOR WIRING CONTROL INTENT.
2. PHOTO CELL LOCATION.
3. INDICATED LIGHTING BRANCH CIRCUIT TO BE PHOTO INITIATED ON, TIME CLOCK OFF. TIME TO BE DETERMINED BY OWNER. REFER TO DETAIL **/E### FOR WIRING DIAGRAM INTENT.
4. .

1 **FIRST FLOOR LIGHTING PLAN**
SCALE: **1/16" = 1'-0"**

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Minneapolis, MN 55432
Phone (763) 571-8000
Fax (763) 571-7210
www.parsonscorp.com

Project

Legacy

Project Address

Engineering Stamp

Project Info

Project:	C15XXXXX
Date:	XX/XX/XX
Project Manager:	X. Xxxxxxx
Project Lead:	X. Xxxxxxx
Designer:	X. Xxxxxxx

Sheet Reference Scale

0 1

BAR IS 1" ON ORIGINAL DRAWING.
IF NOT 1" ON THIS DRAWING
ADJUST SCALE ACCORDINGLY.

Revisions

#	Description	Date
1	Revision	Date

Sheet Name

FIRST FLOOR LIGHTING PLAN

Sheet Number

E201

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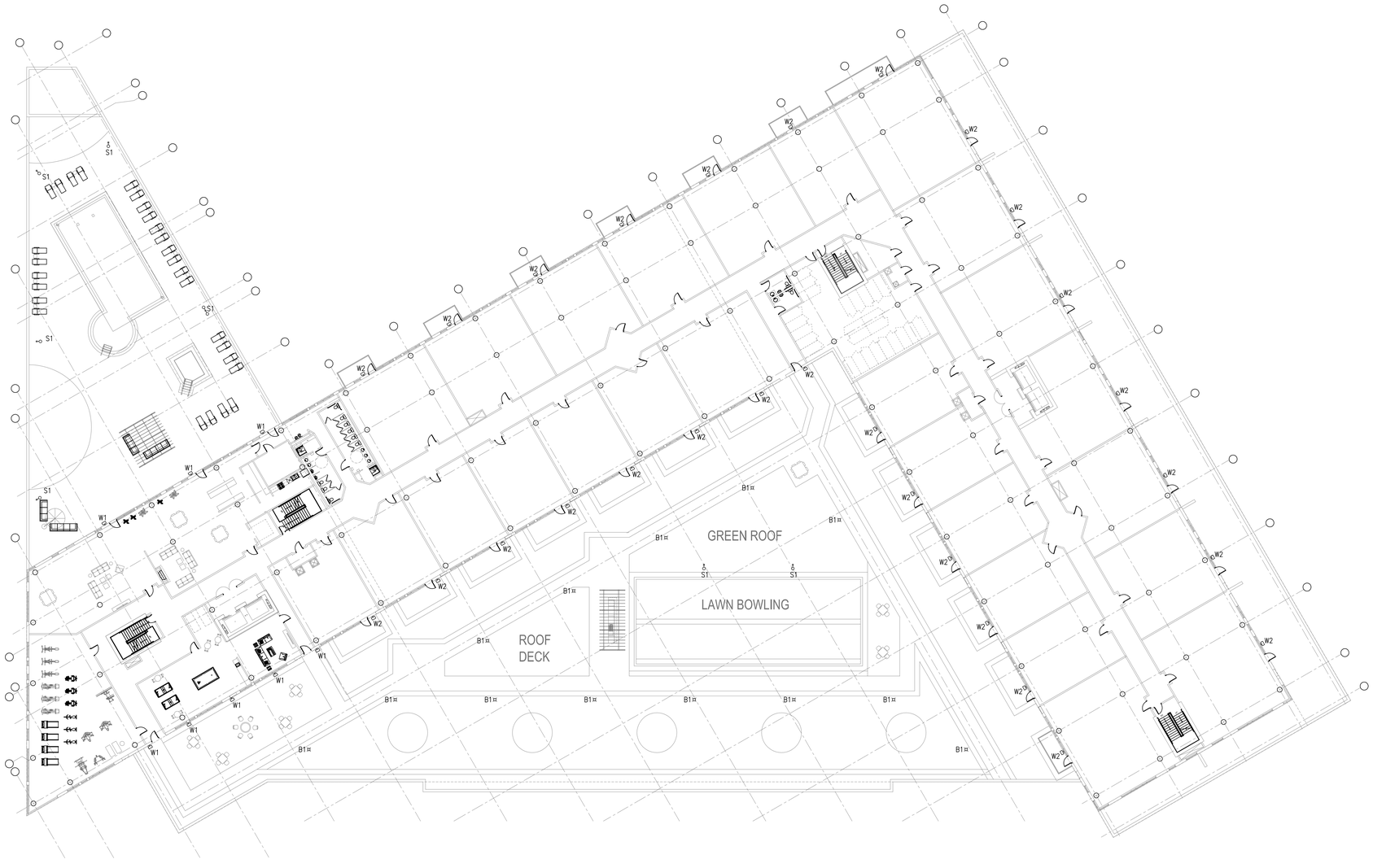
E. PROVIDE SEPARATE NEUTRALS FOR BRANCH CIRCUITS.

SPECIFIC ELECTRICAL NOTES:

1. EXTERIOR LIGHTING CONTROLS. REFER TO DETAIL **/E### FOR WIRING CONTROL INTENT.
2. PHOTO CELL LOCATION.
3. INDICATED LIGHTING BRANCH CIRCUIT TO BE PHOTO INITIATED ON, TIME CLOCK OFF. TIME TO BE DETERMINED BY OWNER. REFER TO DETAIL **/E### FOR WIRING DIAGRAM INTENT.
4. .



1 **SECOND FLOOR LIGHTING PLAN**
SCALE: **1/16" = 1'-0"**

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Minneapolis, MN 55432
Phone (763) 571-8000
Fax (763) 571-7210
www.parsonscorp.com

Project

Legacy

Project Address

Engineering Stamp

Project Info

Project:	C15XXXXX
Date:	XX/XX/XX
Project Manager:	X. Xxxxxxx
Project Lead:	X. Xxxxxxx
Designer:	X. Xxxxxxx

Sheet Reference Scale

0 1

BAR IS 1" ON ORIGINAL DRAWING.
IF NOT 1" ON THIS DRAWING
ADJUST SCALE ACCORDINGLY.

Revisions

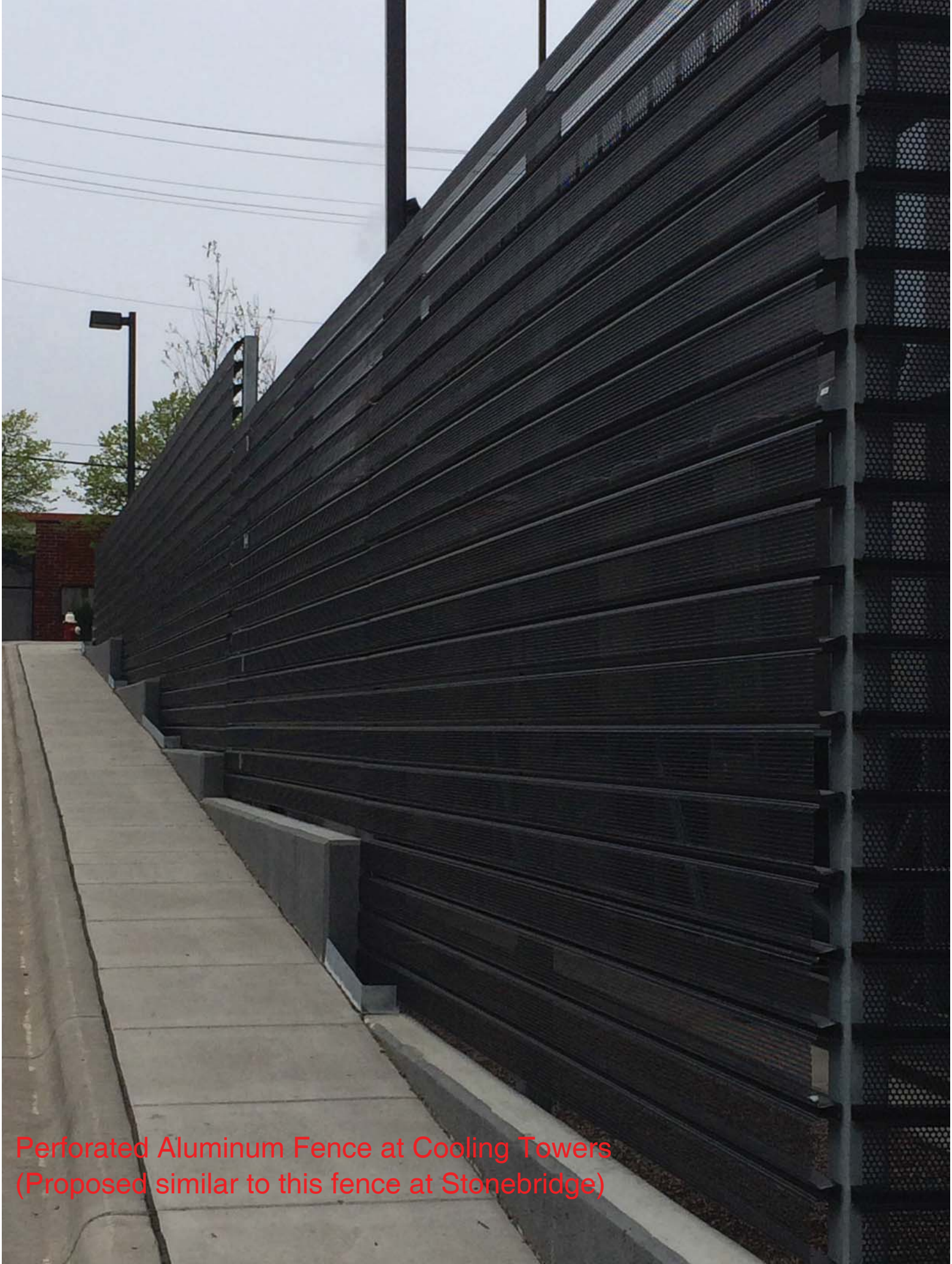
#	Description	Date
1	Revision	Date

Sheet Name

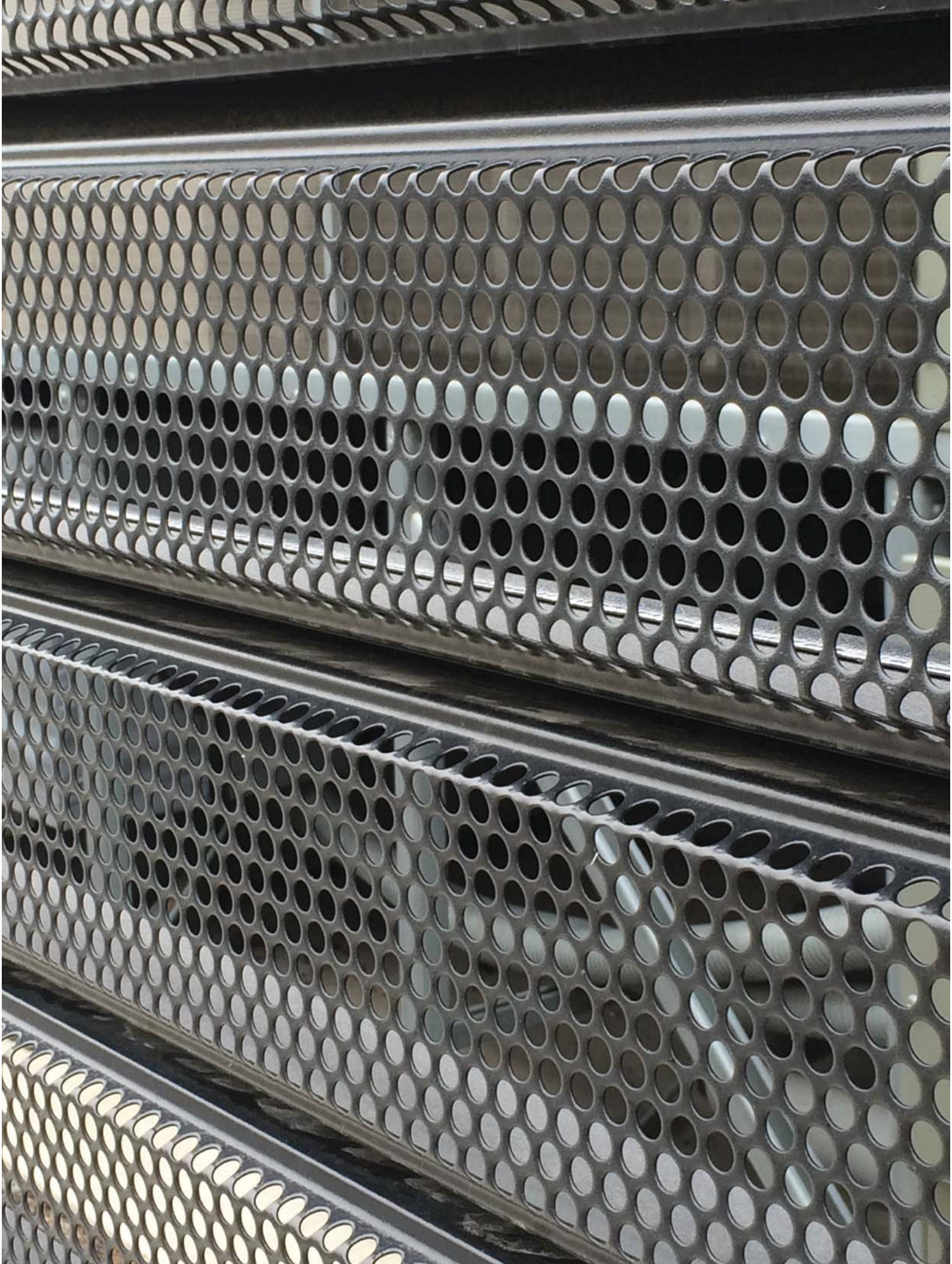
SECOND FLOOR
LIGHTING PLAN

Sheet Number

E202



Perforated Aluminum Fence at Cooling Towers
(Proposed similar to this fence at Stonebridge)

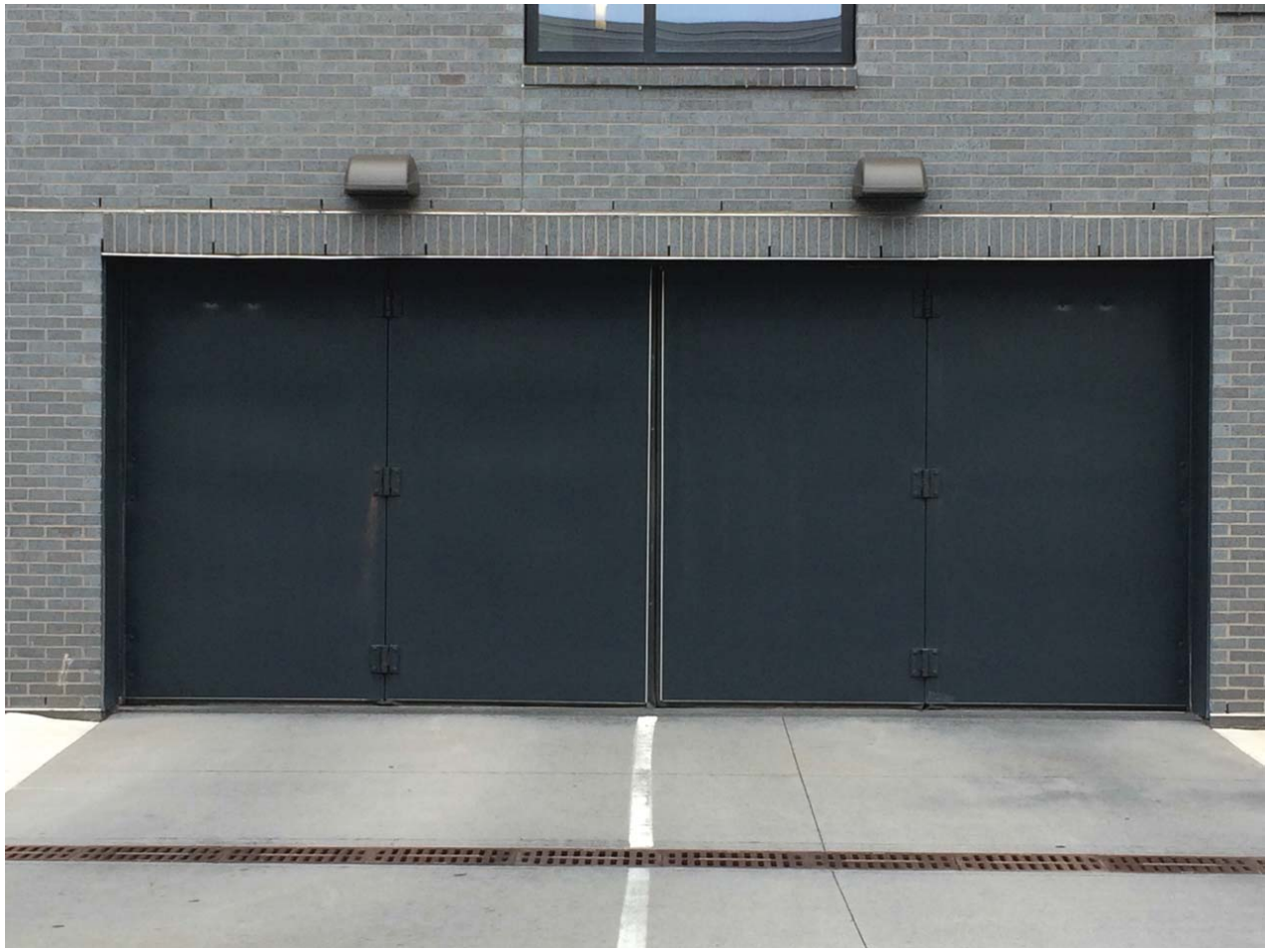


Perforated Aluminum Fence at Cooling Towers
Englarged View

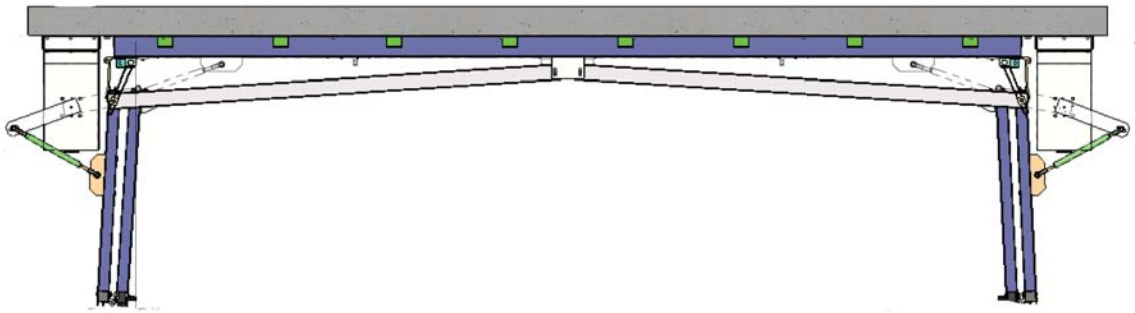
Prefinished Aluminum Railing at Pool Deck
(Proposed similar to this railing at Stonebridge)

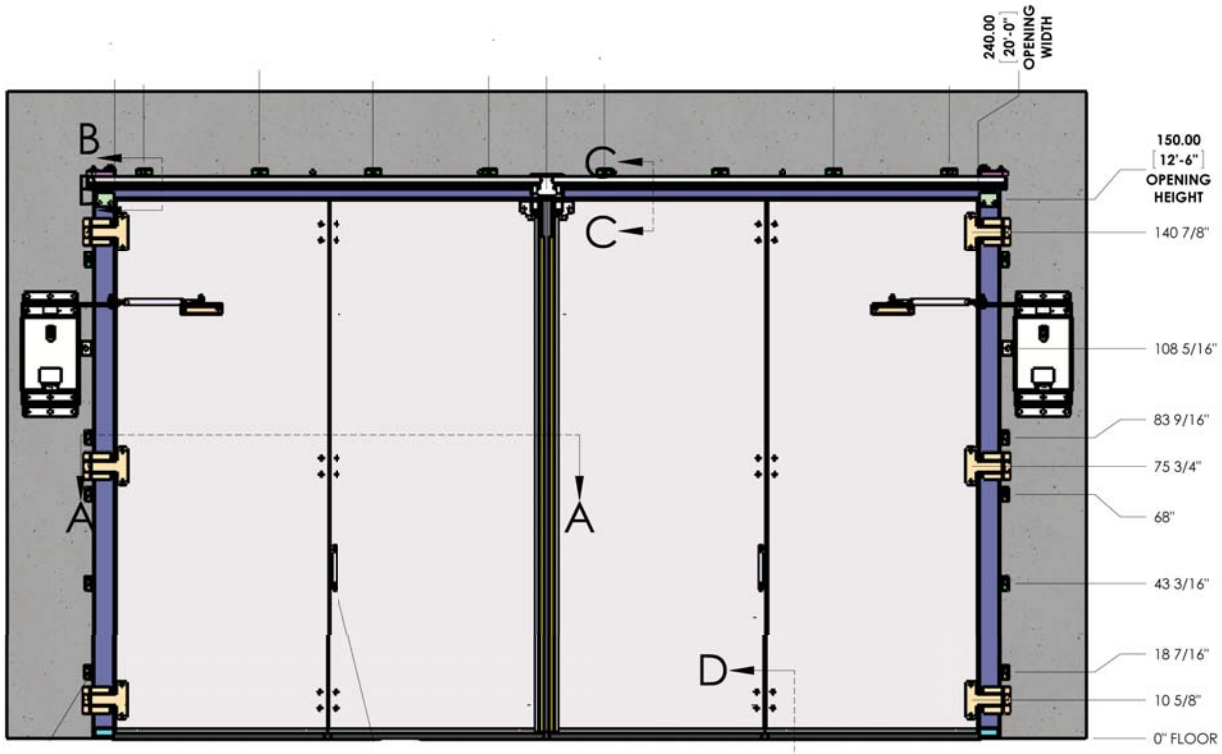
Prefinished Aluminum Railing at Typical Unit Balcony
(Proposed similar to this railing at Stonebridge)

Prefinished Aluminum Railing at Walk-up Units
(Proposed similar to this railing at Stonebridge)

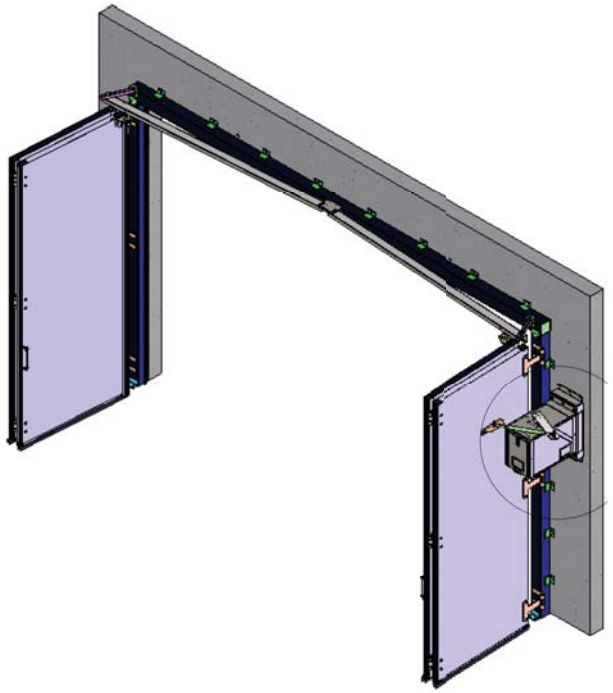


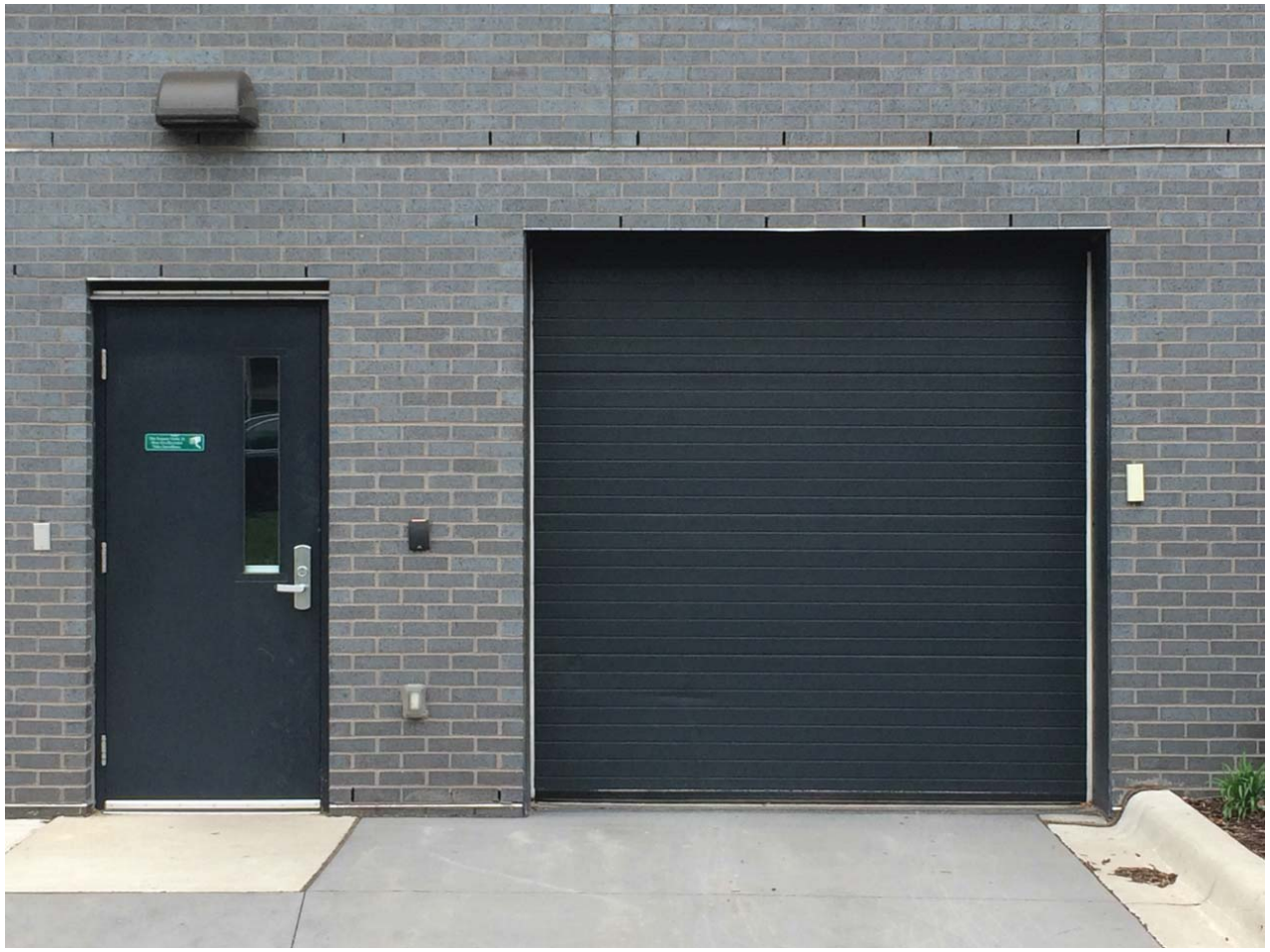
Bi-Fold Doors
(Proposed Similar to this Bi-fold door at Stonebridge)





INTERIOR ELEVATION





Overhead Door
(Similar to this overhead door at Stonebridge)

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the **LEGACY** Condos - 2nd Street Entry and Elevation (Supplemental Images)

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the LEGACY Condos – 2nd Street Entry and 13th Ave Elevation (Supplemental Images)

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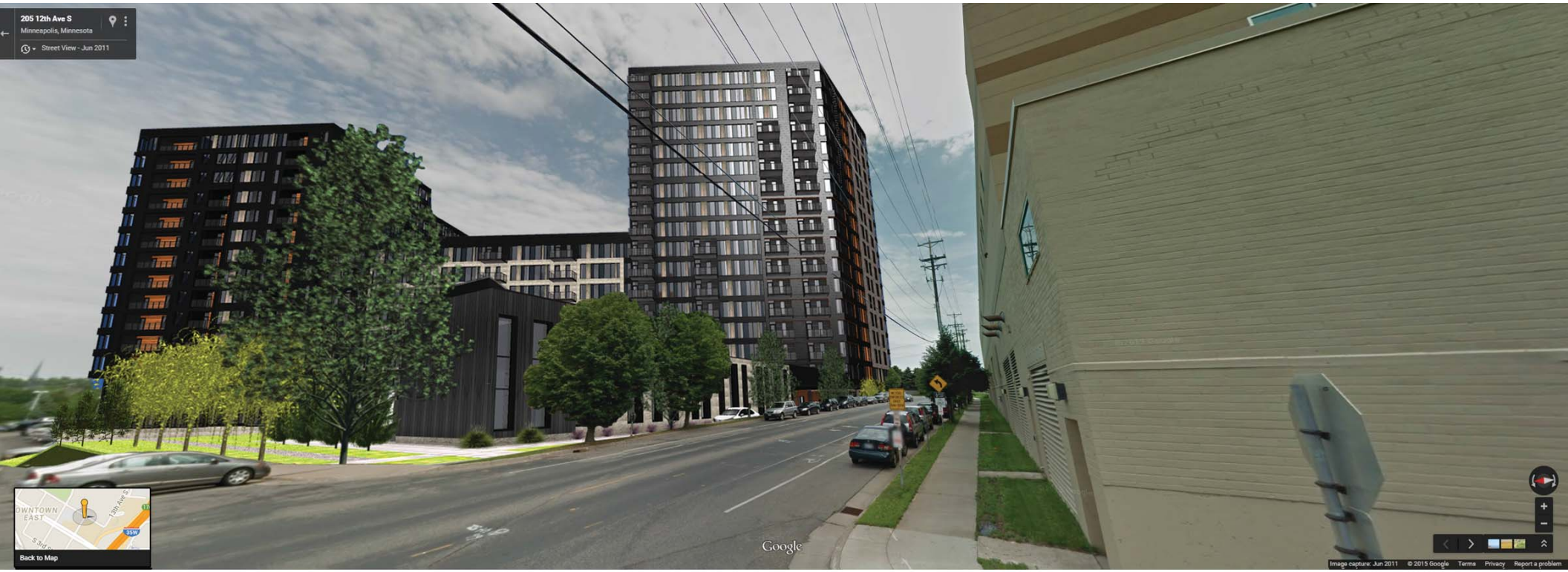
the **LEGACY** Condos – River side view and Stonebridge view (Supplemental Images)



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the **LEGACY** Condos – West Corner 12th Ave S and S 2nd St

©2015 Oertel Architects, Ltd.

the **LEGACY** Condos - Main Building Entry - Port e Cochere

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the **LEGACY** Condos – East Side at 13th Ave

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the **LEGACY** Condos – West River Parkway – North Side / West Side

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the **LEGACY** Condos – Pool Deck and Roof Deck

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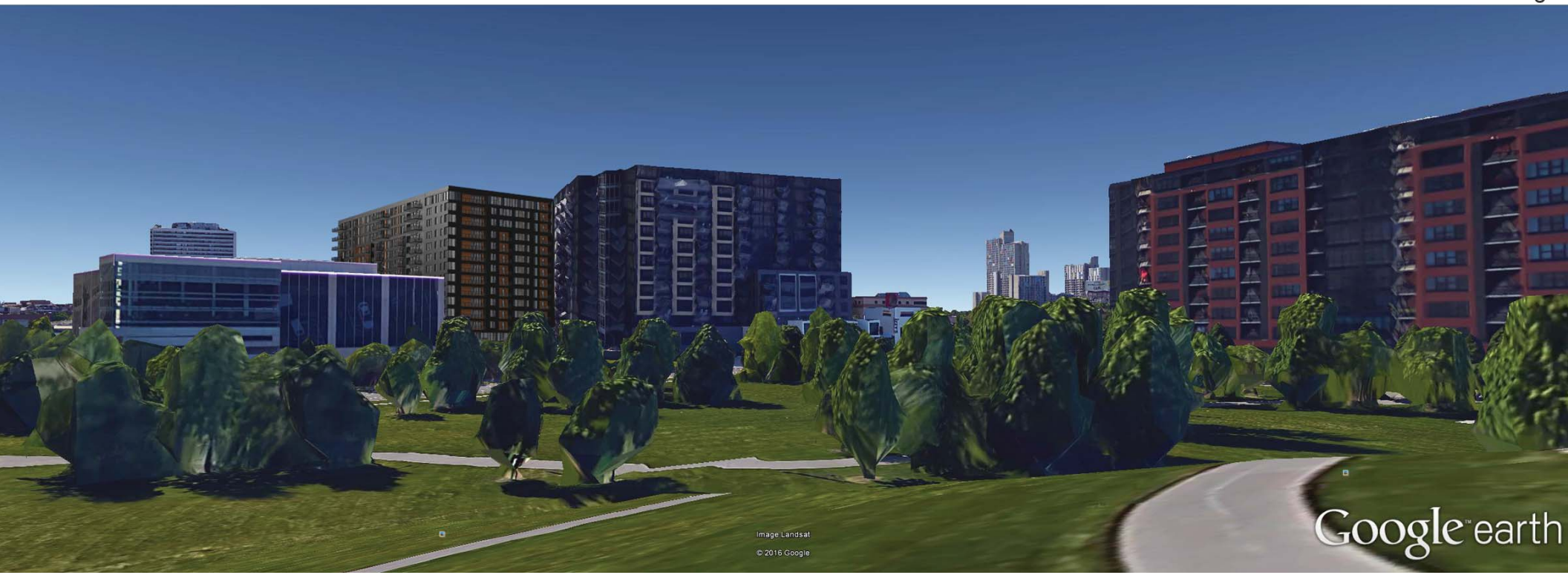
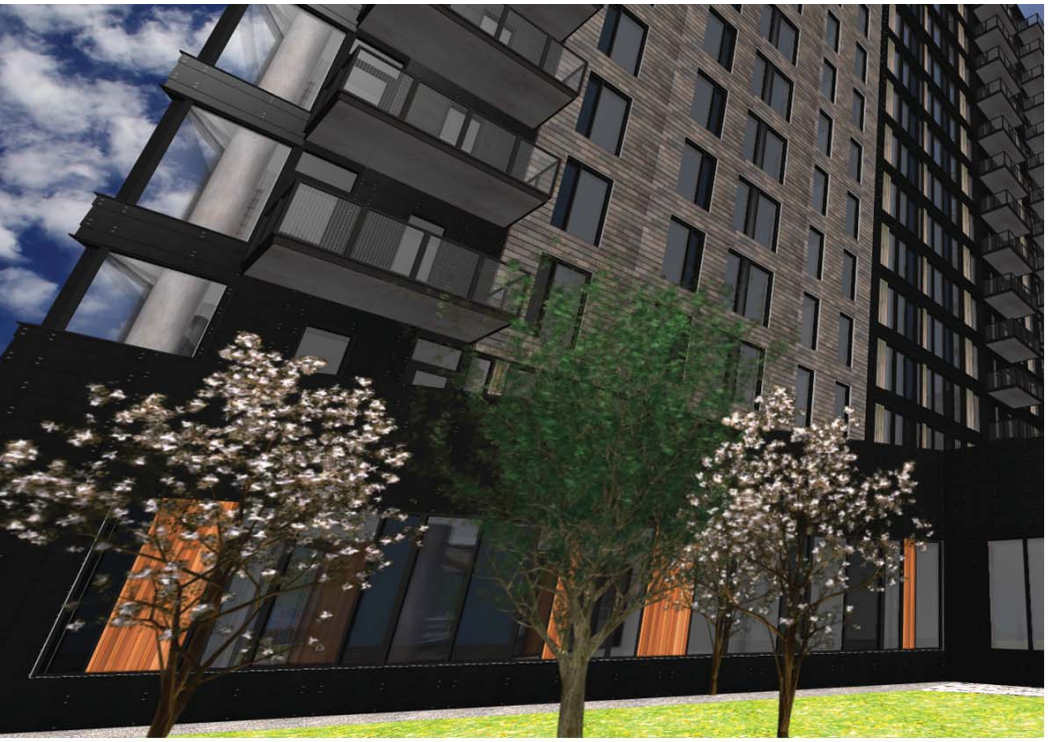
the **LEGACY** Condos - Material Close-ups

©2016 Oertel Architects, Ltd.

the **LEGACY** Condos – Contextual Images





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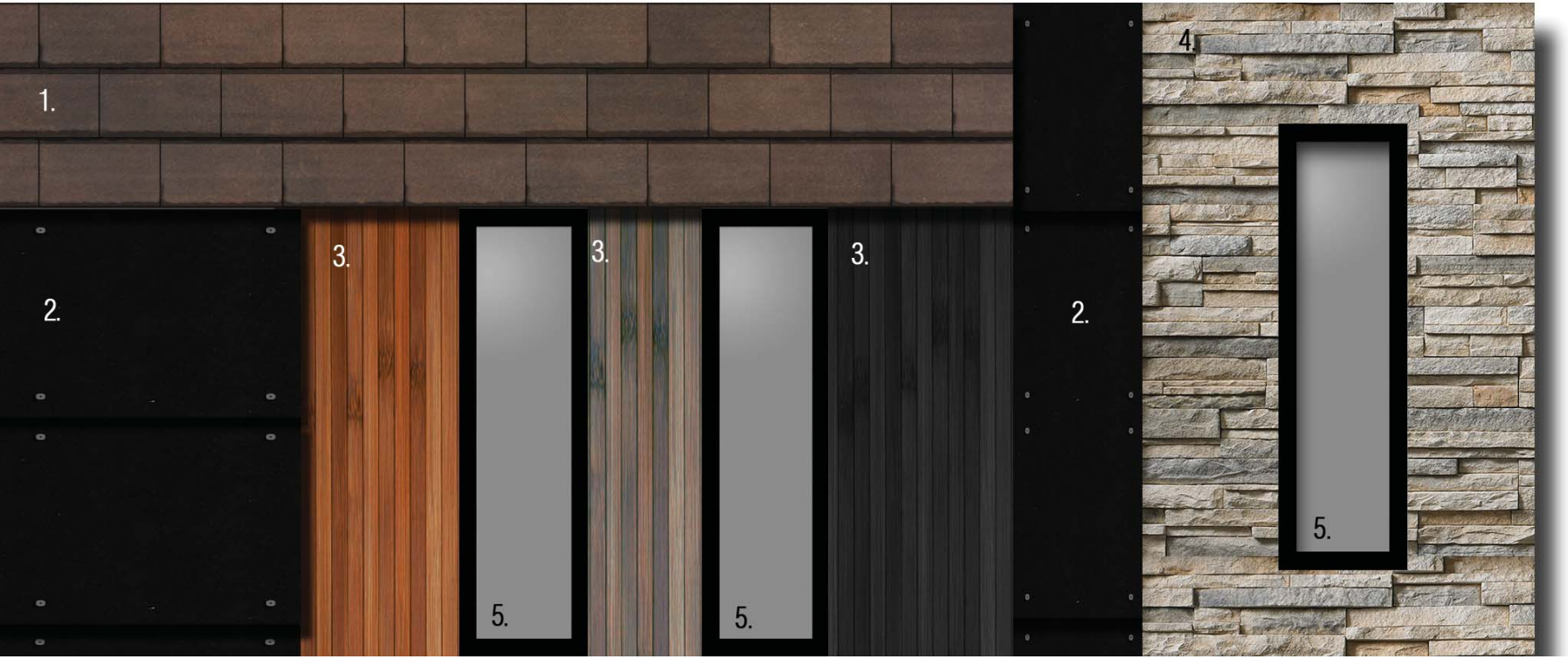
the LEGACY Condos - Material Close-ups & Samples



1. Modular Stainless Steel Metal Panel
   -Light Bronze Color Range
2. Fiber Composite Panel
   -Black w/ exposed fasteners
3. Wood grained composite panel
   -Three colors, various locations
4. Thin set modular stone
5. Aluminum Window System
   -Black w. several glazaing types

Imagery ©2016 Google, Map data ©2016 Google 500 ft

Image capture: Jun 2011 © 2016 Google

2nd Street at west corner of site
Looking East

Google Maps S 2nd St

Image capture: Jun 2011 © 2016 Google

Minneapolis, Minnesota

Street View - Jun 2011

2nd Street
Looking NW

Image capture: Jun 2011 © 2016 Google

13th Avenue
Looking NE

Google Maps 13th Ave S

Image capture: Jun 2011 © 2016 Google

Minneapolis, Minnesota

Street View - Jun 2011

Image capture: Jul 2007 © 2016 Google

2nd Street, looking NW

Image capture: Jul 2007 © 2016 Google

13th Avenue South, looking NE

Image capture: Jun 2011 © 2016 Google

13th Avenue South, looking SW

Image capture: Jun 2011 © 2016 Google

View from West River Parkway, looking South

Imagery ©2016 Google, Map data ©2016 Google 100 ft

Google Maps

Aerial View, looking East

Riverdale Ventures, LLC
3200 Main Street NW
Suite 300
Coon Rapids, MN 55448
763.421.3500

April 7, 2016

Mr. Jacob Frey
Council Member, Ward 3
City Of Minneapolis
City Hall
350 S. 5th St., Room 307
Minneapolis, MN 55415

Re: Applications for the Legacy

Dear Mr. Frey:

This letter is to notify you that Riverdale Ventures, LLC. is seeking approval for the construction of a condominium project consisting of 374 units.

Developer: Riverdale Ventures, LLC is proposing to construct a condominium project in downtown Minneapolis. Jim Stanton, the owner of Riverdale Ventures, LLC has constructed a total of nine (9) other condominium projects in Minneapolis. The first six (6) were located in the North Loop (Lindsay Lofts, Rock Island Lofts, 212 Lofts, Security Warehouse Lofts, 5th Avenue Lofts and Riverwalk), the next two (2) were located in the Mill City District (Bridgewater and Stonebridge) and one (1) project is currently under construction in Downtown East (Portland Tower).

Location: The site is on the corner of 13th Avenue S. and 2nd Street S. The site is currently occupied by the vacant Cenveo building and a 148 stall surface parking lot. The site contains approximately 186,121 s.f. (4.273 acres).

Current property addresses are:
121 12th Avenue South
1220 1st Street South

Zoning: The parcels are zoned I1 with DH, DP and IL overlays. The developer will be requesting to rezone to C3A

Applications Required:

-Rezoning from I1 to C3A. Remove IL Overlay District
-CUP application for PUD
-Height allowance increase from 6 to 17 stories, requested within the PUD app.
-Parking allowance increase from 1.7 to 1.82 parking ratio, requested within the PUD application

-Site Plan Review
-Plat Approval

Building Description: The proposed project is a residential project including 374 condominium units in a stepped building 17/8/14 stories with the units in the building as follows:

First Floor:	24 Units
Second Floor:	26 Units
Third-eighth Floors:	33 Units/Floor
Ninth-11th Floors	22 Units/Floor
12th-14th Floors:	16 Units/Floor
15th-17th Floors	5 Units/Floor
Total Units:	**374 Units**

No commercial use is proposed. Although the unit floor plans have not been finalized the units will range in size from approximately 950 to 3,100 square feet. The exterior of the project is proposed to be stone, glass, metal panel, phenolic (wood grain) panel and fiber cement composite panels and will compliment both the Bridgewater and Stonebridge projects in scale and design.

Building Details: The allowable Floor Area Ratio is 4.0. The site contains 186,121 square feet and the proposed project includes 725,326 square feet of above grade square footage resulting in a FAR of the proposed project of 4.010.

Amenities: The proposed project includes a playground, pet exercise area, pet grooming area, pool and spa, bike racks, a community room, game room, green roof, lawn bowling and an exercise room.

Parking: The proposed project includes one (1) enclosed at grade parking area and three (3) levels of underground parking with a total of 703 parking stalls (17 guest stalls (only 8 allowed in calculation), 14 handicap stalls and 681 residential stalls). As part of the application for this property, Riverdale Ventures, LLC is requesting an increase in the allowable parking ratio from 1.7 to 1.82.

Please call me at (763) 331-4188 or e-mail me at leegard@shamrockcompanies.com if you have any questions.

Sincerely,

Lynn M. Leegard
Vice President

OERTEL ARCHITECTS

Date: March 16, 2016

TO: Downtown Minneapolis Neighborhood Association
Minneapolis Principal City Planner

FROM: Deb Brandwick
Oertel Architects

RE: Legacy

PROJECT SUMMARY

This letter is to notify you that Riverdale Ventures, LLC. is seeking approval for the construction of a condominium project consisting of 374 units and is seeking the support of the Downtown Minneapolis Neighborhood Association. An initial presentation was made to the DMNA on Tuesday February 9th and we now request an opportunity to present the modifications made to the project since that day.

Developer: Riverdale Ventures, LLC is proposing to construct a condominium project in downtown Minneapolis. Jim Stanton, the owner of Riverdale Ventures, LLC has constructed a total of nine (9) other condominium projects in Minneapolis. The first six (6) were located in the North Loop (Lindsay Lofts, Rock Island Lofts, 212 Lofts, Security Warehouse Lofts, 5th Avenue Lofts and Riverwalk), the next two (2) were located in the Mill City District (Bridgewater and Stonebridge) and one (1) project is currently under construction in Downtown East (Portland Tower).

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Dvorak, Hilary A.

From: Wittenberg, Jason W.
Sent: Monday, February 08, 2016 8:23 AM
To: Dvorak, Hilary A.
Cc: Elliott, Beth M.
Subject: FW: The Legacy Condos

FYI. This constituent might think that the upcoming CoW meeting would result in actual approval of the project.

From: Minneapolis 311
Sent: Friday, February 05, 2016 6:32 PM
To: Wittenberg, Jason W.
Subject: The Legacy Condos

Hello Jason,

We received the email below from a resident and were recently told to forward these comments over to you for review to present to the Planning Commission.

City of Minneapolis

Name *	John E. Haggerty
Email *	Jjackhagg@aol.com
Phone	(612) 310-3628
Phone Type	
Address	1120 S. 2nd Street, #1201
City	Minneapolis
State	MN
Zip	55415
Question/Comment *	Greetings. My name is Jack Haggerty, and I am a resident at the Stonebridge Lofts overlooking the site of the newly proposed condo development, The Legacy. I am writing because I was made aware of the details of the new proposal. I was also given a link to the Minneapolismn.gov site where there is a write up of the project and preliminary plans. Unfortunately, we have not been able to access the site to get the materials. I am writing to suggest that there wasn't enough time given to prepare for the February 11 Planning Commission's Committee date to share our concerns about the proposed development -- particularly since we do not have access to the project proposal information and preliminary plans. I am suggesting that that date for the PCC meeting be postponed for some time for the project to be reviewed by the residents affected. Also, when we bought the property at the Stonebridge Lofts, we were told by the city of Minneapolis and the sales representatives at the Stonebridge Lofts represented by RE/Max Realty that the zoning laws for that part of the city was 8 stories. Though, it was said that there weren't any absolute guarantees, we were told that it would be very highly unlikely that variances would be granted. That information, though not the only reason,

weighed heavily on our decision to purchase our condo at the Stonebridge Lofts on the 12th floor overlooking the site in question. I hope to be able to provide more information. But, in the meantime I thought I would send this note to introduce myself as a person of vested interest in the project, and concerned for its future success. Thank you. Jack Haggerty

Previous http://minneapolismn.gov/

This is an email generated from the City of Minneapolis website. * Required fields are indicated with an asterisk.

Thank you,

Jordan Hooks
Customer Service Agent II

City of Minneapolis - 311
3000 Minnehaha Ave
Minneapolis, MN 55406

Office: 612-673-3000
jordan.hooks@minneapolismn.gov

Minneapolis 311 | CELEBRATING 10 YEARS

Kusz, Lisa M.

Subject: RE: Legacy feedback

To: Minneapolis 311@minneapolismn.gov
Subject: Contact Us from public website

City of Minneapolis

Name *	John E. Haggerty
Email *	Jjackhagg@aol.com
Phone	(612) 310-3628
Phone Type	
Address	1120 S. 2nd Street, #1201
City	Minneapolis
State	MN
Zip	55415
Question/Comment *	Dear City Planning Commission Members: The proposal for development of the land in The Downtown East neighborhood of Minneapolis bounded by 2nd Street, and 12th and 13th Avenues near the river is a tremendous undertaking, and affects all of the people of Minneapolis and beyond. I must, at the outset, let you know my motivations for giving my feedback on the proposed development project, The Legacy. One of my perspectives is a selfish one. I am a condominium owner in the Stonebridge Lofts where my current view will be blocked by the newly proposed development, The Legacy. However, and more importantly, my first thought is really how much of a treasure that land where the development of The Legacy is proposed is, and how important it is to be good stewards of that land. The land where the development is slated to take place is adjacent to Saint Anthony Falls Historic District which ends at 2nd Street, and 10th Ave. Given the recent developments in the closing of the Locks near St. Anthony Falls, it would make sense to me that it could be possible to extend the St. Anthony Falls Historic district all the way to where the Lower Lock is including the area bounded by 2nd Street all the way to 13th Avenue. The land in question is very special in more ways as well. In fact, the land at the site of the proposed development, in my opinion, could not only be part of a historical district, but could be recognized as a memorial site (in addition to the memorial at Gold Medal Park) for the I35W bridge collapse due to its very close proximity to that tragic, and historical site. Also, the land in question borders a National Recreation Area which falls under the US National Park Service. Not only is this land a National Recreation Area, but is a site of much natural historical significance. And, in fact, the land is a very sacred place to the Native Americans. My point is that this is a very special plot of land, and to exploit it for commercial gains at the risk of having it lose its special sacred qualities would be tragic. I am hopeful that feedback from many of the groups representing these organizations can give their feedback on this situation -- unless they have already done so. I realize that there has probably been much work done on preparing for this project, but I am not up to speed on the history of the planning on this

space. I apologize if I am bringing up issues that have already been reviewed. But, as a resident of Minneapolis, I felt that it was a personal responsibility to share my thoughts to ensure that at the least you have had a chance to hear my perspective. I look forward to further discussions on this subject and hope to be a part of this solution. It is my goal that a solution be found to this problem that is in the best interest to those affected by the new proposal to develop the land for The Legacy. Thank you.

Previous http://minneapolismn.gov/residents/neighborhoods/residents_downtown_index

This is an email generated from the City of Minneapolis website. * Required fields are indicated with an asterisk.

Dvorak, Hilary A.

From: Doug Verdier <dougv22000@gmail.com>
Sent: Thursday, April 14, 2016 3:39 PM
To: Dvorak, Hilary A.
Subject: Legacy Condo Development

Good Afternoon, Ms. Dvorak,

As a resident owner at RiverWest condo I have been interested in all development proposals in the Historic Mill District for more than 10 years. I have read your Mar. 17, 2016 memorandum on the Legacy Condo project and attended the public meeting of the DMNA Land Use Committee on April 13, 2016 at which a CPED staffer presented an update on the Legacy Condo project and fielded questions from the audience.

Several of the questions I had about the changes to the project from the initial proposal were answered by the updated information. I especially like the change in the "massing" of the building with the tiered look of 3 levels. I do, however, have a number of concerns about the current proposal that were not answered to my satisfaction at the meeting, so I wish to bring them to your attention for consideration prior to the next round of the approval process for this project.

1. The number of elevators for this structure appears inadequate for the density proposed. RiverWest has 415 units in 18 floors and only 4 elevators. One of the elevators is designated as the "freight elevator" for movement of furniture, appliances, etc. whenever there is a move-in or move-out or delivery. Despite a major upgrade in our elevator system a few years ago, there are many times when one of the elevators is "locked" for moves, leaving only 3 for resident use. This does create a bit of a problem during times when many residents are leaving for or returning from work. The location and number of the four planned elevators in the Legacy project does not appear to be well-planned for the density or convenience of the building.

2. The lack of a designated freight elevator and location for moving vans to load/unload seems to have been overlooked with the Legacy plan. RiverWest experiences somewhere between 25 - 30 move-ins/move-outs per year. The designated elevator can be reserved for 4 hour blocks, but this often is inadequate for large household moves, resulting in reservations on more than one day. Contractors working in the building also reserve the elevator for movement of their equipment and supplies, thereby tying up one elevator for an entire day many times. I think that for the convenience of Legacy residents, the developer should plan for at least one additional elevator that could be designated for "freight" and be accessible from the lower areas of the building to provide adequate parking (not on 2d St.) for moving and delivery vans. If this means reducing the total number of units, or making some stacks smaller, in the 17-story building, that would probably be a good trade-off as far as residents are concerned. Having a designated freight elevator also reduces the likelihood of damage to elevator interiors during movement of bulky items.

3. Exterior lighting was discussed briefly during the Q&A, but I did not leave the meeting feeling that any satisfactory decision had been announced. The location of this property is ripe for crime, given the lack of "eyes on the street" and proximity to "getaway" routes. The area is also very dark. Street lights should be required all around the perimeter of the building on 2d St. and 13th Avenue, as well as flood lights on the sides of the building overlooking outdoor amenities and the sidewalks. Public safety is a concern, and the current proposal does not address the lighting issue.

4. One attendee asked about a traffic study in the area. His question was not addressed adequately. Has the City done any sort of traffic study of the entire 2d St. corridor recently? With the addition of a significant number of multi-unit buildings, which means many more pedestrians, bikers, and motor vehicles using the street, I believe that a comprehensive traffic study is warranted. Anyone who tries to use 2d St. from around 12th Avenue to 3rd Avenue during any rush hour notices the high number of vehicles of all sorts and backups on that street. The addition of 374 more units will certainly result in more cars (despite the City's belief that more people will ride bikes or use public

transportation) in the area. A complete traffic study of the entire corridor and feeder streets should be done. This will be critical to be coordinated with the plans for re-design of Washington Avenue. The traffic corridors in this part of town are an entire system that must be analyzed as a whole and not piecemeal, as appears to have been done in the past.

5. No mention was made at the April 13 meeting about any sort of "cap" on number of units in Legacy that could be leased and how many had to be owner-occupied. I realize that this is an issue for the condo association; however, the developer will likely draft the initial Governing Documents for the condo association and ratio of owner-occupied to leased units should be addressed. The survey of residents that was taken in regard to planned projects in the Mill District clearly indicates that there is concern with the number of rental properties that are being built. One aspect of this that has not been fully revealed is the number of rental units that will be allowed in newer condos being built or planned. Without some controls, the area could easily become more rental units than owner-occupied.

I appreciate the work of CPED in assisting and guiding developers and hope that the issues I have raised can be considered as planning goes forward. I will appreciate any response you can provide in advance of any future decisions or meetings regarding the Legacy project as well as the 205 Park Avenue South project.

Best regards,

Doug Verdier

DOWNTOWN MINNEAPOLIS NEIGHBORHOOD ASSOCIATION

40 S. 7th Street, STE 212 PMB 172
Minneapolis, Minnesota 55402
Phone: (612) 659-1279
Online: www.thedmna.org

May 16, 2016

RE: DMNA Support for the Legacy Condo Project

Dear Mr. Stanton,

The DMNA would like to extend its support for the Legacy Condo project as presented to the DMNA Land Use Committee on April 13th, 2016. It is clear that the project team incorporated feedback provided by the community at the previous presentation to the DMNA in February. The team's willingness to meet with the neighborhood on two occasions and readily receive feedback speaks to their respect for the surrounding area.

The project will bring 374 owner occupied residential units to the Mill District, which many residents have long and loudly expressed concerns over a shortage thereof. Generous landscaping (including rain gardens) and expanded boulevard along the 2nd Street townhomes addresses the neighborhood's desire for a green streetscape. The revised plans' better accommodation of dog relief (through a well located exterior relief area as well as interior relief area) addresses the neighborhood's desire to mitigate "doggie damage" to green space in public view and at Gold Medal Park.

In addition to the improvements listed above, the DMNA strongly encourages the project team to do the following:

- Use pedestrian-oriented exterior light fixtures as well as public art that communicate the industrial heritage of the area.
- Explore the establishment of cross easements with the Stonebridge to allow residents to share exterior amenities, such as playgrounds. This would allow the Legacy to offer exterior amenities that complement rather than duplicate those of its sister development, with the end goal of providing a slate of improvements that serve a wide range of ages.
- Provide exterior bike parking for guests well as interior bike maintenance facilities for residents that encourage people to travel to and from the site by bicycle.

There is strong support for the revised design that integrates multiple towers to create greater visual interest. It is understood that additional height is required to accommodate this design change and preserve open space on the site. However, the neighborhood hopes that the allowed height does not set a precedent for future adjacent developments. The site's proximity to I-35W, position as an end cap to the Mill District and setback from the river make a strong case for height at this specific location. The same conditions do not apply to neighboring parcels along West River Parkway.

If the project design changes significantly or construction does not commence by Summer 2017, the DMNA reserves the right to revisit its letter of support.

Regards,
Nick Cichowicz, DMNA Board Chair